

1081 CARLING AVENUE

FORMAL URBAN DESIGN REVIEW PANEL

22.09.09



HOBIN
ARCHITECTURE

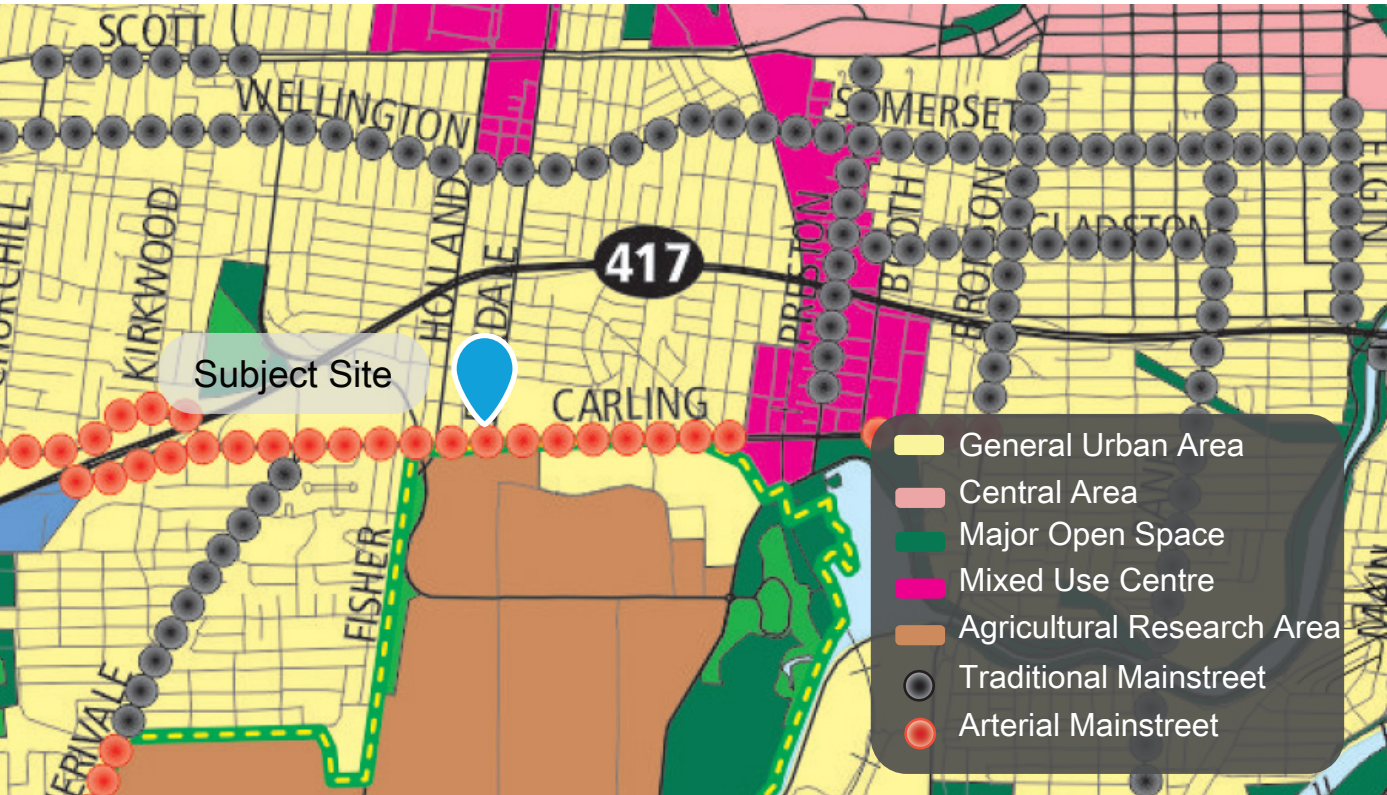
TAGGART
REALTY MANAGEMENT

FOTENN
Planning + Design

CSW

POLICY CONTEXT

OFFICIAL PLAN AND ZONING BY-LAW



Schedule B - Urban Policy Plan



CITY OF OTTAWA OFFICIAL PLAN (2003, as amended)

- Designated Arterial Mainstreet
- **Building Liveable Communities** – Section 2.5.1 describes the basics of liveable communities. This includes good housing, employment, ample greenspace, and a sense of history and culture. The proposed development responds to the seven (7) design objectives outlined in this section.
- **Urban Design and Compatibility** – Section 4.11 identifies policies that set the stage for requiring high quality urban design in all parts of the city and design excellence in design priority areas. Policy considerations include views, building design, massing and scale, outdoor amenity area, and design priority areas.

ZONING BY-LAW (2008-250)

- South portion zoned Arterial Mainstreet, Subzone 10, Special Exception 2196 (AM10[2196])
- North portion zoned Arterial Mainstreet, Subzone 10, Special Exception 2196, Maximum Height 11 metres (AM10[2196] H(11))
- Permitted uses include: apartment dwelling, low-rise and mid-rise
- Proposed to be rezoned to Arterial Mainstreet Subzone 10, Exception XXXX, Schedule YYY (AM10[XXXX] SYYY).
- High Rise Zoning Provisions (under appeal) apply; the proposed development conforms with all required performance standards: rear yard setback above 9 storeys, tower separation (two towers, one lot), and minimum lot area (corner lot)

POLICY CONTEXT

DESIGN GUIDELINES

TRANSIT-ORIENTED DESIGN GUIDELINES

These guidelines apply to all development within 600 metres walking distance of a rapid transit stop or station.

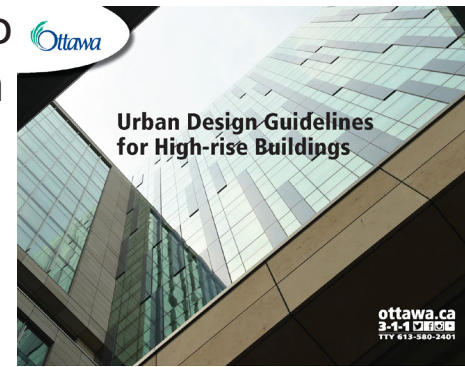


The proposed development meets the following design guidelines, among others:

- Provides transit supportive land uses, mixed use development including high-density residential uses, within a 600-metre walking distance of a rapid transit stop or station (Guideline 1);
- Discourages non transit-supportive land uses that are oriented primarily to the automobile and not the pedestrian, cyclist or transit user (Guideline 2);
- Lays out new pedestrian connections, permitting pedestrian movement throughout the site (Guideline 4);
- Locates buildings close to each other and along the front of the street to encourage ease of walking between buildings and to public transit (Guideline 7);
- Provides underground parking rather than surface parking (Guideline 39).

URBAN DESIGN GUIDELINES FOR HIGH RISE BUILDINGS

These guidelines apply to high-rise buildings, which the Official Plan defines as buildings between 10 and 30 storeys.



The proposed development meets the following guidelines, among others:

- The proposed development is a background building: it enhances existing views and vistas through the placement of the building, height transitions, setbacks, and landscaping (Guideline 1.9);
- The building base defines the street wall context along Carling Avenue (Guideline 1.12);
- The lot is an irregular shape; however, the parkland dedication creates a more regular-shaped lot to permit effective transition measures (Guideline 1.14); and
- The proposed development is within an “emerging downtown district” given its transit corridor and therefore a 20-metre separation to adjacent low-rise is used as a measure of appropriate transition. The lot size permits appropriate transition to the adjacent low-rise neighbourhood to the north (Guideline 1.17).

URBAN DESIGN GUIDELINES FOR DEVELOPMENT ALONG ARTERIAL MAINSTREETS

These guidelines provide urban design guidance at the planning application stage in order to assess, promote and achieve appropriate development along Arterial Mainstreets.



The proposed development meets the following guidelines, among others:

- Locates the new buildings along the public street edges (Guideline 1);
- Uses buildings and landscaping to create a continuous streetscape (Guideline 4);
- Sets the buildings back between 0 and 3 metres (Guideline 6);
- Provides architectural landscaping features at the corner of Carling Avenue and Parkdale Avenue to emphasize the public streets and enhance the streetscape (Guideline 8); and
- Orients the front façade to face the public street and locate front doors to be visible, and directly accessible, from the public street (Guideline 17).

1081 CARLING DESIGN BRIEF

HISTORICAL CONTEXT

1081 Carling Avenue is located in the heart of the Civic Hospital – Central Park neighbourhood, an Ottawa community, containing various building typologies and landscapes. The site is situated between the extensive Central Experimental Farm to the south, residential neighbourhoods to the north and west, and the Ottawa Hospital's Civic Campus to the east. This valued location sits in close proximity to nature via walking and cycling paths reaching Dow's Lake, Ottawa's Dominion Arboretum, the Rideau Canal, and Rideau River. Simultaneously, 1081 Carling Avenue's adjacent infrastructure, such care facilities, retail, community gardens, and schools, provide urban living essentials.

The Civic Hospital-Central Park neighbourhood was settled at the beginning of the nineteenth century. By 1907, the community, formerly known as Bayswater, was annexed by Ottawa. The sprawling territory contains numerous residential pockets. The homes comprising the majority of The Civic Hospital -Central Park neighbourhood are primarily clad in brick. Many of the homes built between 1939 and 1947 were designed by David Youngusband, and belong to the Arts and Crafts movement. This urban fabric dissipates as the residential portions of the neighbourhood begins to sprawl and merge with the expansive Civic Hospital Campus, and Central Experimental Farm.

The site is currently occupied by an eight story office tower containing retail space at the ground level. The tower, built between the late 1960's and early 1970's is

surrounded by at-grade parking. Prior to this condition, the site was divided into smaller parcels for residential dwellings, and a larger lot along Carling Avenue. This south edge of the site meets the public along one of Ottawa's busiest thoroughfares and faces the 427 hectare Experimental Farm. The east portion of the site provides access to the Ottawa Hospital's Civic Campus. Concurrently, the north and west frontages have maintained a cohesive relationship with the single-family residences. The site, historically and currently, has acted as a bridge between the varied typologies of space within the Civic Hospital–Central Park neighbourhood. Therefore, the redevelopment of this site seeks to continue this set of varied associations and connections between buildings and landscape typologies. 1081 Carling Avenue aims to maintain this status as the intersection between these zones while further enhancing the experience of the site and contextual fabric of the Civic Hospital – Central Park neighbourhood.

PROPOSED DEVELOPMENT

The proposed redevelopment, consisting of two residential high-rise buildings and a pedestrian plaza and thoroughfare, intends to activate the encompassing streetscapes surrounding the site. The redevelopment of this site will require the demolition of the existing office tower, and its adjacent surface parking lot.

The proposed towers, one comprised of 25 stories, and the other of 16 stories, both encompass four story podiums. The podium's consistent datum lines and brick cladding, not dissimilar to that of the bordering residential neighbourhood and Ottawa Hospital's Civic campus, aims to maintain a sense of scale and materiality with their surrounding architecture. The ground levels of the building's podiums will house amenity spaces, a reading lounge, gym, and bicycle storage. The utilization of these permeable spaces

and services by future residents and visitors will result in a consistently vibrant relationship between the interior and exterior spaces at grade. To enhance this relationship further, the ground level will be primarily glazed to allow for an activated street front along Carling Avenue, Parkdale Avenue, and Hamilton Avenue. This lively duality between interior and exterior, public and private, will be continued around the interior facades of each tower at the ground floor. The porosity at ground level will emphasize the relationship between the two tower towers at grade. The interior courtyard forms an interactive space for residents of each building. Curved landscaping features paralleling the architectural intent of this space result in a vivid and lush common gathering space.

The west and east tower's ground floor level provides an at grade experience in keeping with the remainder of the podium above, while distinctly reflecting the uniquely ground level amenity spaces anticipated within. The more porous ground level, articulated to be accessible and interactive, in turn acts as a wayfinding clue, indicative of primary entrances, public amenity spaces, and transitions between interior and exterior. Where recessed, the ground level provides additional frontage along Carling Avenue and Parkdale Avenue allowing for greater landscaping to buffer traffic, noise, and wind, while creating safe and enjoyable designated zones for OC Transpo bus service users.

Ease of access to the site is further reinforced by the siting of the two towers, and the landscaping at grade. The site is serviced by OC Transpo bus routes along Carling Avenue and Parkdale Avenue, and is within walking distance of the Carling Station of the O-Train Trillium Line. Additionally, the site is accessed by vehicular traffic, pedestrians, and cyclists. The staggered building masses, and curvature of the site landscaping aids in funneling users on foot into and through the site,

towards entrances, and outdoor amenity spaces. The entrance to the parking garage is located on Hamilton Avenue, along the west façade of the west tower. Each tower's "back-of house" services, such as move-in rooms and waste removal facilities will be accessed by a separate entrance along Hamilton Avenue. Opaque materiality of the building at grade where these services are located aids in the distinction between user's lively amenity spaces and user's service areas.

Continuing upwards, the towers provide density to the neighbourhood, as they contain a total of 411 units. These unit's views benefit from the varied landscape and topography of the city. To the south, units will have an unobstructed view of the Central Experimental Farm, while along the west and north facades, residents will see Ottawa's sunsets and beyond, the Gatineau hills. To the north-east, residents will view the city's downtown core.

The phased development will be completed with four levels of underground parking, containing +/- 322 parking spaces, resulting in a ratio of 0.6 parking spaces for residences and 0.1 for visitors. Additionally, +/- 375 bicycle stalls are included, along with additional bicycle storage at grade, encouraging residents to travel via a sustainable method of transportation.

