

Urban Design Review Panel Report

1822 Bank Street

February 3, 2025

Planning Circulations

Development Review
Planning, Infrastructure and Economic Development Department
City of Ottawa
110 Laurier Avenue West
Ottawa, ON K1P 1J1

Via Email: planningcirculations@ottawa.ca

**RE: Urban Design Review Panel Report
Official Plan Amendment, Zoning By-law Amendment, and Plan of Subdivision Applications
1822 Bank Street**

To Whom it May Concern,

Enclosed, please find responses to the Urban Design Review Panel's comments provided on March 11, 2024, regarding the Official Plan Amendment and Zoning By-law Amendment applications for the property municipally known as 1822 Bank Street, Ottawa.

The Urban Design Review Panel meeting was held on Friday, March 1, 2024, from 9:15 a.m. to 10:15 a.m.

In addition to this cover letter, the following materials are included within the submission package:

/ Urban Design Brief, prepared by Hobin Architecture, dated March 1, 2024.

We trust that this addresses the Urban Design Review Panel Report requirements, and the development can move forward towards approval. Please feel free to contact the undersigned with any additional comments or questions at simpson@fotenn.com.

Respectfully submitted,



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Planner



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Key Recommendations

1. The Panel supports the proposal from a city building perspective.

Response: Acknowledged.

2. The Panel appreciates how the proposal will help incorporate Bank Street into the neighbourhood, improving the corner condition of the intersection and becoming a precedent for future integration along Bank Street.

Response: Acknowledged.

3. The Panel supports the location, orientation, and positioning of the park along with the pedestrian access points proposed through this plan.

Response: Acknowledged.

4. The Panel has some concerns with the heights of Building 5 and Building 2, with Building 5's height appearing too tall and having a negative effect on the residential neighbourhood to the west and the park, and Building 2 appearing capable of taking on more height if necessary. Consider reallocating the heights and densities of these two buildings.

Response: The height of Building 5 at the southwest corner (now referred to as Building 1) has been reduced in height to 24 floors to provide a more sensitive transition to the adjacent neighbourhood to the west. Additional height has been added to Buildings 2 and 3 (corner of Bank and Walkley and facing Bank Street mid block) to shift the development density away from the low-rise neighbourhoods.

5. The Panel suggests potentially rotating Building 1 on the corner of Walkley Road and Bank Street to have its longer frontage facing along Bank Street rather than along Walkley Road.

Response: The applicant explored this option and found that it compromised the separation distance between buildings facing Bank Street. However, the podium of this building (Building 1) has been reshaped to better frame the proposed POPs and provide a better relationship to Bank Street.

6. The Panel suggests reducing the impact of the car on the public realm by disconnecting the private road along one side of the park and/or minimizing the use of the private road for parking and access to the buildings.

Response: The private road now provides access only to the passenger loading functions at the lobbies and for building services and loading. Below grade parking access is limited to two points along the proposed public road to the west edge of the site. Additionally, the design team has further refined the private road surface treatments to ensure that it is perceived less as a roadway and more as mixed use space where vehicle drivers are permitted, at low speed, and in respect of the right of way of pedestrian users.

7. The Panel recommends the podium heights could be lowered to better relate to the scale of the pedestrian environment.

Response: The podium heights have been reduced to 4 stories for Buildings 1, 3, and 4. For Building 2 (corner of Bank and Walkley), the podium is proposed to be 5 stories to provide a stronger anchoring form at the key intersection.

Site Design and Public Realm

8. The Panel sees great potential for placemaking in this project.

Response: Acknowledged.

9. The Panel appreciates how the redevelopment of the site reconciles the existing grade challenges on site.

Response: Acknowledged.

10. The Panel appreciates the “funnel effect” of pedestrian spaces between buildings, and the potential for a strong pedestrian experience that opens up to the central park space is positive.

Response: Acknowledged.

11. The Panel appreciates the central location of the park and the overall disposition of the buildings on the site. Further consider the heights of the towers to allow for more light to penetrate into the park.

Response: Acknowledged.

12. The Panel suggests the POPS next to Building 1 could be more of a corner POPS that provides a diagonal connection from Building 1 through to the park.

Response: Noted. The design of the POPs has been reworked to provide a more direct pathway from the intersection between Buildings 2 and 3 and into the interior park.

13. The Panel has concerns with the access and traffic flows at the south-west corner along Walkley Road. Consider looking at flows in more detail and improving that intersection for better all-around safety, but especially for pedestrians that will be crossing there often to get to the transit station.

Response: In conjunction with our traffic engineers and the City of Ottawa, there are improvements being proposed to the intersection to better handle the traffic flow and delineation between the public road and the existing private road to the west.

14. The Panel questions whether the location of the loading and servicing for Building 5 is appropriate. More consideration ought to be given to that south-western corner and how it connects with the access off Walkley Road, from both the vehicle and pedestrian perspectives.

Response: The loading and servicing for Building 1 (formerly Building 5) has been relocated away from the intersection allowing for an improved POPs anchoring the corner. Additionally, a secondary building entrance has been added to face the intersection.

15. The Panel suggests providing a robust landscape buffer along the western edge of the site to create a better condition/transition with the neighbourhood to the west. Consider a strong tree-lined western edge.

Response: The new public right of way has been adjusted to provide sufficient space for a row of trees bordering the west property line.

16. The Panel suggests perhaps eliminating the vehicle parking along the private road's edge in order to maintain the private road's integrity as a pedestrian-first, woonerf style, space and provide a safer connection between the buildings and the park.

Response: The design team considered this, however, we felt that short-term convenience parking is necessary to support the passenger drop off from taxis and ride-shares that has become an integral part of multi-residential projects with lower vehicle ownership residential mixes. Additionally, these parking spaces are needed to allow the viability of the retail / commercial spaces. We have reduced the amount of space from the previously proposed +/-40 spaces to 20 spaces.

17. Some Panel members recommend forgoing the mid-rise option in/along the park space.

Response: The option for a midrise block has been removed.

18. The Panel discourages the mixing of private amenity and public open space within the park area. Consider opportunities to expand the podiums of the buildings in order to place private amenity spaces on the podium rooftops instead of in/next to the park space.

Response: The current proposal respects the public / private divide set up by the private road. All area within area bounded by the public and private roads is now proposed to be part of the publicly accessible open space.

19. The Panel supports the proposed POPS parkette along Bank Street.

Response: Acknowledged.

20. The Panel has concerns with placing roads on all sides of the park space, while still appreciating that the private street condition is proposed to be more woonerf-like.

Response: Acknowledged. The design team seeks to minimize the traffic on the woonerf portion by limiting it to passenger drop off and essential building servicing. The design of the driving surfaces will be such to reinforce the idea that is a space where vehicle drivers are permitted, at low speed, and in respect of the right of way of pedestrian users.

21. The Panel has concerns with the potential for vehicles to use these roads to cut through the site.

Response: The design team shares this concern. As such, we seek to present a design that minimize the appeal and advantage of using this cut through. However, we do not feel that eliminating the right-in / right-out from Bank Street is prudent response as it would further pressure the intersection on Walkley Road.

22. One Panel member suggests the private road section along the west side of Building 2 could be eliminated and the park could take over that space, extending the park up to Building 2.

Response: The design team looked that this option, however we found that the necessary turning space to support loading and servicing increased substantially in the event that we provide dead-end access. Having the complete private road loop was deemed to provide the lowest impact space for vehicle movements.

23. One Panel member suggests the loading and parking accesses of Building 2 and Building 1 could be shared and the private road could be truncated more than it currently is at that location, understanding that this is assuming fire access could come off of Bank Street or the new public road on the north side.

Response: The loading and servicing space for Buildings 1 and 2 are now shared in a single point between the buildings. Likewise, the loading and servicing for Buildings 3 and 4 are in close proximity. These changes seek to limit the amount the site area that is impacted by truck movements.

Built Form & Architecture

24. The Panel appreciates that this proposal will be transformative for this area of Bank Street as an important TOD.

Response: Acknowledged.

25. The Panel appreciates the opportunity for this proposal to improve the grading and knit Bank Street back into the neighbourhood.

Response: Acknowledged.

26. The Panel suggests Building 1 be oriented with its longer frontage along Bank Street, rather than along Walkley Road. Consider rotating Building 1 approx. 90 degrees to achieve longer frontage along Bank Street.

Response: The design team has made revisions to the podium of Building 2 (formerly Building 1) to reinforce its relationship to Bank Street. The retail/ commercial spaces now face toward Bank St. The building edge at the podium is now aligned with Bank, and the landscape concept reinforces the desire line for pedestrians from the intersection toward the interior of the site.

27. The Panel has some concerns with the 6-storey podium heights as it diminishes the relationship with the public realm. The Panel suggests a lower podium height, such as 4-storeys, would be more beneficial to the public realm and more comfortable for pedestrians, particularly when setting a standard for this section of Bank Street.

Response: The podium heights have been reduced to 4 stories for Buildings 1, 3, and 4. For Building 2 (corner of Bank and Walkley), the podium is proposed to be 5 storeys to provide a stronger anchoring form at the key intersection.

28. The Panel suggests Building 2 on the eastern edge of the site should go taller, bumping right up to meet the 45-degree angular plane shown on p.34 of presentation and perhaps even beyond it.

Response: Building 3 (formerly Building 2) has been increased in height to 33 storeys. This change offsets the reduction in height on Building 1 (formerly building 5) at the southwest corner.

29. The Panel recommends Building 5 on the south-west corner on the site be reduced in height, and an alternate building mass with a lower height explored, as it has the most negative impact on the proposed park as well as on the established single-family residential neighbourhood to the west. Consider giving the park more ingredients for success by reducing long shadows over park space.

Response: Building 1 (formerly Building 5) has been reduced in height to 24 storeys to provide a better transition to the low-rise neighbourhood to the west. This change also makes a margin difference in shadowing on the parking in the late morning to noon period.

1822 Bank Street

OTTAWA URBAN DESIGN REVIEW PANEL

MARCH 1, 2024



FOTENN
Planning + Design

Ferris + Associates
a division of NAK DS Inc



BGO

DESIGN BRIEF

HISTORICAL CONTEXT

The site, 1822 Bank Street is in the southern part of Ottawa inside the Greenbelt. As with many of the Outer Urban areas of the city, the land use was primary agricultural and rural residential prior to the 1950s. By the early 1960s, much of the west side of Bank Street had been developed into a mix of light industrial (warehousing) and large commercial such as car dealerships. Development of the development site also occurred during this period, although the current commercial mall building was not constructed until around 1970.

The single family home focused residential neighbourhoods to the northwest and to the east (on the other side of Bank Street) were primarily developed in the early 1960s while the addition of some larger apartment buildings in the late 1970s. Also in the late 1960s, the development of the shopping plaza northeast of the site at Bank and Altavista began with the construction of what is now the grocery store. The balance of that mall followed around 1970.

To the immediate north of the site, the existing office block and strip mall were constructed between the late 1970s and late 1980s. Like the subject site, little has changed on that site in the following years to the current time.

As the former railway uses to the west of the site transitioned from freight focus, the parcel of land freed up by the closure of the spur curve of rail line following what is now Glenhaven Private was redeveloped into a small enclave of townhomes and a row of townhomes boarding the west property line in the early 2000s. This period also included Ottawa's earliest foray into light rail with the opening of the O-Train on the former freight corridor to the west. Walkley, however, was not served by a station at that time.

The South Transitway brought a rapid transit station to the west of the site in the early 1990s providing easy connection to downtown via Hurdman Station. As of 2024, the service at this station to Hurdman and to Riverside south is expected to be replaced by a new LRT line that is currently under construction.

Construction of this LRT and the accompanying Walkley Station has provoked a new wave of development on the lands immediately adjacent to the station. To date a hotel and two apartment buildings have been developed on the south side of Walkley road on Anand Private. However, along Bank Street, the development pattern of any recent construction has largely followed the ground / parking orient large format commercial and light industrial that has set the urban pattern since 1960.

Bank Street is scheduled to undergo a major infrastructure renewal and urban transformation in the later half of the 2020s with a focus of making the street a 'Complete Street' to better accommodate a variety of mobility options including active transposition. This change is set to align with redevelopment of this property (and others) to a higher density, more mixed use and less car focus development pattern, particularly at site in close proximity to transit.

The scale of this parcel, at 17,300m² presents a unique opportunity to build new and more urban community capitalizing on the existing set of amenities such as grocery and service stores, employment areas, and access to transit. The parcel size is sufficient to allow the opportunity to create elements of public realm that serve the new development and surrounding community.

CURRENT SITE CONTEXT

WESTWARD – The site abuts the back yards of a row of townhomes constructed along the former rail corridor that now forms Glenhaven Private. To the west along Walkley is an enclave of townhomes also developed at the same time. Northwest is a 1960s low-rise, single family home neighbourhood. Further west, the Airport Parkway provides a linear open space and transportation corridor linking the Airport to the core.

EASTWARD – Bank Street forms the eastern boundary of the site. Immediately to the east is a car dealership, while northeast is a shopping plaza and southeast (south of Walkley) is a series of small commercial uses including a gas station.

NORTHWARD – North of the site is an existing shopping centre and three storey office building. Like the subject site, this parcel is both of significant size and depth and is anticipated to support future intensification and change. There are also a couple small (and older) stand-alone commercial buildings border the site. The nearest houses a small restaurant, while the second houses a pharmacy and treatment centre and financial support services.

SOUTHWARD – The Southern edge of the site is bounded by Walkley Road. This arterial serves as a significant east-west link in the south portion of the city, extending from Mooney's Bay in the west to highway 417 in the east. As such, there are significant traffic volumes at the Bank Street and Walkley Road intersections. A proposed new intersection at Walkley Road and the Airport Parkway (late 2020s) will influence some of that volume. On the south side of Walkley Road, there is a stand-alone drugstore and to the southwest, the aforementioned hotel and new apartment buildings adjacent to the new LRT station.

MASTER PLAN CONCEPT

The fundamental principle shaping the master plan is the development of a mixed-use transit-oriented community that is centered around a large and vibrant public open space creating a sense of place on an otherwise generic commercial arterial street. This public space is intended to provide focus to the new community and will be a source of vibrancy and animation at the heart of this new micro urban core. It will provide additional exterior green space for the residents and visitors of this development to enjoy and play. Smaller "POPs," will vary in their identities but will all play an important role in connecting the new development to the surrounding streets and reinforcing desire lines for movement through the site. Studying the patterns and impacts of prevalent winds and sun exposure in these spaces is essential in creating quality spaces that are enjoyable throughout the various seasons.

The redevelopment of this site proposes a series of high-rise buildings of varying heights. While considering our proposed massing along with existing and future context, we have reduced the building heights along the western edge of our site and allowed for taller forms at the south and

DESIGN BRIEF

southeast to reinforce the importance of the key intersection of Bank and Walkley. The position of the open space along the west side of the site and central ensures ample sunlight in the afternoon and evening and provides the maximum amount of view to green space toward the west. The position of highest densities towards the southeast also provides transition down to the low-rise neighbourhoods to the west. The intent of the architectural design is that the ground floor uses adjacent to public spaces are to be actively programmed with active uses such as amenity, lobbies, and strategic small retail spaces. This includes ground floor spaces facing Bank Street, the open space, and the POPs at Bank and Walkley and at the northeast corner of the site.

KEY DESIGN CONSIDERATIONS

The following design considerations should serve as guidelines to help shape both the public realm and the built environment in order to achieve the desired mixed-use environment.

BUILDING FORM - PODIUMS AND TOWERS

The envisioned development positions itself to support the new LRT station southwest of the site. The high-rise building typologies will address the anticipated density for the development. The development will consider scale, materiality, relationships to both the open spaces and the built environment, along with its sun and shadow impacts. Taller built forms will be consistent with the city's high-rise design guidelines in terms of podium heights, floor plate sizes, and tower separation and will address the three distinct building elements listed in the guidelines: the ground level edge condition and podium, the main body of the tower and the articulation of the top.

PLACEMAKING

Creating a strong public realm will be critical to the success of this development. Establishing a framework to achieve a quality public realm is foundational to the development of each future phase of development on this site. The following themes are inherent to proper functioning of the public realm and should be carefully considered in order to successfully establish a quality urban fabric where people will want to live, work and play.

In the context of a large development such as this one, it is critical to consider the hierarchy of streets, public open spaces and linkages between these spaces to ensure that the development is not designed in isolation, but rather in a manner that relates to its current and future context. The central open space area of the development will serve as a placemaking anchor. This large spaces is intended to be the “living room” for this new community. Adjacent buildings should anticipate ground level uses that will support the animation of the open public space. The primary landscape components are comprised of a: Open Space, Interior streets, Residential Thresholds, and POPs.

The “backyard” of the site, the “Open Space” is a recreational greenspace consisting of large grass areas and pathways to draw users through the site. Space for playful structures further distinguishing the pathway while providing opportunities for lighting and sound or music, and various seating opportunities. This cheerful space, centrally located, animates the entirety of the site.

Within this space, there is also the possibility to incorporate an amenity building for use of the development and neighbouring communities.

BUILDING EDGES

Throughout all our projects, we take great care in ensuring that the interface between our proposed built environment responds to and supports the existing fabric in which it is inserted in. Understanding and acknowledging the existing context through thoughtful architectural expression is critical to the way people experience and interact with the built environment. The architectural expression should respond to the context through appropriate scale of podium elements and use of complimentary materials. As previously mentioned, the architectural expression at grade is often driven by internal uses and is therefore critical to carefully plan internal uses appropriately along active edges of the building.

INTERIOR STREETS

Creating a strong and rich built fabric will be important given the lack of urban context around the 1822 Bank Street site. As previously mentioned, careful consideration needs to be given to the pedestrian experience and the relationship of streetscapes to buildings. The various ground level uses fronting onto streets must contribute positively to the ground level edge conditions and be appropriately scaled to complement the

pedestrian experience. Additionally, using durable materials such as masonry and stone will provide a sense of familiarity and tactile quality to the users.

PATHWAYS TO TRANSIT

Creating and maintaining a strong pedestrian experience is a vital aspect for the vision of this site. Through the day we should anticipate that all the new streets and pedestrian ways will be regularly used and should be designed as such. Understanding the hierarchy of the street – be it the new public street or the more pedestrian private - is important in prioritizing the various pedestrian experiences by designing appropriately scaled edge conditions supported by complementary programming along those streets. Bank Street and Walkley Road are busy streets which move a large volume of vehicles throughout the day. In contrast, our internal street will have a different relationship to our site and should ideally become secondary to the function of the site rather than a busy thoroughfare. Promoting various uses along the internal street will contribute to improving the pedestrian experience and overall street animation.

OTHER CONSIDERATIONS - TRAFFIC, PARKING & SERVICING

We anticipate several phases of underground parking structures to fulfill the needs of the proposed and future uses on the site. Given the proximity to the new Trillium Line LRT and existing public transit, parking requirements may be lower than similarly scaled developments in less connected locations. We believe that the local neighbourhood amenities and renewed Bank Street will support a preference to active transport for many residents. Consideration will need to be given to servicing, shipping and receiving for commercial tenants and operational functions for residential uses.

IN SUMMARY

We believe through thoughtful design and planning our vision will bring renewed vitality to the area while taking advantage of the unique proximity to the current and future transit available to the community. This unique development will offer a truly complete community lifestyle while becoming a vibrant and exciting addition to Bank Street South's evolving urban fabric.

PROPOSAL OVERVIEW



Key

- Open Space Opportunity*
- Pedestrian Access Point
- Pedestrian Desire Lines
- Future Cycle Tracks
- Vehicle Movement - Public
- Vehicle Movement - Private
- Loading Space

*Open Space Opportunity TBD

Proposed Development Stats

Building Area: 1.161 - 1.221M sq.ft.
Proposed Heights: 6 - 40 storeys
Number of Dwelling Units: 1,100 - 1,200
Parking Spaces: 43 surface + 800 below grade
Bicycle Parking: 600 spaces
Public Realm: 25 - 30% of Site Area (43,000 - 55,000 sq.ft)
Total Green Area: 32 - 36% of Site Area (65,200 - 68,200 sqft) including green roofs

OFFICIAL PLAN

The subject site is located within the **Outer Urban Transect** in the **City of Ottawa Official Plan**. The Outer Urban Transect applies to lands within the green-belt that have a classic suburban model of development. The objective of the Outer Urban Transect is to introduce more viable public transit and active mobility options and encourage more diverse housing forms.

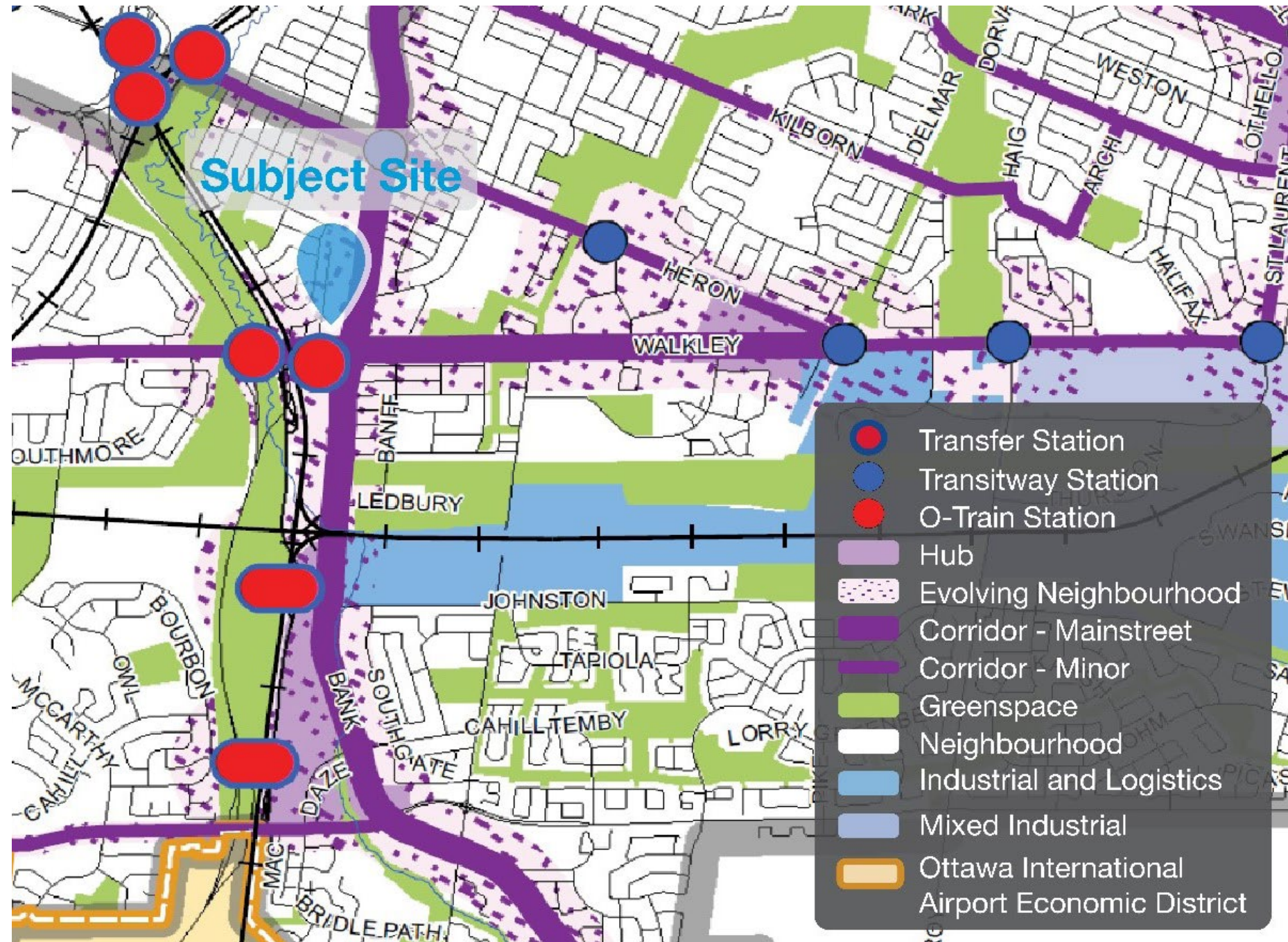
The subject site is designated **Mainstreet Corridor**. The Corridor designation applies to bands of land along specified streets whose planned function combines a higher density of development, a greater degree of mixed uses and a higher level of street transit service than abutting Neighbourhoods, but lower density than nearby Hubs.

Permitted building heights within the Mainstreet Corridor designation are **up to high-rise (40 storeys)** on corridors with a width greater than 30 metres (including Bank Street adjacent to the subject site). Permitted heights are subject to appropriate height transitions, setbacks and angular planes.

Development along Corridors should generally be located to frame the street, park, or greenspace and should provide an appropriate setback within the street context, with clearly visible public entrances from public sidewalks.

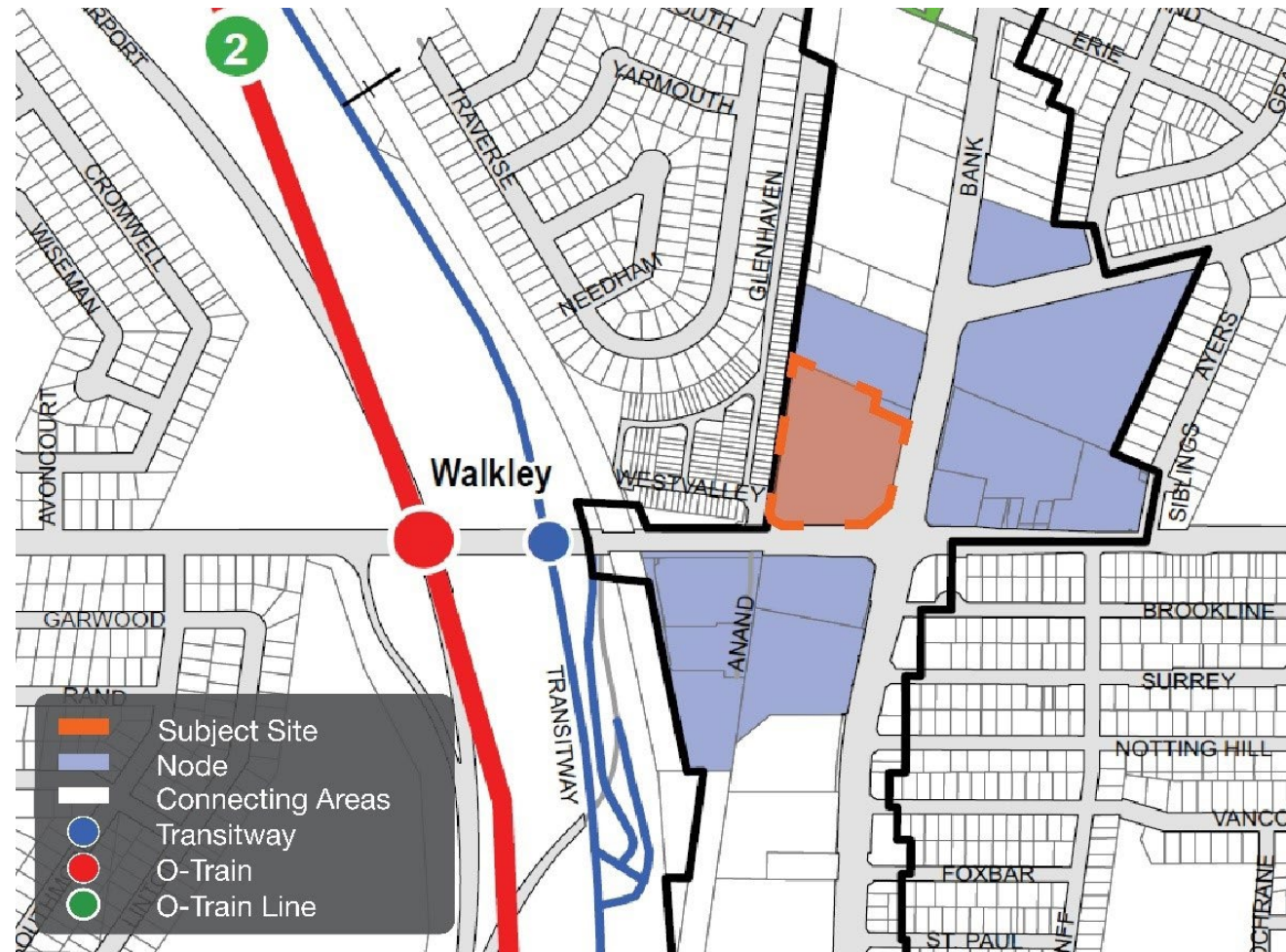
The proposed site design contributes to the evolution of a **15-minute neighbourhood** by:

- Locating buildings and store entrances along public streets (Bank Street and Walkey Road).
- Establishing an internal circulation pattern that includes direct and safe street and multi-use path connections to the surrounding built and planned urban fabric.
- Including a public street grid and pedestrian and cycling network to maximize connectivity to the surrounding street network and transit, with vehicular parking located underground.
- Building arrangement and design includes facade treatments, articulation, building materials and site furnishings that are comfortable at the pedestrian scale.



BANK ST SECONDARY PLAN

The **Bank Street South Secondary Plan** guides the long-term design and development of the portion of Bank Street between Riverside Drive and the Walkley rail corridor, and provides direction on land designations, built form, design and mobility. Section 1.4 identifies the **General Land Use and Design Policies** which provide direction for the entire Bank Street South planning area, with the proposed development incorporating the following:

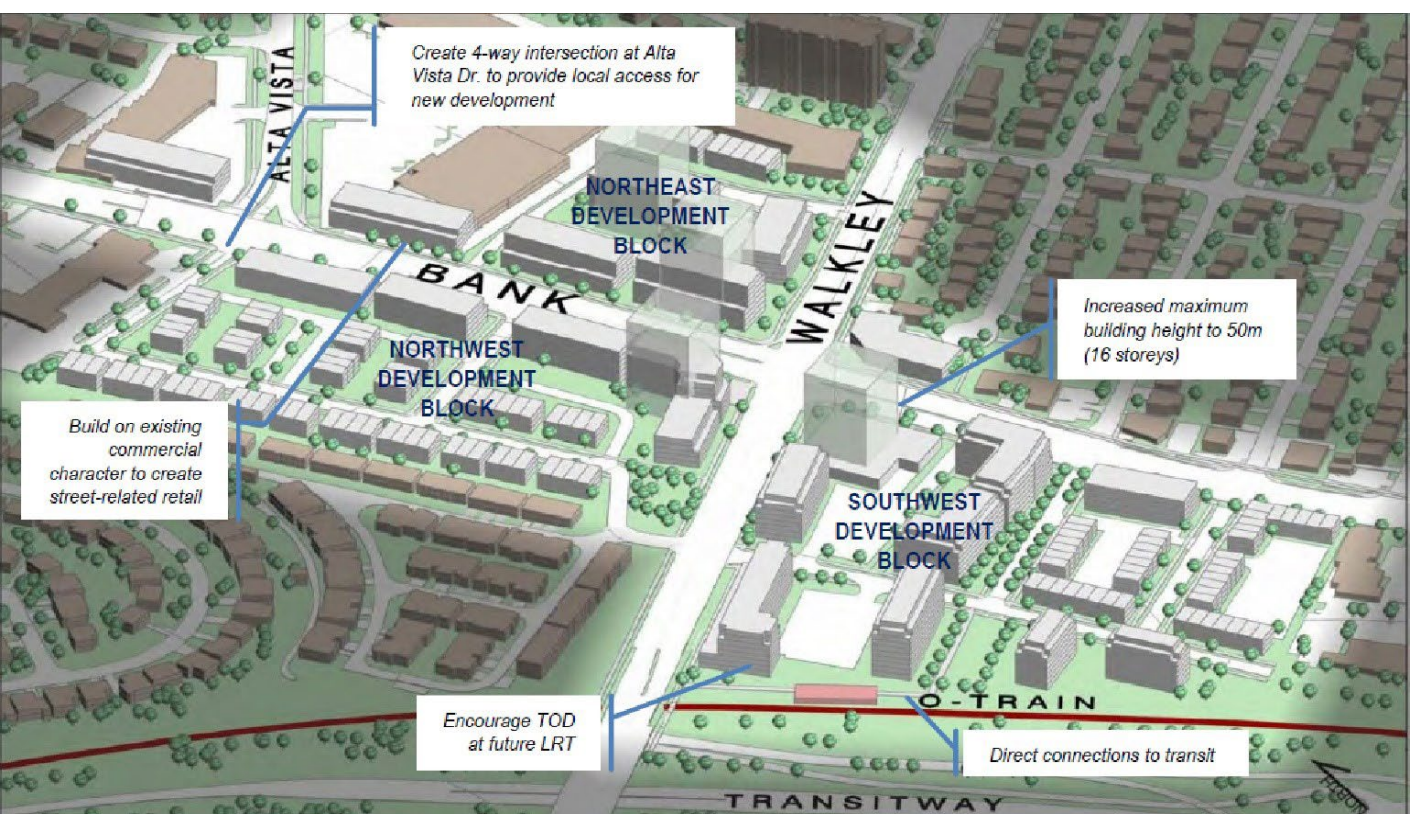


- The proposed **mix-use development** will include street-related retail uses on the ground floor along the Bank Street frontage, with residential uses located on the upper floors of the towers.
- A **well-defined street wall** of four storeys has been proposed along Bank Street to create a visually continuous streetscape and a strong street edge. The buildings along Bank Street will include entrances that are easily accessible directly from the sidewalk for pedestrians.
- **Height transitions** have been maintained between the proposed high-rise buildings and the existing low-rise residential townhomes on Glenhaven Private through generous separation space between buildings and step backs at the upper storeys.
- Parking has been screened from residential properties and the public realm through **below-grade parking** accessible via Bank Street and Walkley Road. On-street parallel parking is proposed internal to the site along both public and private streets to promote the viability of sidewalk-oriented retail.
- **Street trees** are proposed along Bank Street and Walkley Road to establish an avenue of mature trees which gives character, identity and distinction to Bank Street as an important pedestrian and automobile route.

The Secondary Plan segments the area into three distinct Nodes, with the subject site falling within **Node 3: Walkley Road**. Section 3.3 notes this area is to evolve into a medium- to high-density transit-oriented development area with higher-density buildings located on the north side and/or the southwest side of the Walkley Road and Bank Street intersection. The proposed development has responded to these guidelines by:

- Integrating pathways from Bank Street through the site to the future transfer station located at Walkley Road to **enhance the connectivity** of the area.
- Reconfiguring the existing intersection at Glenhaven Private to Walkley Road to **provide safe access** to the existing residential subdivision while accommodating the proposed development's new local street along the northern edge of the property.

COMMUNITY DESIGN PLAN AND ZONING



COMMUNITY DESIGN PLAN

The **Bank Street Community Design Plan** (CDP) is the Council-approved guide to the long-term growth and development of the portion of Bank Street from Riverside Drive to the CN rail line located south of Walkley Road, aimed at creating a framework for the future development of these lands. The subject site is located within Node 3: Bank Street near Walkley Road and Alta Vista Drive, with the following guidelines incorporated into the proposed development:

- Locating high-rise buildings along the north side of the Bank Street / Walkley Road intersection.
- Providing an east-west pedestrian connection through the subject site from Bank Street to the future LRT station at Walkley Road.
- Reconfiguring the existing Walkley Road connection to Glenhaven Private to accommodate the new local street internal to the site.
- Improving the pedestrian environment by including a large, central park within the site, and providing landscaping along the public ROW along Bank Street and Walkey Road to make the walking environment more appealing.



ZONING BYLAW

1822 Bank Street is zoned **Arterial Mainstreet, Subzone 8 (AM8)** in the City of Ottawa Comprehensive Zoning By-law (2008-250). The purpose of the Arterial Mainstreet zone is to:

- Accommodate a broad range of uses including retail, service commercial, offices, residential and institutional uses in mixed-use buildings or side by side in separate buildings; and
- Impose development standards that will promote intensification while ensuring that they are compatible with the surrounding uses.
- The AM zone is a flexible zone permitting a wide range of residential and non-residential uses to support mixed-use redevelopment.

APPLICABLE DESIGN GUIDELINES



Urban Design Guidelines for High-Rise Buildings

These guidelines address the **design of high-rise buildings** (10+ storeys) in **relation to their context, built form, and impact on pedestrian realm**. The following design guidelines are applicable to the development:

- The base of the buildings define the street wall context along Bank Street and Walkley Road.
- The proposed towers provide separation distances greater than 23 metres and respect the angular plane from the towers along Walkey Road to the existing townhomes along Glenhaven Private.
- The proposed towers also reflect the preferred tower floor plate size of approximately 750 square metres.
- The proposed development would achieve the desired base-middle-top expression.
- Parking is provided below-grade as well as on-street parallel parking.
- The subject property will continue to work to satisfy guidelines regarding building form and public realm as the concept is further developed.



Transit-Oriented Development Guidelines

These guidelines apply to all development **within 600 metres walking distance of a rapid transit stop or station**.

The subject site is located less than 300 metres walking distance to the Walkley Light Rail Transit (LRT) station, and the proposed development responds to the following design guidelines:

- Provides transit supportive, mixed-use development including high density residential uses, within 300 metres walking distance of a rapid transit stop or station.
- Lays out new pedestrian connections, permitting pedestrian movement throughout the site from Bank Street through to Walkley Road and beyond.
- Locates buildings close to each other and along the front of the street to encourage ease of walking between buildings and to public transit.
- Discourages surface parking through the inclusion of an underground parking garage.
- Transitions in scale between the higher density development along Walkey Road and the adjacent low-rise neighbourhood along Glenhaven Private, achievable through setbacks, stepbacks, and adequate separation distances between the towers and low-rise buildings

APPLICABLE DESIGN GUIDELINES



Urban Design Guidelines for Development on Arterial Mainstreets

These guidelines provide urban design guidance at the planning application stage in order to assess, promote and achieve appropriate development along Arterial Mainstreets. The proposed development meets the following guidelines, among others:

- Locates the new buildings along the public street edges and aligns the streetwall buildings with the existing built form or with the average setback of the adjacent buildings in order to create a visually continuous streetscape.
- Provides architectural landscaping features at the corner of Bank Street and Walkey Road to emphasize the public streets and enhance the streetscape.
- Orients the front façade to face the Bank Street and Walkley Road and locate front doors to be visible, and directly accessible, from these streets.
- Creates attractive public and semi-public outdoor amenity spaces through the creation of a central park.
- Incorporates setbacks on the upper floors of taller buildings to help achieve a human scale and more light on the sidewalks

Bird Safe Design Guidelines

Minimizing the Transparency and Reflectivity of Glazing:

- Avoiding monolithic, undistinguished expanses of glazing;
- Incorporating visual interest or differentiation of material, texture, colour, opacity, or other features to fragment reflections.
- Where green roofs, rooftop gardens or terraces are included in a design, any adjacent glazing should also be treated to a height of 4 metres from the surface of the roof or terrace or the height of the adjacent mature vegetation, whichever is greater.

Avoiding or Mitigating Design Traps:

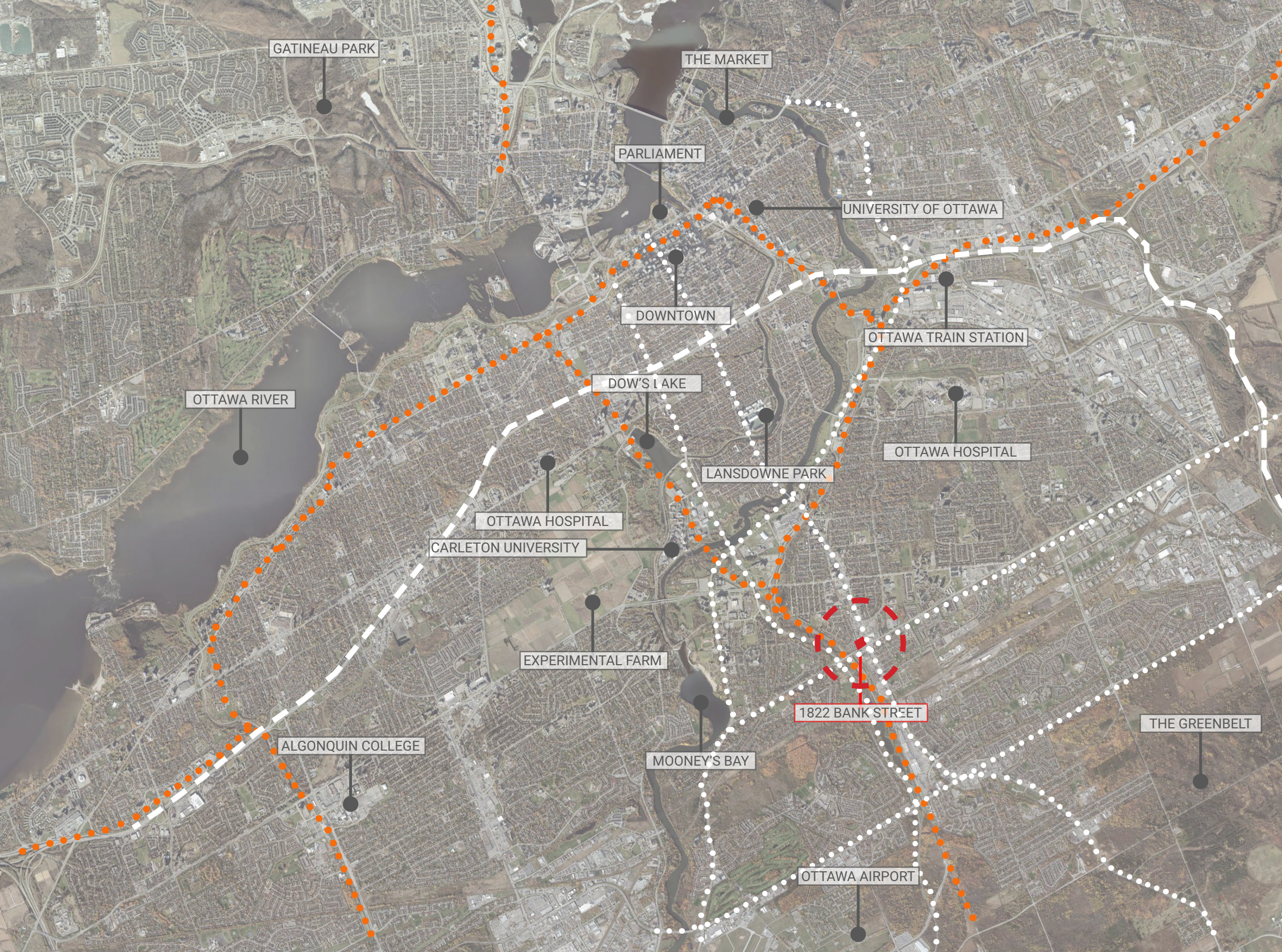
- All glazing that could create a fly-through, mirror maze or black hole effect should use bird-safe glass or integrated protection measures;
- Glass corners should be treated to render them bird-safe for at least 5 metres in each direction;

Creating Safe Bird-Friendly Landscaping:

- Designing landscape plantings to minimize reflections of trees and shrubs in nearby buildings.
- Minimizing the reflection of rooftop landscapes in adjacent building features or surrounding properties.
- Avoiding locating ornamental fountains, ponds, stormwater retention basins, wetlands, swales or related infrastructure near glass façades or windows.