BUILT FORM & URBAN FABRIC

The building massing for the two towers is comprised of three designated components: the porous and ground level, the more opaque remainder of the podium floating above, and the towers, elongated by vertical bands of masonry and glazing drawing one's eye towards the sky. The architectural expression of each of these three components require demonstrating a sensitivity to the abutting neighbourhood, and a sense of stateliness for the development's future residents. The buildings themselves, acting as sisters, as opposed to identical twins, follow the same design principles, while individually addressing the needs of their location on the site, orientation, and internal spaces.

At grade, the street and interior courtyard space is activated for a dynamic experience. At five meters in height, this level, sheltered by the above podium, reflects the neighbourhood scale. Wide, unobstructed sidewalks, large planters containing trees, bench seating, and varied paving materials continue this concept forward, allowing pedestrians to weave through the site leisurely. The inner courtyard space between the towers, protected by taller planters and mature trees, results in a designated zone to be used primarily by residents. The sidewalks parallel to Parkdale Avenue and Hamilton Avenue will contain mature tree planters and varied landscaping pavers to create pleasant entry points to the site.

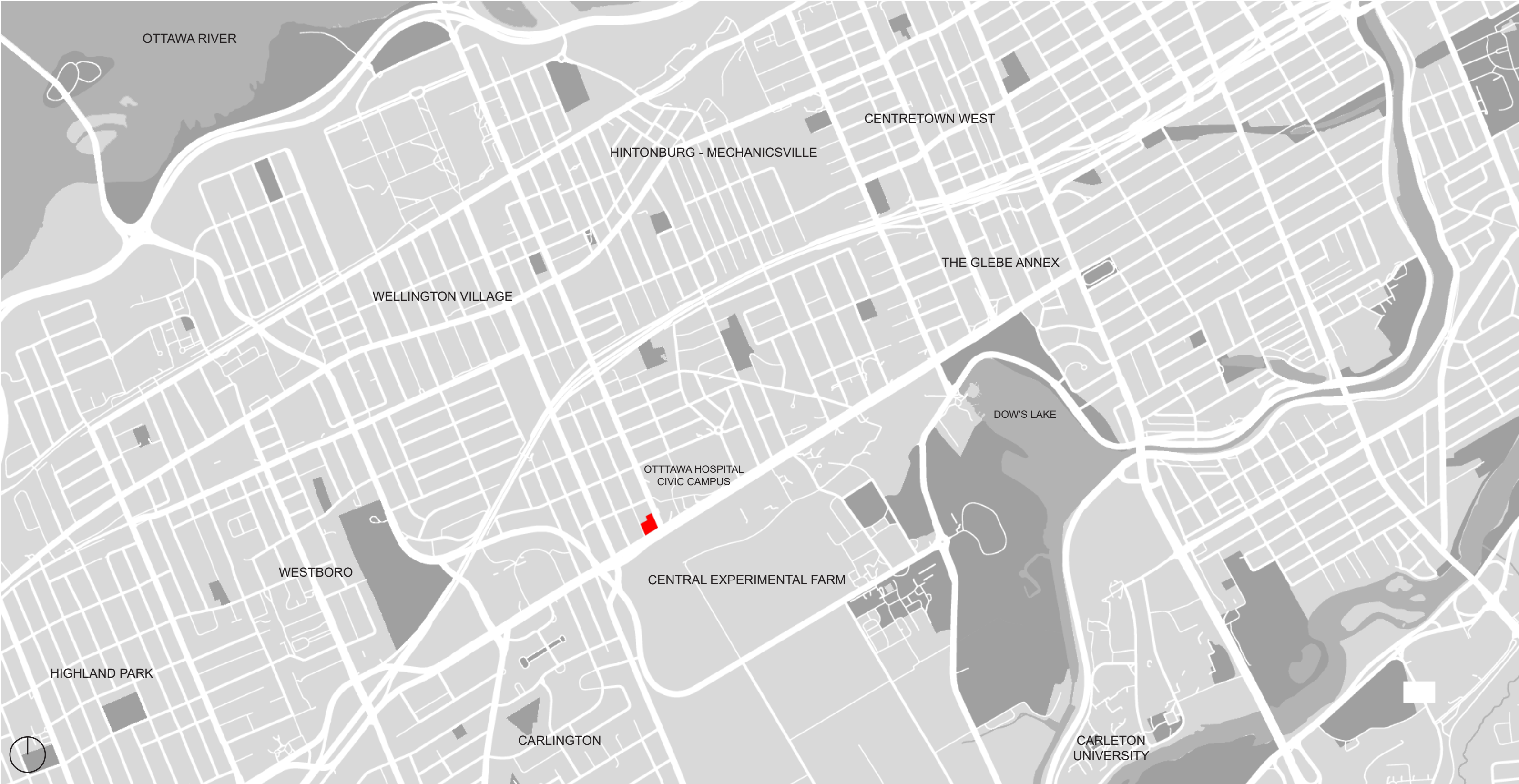
Levels two through four of the podiums, are punctuated by a variety of windows, encapsulated by masonry, and a combination of inset and juliet balconies floating above grade. The slender towers present the third and final condition of the building as they emerge from the heavier podium level. The narrowness of the floorplates is further emphasized by the vertical components

demarcating glazed sections of the facades. The mechanical penthouse is encapsulated by the materiality of the towers for an uninterrupted form.

Both towers are terraced towards the north to provide a less daunting relationship between the residential neighbourhood and the site. This stepped form also allows for rooftop amenity or private terrace space with idealistic views. Outdoor spaces are continuous throughout the tower, as a variation of projecting, recessed, and juliet balconies aid in activating the tower facades.

These three primary components individually and collectively address the task of maintaining the site's unique location. This redevelopment aims to provide a residential hub for the Civic Hospital – Central Park neighbourhood, while embracing it's greater role within the city of Ottawa, as an intersectional link between a variety of building and landscape typologies.

CONTEXT CITY SCALE



CONTEXT REGIONAL SCALE



LEGEND:

- FUTURE LRT LINE
- EXISTING BUS ROUTES
- FUTURE LRT STATION
- EXISTING BUS STOPS

CONTEXT

NEIGHBOURHOOD SCALE

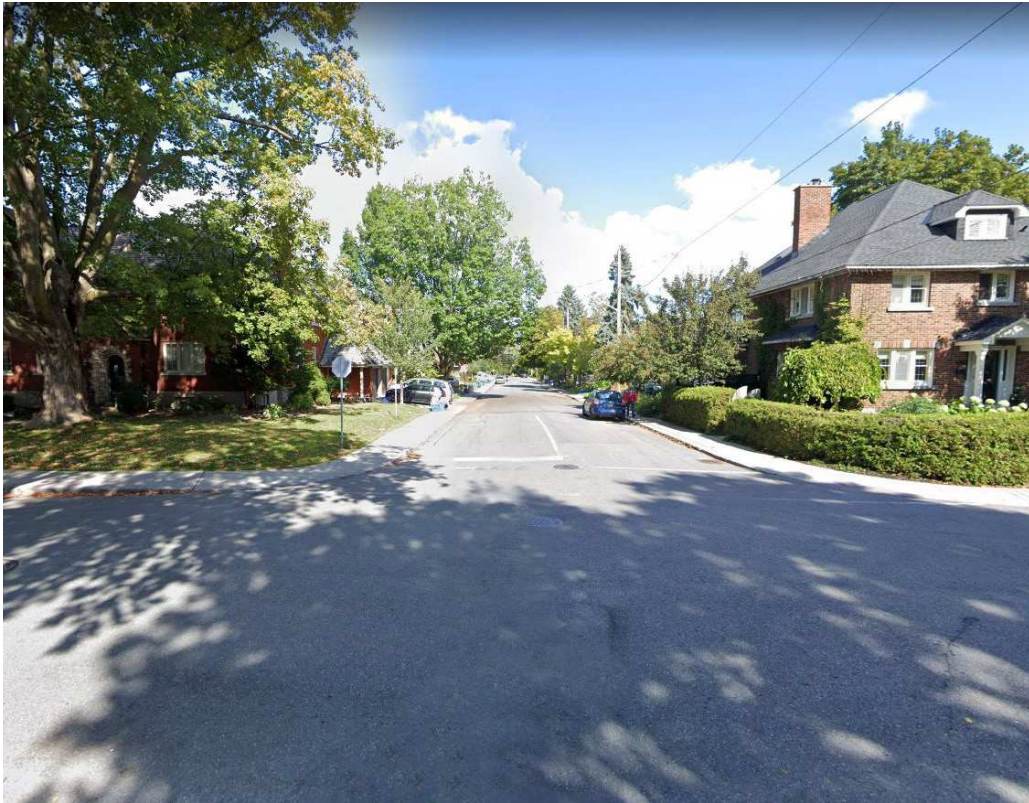
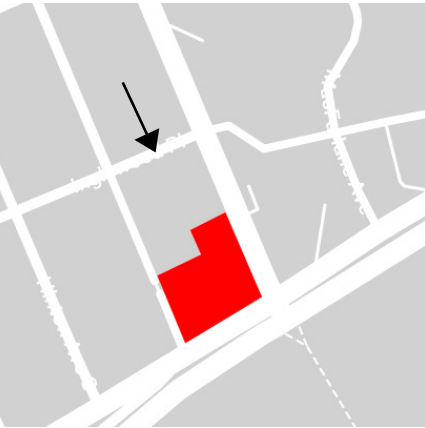