CONTEXT

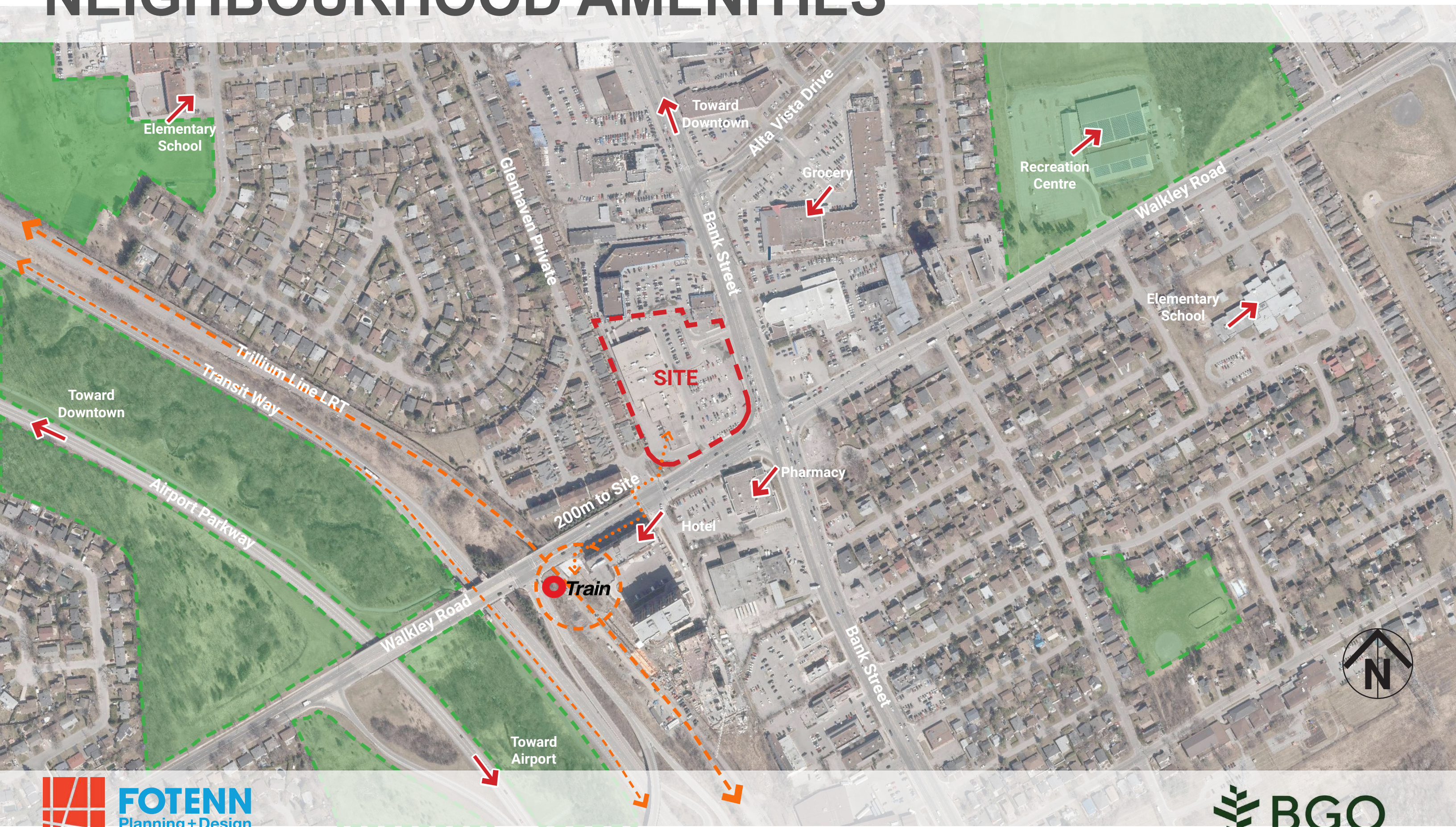


MAP KEY

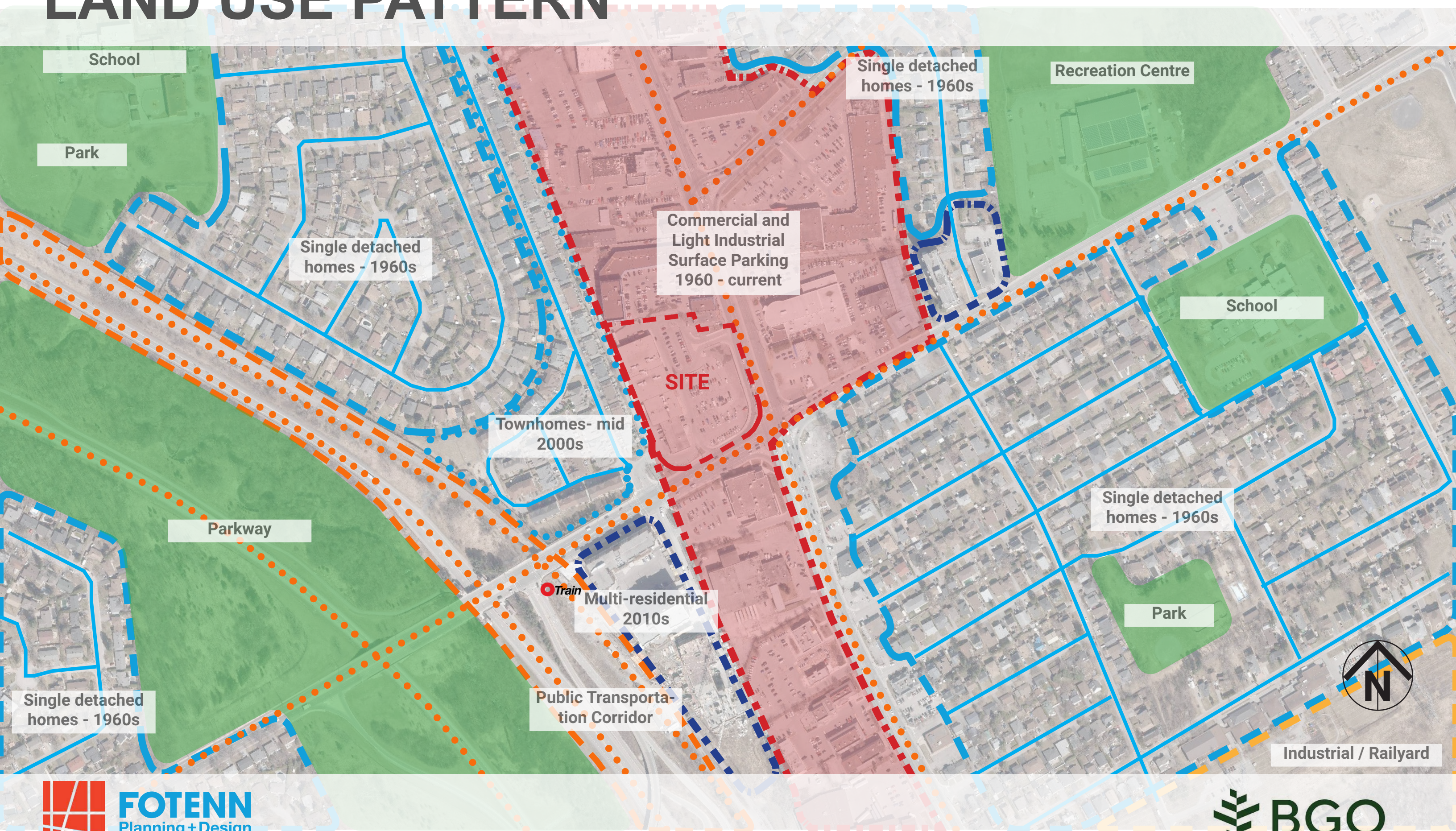
- PLACE
- POINT OF INTEREST
- SITE LOCATION
- RAPID TRANSIT
- MAJOR ROAD
- HIGHWAY



NEIGHBOURHOOD AMENITIES



LAND USE PATTERN



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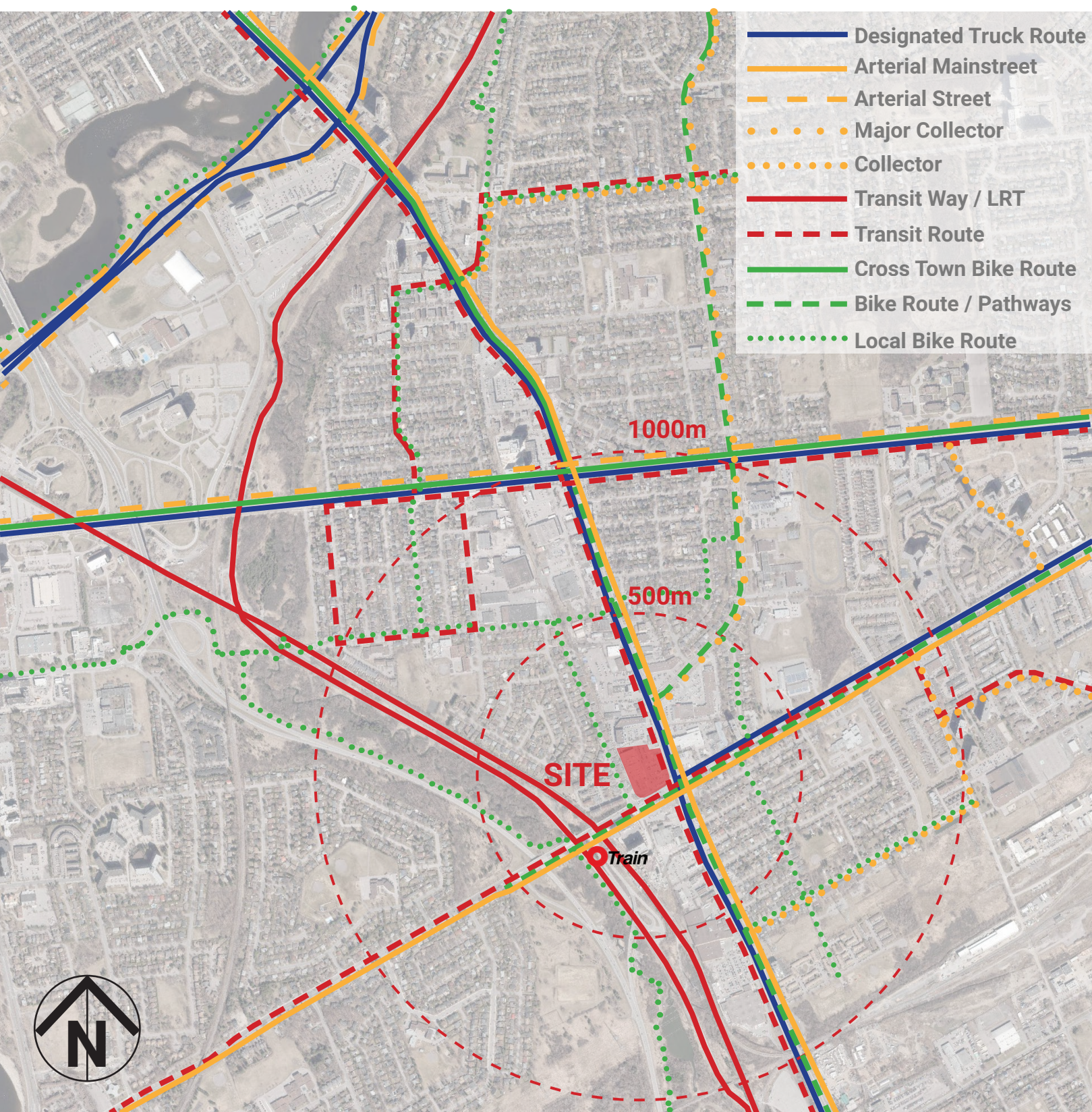
HOBIN
ARCHITECTURE

Ferris + Associates
a division of NAK DS Inc



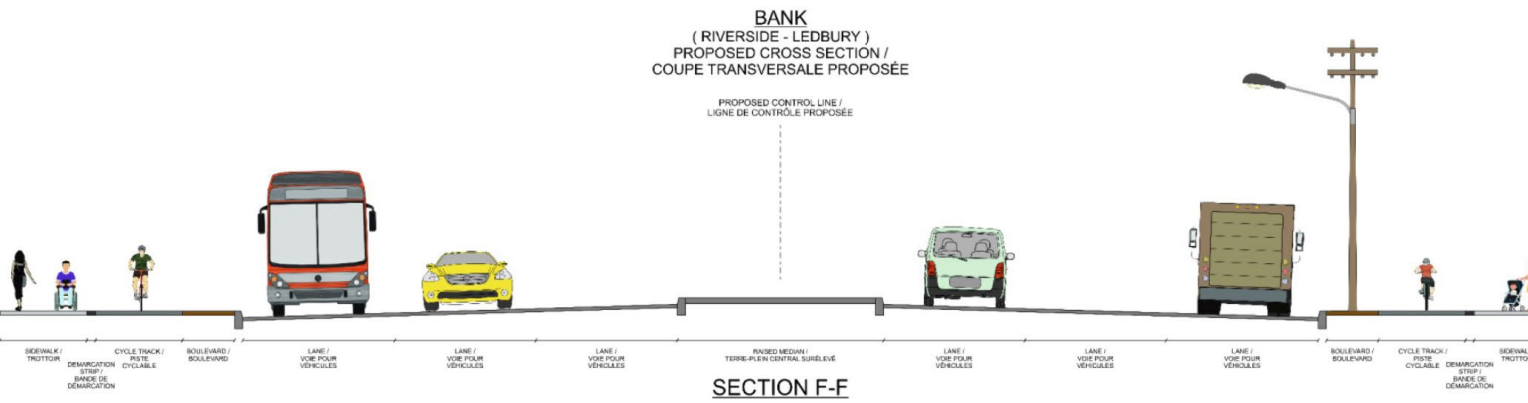
BGO

TRANSPORTATION

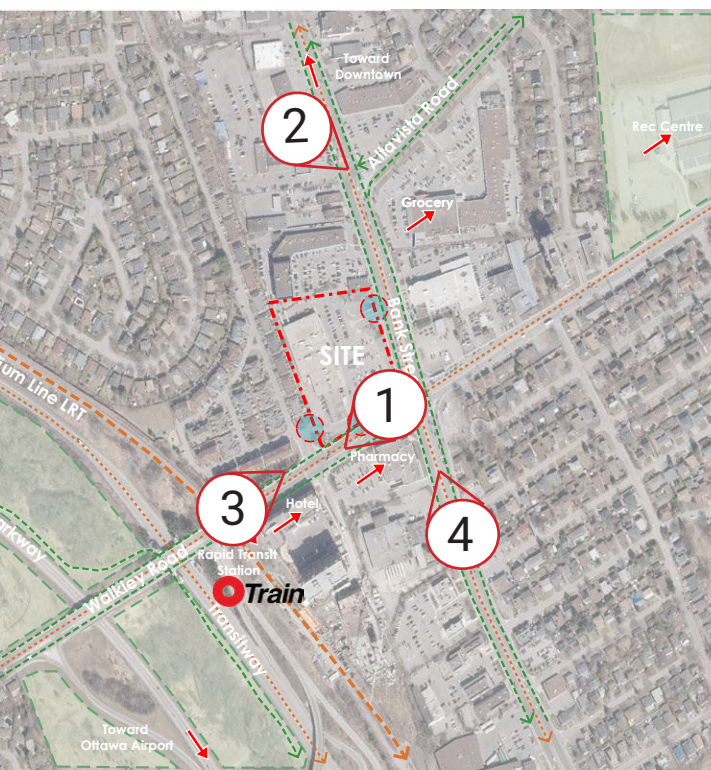


Bank Street Renewal Project - Phase 2

- Proposed Start ~2028
- New below grade services
- Road section redesign to Complete Street standard with Protected Intersections
- New Active Transportation Infrastructure



IMAGES OF THE SURROUNDING AREA



1. View from Walkley toward Anand Private



2. View toward southeast corner of Alta Vista

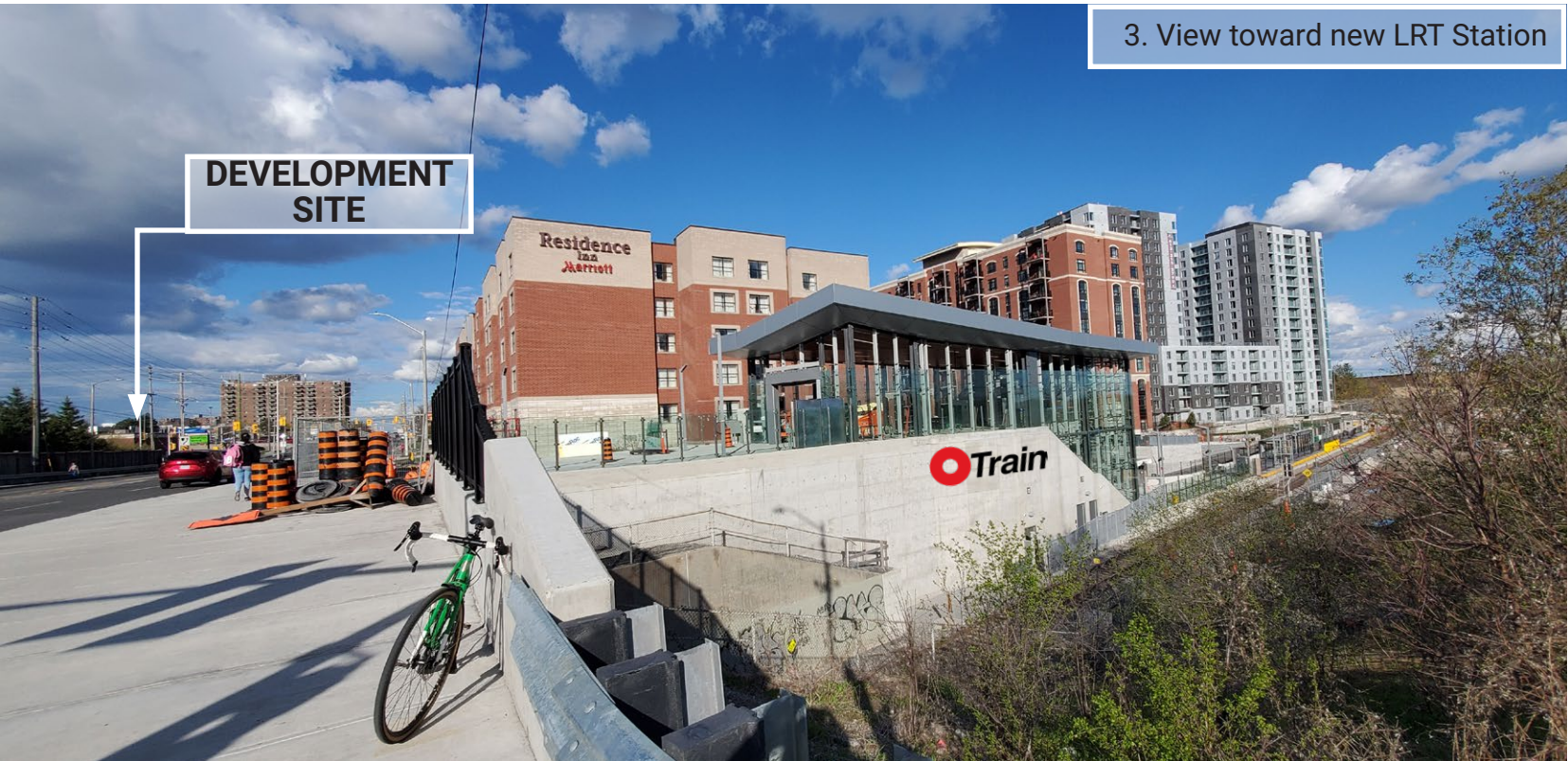


3. View Anand Private looking East

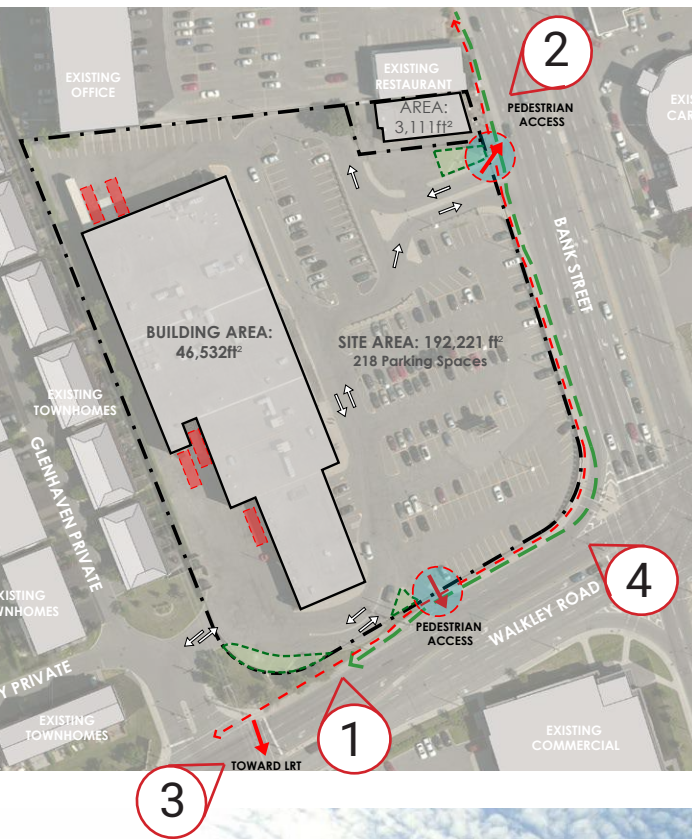


4. View on Bank Street looking North

IMAGES OF THE SURROUNDING AREA



IMAGES TOWARD THE SITE



1. View from west on Walkley near LRT entrance



2. View from Bank and Walkley



3. View from LRT Entrance



4. View from Bank and Walkley

EXISTING CONDITIONS



TOTAL GROSS AREA: 46,532 sq.ft.

Commercial Use

- 1 Storey
- Multiple Retail Units

Parking (Surface)

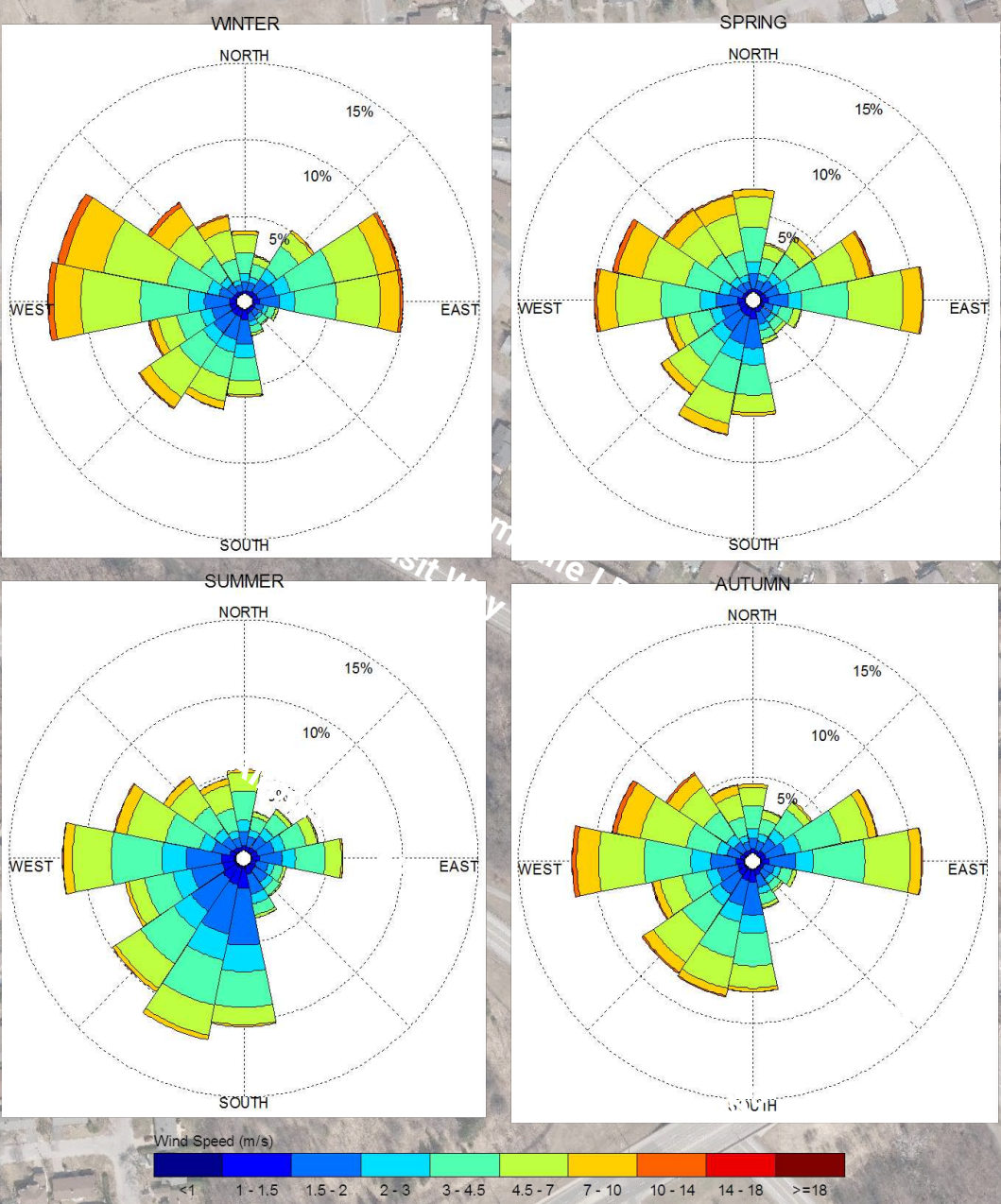
- 218 spaces
- 4.7 spaces per 1000 sq.ft.