CONTEXT

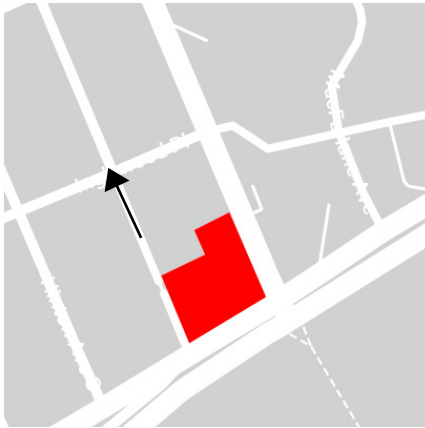
NEIGHBOURHOOD SCALE



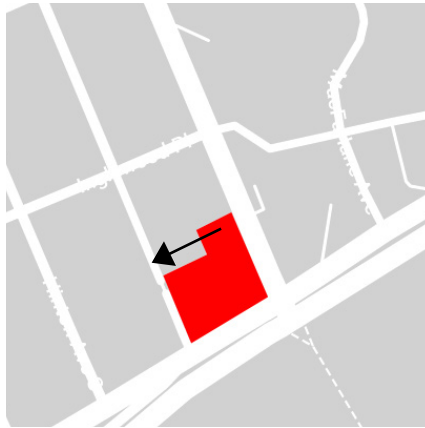
INGLEWOOD PLACE



INTERSECTION OF HAMILTON AVENUE AND
INGLEWOOD PLACE



HAMILTON AVENUE SOUTH

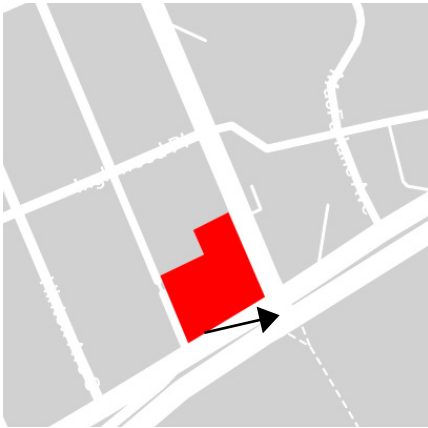


CONTEXT

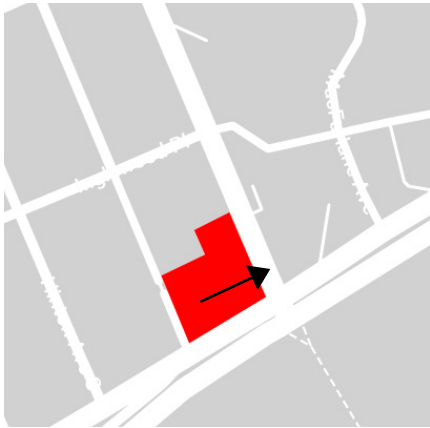
NEIGHBOURHOOD SCALE



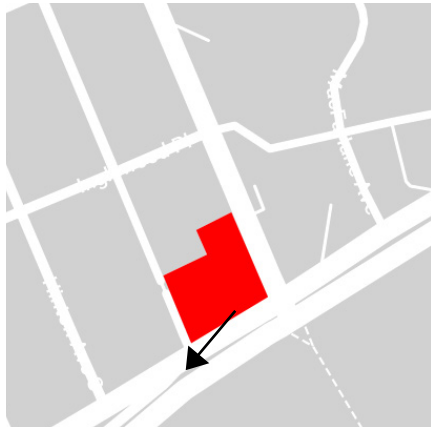
VIEW ALONG CARLING AVENUE, LOOKING EAST,
INTERSECTION OF PARKDALE AND CARLING



PARKDALE AVENUE



VIEW ALONG CARLING AVENUE,
LOOKING WEST



CONTEXT FUTURE DEVELOPMENT OF CARLING AVENUE



PUBLIC CONSULTATION WHAT WE HEARD - MAJOR THEMES

Land Use

Desire for commercial/retail, mixed-use

Building Typology

Condo vs. Apartment

Height + Density

Compatibility with the Low-Rise Residential Neighbourhood

Transition, form, materiality, heritage context

Park/Greenspace

Scale, orientation, location and proximity to Experimental Farm

Community Benefits

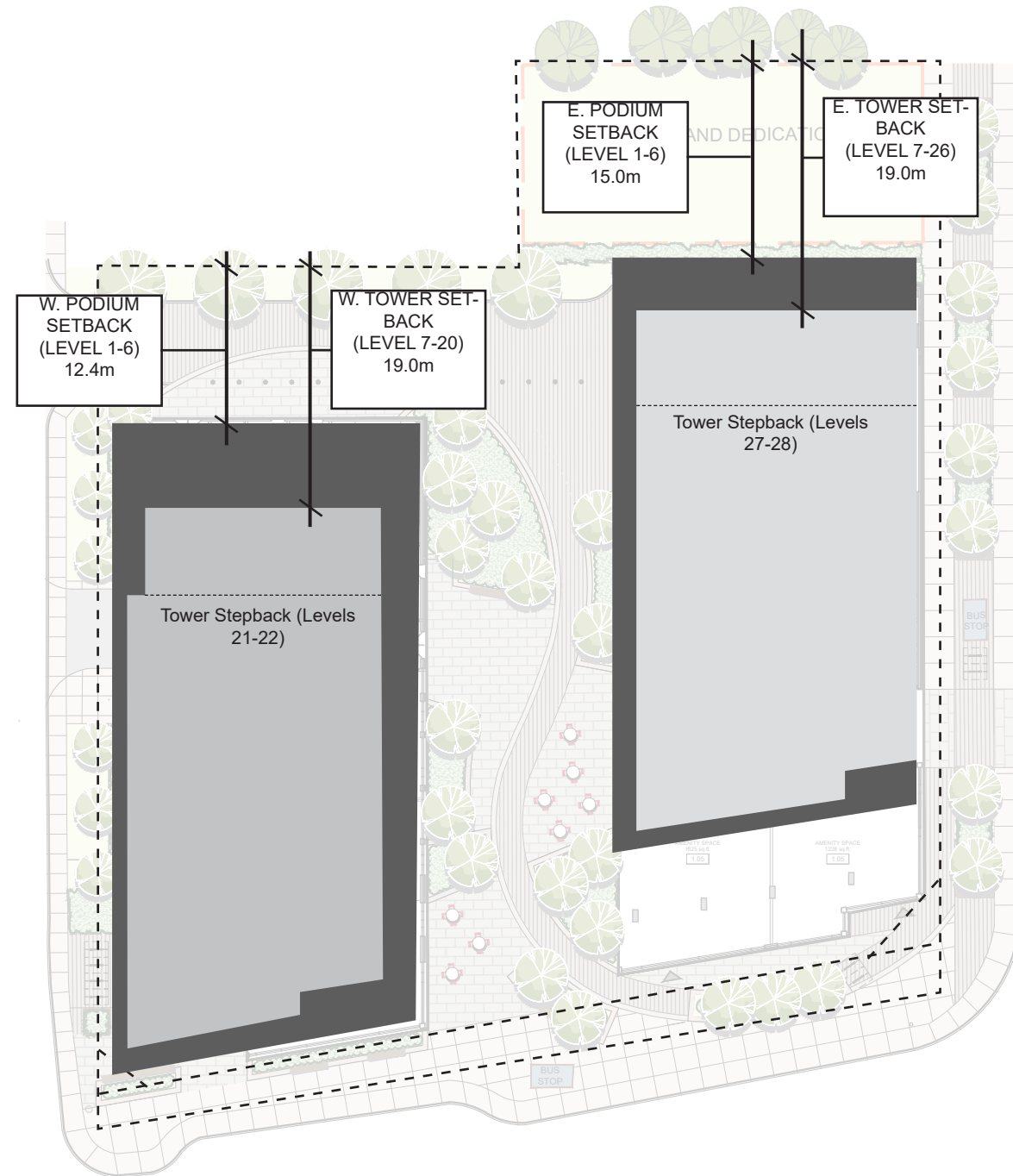
Achieving Better Transition

ZBLA vs. As-Of-Right

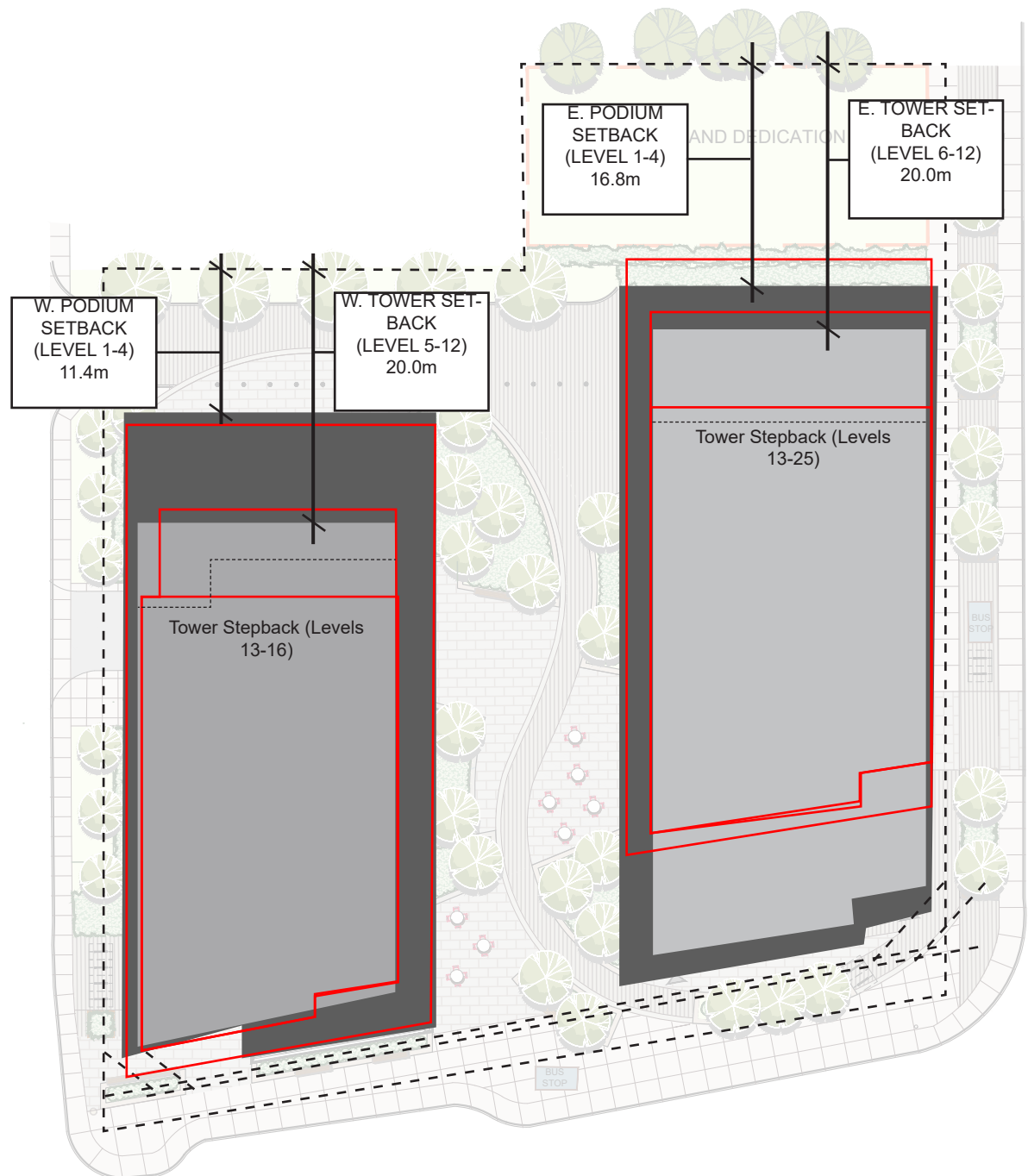
Permitted Height vs. Additional Height

DESIGN EVOLUTION SITE PLAN

— Previously Proposed Building Outline



Previously Proposed Concept



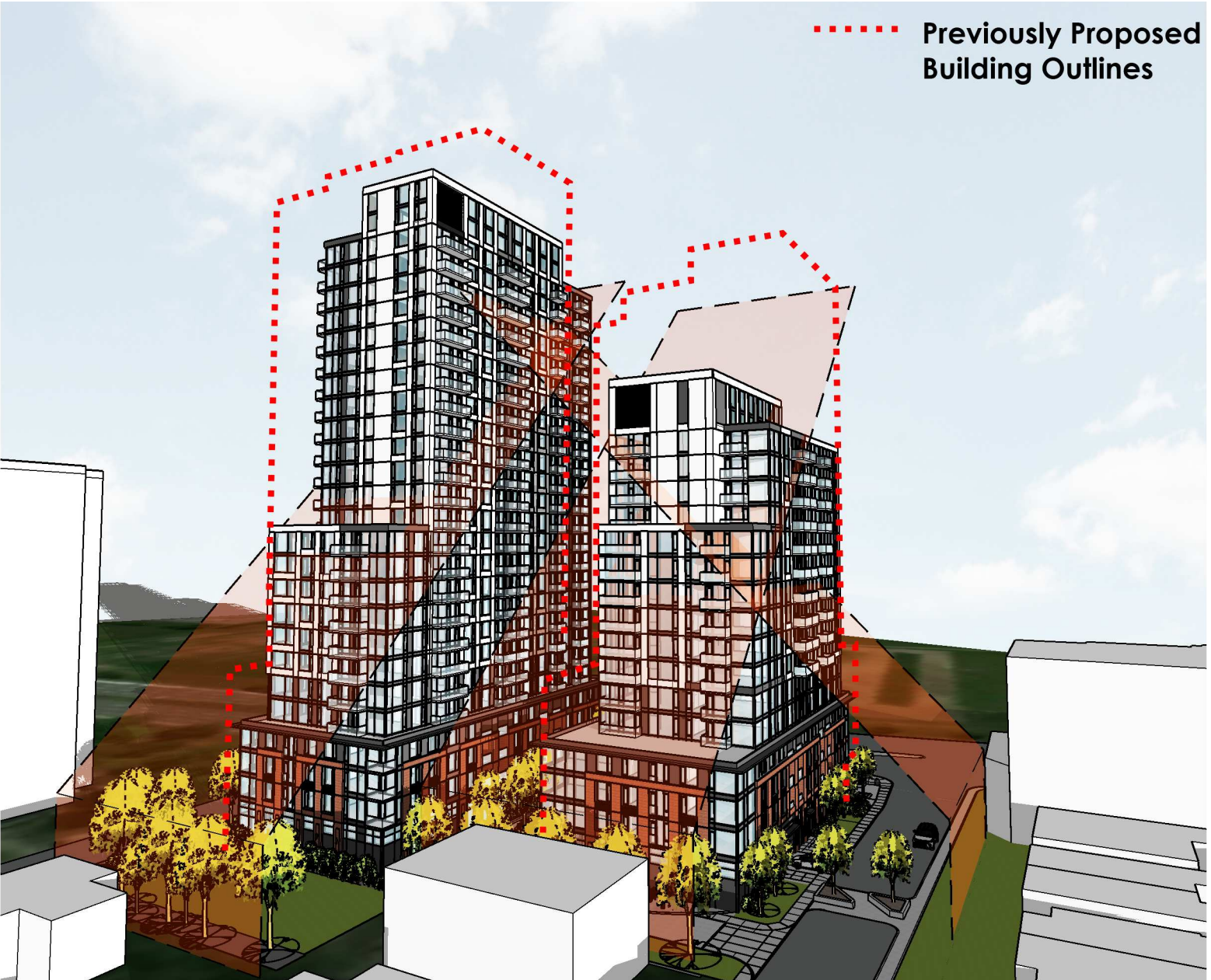
Currently Proposed Concept

DESIGN EVOLUTION

RELATION TO ANGULAR PLANE



Previously Proposed Concept

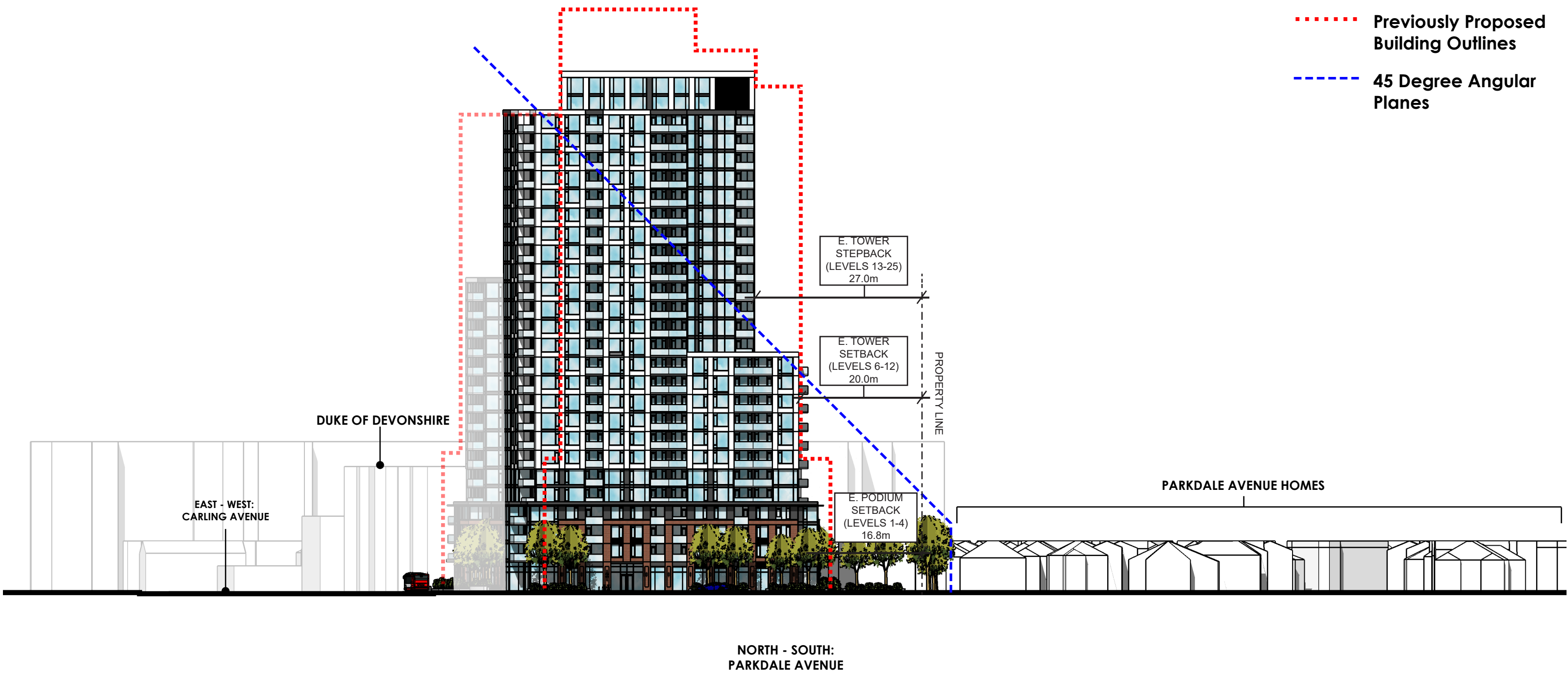


Currently Proposed Concept

DESIGN EVOLUTION SITE CONTEXT SECTION/ELEVATION

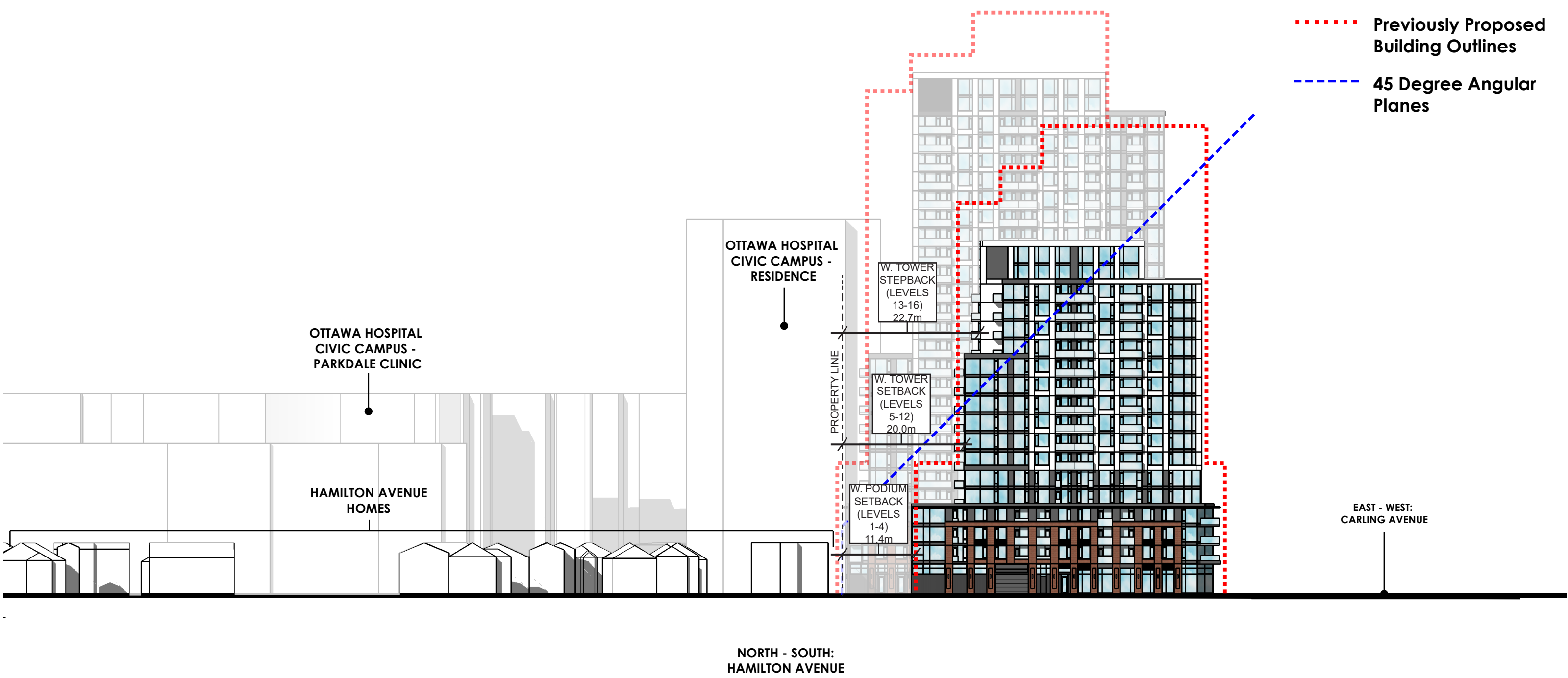


DESIGN EVOLUTION SITE CONTEXT SECTION/ELEVATION



- Previously Proposed Building Outlines
- - - - 45 Degree Angular Planes

DESIGN EVOLUTION SITE CONTEXT SECTION/ELEVATION



DESIGN EVOLUTION PREVIOUS CONCEPT WITH PROPOSED HEIGHT REDUCTION



Diagrammatic representation of height reduction and area removed from the East and West towers.

DESIGN EVOLUTION PREVIOUS CONCEPT WITH PROPOSED PODIUM REDUCTION



DESIGN EVOLUTION PREVIOUS CONCEPT WITH PROPOSED PODIUM REDUCTION



Diagrammatic representation of the reduction in podium height in the East and West Towers.

PROPOSED DEVELOPMENT

DEVELOPMENT STATISTICS