- Landscape Space
- Pedestrian Access Point
- Pedestrian Desire Lines
- Future Cycle Tracks
- Vehicle Movement - Private
- Loading Space

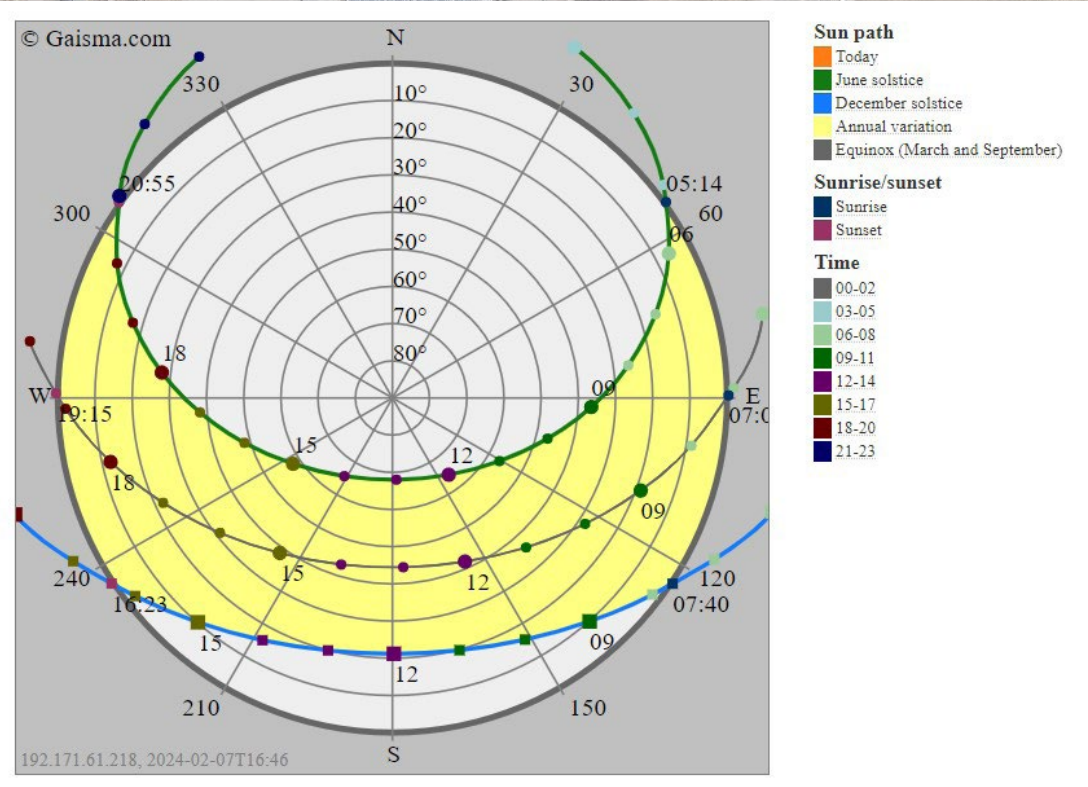
SITE MICRO CLIMATE

PREVAILING WINDS

MACDONALD-CARTIER INTERNATIONAL AIRPORT, OTTAWA, ONTARIO



SUN PATH



SOLAR ENERGY

Ottawa, Canada - Solar energy and surface meteorology												
Variable	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec
Insolation, kWh/m²/day	1.48	2.37	3.49	4.42	5.08	5.6	5.57	4.86	3.6	2.29	1.48	1.23
Clearness, 0 - 1	0.45	0.49	0.5	0.48	0.47	0.49	0.5	0.5	0.46	0.41	0.4	0.43
Temperature, °C	-10.69	-9.35	-3.74	4.81	12.71	18.43	20.97	19.77	15.07	7.8	0.81	-6.8
Wind speed, m/s	4.4	4.28	4.44	4.34	4.21	4.14	3.75	3.78	4.15	4.27	4.46	4.41
Precipitation, mm	59	58	62	65	73	83	88	86	80	71	84	82
Wet days, d	14.2	11.5	11.4	11	12.1	12.2	11.4	11.3	12	12.2	14.3	15.5

DESIGN PROPOSAL



HOBIN
ARCHITECTURE

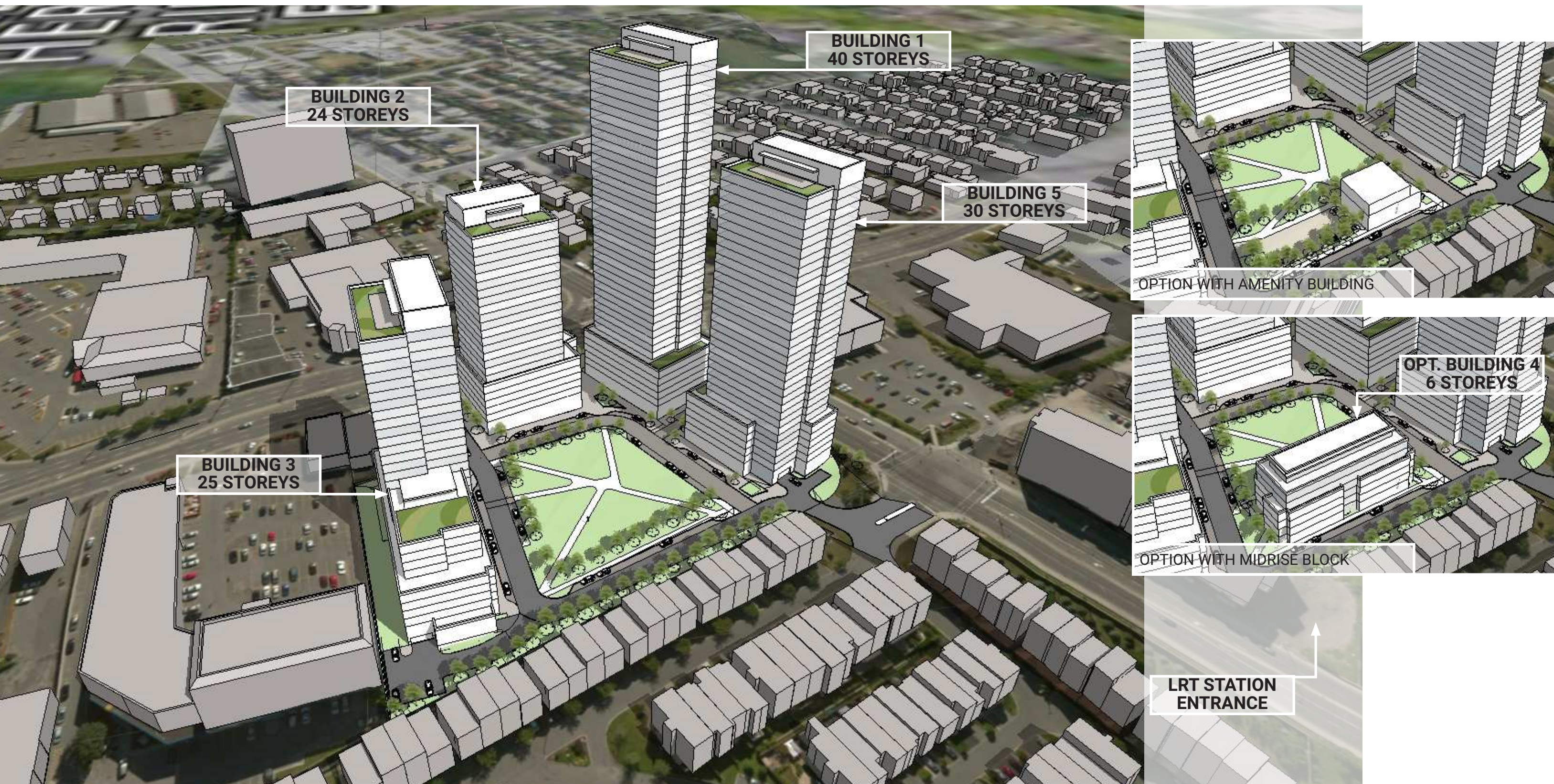
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BGO

PROPOSED MASSING



CONCEPT PLAN



TOTAL GROSS: 1,161,243sq.ft.
TOTAL NET : 938,899sq.ft.
TOTAL UNITS: +/- 1,155

BUILDING 1 (40 Storeys - Mixed-Use)
CGA: 361,630 sq.ft.
GLA: +/- 293,157 sq.ft.
UNITS: +/- 360 units

BUILDING 2 (24 Storeys - Mixed-Use)
CGA: 216,090 sq.ft.
GLA: +/- 173,814 sq.ft.
UNITS: +/- 209 units

BUILDING 3 (25 Storeys - Mixed-Use)
CGA: 304,159 sq.ft.
GLA: +/- 246,130 sq.ft.
UNITS: +/- 298 units

BUILDING 5 (30 Storeys - Rental)
CGA: 279,364 sq.ft.
GLA: +/- 225,798 sq.ft.
UNITS: +/- 288 units

Optional (6 Storeys - Rental / Seniors)
CGA: 60,636 sq.ft.
GLA: +/- 46,441 sq.ft.
UNITS: +/- 59 units

COMPARISON



Existing Conditions
Building Area: 46,532 sq.ft.
Number of Homes near Transit: Zero
Surface Parking Spaces: 218
Public Realm: Zero
Total Green Area: 1.4% of Site Area (2,650 sq.ft.)



Proposed Development Options
Building Area: 57,800 sq.ft
Gross Area: 1.161 - 1.221M sq.ft.
Number of Homes near Transit: 1,100 - 1,200
Surface Parking Spaces: 43 + below grade parking
Public Realm*: 25 - 30% of Site Area (43,000 - 55,000 sq.ft)
Total Green Area*: 32 - 36% of Site Area (65,200 - 68,200 sqft)

*including green roofs; open space opportunity to be determined

BGO PRECEDENT PROJECTS



HOBIN PRECEDENT PROJECTS



CITY PARK DRIVE REDEVELOPMENT (RIOCAN)



LANDSDOWNE PARK - THE RIDEAU



1640 CARLING AVENUE REDEVELOPMENT (RIOCAN)

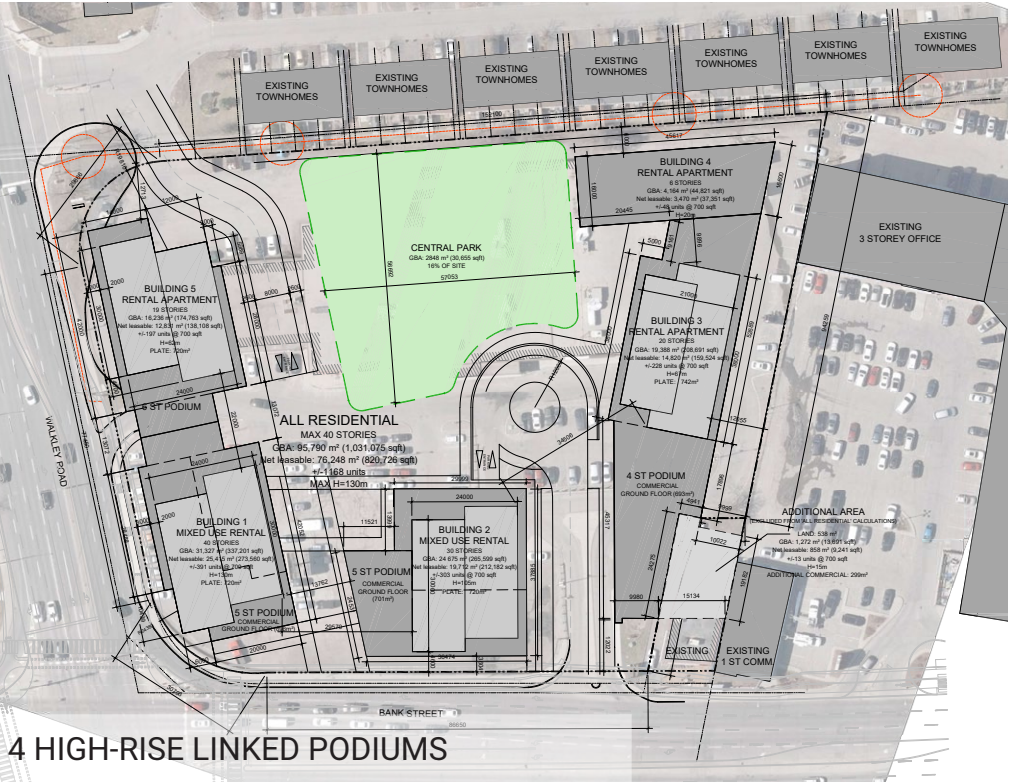
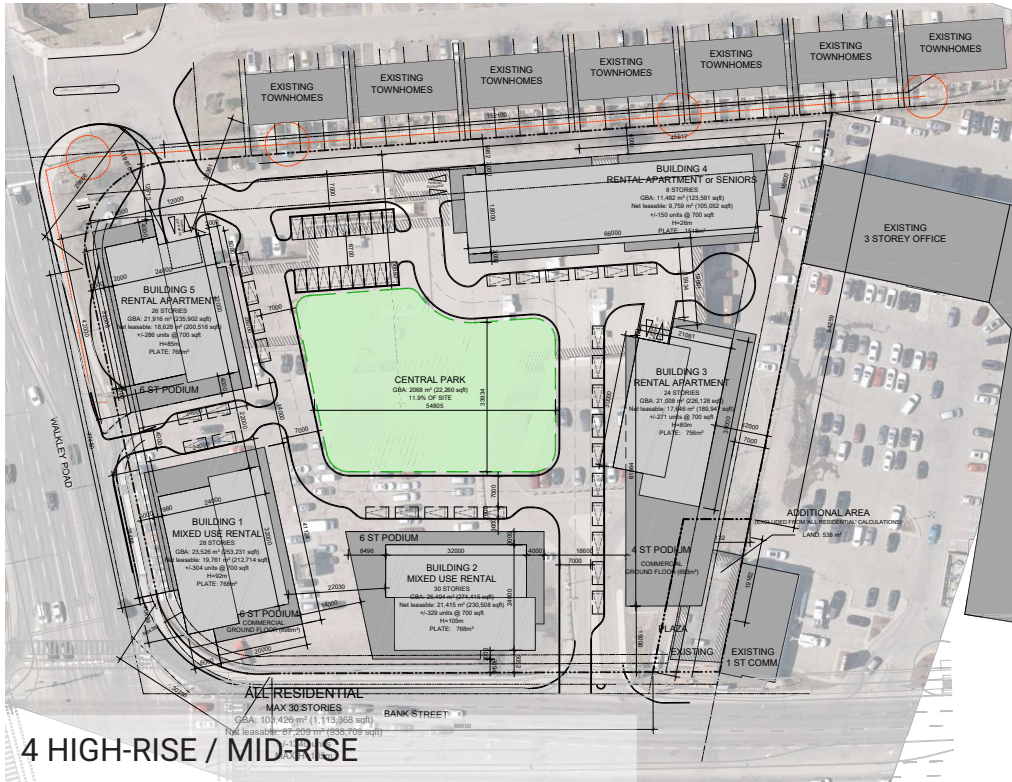


GLADSTONE AND LORETTA REDEVELOPMENT



LANDSDOWNE PARK - THE VIBE

ALTERNATIVE OPTIONS EXPLORED



KEY DESIGN NARRATIVES

BUILDING FORM - PODIUMS AND
TOWERS

PLACEMAKING

BUILDING EDGES

INTERIOR STREETS

PATHWAYS TO TRANSIT



HOBIN
ARCHITECTURE

FOTENN
Planning + Design

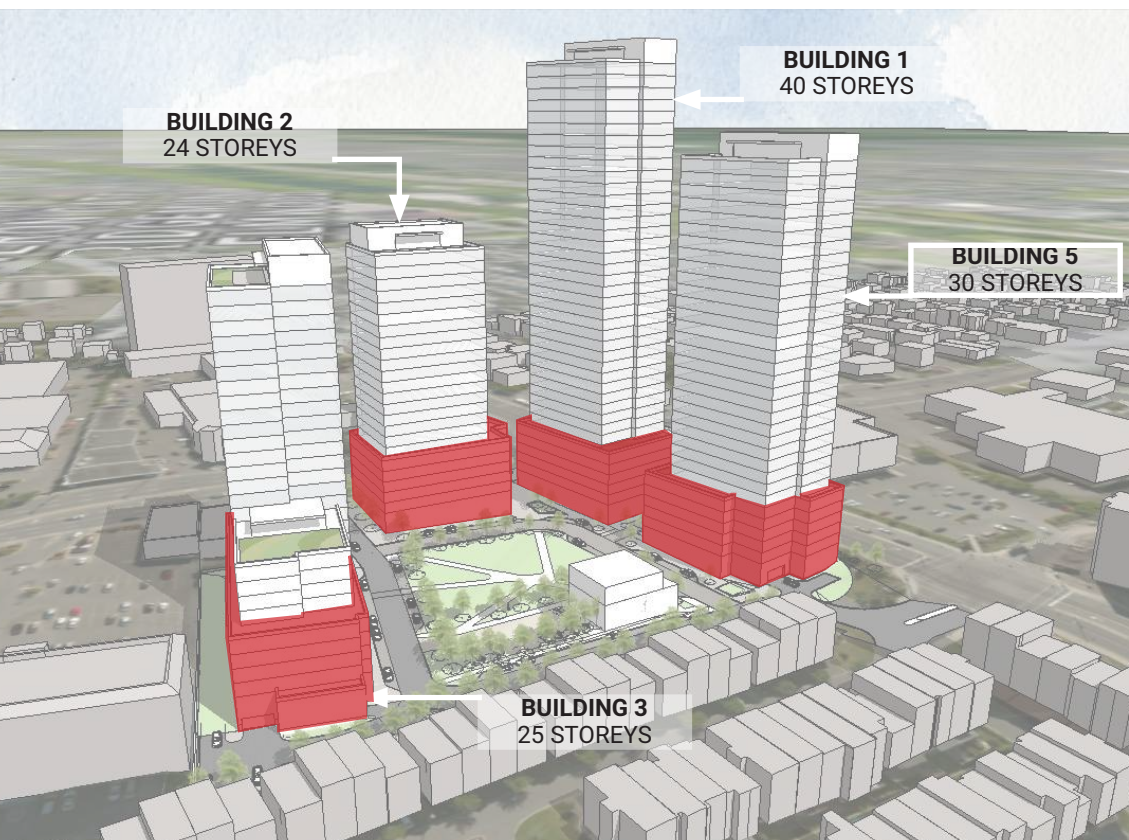
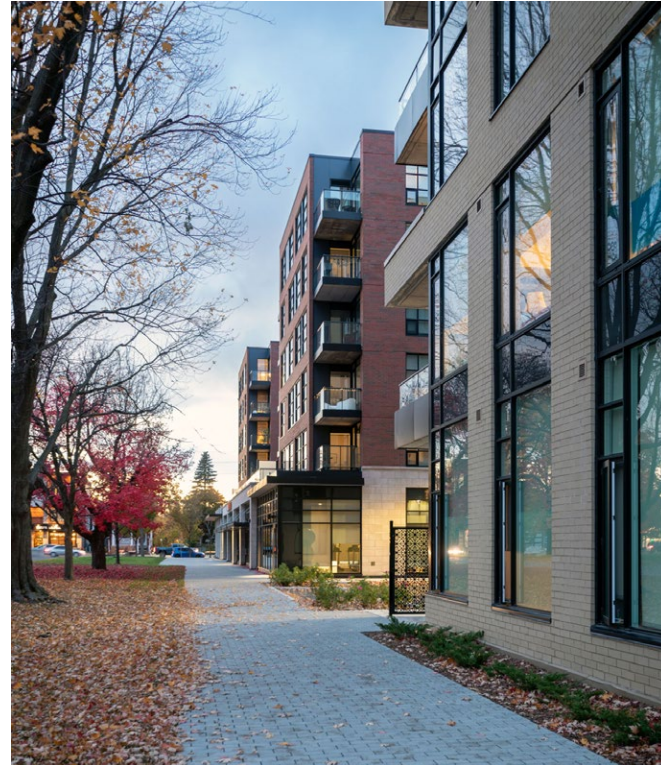
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BUILT FORM - PODIUM AND TOWER

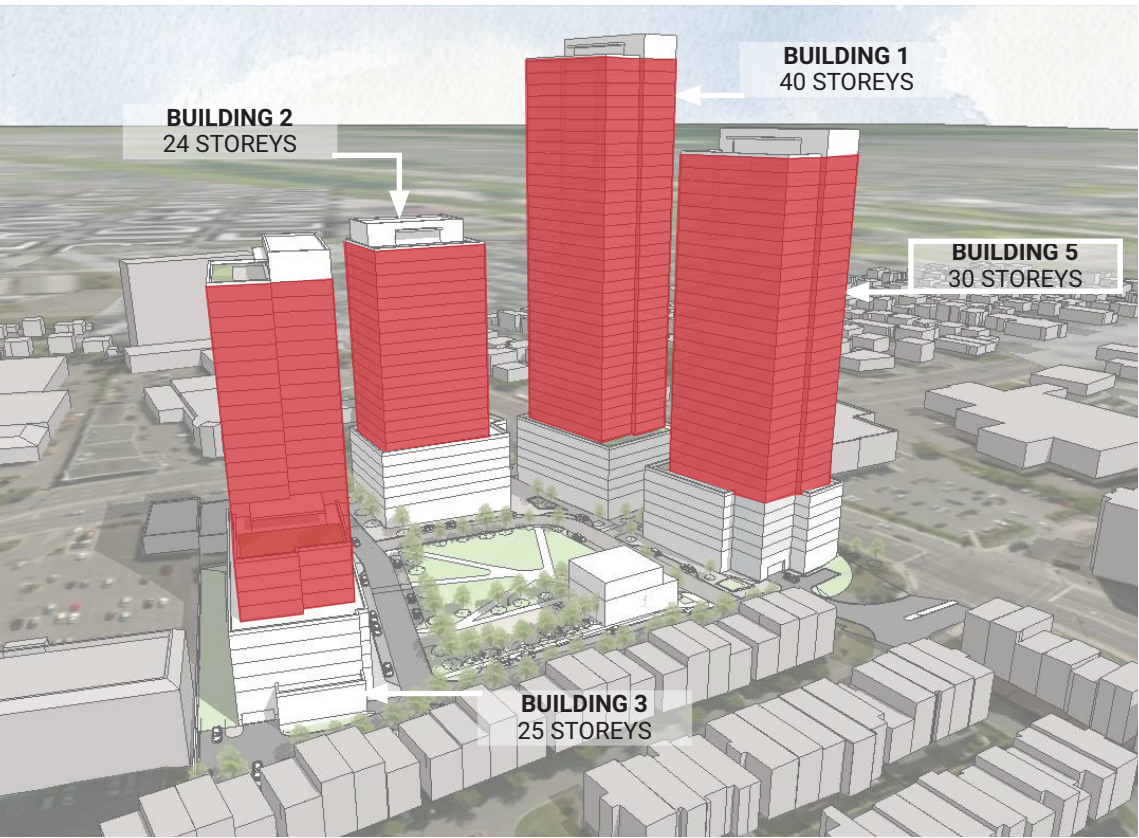
The high-rise built form addresses the three distinct building elements listed in the City's high-rise design guidelines; the ground level edge condition and podium, the main body of the tower and the articulation of the top.

- 1 Street Edge + Podium Condition
- 2 Tower Body
- 3 Top



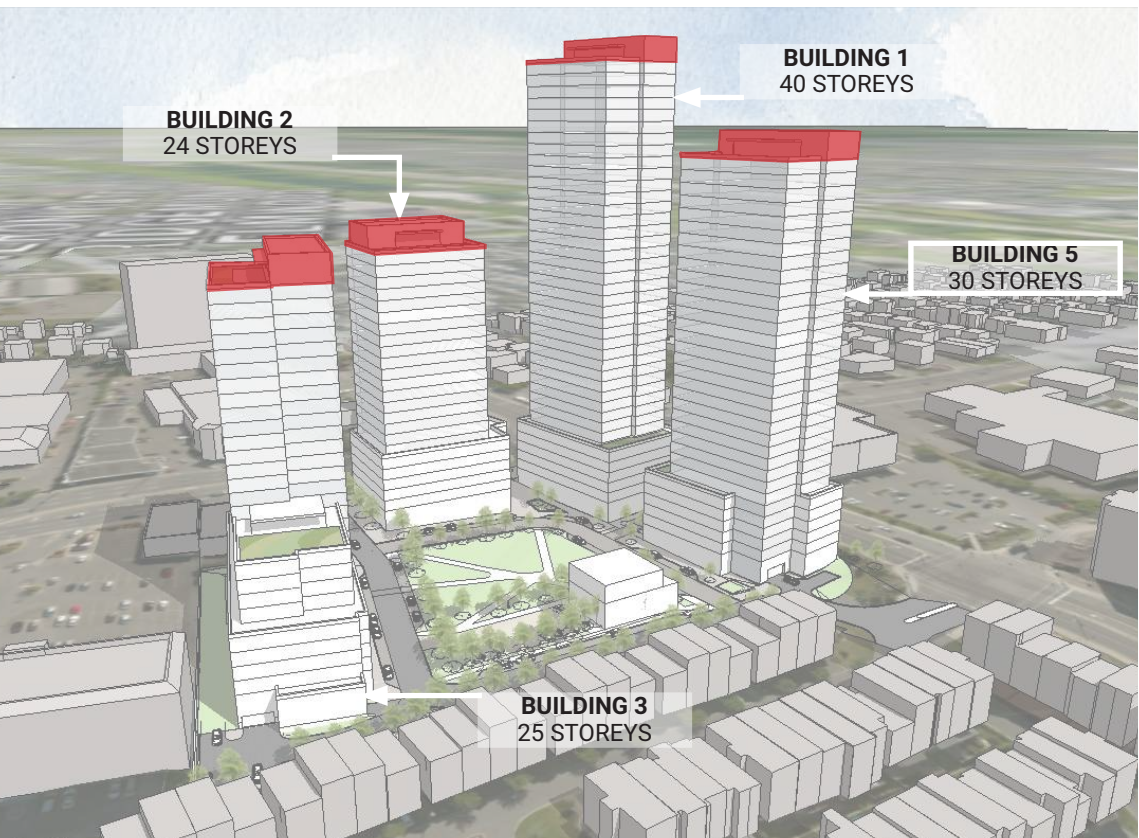
BUILT FORM - PODIUM AND TOWER

- 1 Street Edge + Podium Condition
- 2 Tower Body
- 3 Top



BUILT FORM - PODIUM AND TOWER

- 1 Street Edge + Podium Condition
- 2 Tower Body
- 3 Top



PLACEMAKING

Open Space

The “backyard” of the site, the “Open Space” is a recreational greenspace consisting of large grass areas and pathways to draw users through the site. Space for playful structures further distinguishing the pathway while providing opportunities for lighting and sound or music, and various seating opportunities. This cheerful space, centrally located, animates the entirety of the site. This space would provide additional animation and amenity to the development and surrounding community.



*Open Space Opportunity TBD



BUILDING EDGES

The design of the building podiums will be to support active use and animation at grade. This will include amenities, lobbies, and small retail spaces where applicable. Design will provide for future flexibility as the street evolves and the need for increased amounts of local retail grows.

- Small Retail or Commercial
- - - Building Active Uses



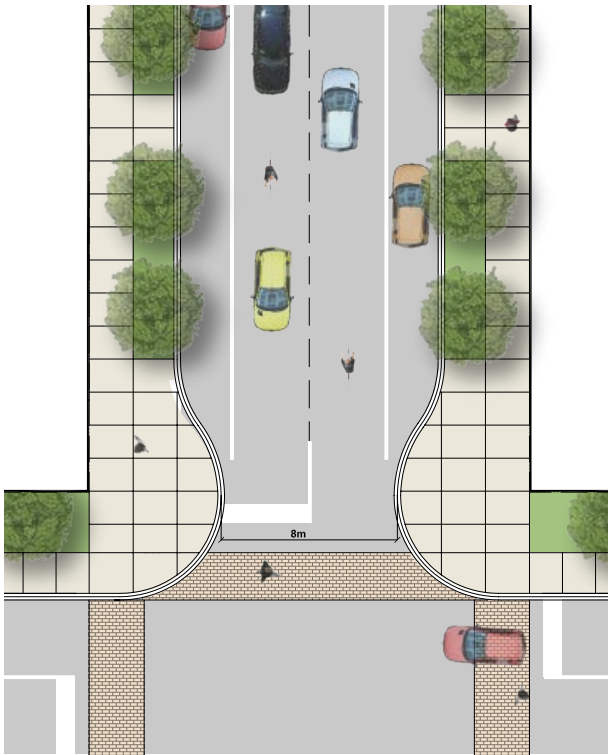
*Open Space Opportunity TBD



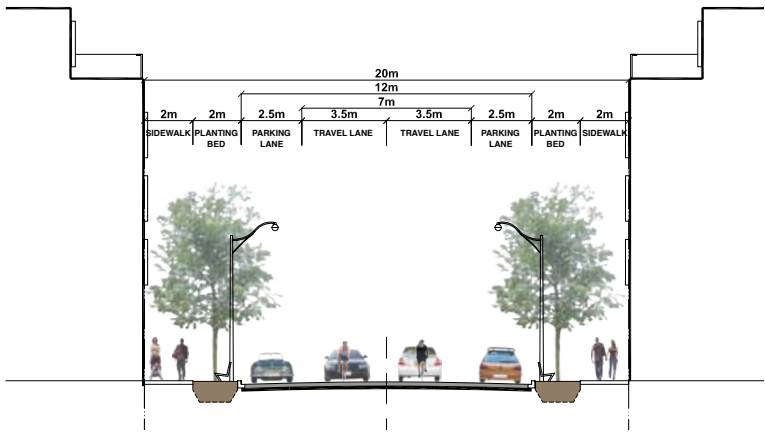
INTERIOR STREETS

Interior Streets

The interior streets, designed to contrast the high volume municipal thoroughfares of Bank Street and Walkley Road provide many functional purposes. The streets separate the park from the Southern and Eastern buildings, and provide at-grade parking and access to below grade parking.



The internal street is proposed to be 16.5m R.O.W. providing sufficient space for both pedestrians and vehicular traffic to circulate while maintaining a green landscape buffer along the street edges.



*Open Space Opportunity TBD



PATHWAYS TOWARD TRANSIT

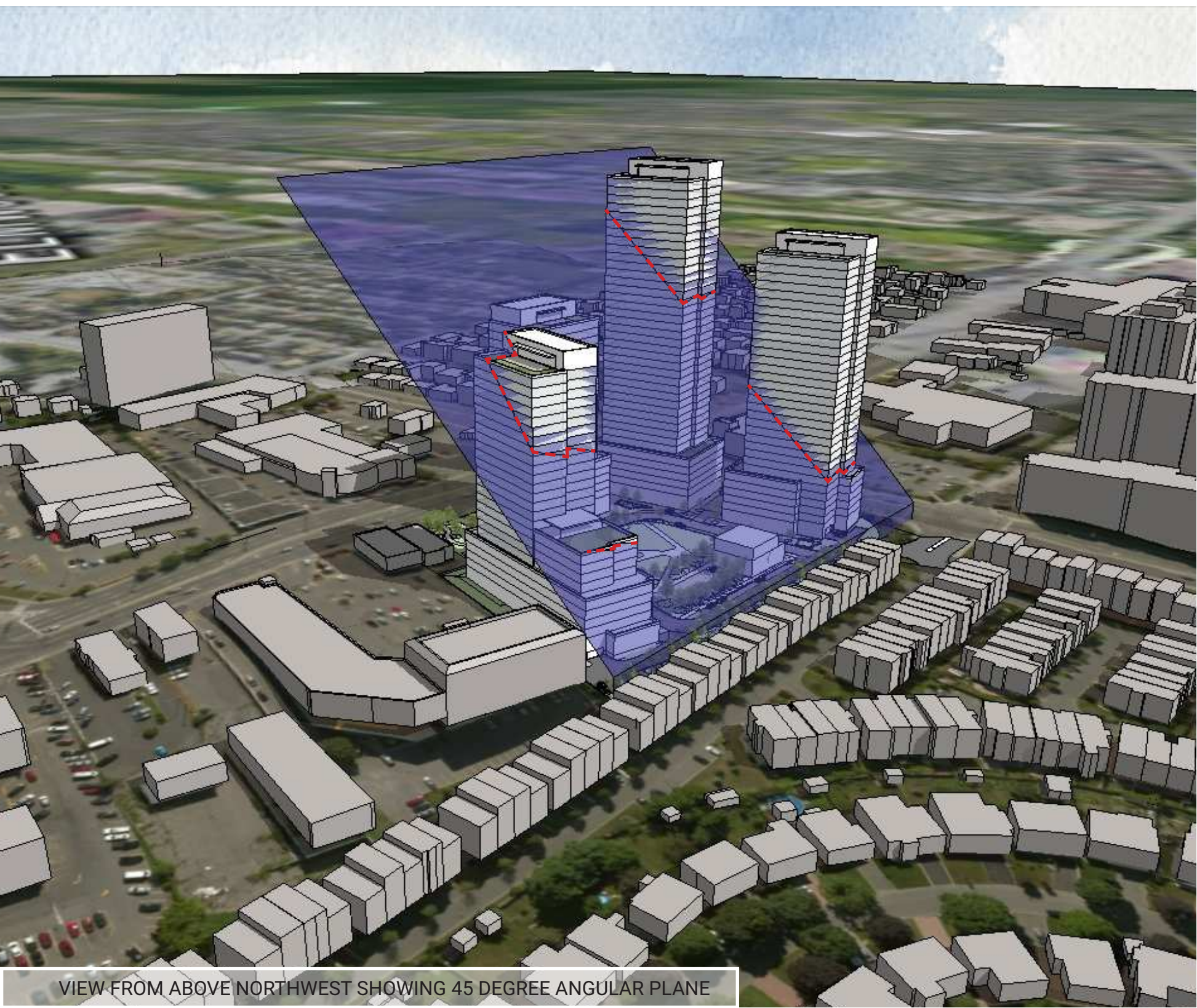
The proposed development provides opportunities for developing desire lines for movement through the site - both for residents and for visitors from the surrounding community. There is a strong desire line toward the Walkley LRT station that asks for a diagonal path from the north east to the south west and bring people into and through the Open Space.



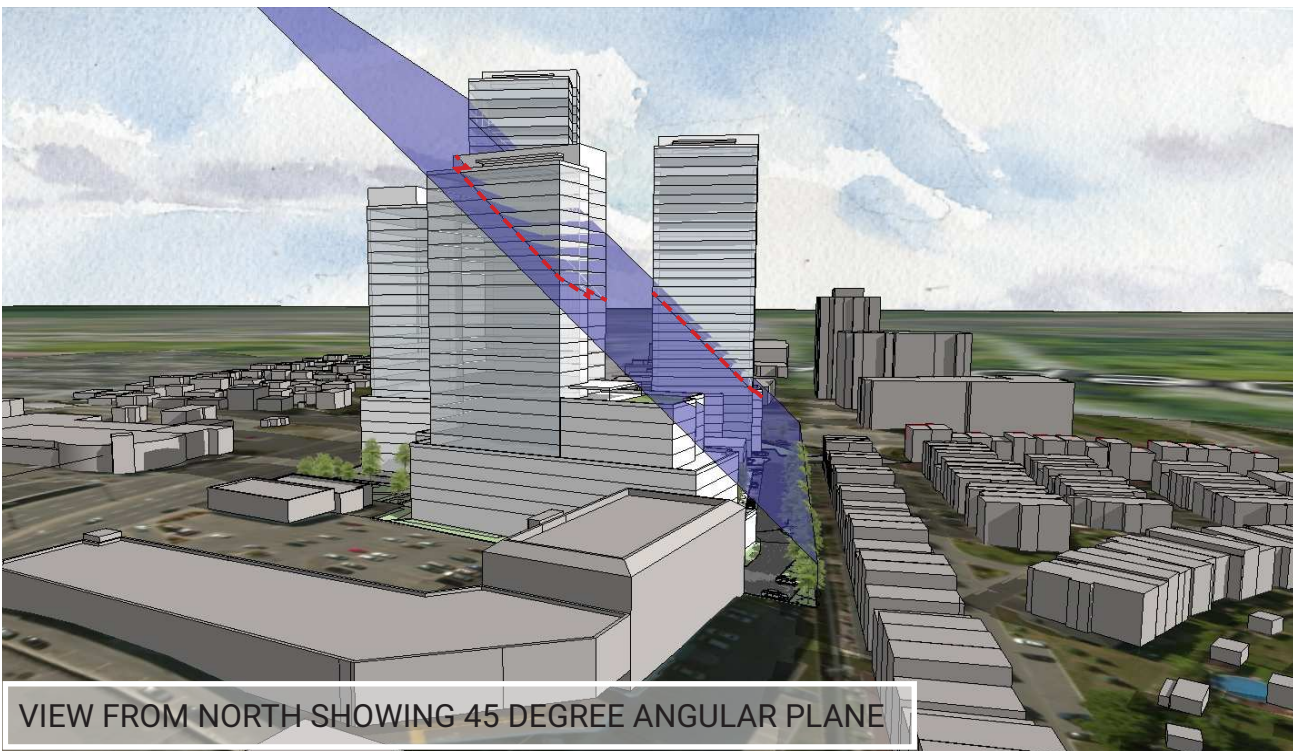
*Open Space Opportunity TBD



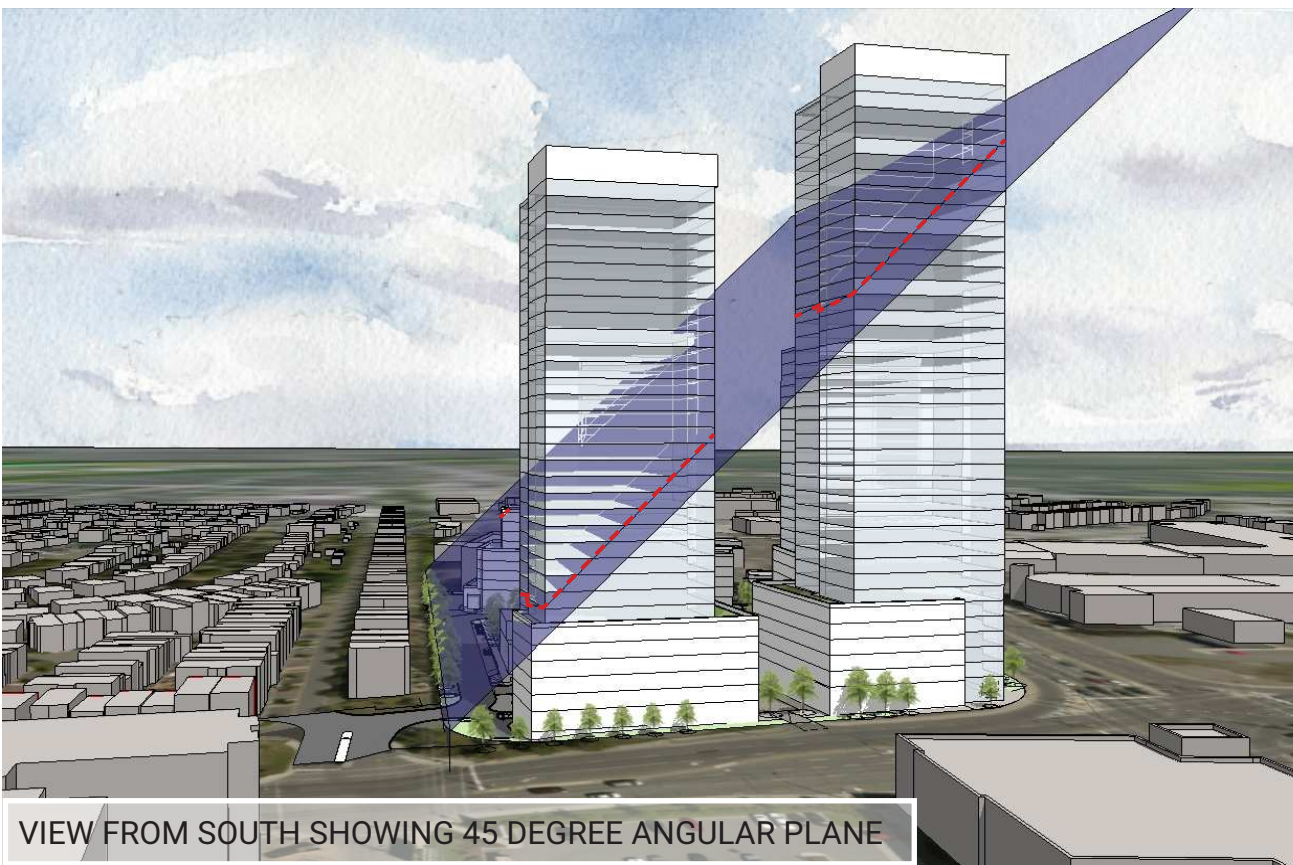
RELATIONSHIP TO ADJACENT NEIGHBOURS



VIEW FROM ABOVE NORTHWEST SHOWING 45 DEGREE ANGULAR PLANE



VIEW FROM NORTH SHOWING 45 DEGREE ANGULAR PLANE



VIEW FROM SOUTH SHOWING 45 DEGREE ANGULAR PLANE

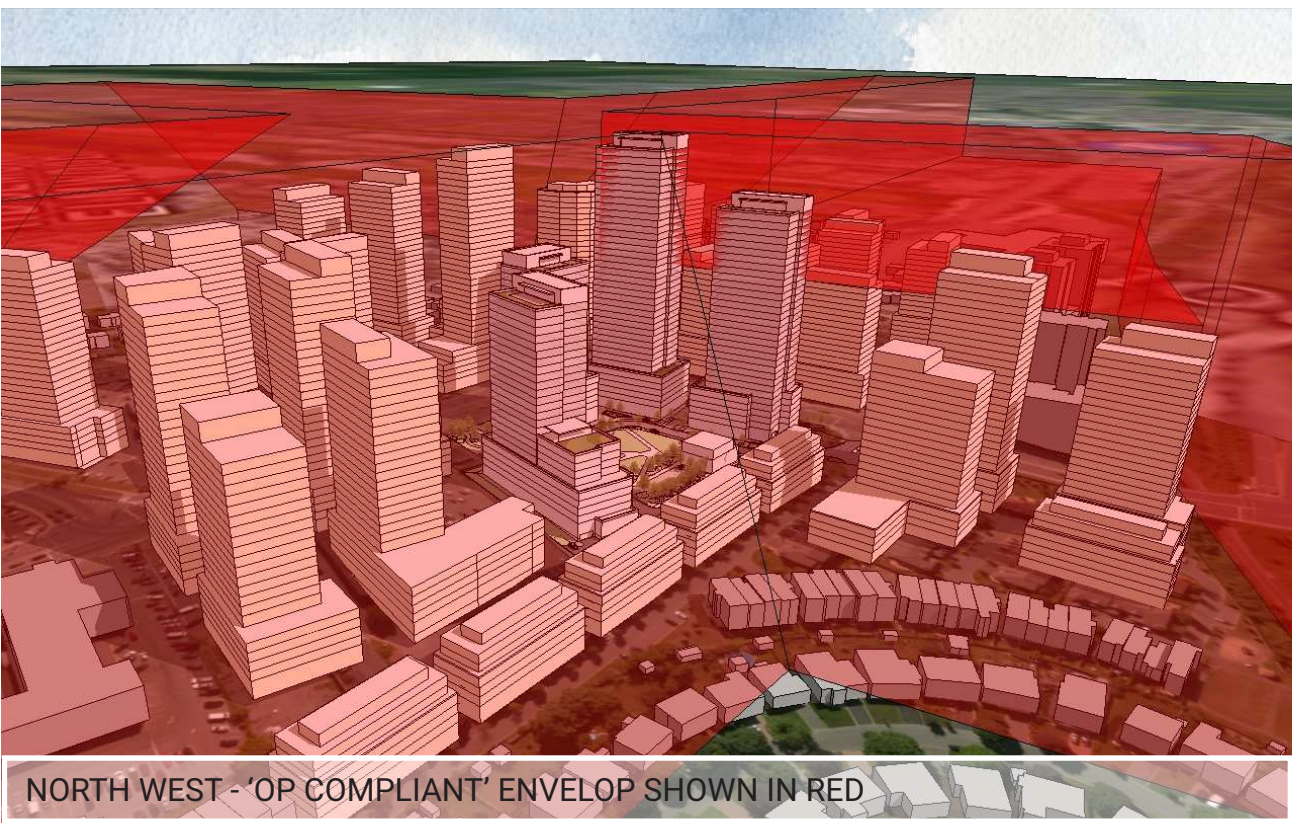
FUTURE DEVELOPMENT POTENTIAL



NORTH WEST - PROPOSED WITH FUTURE 'OP COMPLIANT' DEVELOPMENT



NORTH WEST - PROPOSED IN EXISTING CONDITIONS



NORTH WEST - 'OP COMPLIANT' ENVELOP SHOWN IN RED

FUTURE DEVELOPMENT POTENTIAL



SOUTH EAST - PROPOSED WITH FUTURE 'OP COMPLIANT' DEVELOPMENT



NORTH - PROPOSED WITH FUTURE 'OP COMPLIANT' DEVELOPMENT



SOUTH WEST - PROPOSED WITH FUTURE 'OP COMPLIANT' DEVELOPMENT

SUSTAINABILITY APPROACH

BGO JOINED THE NET ZERO ASSET MANAGERS INITIATIVE

In 2021, BGO joined the Net Zero Asset Managers (NZAM) initiative, committing to support the goal of net zero greenhouse gas (GHG) emissions by 2050 or sooner.¹