TOTAL GROSS: 358,574 SQ.FT. (33,313 SQ.M.)
TOTAL NET: 283,080 SQ.FT. (26,299 SQ.M.)
TOTAL NO. OF UNITS: 411

WEST TOWER (16 STOREYS):

GFA: 131,753 SQ.FT. (12,240 SQ.M.)
 NET: 100,709 SQ.FT. (9356 SQ.M.)

UNIT STATISTICS:

	STUDIO	1 BED	1 BED+	2 BED	2 BED+	TOTAL
NO.	16	71	3	46	7	143
RATIO	11%	50%	2%	32%	5%	

AVG. SQ.FT. = 704 SQ.FT. (65 SQ.M.)

EAST TOWER (25 STOREYS):

GFA: 226,821 SQ.FT. (21,072 SQ.M.)
 NET: 182,371 SQ.FT. (16,943 SQ.M.)

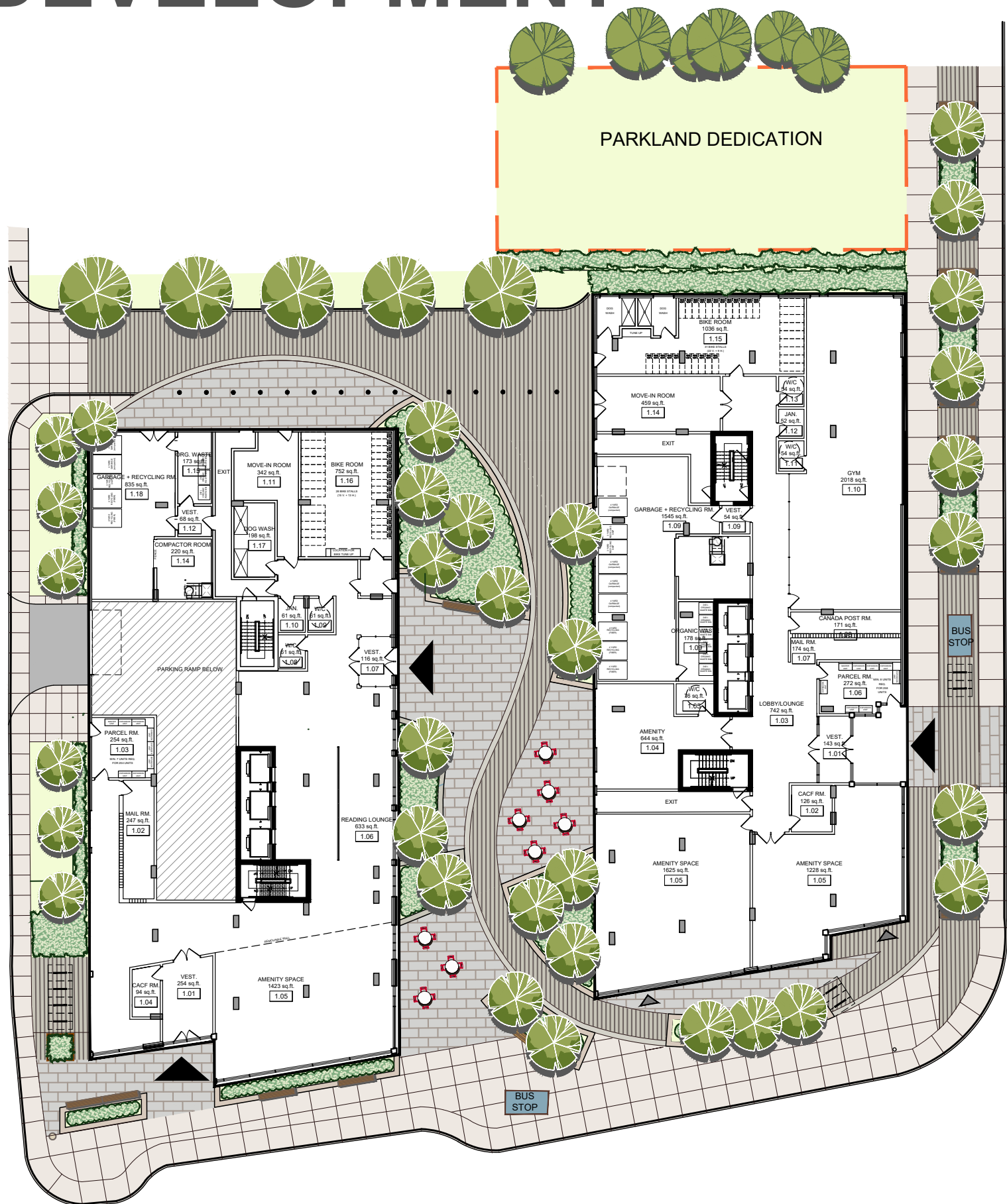
UNIT STATISTICS:











	STUDIO	1 BED	1 BED+	2 BED	2 BED+	TOTAL
NO.	37	132	21	54	24	268
RATIO	14%	49%	8%	20%	9%	

AVG. SQ.FT. = 680 SQ.FT. (63 SQ.M.)

- LEGEND:**
- PROPOSED CONCRETE PAVING
 - PROPOSED UNIT PAVING - TYPE 1
 - PROPOSED UNIT PAVING - TYPE 2
 - CONCRETE SEATING WALL
 - SHRUB PLANTING
 - TOPSOIL & SOD
 - WOOD BENCH
 - PROPOSED LIGHT POST
 - PROPOSED TREE
 - EXISTING TREE TO BE RETAINED

PROPOSED DEVELOPMENT



- LEGEND:**
-  PROPOSED CONCRETE PAVING
 -  PROPOSED UNIT PAVING - TYPE 1
 -  PROPOSED UNIT PAVING - TYPE 2
 -  CONCRETE SEATING WALL
 -  SHRUB PLANTING
 -  TOPSOIL & SOD
 -  WOOD BENCH
 -  PROPOSED LIGHT POST
 -  PROPOSED TREE
 -  EXISTING TREE TO BE RETAINED



DESIGN NARRATIVES

BUILT FORM & URBAN FABRIC
PUBLIC REALM AND STREET ANIMATION
SKYLINE IMPACT
CONNECTIVITY

PROPOSED DEVELOPMENT



BUILT FORM



- 1 - GROUND LEVEL
- 2 - PODIUM
- 3 - TOWER
- 4 - PENTHOUSE

BUILT FORM



1 - GROUND LEVEL
2 - PODIUM



1081 CARLING AVENUE
Urban Design Review Panel

BUILT FORM



BUILT FORM



1 - GROUND LEVEL
2 - PODIUM



1081 CARLING AVENUE
Urban Design Review Panel

BUILT FORM



3 - TOWER

BUILT FORM



URBAN FABRIC



Both the East and West towers utilize existing context buildings as points of reference. The datum lines delineating the podium levels have been abstracted from neighbouring building heights. While the proposed site buildings aim to borrow influential massing ratios from Carling Avenue's frontage, it also aims to create a new precedent, defining a manner in which future building's along Carling Avenue will be able to enhance street presence, welcome the public realm and activate the thoroughfare.

URBAN FABRIC



PUBLIC REALM + STREET ANIMATION



PUBLIC REALM + STREET ANIMATION



PUBLIC REALM + STREET ANIMATION



PUBLIC REALM + STREET ANIMATION



SKYLINE IMPACT



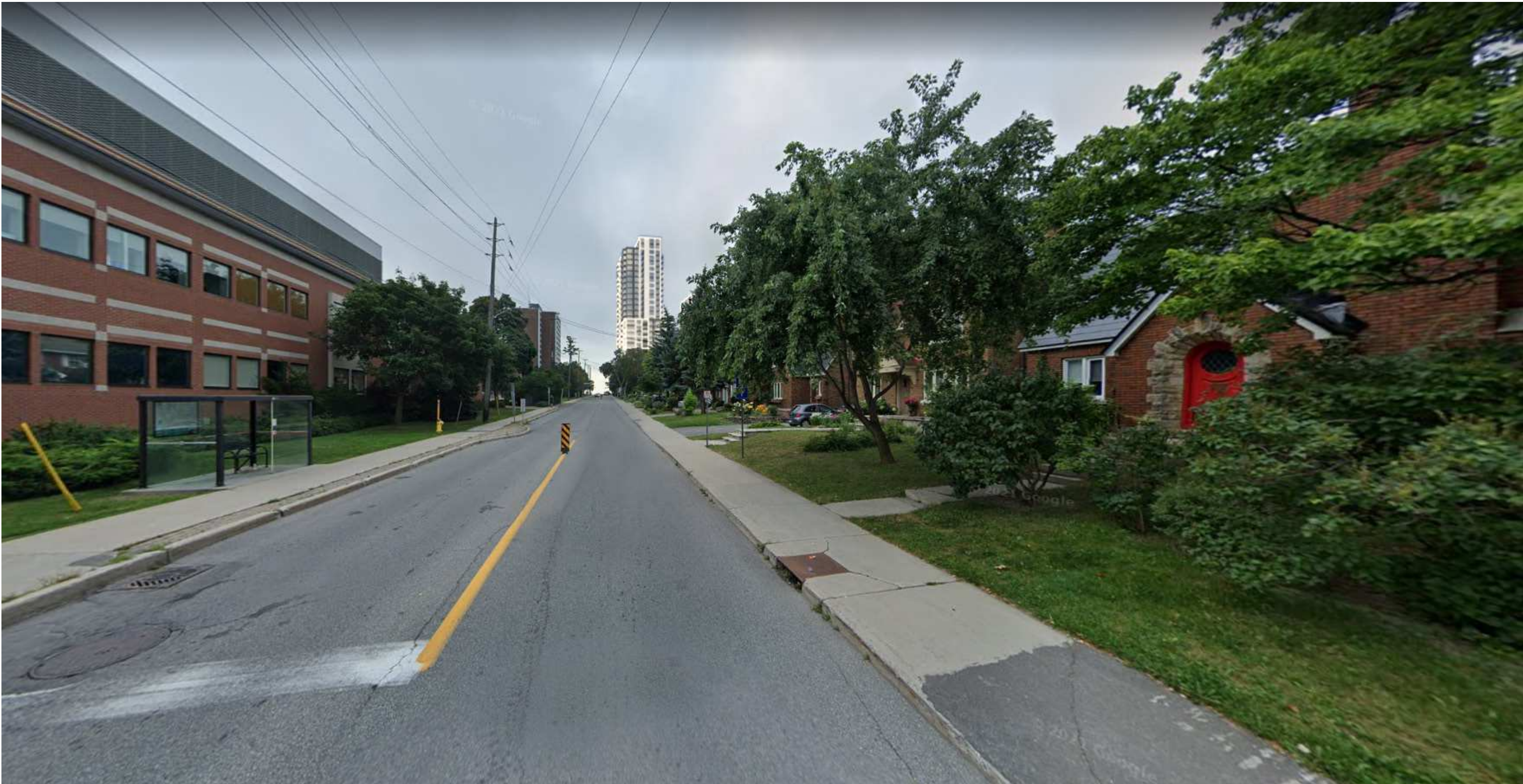
SKYLINE IMPACT VIEW EAST ALONG CARLING AVENUE



SKYLINE IMPACT VIEW WEST ALONG CARLING AVENUE



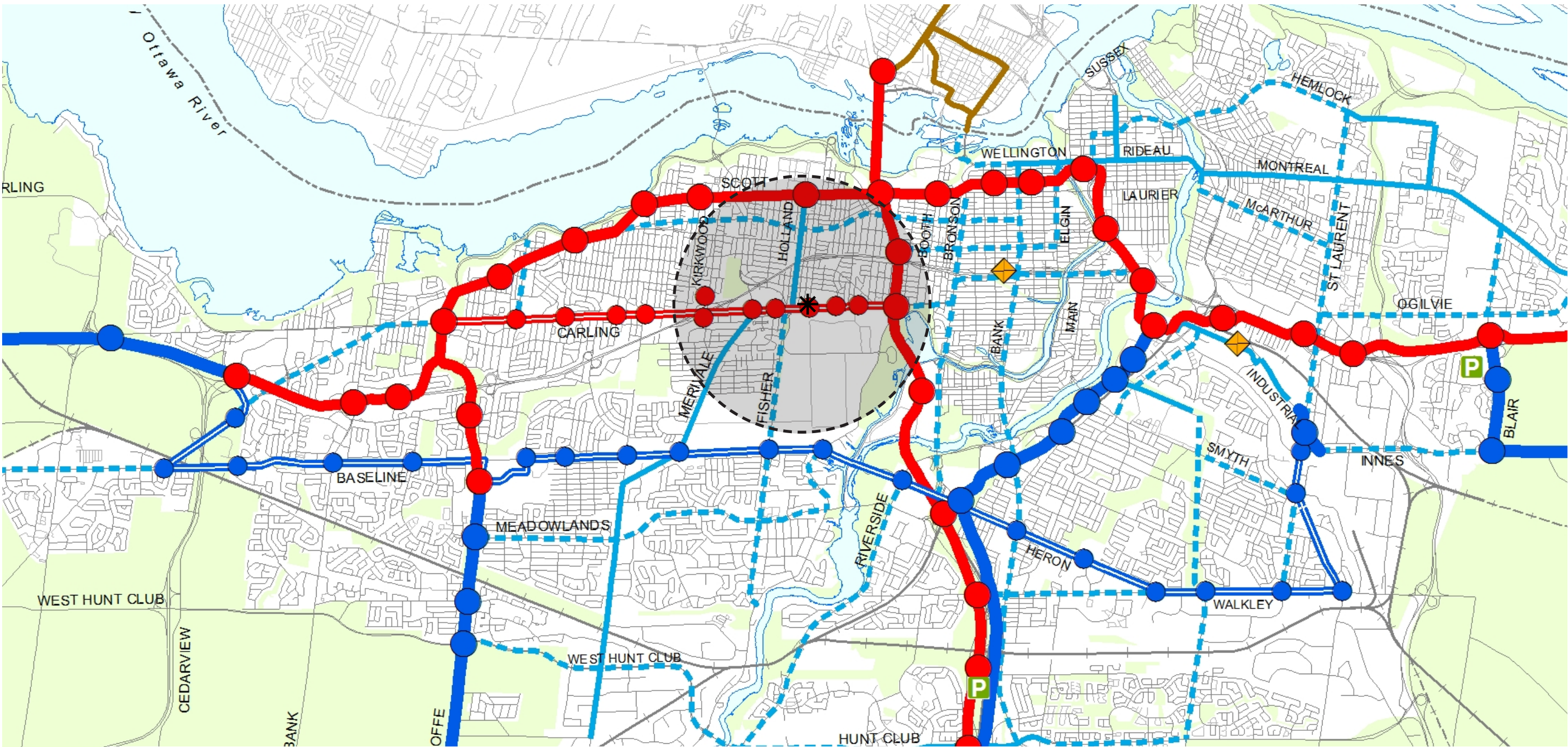
SKYLINE IMPACT VIEW FROM PARKDALE AVENUE + RUSKIN STREET



SKYLINE IMPACT VIEW FROM THE CENTRAL EXPERIMENTAL FARM



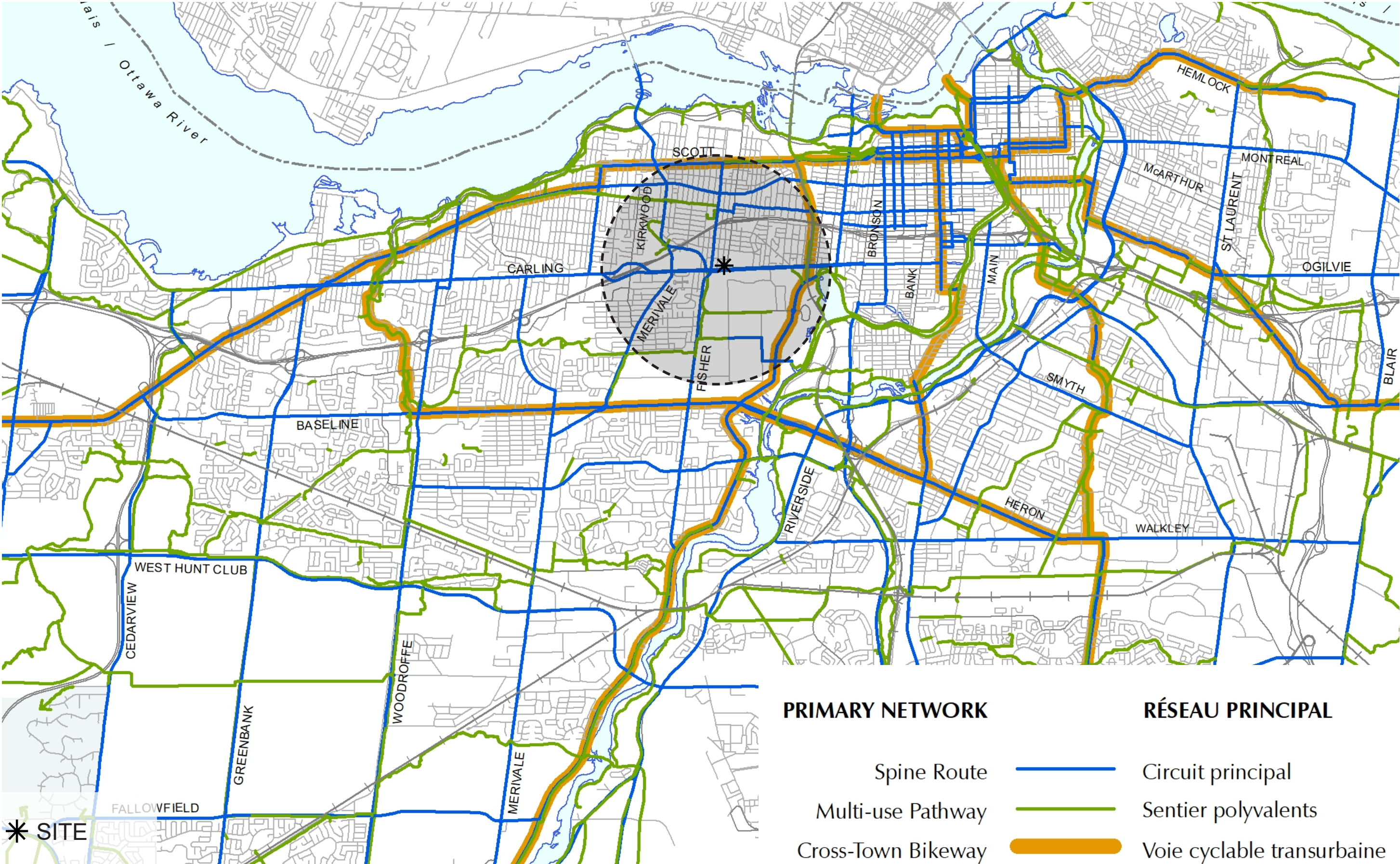
CONNECTIVITY SCHEDULE D - RAPID TRANSIT NETWORK



RAPID TRANSIT	TRANSPORT EN COMMUN RAPIDE	Park and Ride	P Parc-O-Bus
Light Rail Transit (LRT) - Grade Separated Crossings	Transport en commun par train léger (TCTL) - passages étagés	Transit Station - rail	Station du transport - train
Light Rail Transit (LRT) - At-Grade Crossings	Transport en commun par train léger (TCTL) - passages à niveau	Transit Station - bus	Station du transport - autobus
Bus Rapid Transit (BRT) - Grade Separated Crossings	Transport en commun rapide par autobus (TCRA) - passages étagés	Conceptual Future Transit Corridor	Avenir conceptuel - Couloir de transport en commun
Bus Rapid Transit (BRT) - At-Grade Crossings	Transport en commun rapide par autobus (TCRA) - passages à niveau	Inter-regional Stations	Stations interrégionales
TRANSIT PRIORITY	PRIORITÉ AU TRANSPORT EN COMMUN	Potential Rail Yard	Cour de tirage possible pour trains
Transit Priority Corridor (Continuous Lanes)	Corridor donnant priorité au transport en commun (voies continues)	Gatineau Rapibus	Rapibus de Gatineau
Transit Priority Corridor (Isolated Measures)	Corridor donnant priorité au transport en commun (mesures isolées)		