BGO'S FIRM-LEVEL NET ZERO 2030 INTERIM TARGETS²

Scope 1 & 2 GHG emissions (fuel and electricity for corporate offices)

- **72.6% reduction** in GHG emissions intensity

Scope 3 emissions (including indirect GHG emissions, including our financial emissions)

- **50% reduction** in GHG emissions intensity for multi-family residential buildings
- **57% reduction** in GHG emissions intensity for commercial buildings

¹ The Net Zero Asset Managers ("NAZM") initiative is an international group of asset managers committed to supporting the goal of net zero greenhouse gas emissions by 2050 or sooner, in line with global efforts to limit warming to 1.5 degrees Celsius; and to supporting investing aligned with net zero emissions by 2050 or sooner. As a signatory to the NAZM initiative, BGO has pledged to work in partnership with asset owner clients on decarbonisation goals, consistent with an ambition to reach net zero emissions by 2050 or sooner across all assets under management. More information is available at <https://www.netzeroassetmanagers.org>.

² vs. 2019 Baseline. Scope 1 emissions are direct emissions that originate at asset managed properties. These include natural gas and fuel oil consumption for space heating, water heating and, in some cases, cooking. Emissions resulting from refrigerants used on-site are outside of the reporting scope. Scope 2 emissions are indirect emissions from purchased electricity, steam and chilled water that is consumed at asset managed properties but generated elsewhere. Emissions from sub-metered tenant consumption are outside of BGO's organizational boundary and are therefore not included as Scope 2 emissions. Scope 3 emissions are reported for water consumption, waste generation, and tenant sub-metered energy consumption at properties. While sub-metered tenant consumption is the responsibility of tenants, it is reported as Scope 3 (other indirect emissions) for completeness and comparability of overall emissions to historical years where submeter-based billing was not present. Environmental performance (greenhouse gas emissions, energy, water, waste, and green building certifications) data for BGO's global debt series and mortgage investments, European and Asian Core Plus and Value-Add strategies, Asian and European separate accounts, and Asian debt series has been excluded. BGO continues to expand the environmental performance data collection process globally and implementing a consistent data collection approach across the firm. The environmental performance data reported excludes the greenhouse gas emissions, energy, water, waste data associated with tenant-paid invoices.

³ Learn more about BentallGreenOak's awards here: <https://www.bentallgreenoak.com/sustainable-investing.php>

BGO PATH TO NET ZERO



APPENDIX

RESPONSE TO URBAN DESIGN COMMENTS
PROJECT STATISTICS
SITEPLAN
SITEPLAN OPTIONS
TOPOGRAPHIC SURVEY
BELOW GRADE PARKING CONCEPT
SITE SECTIONS
SUN SHADOW STUDIES



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ARCHITECTURE

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RESPONSE TO URBAN DESIGN COMMENTS

NOTE FROM THE APPLICANT: The Urban Design comments will be evaluated in consultation with feedback from all stakeholders, and on completion of a detailed due diligence program; which are both in progress. We will provide a more detailed reply following.

34. The following elements of the preliminary design are appreciated:

- a. Mixed uses along Bank Street and residential use on Walkley and in the interior of the site.
- b. Consideration of place-making for future residents of the development through the provision of parks and POPS.
- c. Potential pedestrian porosity of the site.
- d. Public and private streets within the site to organize access to buildings.

Applicant: Noted. Thanks.

35. The following elements of the preliminary design are of concern/unconvincing:

- e. The overall appearance of being an enclave and lack of connectivity with the rest of the broader community (particularly the lack of consideration of the planned context).

Applicant: We feel that creation of a sense of immediate community and a focal point is essential to the success of the project. In their current forms, both Bank St. and Walkley are relatively hostile roads – high traffic volume and low-quality public realm. While the proposed Bank Street Reconstruction will improve the public realm to a degree, there is a necessity for creating a public space that is separated from, at a fine scale/grain, and more landscaped than is possible on Bank or Walkley. The idea of the ‘central green space’ seeks to achieve this by making a second frontage that breaks from the busy-ness of the two major arteries. We feel that to ensure that this is not an enclave, the inclusion of a public right of way accessing this green space and through the site is important. Likewise, the recognition of desire lines through the site to access transit is enshrined in the design. Further multiple points of views inward and pedestrian porosity into the central space are provided from Bank and Walkley. These serve to invite pedestrians into the site – breaking down any sense of enclave.

- f. The lack of street connectivity with the future development north of the site, which is a requirement of the Secondary Plan.

Applicant: The design team is reviewing options for providing the possibility of future connectivity to the north.

- g. The complicated, irregular, and potentially unsafe design at Walkley, Glenhaven, West Valley, and the proposed new public street intersection. The new public street makes a sharp turn within a short distance from an existing complicated intersection. The condition is further complicated by the location of the loading and parking ramp of Building 5.

Applicant: The primary issue with the Glenhaven, West Valley and new street intersection is the length of the throat to Walkley Road. Our traffic engineers are reviewing options for improvements to the design of the intersection to address existing and new traffic volumes.

- h. The proposed loading spaces between Buildings 1 and 2 and Buildings 1 and 5, which can potentially compromise pedestrian experience.

Applicant: Noted. We are currently early in the design stages of the project, focusing currently on massing and building location level issues. As of currently, the final locations of loading spaces are still under review within the design team as a whole; taking into account priorities related to serviceability, phasing, pedestrian safety, and public space impacts.

- i. Building 3 –
 - Although the stepping of the building is appreciated, the overall floor plate of the building above podium appears to be massive.
 - The 10m setback of the high-rise portion to the property line to the north is too small which will compromise development potential on the abutting lot and quality of life for all.

Applicant: This will be explored further as the design is developed.

- j. Building 5 –

- There is a lack of transition to the adjacent low-rise residential area. The provision of a 25m separation is insufficient for providing effective transition between a 30-storey tower and the 3-storey town homes.

Applicant: This relationship reflects the tension between the OP recognized heights and target density and previous development in areas adjacent to major transit stations and Mainstreet corridors

- The tower and the six-storey podium, located south of the proposed park, will cast wide and long shadows in the new park.

Applicant: Please refer to the shadow studies included in the UDRP submission. The angle of the grid and the location of the park near the west property line favours sunlight in the park in the afternoon throughout the year.

- k. The park
 - Surrounded by roads on all four sides and residential fencing on the west side, the quality and experience of the park remain unconvincing.
 - There is very limited visibility from either Bank Street or Walkley Road.

Applicant: We are only proposing a public road on the north and west side of the park. This responds to the City requirements that all new parks abut a public road. The Private Road on the south and east side are intended to serve only for the requires of loading / drop off and will not carry significant vehicle traffic. These will be designed as low-speed, pedestrian priority spaces (woonerf or living street) – effectively an extension of the hard landscaping from the proposed building faces to the greenspace.

Preliminary Suggestions (site plan, public realm, and built form)

37. It is crucially important to explore and study block plan for the northwest quadrant of Bank and Walkley following directions of the Secondary Plan. A realistic block plan can offer clarity and provide guidance to the design of this site, including street and pathway connections, provision of parks and open spaces, and managing

RESPONSE TO URBAN DESIGN COMMENTS

relationship between neighbouring sites. Such a block plan can serve as a basis for the creation of a plan of subdivision. The attached diagrams illustrate how such a block plan may look like. Please note urban design is not recommending any of these block plan options. The purpose of these diagrams is to simply demonstrate that there may be a range of options and it is necessary to explore block plan options to inform the design of the site.

Applicant: In the UDRP submission deck we have included 'Future Development Potential' massing studies that look at how the blocks to the north may be developed out under the new OP frames and in a pattern that contemplates the extension a street northward to Alta Vista Drive.

38. Design a well-connected neighbourhood that is integrated into the surrounding broader community. While practical merits of being a standalone enclave can be appreciated at this location, perhaps in the near term, future residents and business will benefit when the site is integrated into the future the broader community.

Applicant: Our design recognizes that the neighbourhood is one that will increasingly evolve. Our overall site layout for multiple connections from Bank and Walkley into the site.

39. Design a functional, animated, bright, and visible park with optimal microclimate conditions year-round.

Applicant: Noted. We have brought Ferris + Associates Landscape Architects onto our team to help guide the development of the outdoor hard and soft landscape spaces.

40. Ensure street design is safe and satisfies practical requirements.

Applicant: Noted. Our priorities for the streets (public and private) are to ensure controlled traffic volumes, minimize or eliminate commuter cut through, control speed, and ensure the most inviting pedestrian environment.

41. Ensure effective built form transition, guided by the application of angular planes.

Applicant: Please refer to the angular plane diagrams included in the UDRP submission set. We feel that the proximity to the transit station and future development potential under the OP justifies some elements of the development breaking the 45 degree angular planes. The trade-off the proposed development offers is providing the majority of the western edge as greenspace. This minimizes the number of the existing townhomes impacted by overlook / shadowing.

42. Continue to explore the design of the space between the buildings, particularly Buildings 1 and 2, and Buildings 1 and 5. These spaces should be animated pedestrian realm not locations of back of the house functions.

Applicant: Noted. The design of the loading / back of house functions is still being explored for these buildings.

43. Ensure at grade commercial spaces are leveled with the sidewalk along Bank Street.

Applicant: Our preliminary sections (included in the UDRP submission) provide stepping in the ground floors of some of buildings to ensure a match at key points between the exterior grades along Bank and Walkley and the interior spaces. This will support mixed use functions such as small retail/services, exterior accessed amenity spaces, and lobby spaces with level access. Due to the ~4.5m grade drop across the site, we have prioritized at-grade access along Bank Street and at the corner of Bank and Walkley. Where the grade falls more steeply, our design objective is to provide street facing amenity uses, serviced by internal access, to provide active use at / near the street level.

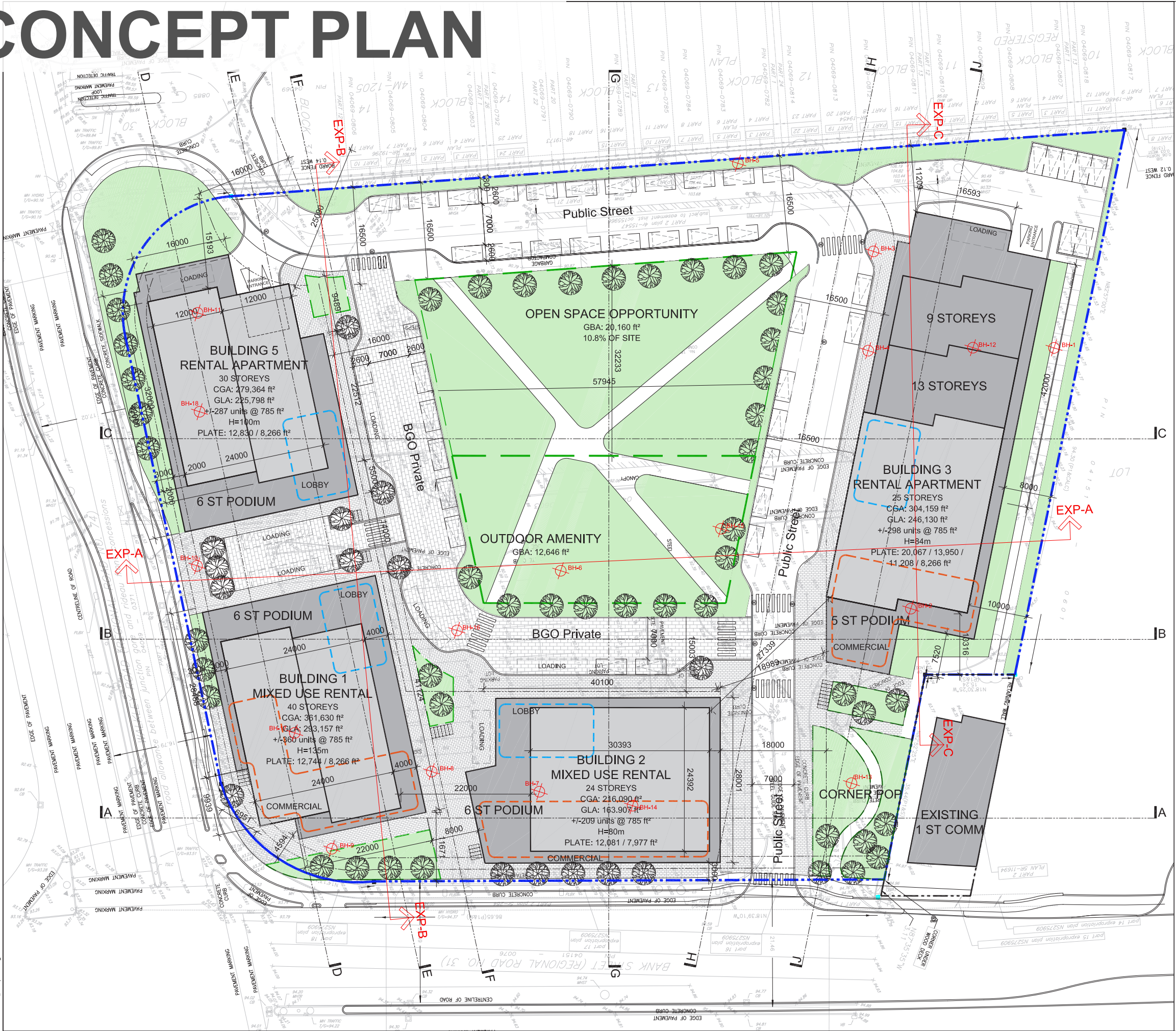
PROJECT STATISTICS

Site Area: 186,537 sq.ft.

OPTION	Full centre green space	Community building	Centre mid-rise building
CONSTRUCTED GROSS AREA	1,161,243sq.ft	1,167,701sq.ft	1,221,879sq.ft
LEASEABLE AREA ⁽¹⁾	938,899sq.ft	944,195sq.ft	985,341sq.ft
OPEN SPACE	32,633sq.ft	19,500sq.ft	19,500sq.ft
ROAD AREA	38,568sq.ft	38,568sq.ft	38,568sq.ft
FSI	6.22	6.25	6.55
UNIT COUNT ⁽²⁾	1,155	1,155	1,214
PARKING SPACES ⁽³⁾	577	577	607
VISITOR PARKING ⁽³⁾	115	115	121
BICYCLE PARKING ⁽³⁾	577	577	607
AMENITY SPACE ⁽³⁾	74,547sq.ft	75,490sq.ft	74,547sq.ft

(1) Based on 82% efficiency
(2) Average unit size of 785 sq.ft
(3) Zoning Minimum requirements

CONCEPT PLAN



PROPOSED DEVELOPMENT STATS		
	REQUIRED	PROPOSED
PROJECTED NUMBER OF UNITS		± 1,154
SETBACK ALONG BANK STREET	0m	0m
SETBACK ALONG WALKLEY ROAD	0m	0m
REAR YARD SETBACK	7.5m	7.5m
INTERIOR SIDEYARD SETBACK	0m	0m
MAXIMUM HEIGHT		135 m (MAX)
NUMBER OF STOREYS		MAX 40
TOTAL GROSS BUILDING AREA		107,883 m²
TOTAL NET LEASEABLE AREA (ZBL)		87,227 m²
TOTAL PARKING (BELOW GRADE)		815
VISITORS @ 0.1/UNIT	115	115
RES @ 0.5/UNIT	577	700
BICYCLE PARKING		
RES @ 0.5/UNIT	577	577
COMMERCIAL	TBD	TBD

NOTE: ALL EXISTING SITE INFORMATION AS PER TOPOGRAPHICAL SURVEY PLAN DATED MAY 23, 2023 PREPARED BY STANTEC GEOMATIC LTD.



PROJECT TEAM

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FOTENN PLANNING AND DESIGN
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SURVEYOR
STANTEC GEOMATICS LTD
BRIAN WEBSTER
T 613.722.4420

no.	date	revision
1		ISSUED FOR INFORMATION

It is the responsibility of the appropriate contractor to check and verify all dimensions on site and report all errors and/or omissions to the architect.

All contractors must comply with all pertinent codes and by-laws.

Do not scale drawings.

This drawing may not be used for construction until signed.

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PROJECT / LOCATION

BGO BANK AND WALKLEY
1820 BANK STREET

DRAWING TITLE:

CONCEPT SITE PLAN

DRAWN BY:
DV

DATE:
23-11-13

SCALE:
1:300

PROJECT:
2230

DRAWING NO.:

A1.00

REVISION NO.:

OPTION COMMUNITY BUILDING



PROJECT TEAM

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LANDSCAPE ARCHITECT
COMPANY
NAME
T 000.000.0000

TRANSPORTATION ENGINEER
CGH TRANSPORTATION
ANDREW HARTE
T 613.697.3797

ENVIRONMENTAL ENGINEER
EXP ENGINEERING
NAME
T 613.688.1899

GEOTECHNICAL ENGINEERING
EXP SERVICES INC.
CHRIS KIMMERLEY
T 613.688.1899

SURVEYOR
STANTEC GEOMATICS LTD
BRIAN WEBSTER
T 613.722.4420

1		ISSUED FOR INFORMATION
no.	date	revision

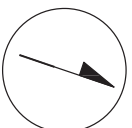
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LOCATION PLAN



PROPOSED DEVELOPMENT STATS

	REQUIRED	PROPOSED
PROJECTED NUMBER OF UNITS		
SETBACK ALONG BANK STREET	0m	
SETBACK ALONG WALKLEY ROAD	0m	
REAR YARD SETBACK	7.5m	
INTERIOR SIDEYARD SETBACK	0m	
MAXIMUM HEIGHT		
NUMBER OF STOREYS	MAX 40	
TOTAL GROSS BUILDING AREA		
TOTAL NET LEASEABLE AREA (ZBL)		
TOTAL PARKING (BELOW GRADE)		
VISITORS @ 0.1/UNIT	115	
RES @ 0.5/UNIT	577	
BICYCLE PARKING		
RES @ 0.5/UNIT	577	
COMMERCIAL	TBD	

NOTE: ALL EXISTING SITE INFORMATION AS PER
TOPOGRAPHICAL SURVEY PLAN DATED MAY 23, 2023
PREPARED BY STANTEC GEOMATIC LTD.

PROJECT/LOCATION:

BGO BANK AND WALKLEY

1820 BANK STREET

DRAWING TITLE:

CONCEPT SITE PLAN

DRAWN BY:	DA
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DV 23

A1.01

REVISION NO.:

City File Number: (D00-00-00-0000)

23-11-13
1822 Bank_siteplan.dwg

OPTION MID-RISE BUILDING



PROJECT TEAM

ARCHITECTURAL
HOBIN ARCHITECTURE
DOUG VAN DEN HAM
T 613.238.7200 EXT.115

PLANNING
FOTENN PLANNING AND DESIGN
PAUL BLACK
T 613.730.5709

CIVIL ENGINEER
DSEL
ADAM FOBERT
T 613.222.9493

LANDSCAPE ARCHITECT
COMPANY
NAME
T 000.000.0000

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STANTEC GEOMATICS LTD
BRIAN WEBSTER
T 613.722.4420

1		ISSUED FOR INFORMATION
no.	date	revision

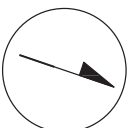
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LOCATION PLAN



PROPOSED DEVELOPMENT STATS

	REQUIRED	PROPOSED
PROJECTED NUMBER OF UNITS		
SETBACK ALONG BANK STREET	0m	
SETBACK ALONG WALKLEY ROAD	0m	
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BICYCLE PARKING		
RES @ 0.5/UNIT	577	
COMMERCIAL	TBD	

NOTE: ALL EXISTING SITE INFORMATION AS PER
TOPOGRAPHICAL SURVEY PLAN DATED MAY 23, 2023
PREPARED BY STANTEC GEOMATIC LTD.