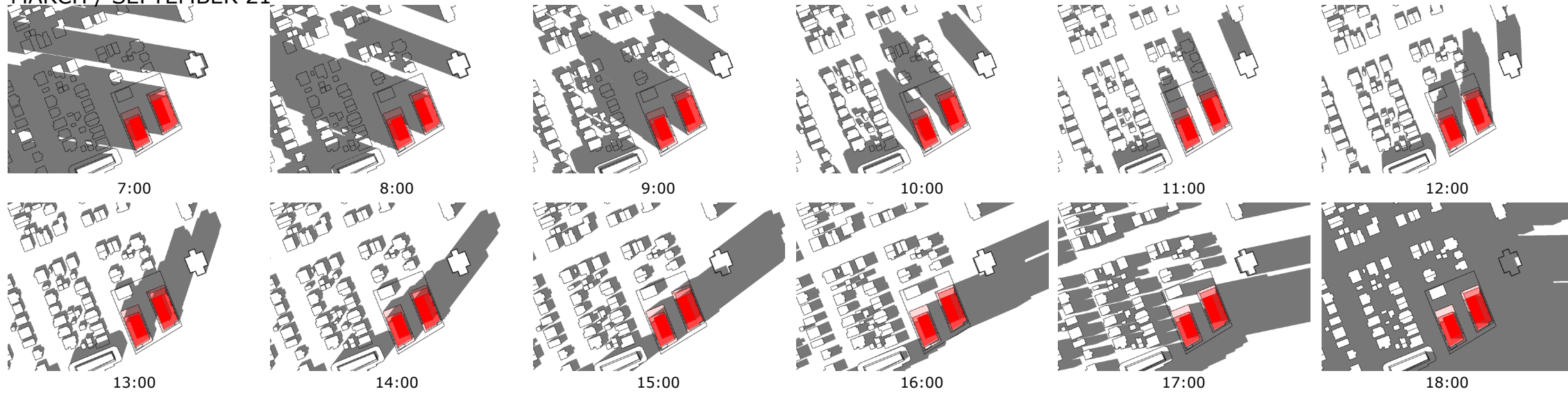
* SITE

CONNECTIVITY SCHEDULE C - PRIMARY URBAN CYCLING NETWORK

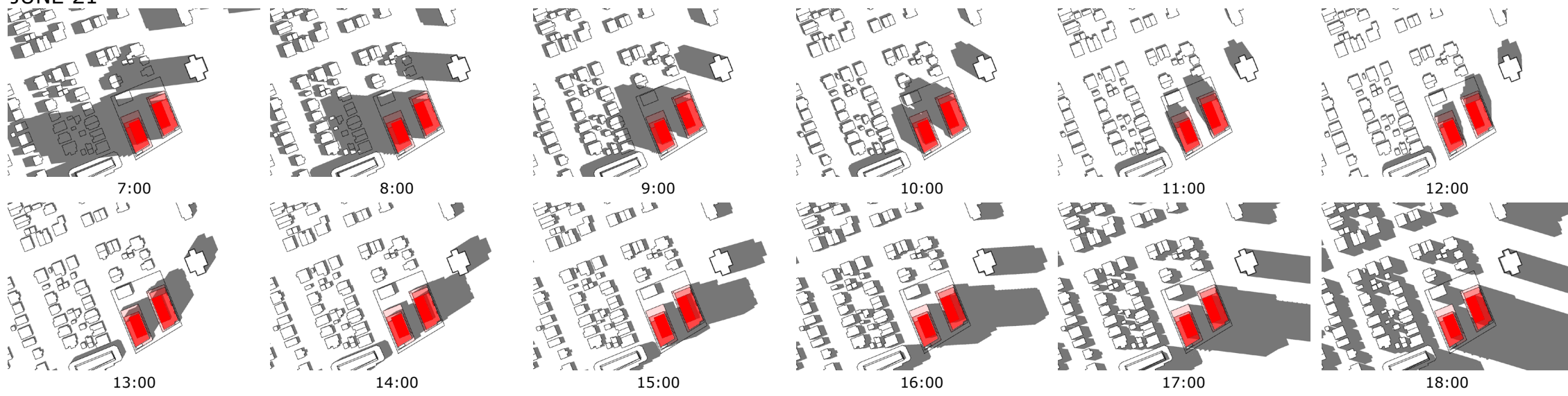


SUN / SHADOW STUDY

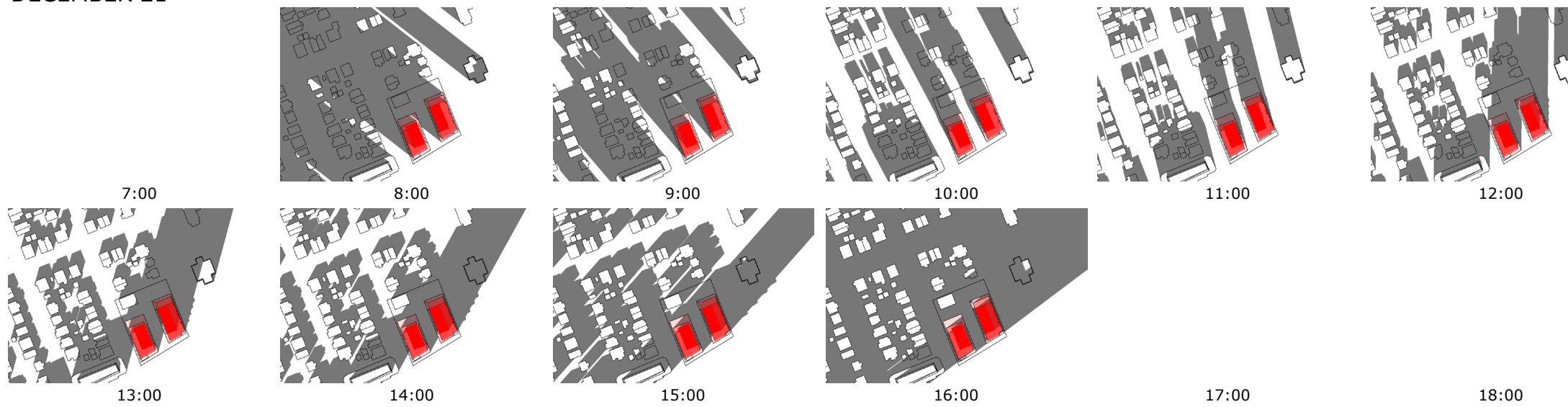
MARCH / SEPTEMBER 21



JUNE 21



DECEMBER 21



THANK YOU



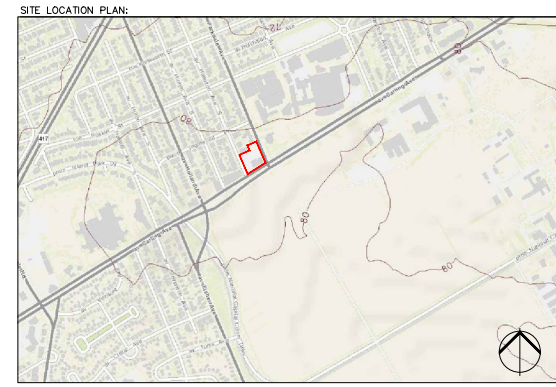
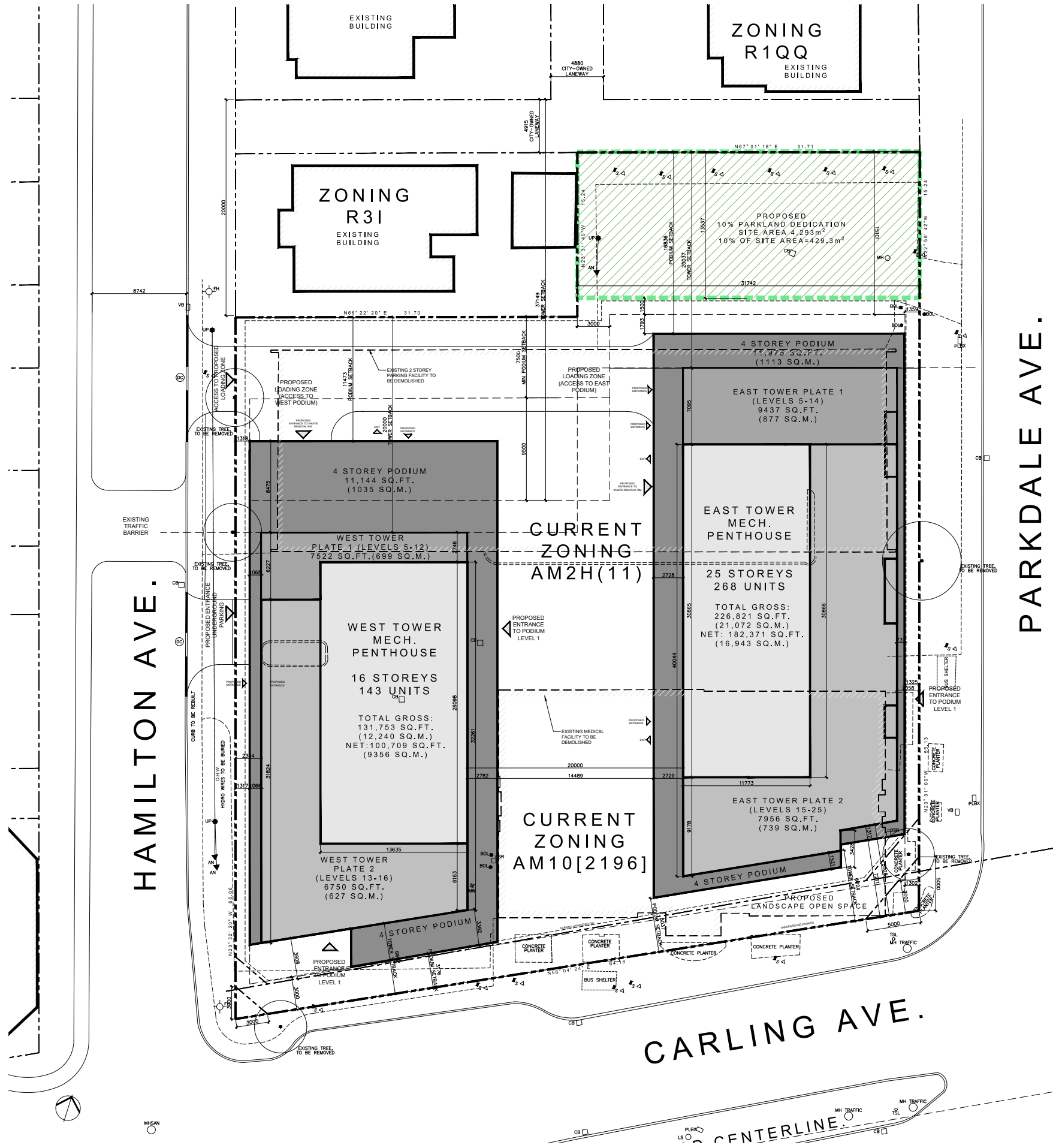
HOBIN
ARCHITECTURE



CSW

An architectural line drawing of a modern multi-story building with a courtyard. The building features large glass windows and balconies. The courtyard has a paved area with a grid pattern, several trees, and a bicycle rack with a bicycle. The drawing is in a light, sketchy style.

APPENDIX
SITE PLAN
FLOOR PLANS
ELEVATIONS
LANDSCAPE PLAN



ZONING NOTES:

CURRENT ZONING: NORTH PARCEL: AM2 H(11), SOUTH PARCEL: AM10[2196]

LOT AREA: 4,293 m²

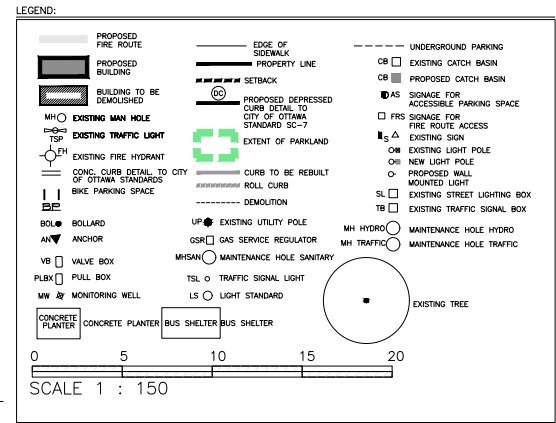
DEVELOPMENT STATS PHASE 1

	REQUIRED	PROPOSED
LOT WIDTH		63.44m
LOT DEPTH		70.3m IRREGULAR
UNITS		268
EAST TOWER UNITS		143
WEST TOWER UNITS		411
FRONT YARD SETBACK	CARLING AVENUE	3 m VARIES = 3.1m - 5m
REAR YARD SETBACK		7.5 m VARIES = 11.4m - 16.5m
CORNER SIDE YARD SETBACK	PARKDALE AVE.	1.3 m
CORNER SIDE YARD SETBACK	HAMILTON AVE.	1.3 m
MAXIMUM HEIGHT		30 m
EAST TOWER		77 m
WEST TOWER		50 m
NUMBER OF STOREYS		25 STOREYS
EAST TOWER		16 STOREYS
WEST TOWER		16 STOREYS
BUILDING AREA (TOTAL GROSS)		
EAST TOWER		+/- 21,072 m ²
WEST TOWER		+/- 12,240 m ²