PROJECT/LOCATION:

BGO BANK AND WALKLEY

1820 BANK STREET

DRAWING TITLE:

CONCEPT SITE PLAN

DRAWN BY: DV	DATE: 23-11-13
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SCALE:
1:300

PROJECT: 2230

DRAWING NO. 1
A 1 00

A1.02

REVISION NO.:

City File Number: (D00-00-00-0000)

23-11-13
1822 Bank_siteplan.dwg

TOPOGRAPHY

SURVEYOR'S REAL PROPERTY REPORT
PART 1 - PLAN OF SURVEY

**PART OF LOT 23
JUNCTION GORE**
(GEOGRAPHIC TOWNSHIP OF GLOUCESTER)

CITY OF OTTAWA

Scale 1:400
0 10 20 30 METRES

Stantec Geomatics Ltd.

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METRIC CONVERSION
DISTANCES AND COORDINATES SHOWN ON THIS PLAN ARE IN METRES
AND CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048

BEARING NOTE
BEARINGS ARE GRID, DERIVED FROM CAN-HEAT VRS NETWORK GPS OBSERVATIONS ON NCC HORIZONTAL CONTROL MONUMENTS 19773035 AND 19680191, CENTRAL MERIDIAN, 76° 30' WEST LONGITUDE MTM ZONE 9, NAD83 (ORIGINAL).

19773035 N:5006060.42 E:324888.04
19680191 N:5033564.26 E:388064.94

NOTE
THIS PLAN OF SURVEY IS TO BE READ IN CONJUNCTION WITH THE REPORT SUMMARY NOTED AS PART 2 HEREON.
THIS REPORT CAN ONLY BE UPDATED BY THIS OFFICE. NO ADDITIONAL PRINTS OF THIS ORIGINAL REPORT WILL BE ISSUED SUBSEQUENT TO THE DATE OF CERTIFICATION.
ALL TIES ARE MINIMUM UNLESS OTHERWISE NOTED.
ALL TIES TO CURVED BOUNDARY ARE RADIAL TO ARC.
RISK OF UNDERGROUND SERVICES, MONUMENTATION PLANTED ACCORDINGLY.

PART 2
This Report was prepared for Griffin Brockman and the undersigned accepts no responsibility for the use by other parties.

- REGISTERED RIGHTS-OF-WAY/EASEMENTS**
Subject to easement as in Inst. No. 0T55584. See Plan.
- PROPERTY IMPROVEMENTS**
This is a foundation survey only.
- COMPLIANCE WITH MUNICIPAL ZONING BYLAWS**
Compliance is not certified by this report.
- ADDITIONAL REMARKS**
The building ties are to the unpargeted concrete foundation walls.

LEGEND		
■	DENOTES	FOUND MONUMENTS
□		SET MONUMENTS
IB		IRON BAR
IBR		ROUND IRON BAR
SIB		STANDARD IRON BAR
SSIB		SHORT STANDARD IRON BAR
CC		CUT CROSS
CP		CONCRETE PIN
WIT		WITNESS
PIN		PROPERTY IDENTIFICATION NUMBER
MEAS		MEASURED
INST		INSTRUMENT
PROP		PROPORTIONED
OU		ORIGIN UNKNOWN
SG		STANTEC GEOMATICS LTD.
PI		PLAN BY ANNIS, O'SULLIVAN, VOLLEBEK LTD.
		DATED APRIL 10, 2002
▲	AN	ANCHOR
●	BOL	BOLLARD
CB	CB	CATCH BASIN
DRN	DRN	DRAIN
GV	GV	GAS VALVE
HYD	HYD	FIRE HYDRANT
JBX	JBX	JUNCTION BOX
LS	LS	LIGHT STANDARD
MH	MH	MAINTENANCE HOLE UNIDENTIFIED
MHB	MHB	MAINTENANCE HOLE BELL
MHF	MHF	MAINTENANCE HOLE FIBRE OPTIC
MHH	MHH	MAINTENANCE HOLE HYDRO
MHSAN	MHSAN	MAINTENANCE HOLE SANITARY
MHSTM	MHSTM	MAINTENANCE HOLE STORM
MHT	MHT	MAINTENANCE HOLE TRAFFIC
OW	OW	OBSERVATION WELL
SN	SN	SIGN
UP	UP	UTILITY POLE
○		TREE DECIDUOUS (D.B.H. SHOWN)
— CHW —	CHW	OVERHEAD WIRE

SURVEYOR'S CERTIFICATE
I CERTIFY THAT:
1. THIS SURVEY AND PLAN ARE CORRECT AND IN ACCORDANCE WITH THE SURVEYS ACT.
2. THE SURVEYORS ACT AND THE REGULATIONS MADE UNDER THEM.
3. THE SURVEY WAS COMPLETED ON THE 3rd DAY OF NOVEMBER, 2023.

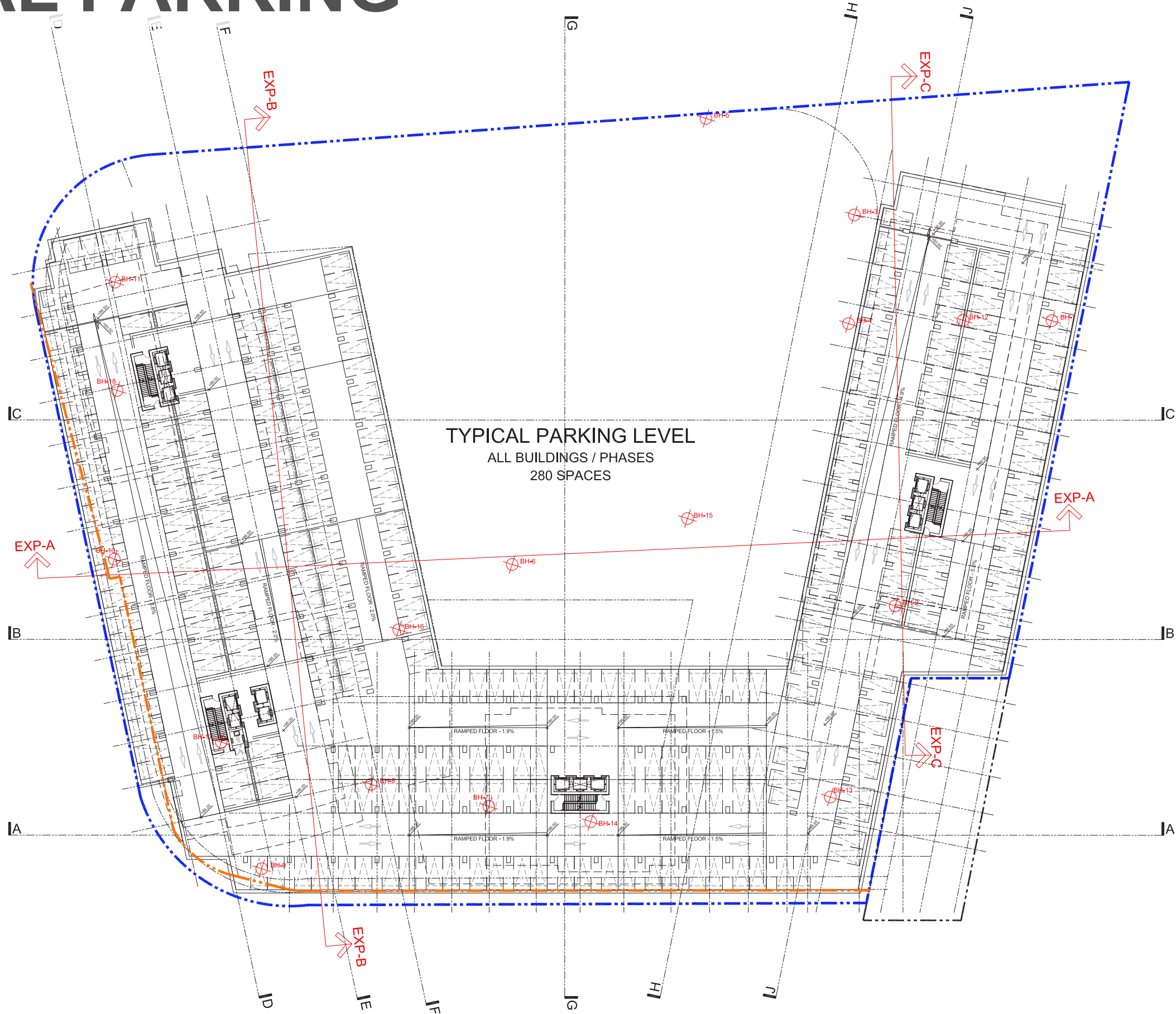
DATE _____ BRIAN J. WEBSTER
ONTARIO LAND SURVEYOR

SRO MAP COORD.= 370246, 5025996

Stantec Geomatics Ltd.
CANADA LANDS SURVEYORS
ONTARIO LAND SURVEYORS
1331 CLYDE AVENUE, SUITE 300
OTTAWA, ONTARIO, K2C 3G4
TEL. 613.722.4420
stantec.com

DRAWN: ZF CHECKED: BW PM: BW FIELD: AW PROJECT No.: 161614709-110

TYPICAL PARKING



no.	date	revision
1		ISSUED FOR UDRP

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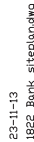
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F: 613 285-2006
E: mail@hobinarc.com
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HOBIN
ARCHITECTURE

PROJECT / LOCATION:		
BGO BANK AND WALKLEY 1820 BANK STREET		
DRAWING TITLE:		
CONCEPT PARKING PLAN		
DRAWN BY:	DATE:	SCALE:
DV	23-11-13	1:300
PROJECT:		DRAWING NO.:
2230		A2.00
		REVISION NO.:

1



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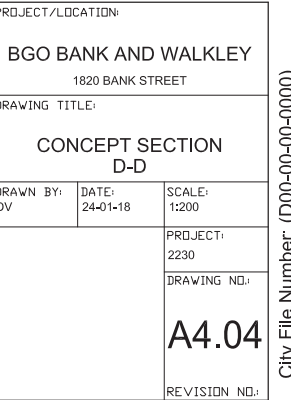
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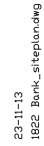
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HOBIN
ARCHITECTURE

City File Number: (D00-00-00-0000)





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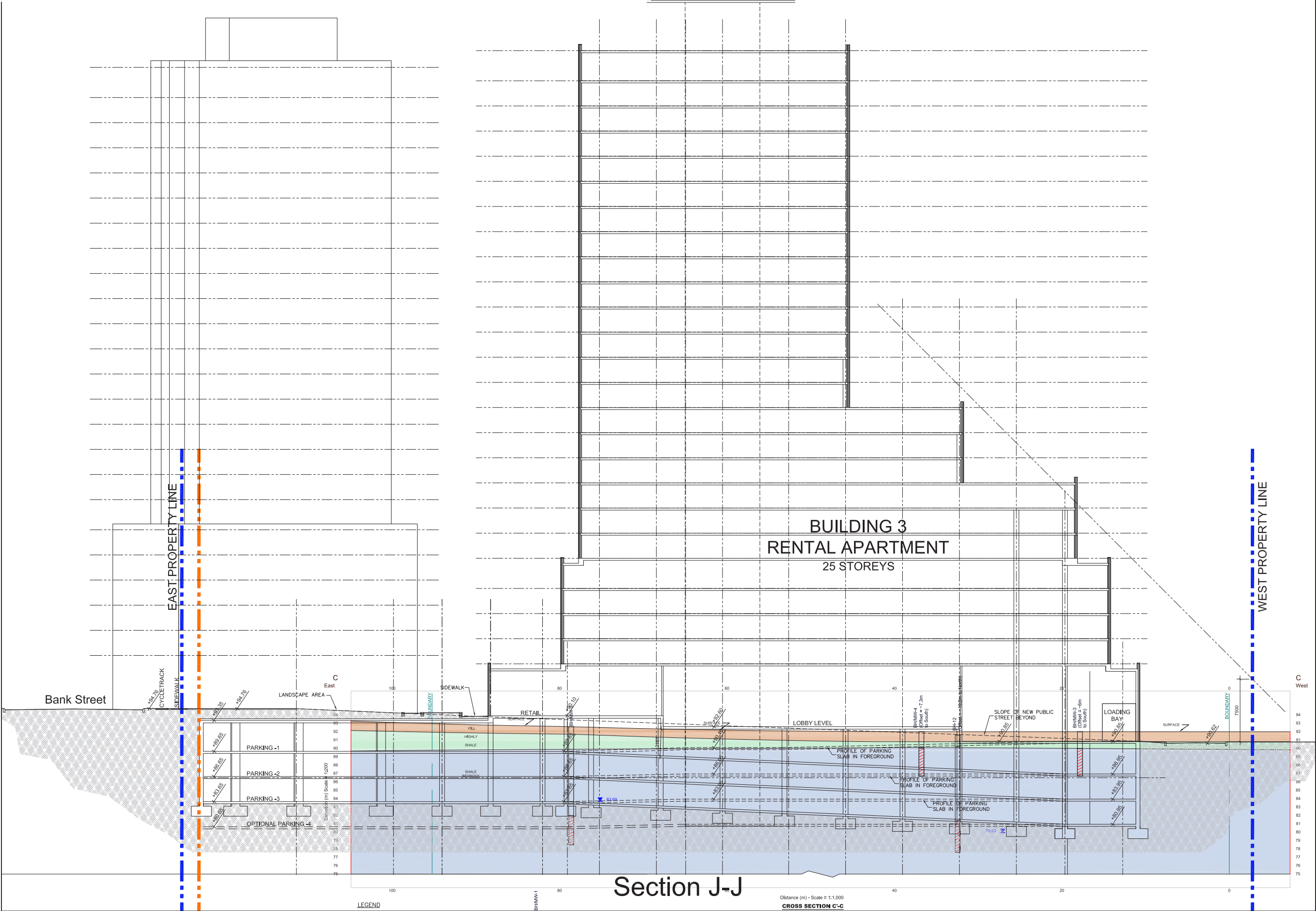
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ARCHITECTURE

City File Number: (D00-00-00-0000)

PROJECT SECTIONS



1	ISSUED FOR INFORMATION
no.	date
revision	

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E: mail@hobinarc.com
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HOBIN
ARCHITECTURE

PROJECT / LOCATION:
BGO BANK AND WALKLEY
1820 BANK STREET

DRAWING TITLE:
CONCEPT SECTION
J-J

DRAWN BY:
DV

DATE:
24-01-18

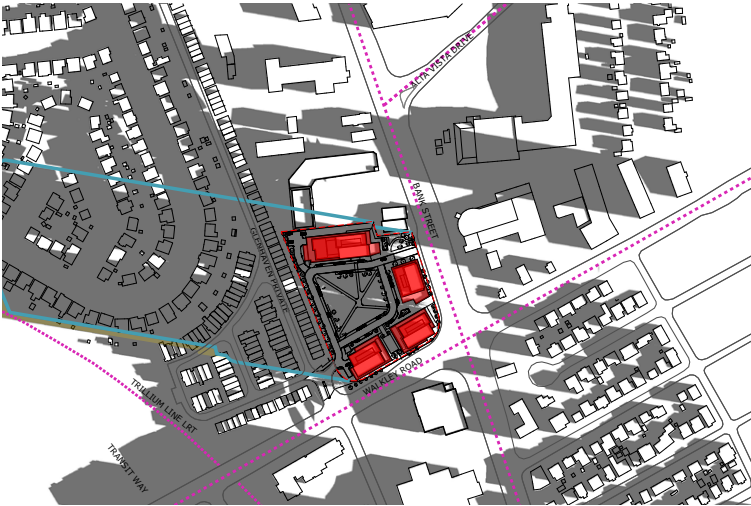
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PROJECT:
2230

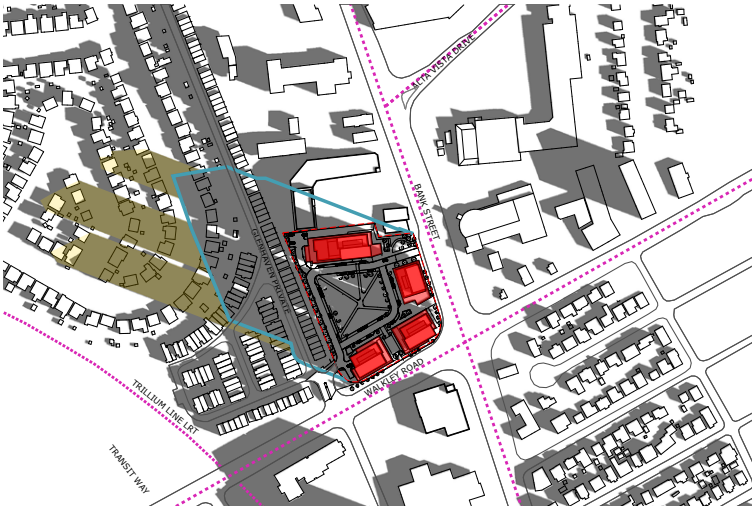
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REVISION NO.:

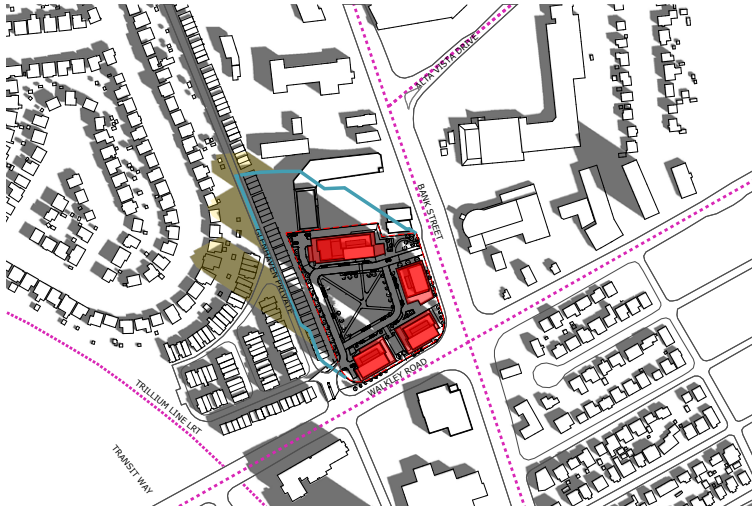
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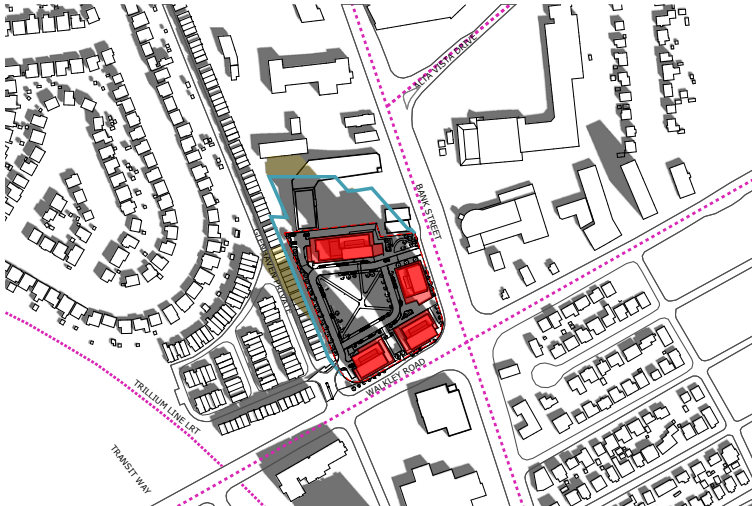
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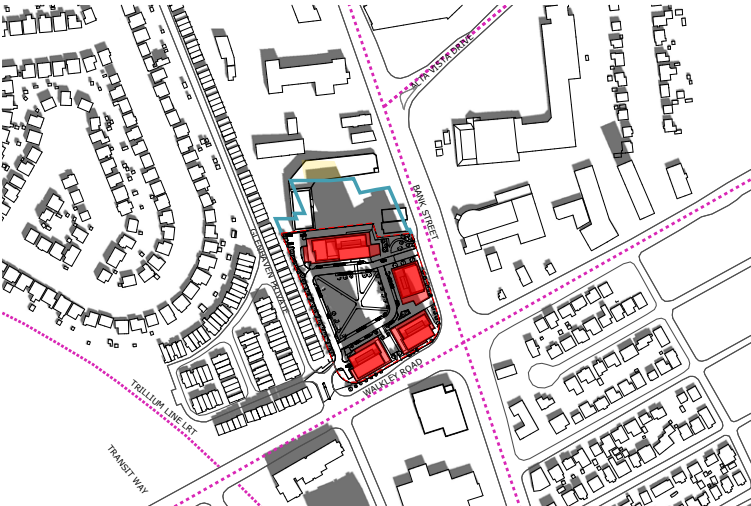
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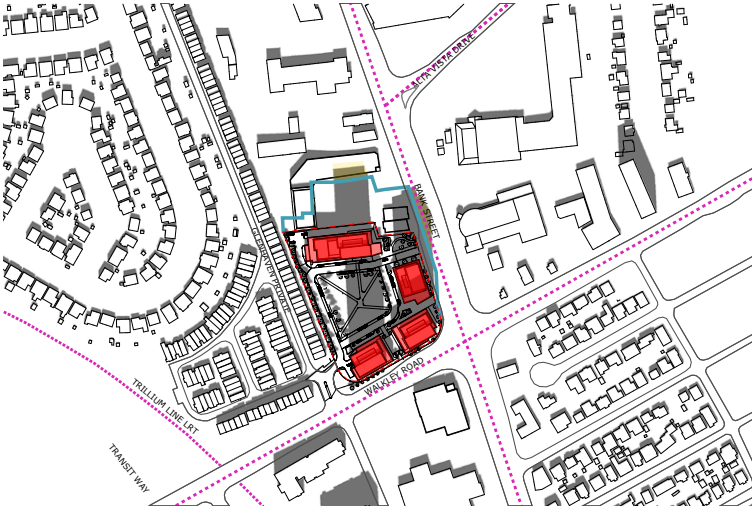
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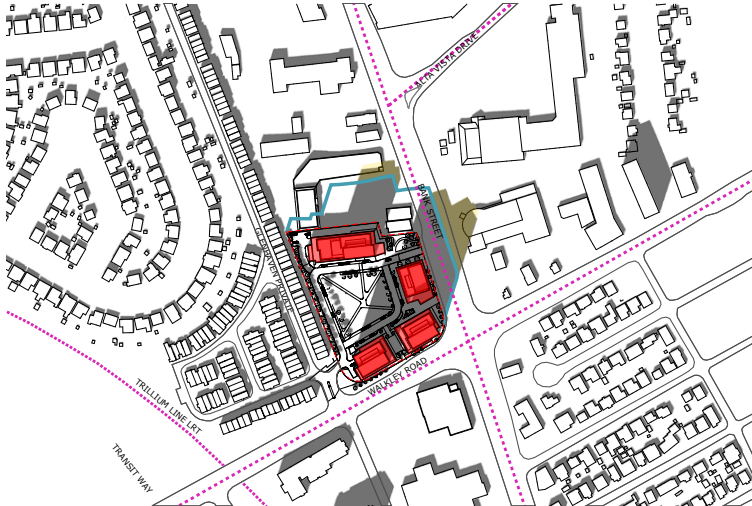
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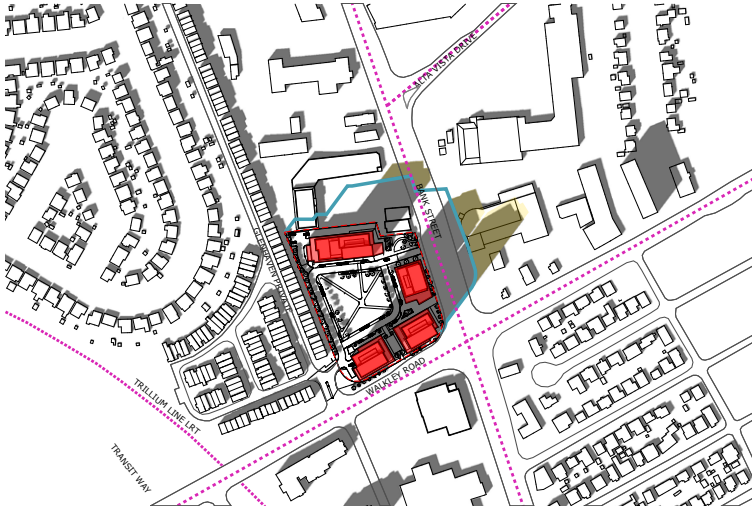
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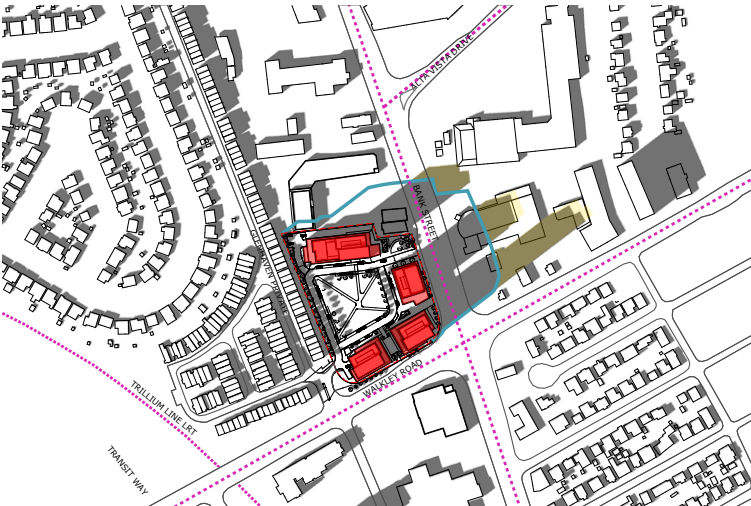
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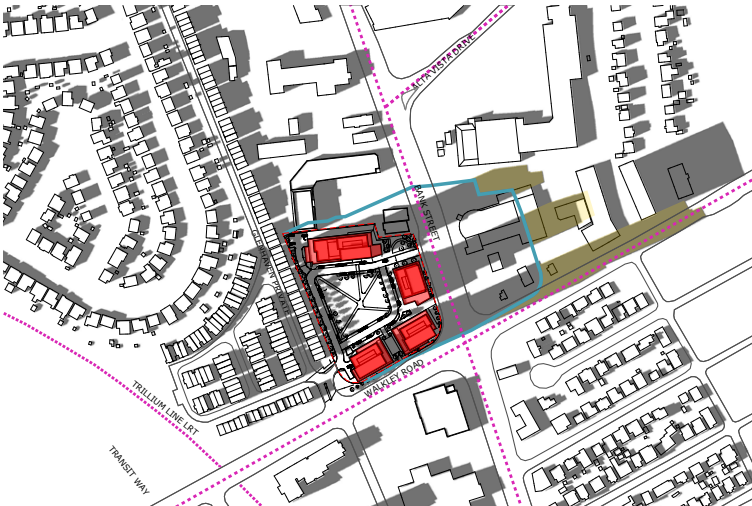
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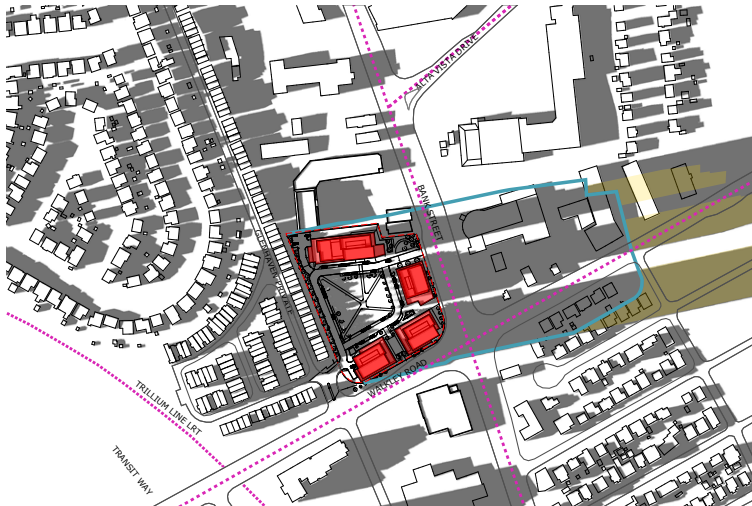
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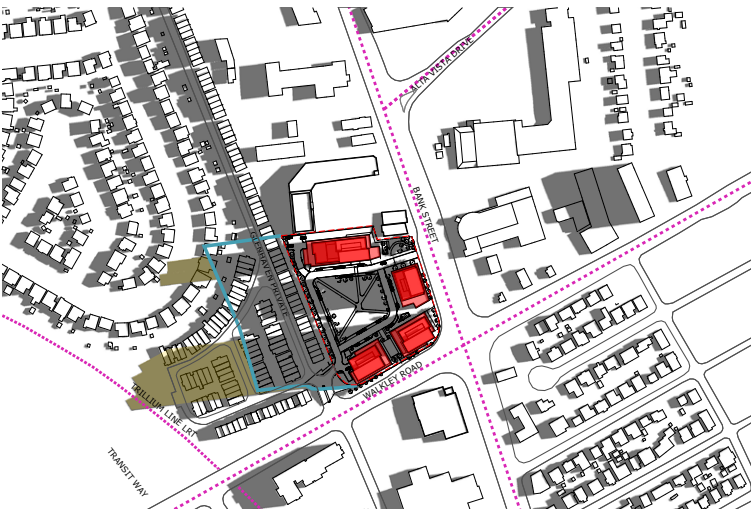
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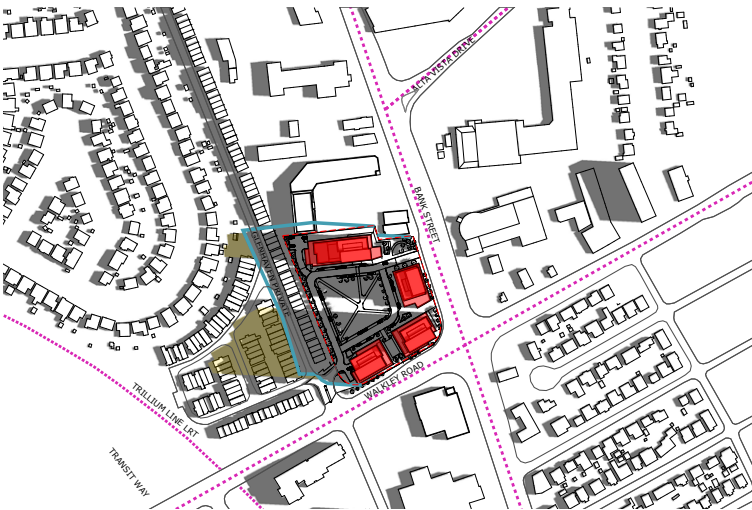
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SUN SHADOWS

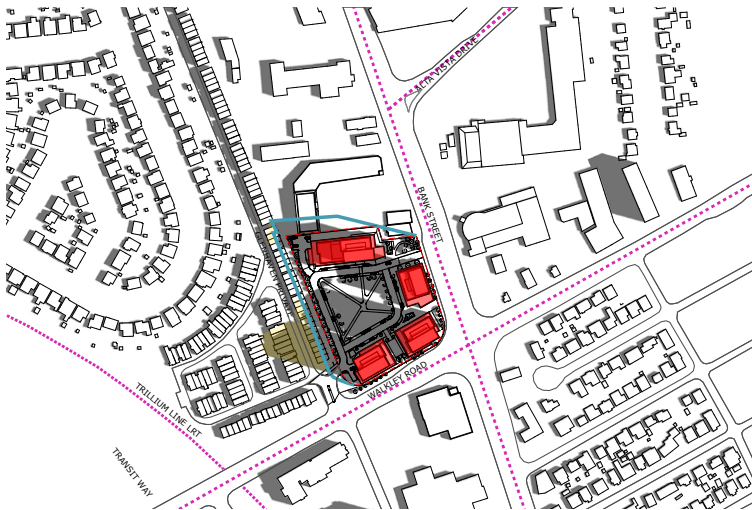
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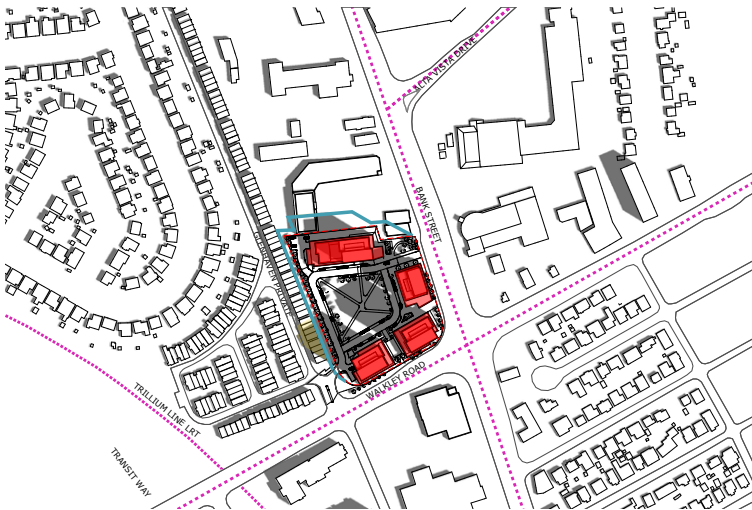
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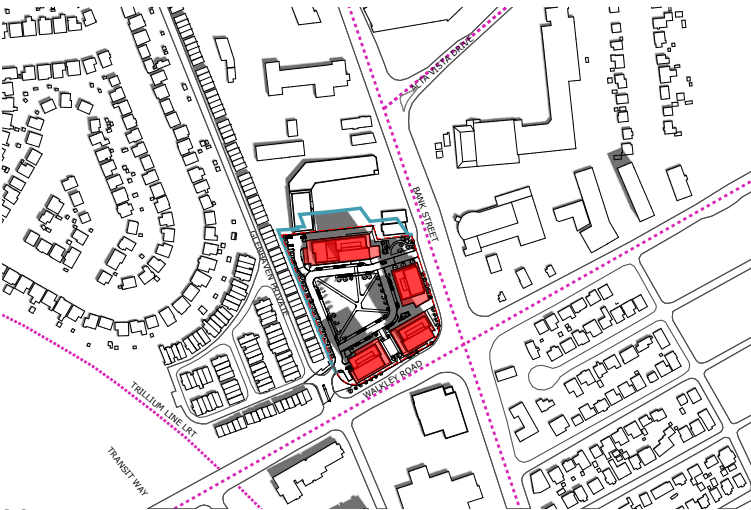
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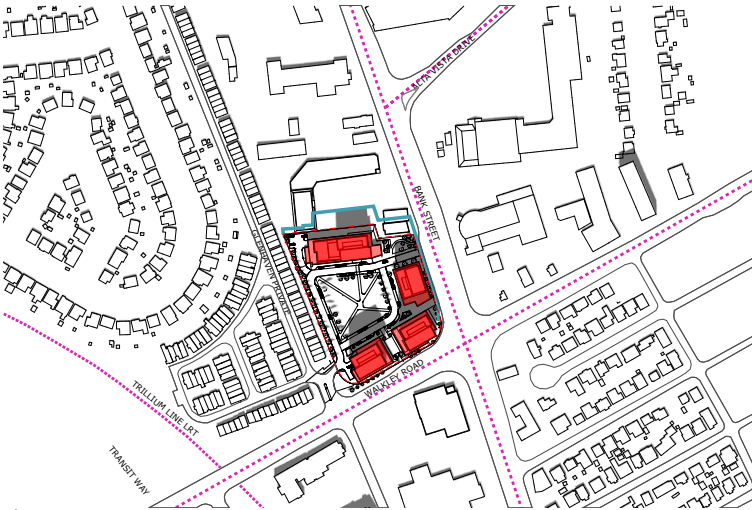
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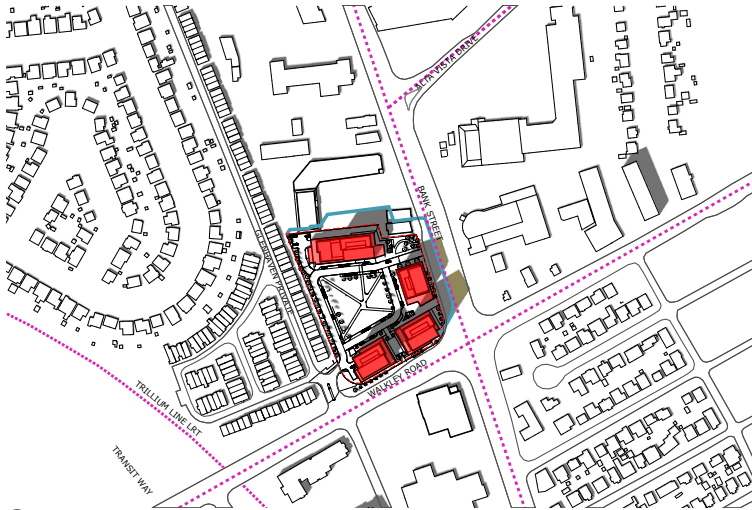
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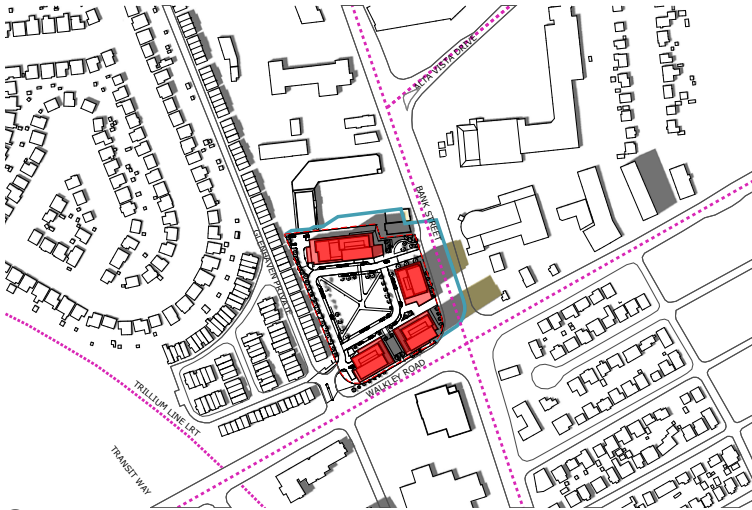
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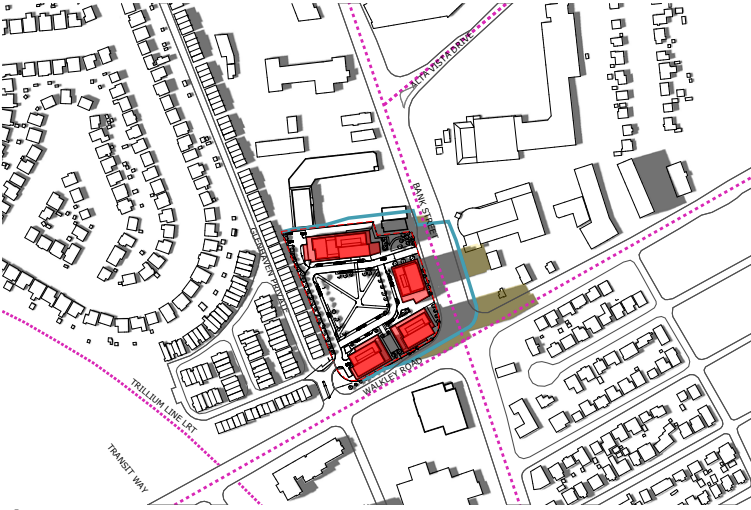
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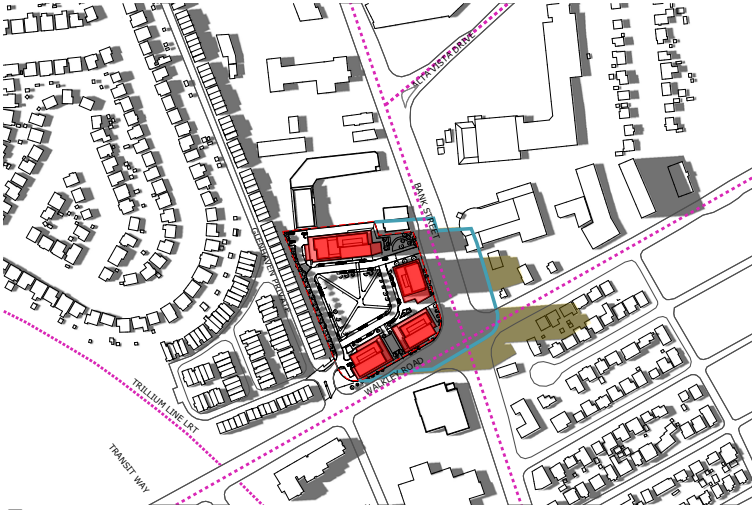
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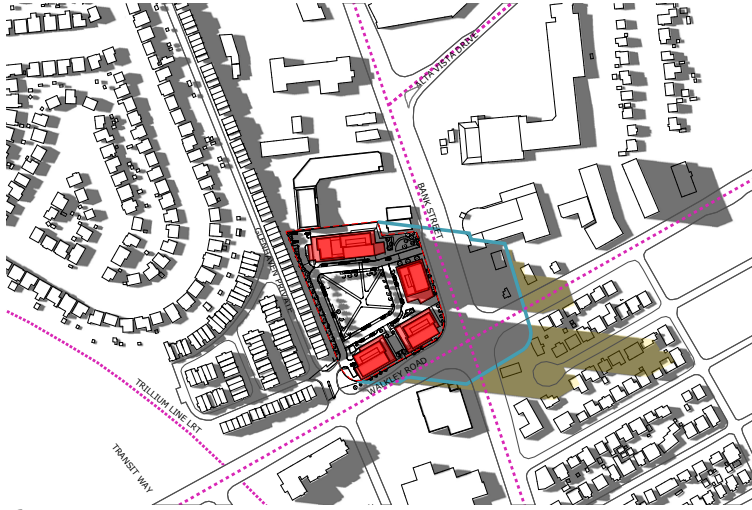
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7 pm

SUN SHADOWS WINTER SOLSTICE

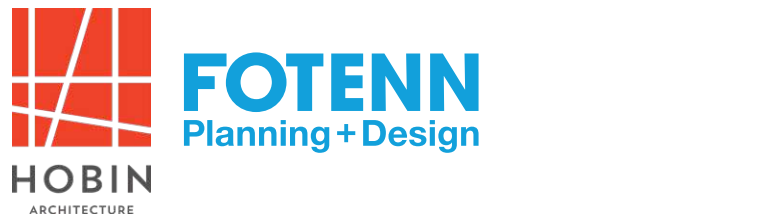
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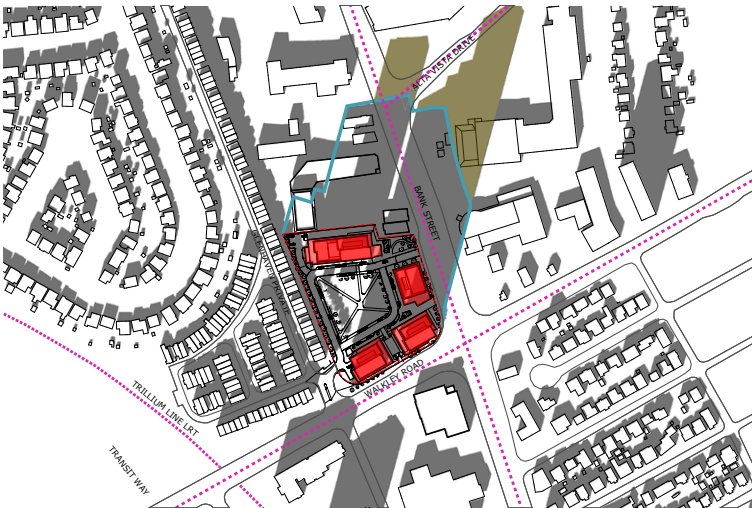
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5 pm



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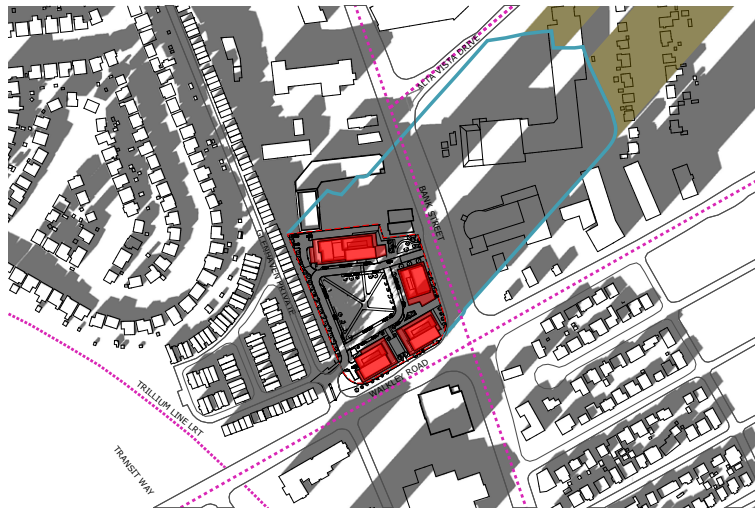
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6 pm



11 am



3 pm



7 pm