NOTE: ALL EXISTING SITE INFORMATION AS PER SITE SURVEY PLAN DATED JUNE 12, 2018 AND PREPARED BY STANTEC

PARKING SUMMARY

REQUIRED	Phase 1		Phase 2		Total	Bicycle Parking	Total
	Units	Spots	Units	Spots			
Units	125	288	211	211	411		411
Units required by 14	117	262	199	199	398		398
Parking Req. (1st 15m)	88	131	200	200	398		398
Parking Req. (1st 15m)	14	27	41	41	82		82
TOTAL	102	158	241	241	482		482
PROVIDED	Phase 1		Phase 2		Total		Total
P1	125	288	211	211	411		411
P2	125	288	211	211	411		411
P3	125	288	211	211	411		411
P4	125	288	211	211	411		411
TOTAL	125	288	211	211	411		411



PROJECT TEAM

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STANLEY WEBSTER
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no.	date	revision

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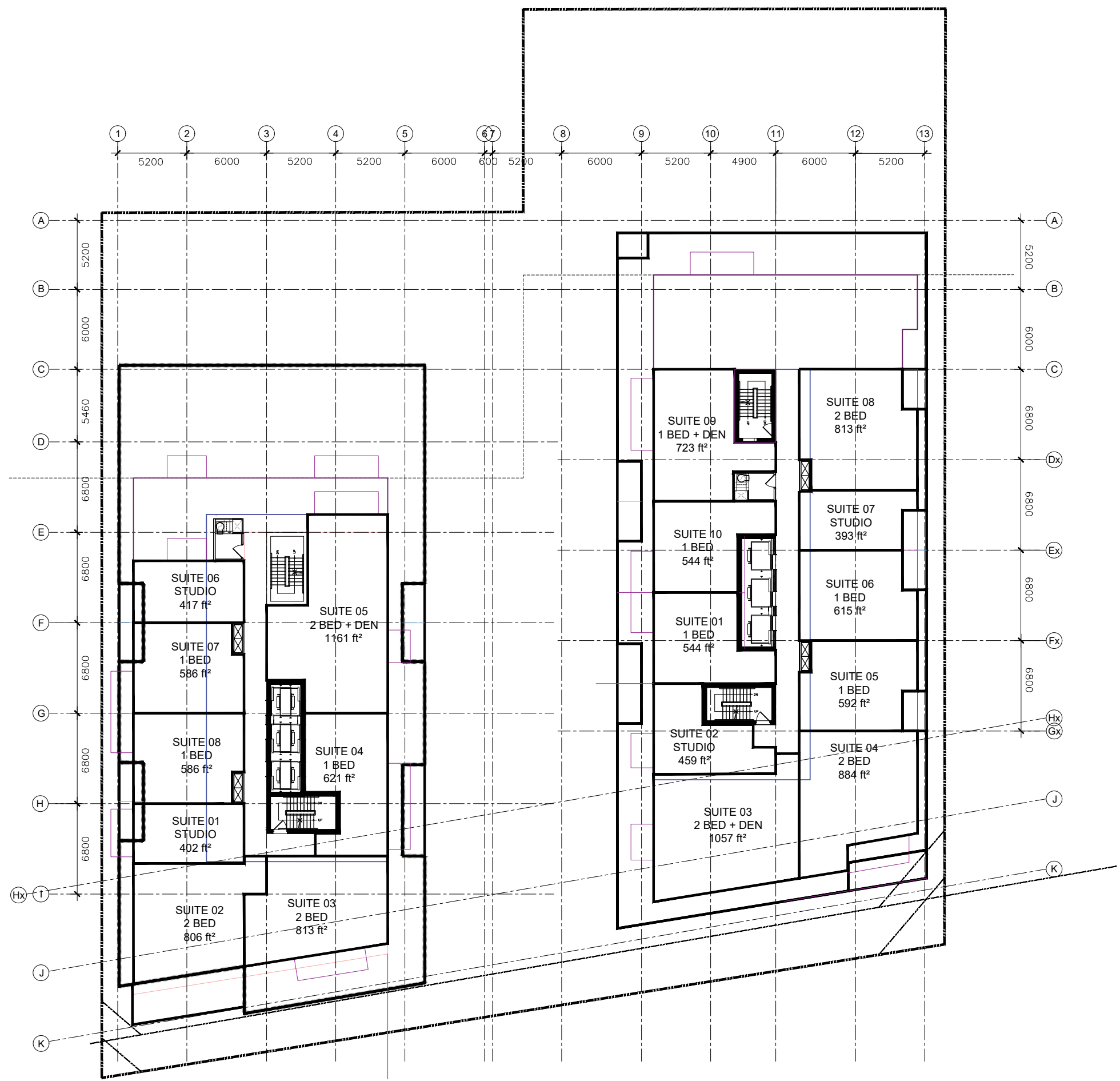


HOBIN
ARCHITECTURE

PROJECT/LOCATION:
TAGGART REALTY
1081 CARLING AVE.
OTTAWA, ON

DRAWING TITLE:
TYPICAL TOWER
LEVEL 2 PLAN

DRAWN BY: PB	DATE: 22.08.15	SCALE: 1:150
PROJECT: 2117		DRAWING NO.: A206
REVISION NO.:		



1 TYPICAL TOWER LEVEL 2 PLAN
A0.01 1:150

None

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LEGEND:

NEW BUILD:

AP-1 ALUMINUM PANEL 1

AP-2 ALUMINUM PANEL 2

BV-1 BRICK VENEER

PC PRECAST CONCRETE

GR GLASS RAILING

MS-1 METAL SIDING 1

SP SPANREL PANEL

VG VISION GLASS

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HOBIN
ARCHITECTURE

PROJECT/LOCATION:

1081 CARLING AVENUE

DRAWING TITLE:

ELEVATIONS

DRAWN BY:

PB

DATE:

220815

SCALE:

1:200

PROJECT:

2117

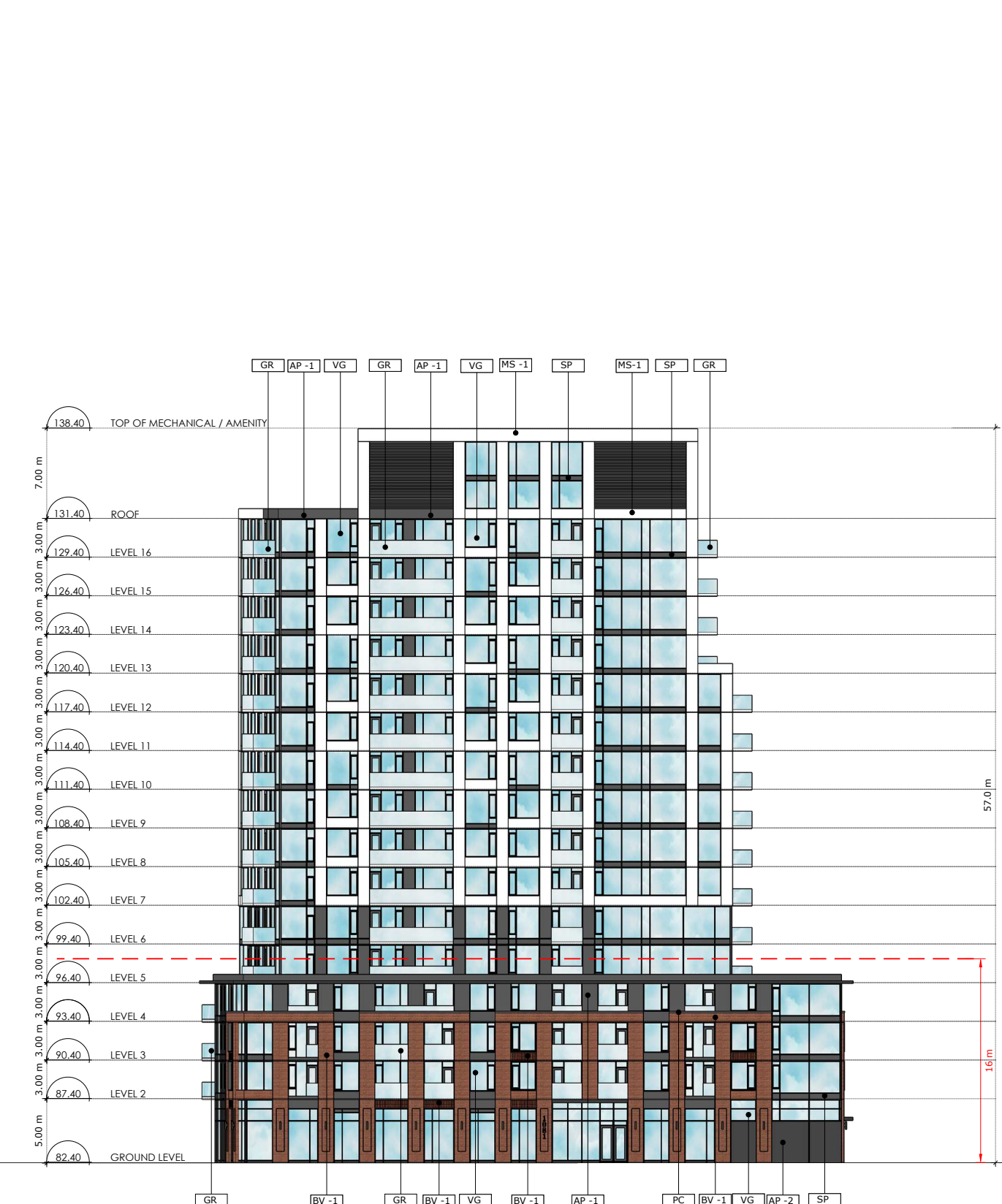
DRAWING NO.:

A3-00

REVISION NO.:



1 EAST ELEVATION
Scale: 1: 200



2 EAST ELEVATION - WEST TOWER
Scale: 1: 200

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no.	date	revision

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LEGEND

NEW BUILD	
AP-1	ALUMINUM PANEL 1
AP-2	ALUMINUM PANEL 2
BV-1	BRICK VENEER
PC	PRECAST CONCRETE
GR	GLASS RAILING
MS-1	METAL SIDING 1
SP	SPANDREL PANEL
VG	VISION GLASS

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HOBIN
ARCHITECTURE

PROJECT/LOCATION:

1081 CARLING AVENUE

DRAWING TITLE:

ELEVATIONS

DRAWN BY: PB

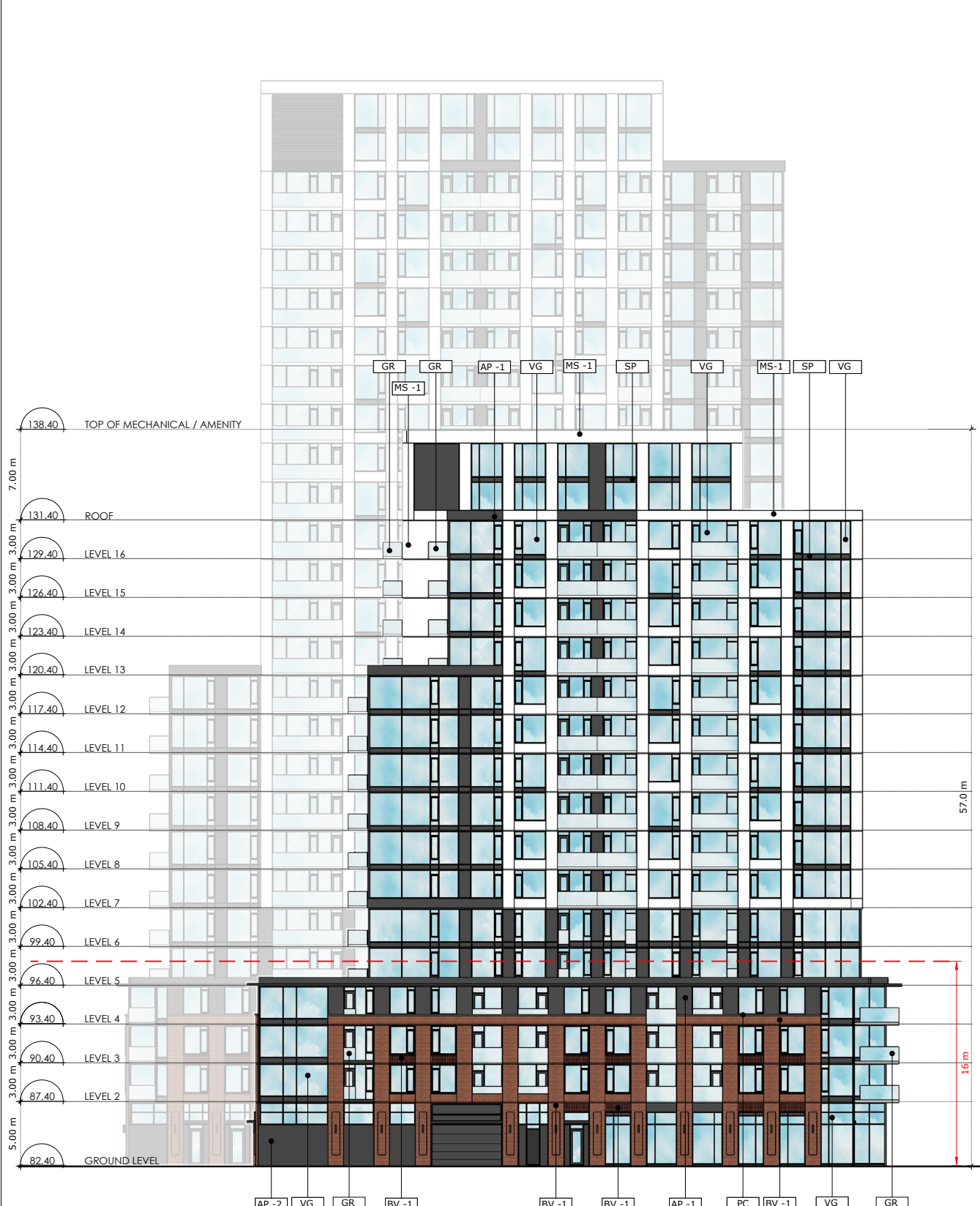
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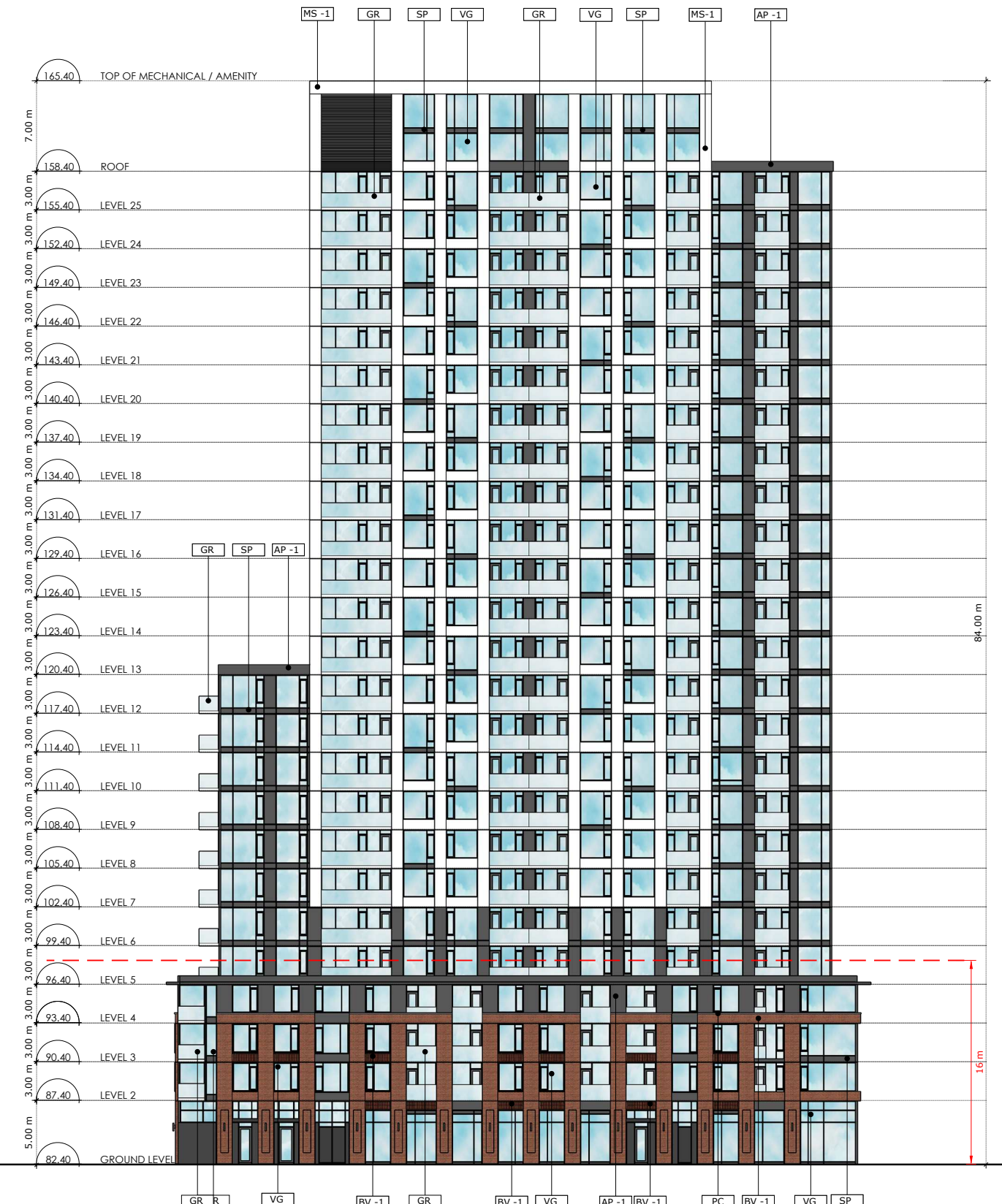
PROJECT: 2117

DRAWING NO.: A3-01

REVISION NO.:



1 WEST ELEVATION
Scale: 1: 200



2 WEST ELEVATION - EAST TOWER
Scale: 1: 200

PROJECT TEAM

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no.	date	revision

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LEGEND

NEW BUILD	
AP-1	ALUMINUM PANEL 1
AP-2	ALUMINUM PANEL 2
BV-1	BRICK VENEER
PC	PRECAST CONCRETE
GR	GLASS RAILING
MS-1	METAL SIDING 1
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HOBIN
ARCHITECTURE

PROJECT/LOCATION:

1081 CARLING AVENUE

DRAWING TITLE:

ELEVATIONS

DRAWN BY:	DATE:	SCALE:
PB	220815	1:200

PROJECT:
2117

DRAWING NO.:
A3-02

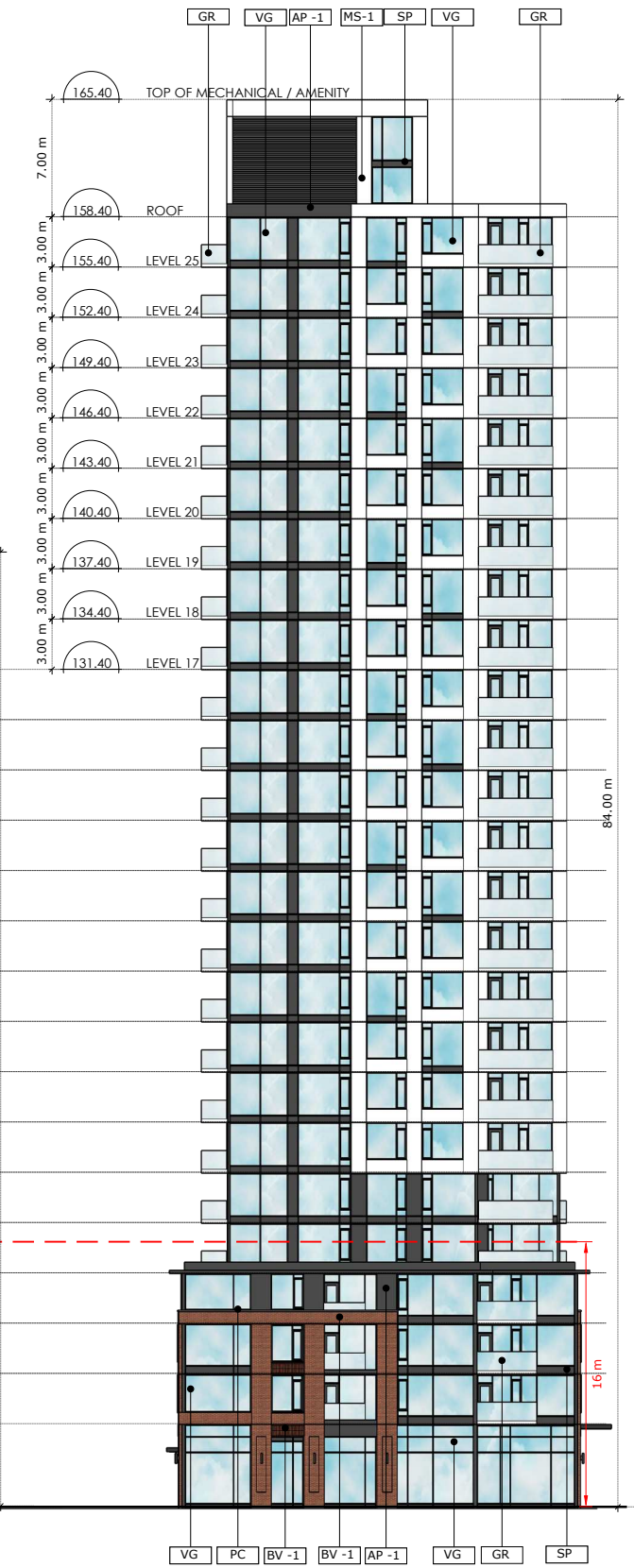
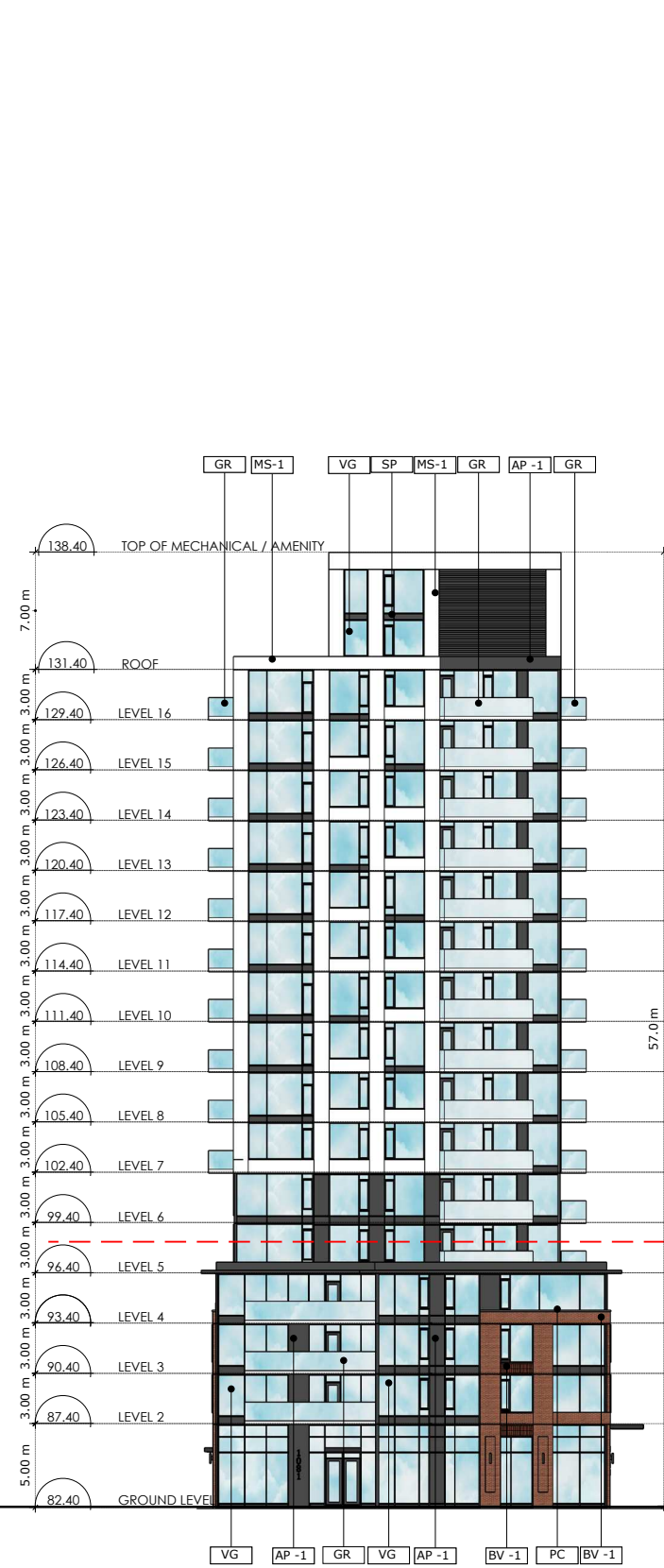
REVISION NO.:



1 NORTH ELEVATION
A3.03 Scale: 1: 200



2 SOUTH ELEVATION
A3.03 Scale: 1: 200



86.00 m

57.0 m

16 m












57.0 m

16 m

Last Save: 2022-08-17 12:28:59 PM
File Location: Z:\Projects\2100-01_1081 Carling Avenue\Drawings\Presentation\2100-01_1081 Carling Landscape_Presentation03.dwg



LEGEND:

-  PROPOSED CONCRETE PAVING
-  PROPOSED UNIT PAVING - TYPE 1
-  PROPOSED UNIT PAVING - TYPE 2
-  CONCRETE SEATING WALL
-  SHRUB PLANTING
-  TOPSOIL & SOD
-  WOOD BENCH
-  BIKE PARKING
-  PROPOSED LIGHT POST
-  PROPOSED TREE
-  EXISTING TREE TO BE RETAINED

Key Plan

2	ISSUED FOR REVIEW	04.08.2022	
1	ISSUED FOR REVIEW	19.08.2021	
No.	Revision	Date	

North: Stamp:



Landscape Architecture
Urban Design
Site Planning
Recreation and Park Planning
Project Management

319 McRae Avenue, Suite 502, Ottawa, Ontario, K1Z 0B9
Tel: (613) 729-4536

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 - check and verify all dimensions on site;
 - report all errors and/or omissions to the landscape architect;
 - comply with all pertinent codes and by-laws;
 - check and verify locations of all underground services with all local utilities prior to any digging.

Project: **1081 CARLING**

OTTAWA, ON
OTTAWA, ONTARIO

Drawing: **LANDSCAPE PLAN**

Scale:	Date:
Design By: SE	Drawn By: SE
Project Number: 2100-01	Sheet Number: L 1.0