



Certification Form for Transportation Impact Assessment (TIA) Study Program Manager

TIA Plan Reports

On April 14, 2022, the Province's Bill 109 received Royal Assent providing legislative direction to implement the More Homes for Everyone Act, 2022 aiming to increase the supply of a range of housing options to make housing more affordable. Revisions have been made to the TIA guidelines to comply with Bill 109 and streamline the process for applicants and staff.

Individuals submitting TIA reports will be responsible for all aspects of development-related transportation assessment and reporting, and undertaking such work, in accordance and compliance with the City of Ottawa's Official Plan, the Transportation Master Plan and the Transportation Impact Assessment (2017) Guidelines.

By submitting the attached TIA report (and any associated documents) and signing this document, the individual acknowledges that they meet the four criteria listed below.

Certification

- I have reviewed and have a sound understanding of the objectives, needs and requirements of the City of Ottawa's Official Plan, Transportation Master Plan and the Transportation Impact Assessment (2017) Guidelines (Update Effective July 2023);
- I have a sound knowledge of industry standard practice with respect to the preparation of transportation impact assessment reports, including multi modal level of service review;
- I have substantial experience (more than 5 years) in undertaking and delivering transportation impact studies (analysis, reporting and geometric design) with strong background knowledge in transportation planning, engineering or traffic operations; and

City of Ottawa
Transportation Engineering Services
Planning, Real Estate and Economic Development
110 Laurier Avenue West, 4th fl.
Ottawa, ON K1P 1J1
Tel. : 613-580-2424
Fax: 613-560-6006

Revision Date: June, 2023

Transportation Impact Assessment Guidelines

I am either a licensed or registered¹ professional in good standing, whose field of expertise [check appropriate field(s)]:

is either transportation engineering

or transportation planning.

Dated at this day of , 20.

(City)

Name:

Professional Title:

B. Byvelds

Signature of Individual certifier that they meet the above four criteria

Office Contact Information (Please Print)

Address:

City / Postal Code:

Telephone / Extension:

E-Mail Address:

Stamp

¹ License of registration body that oversees the profession is required to have a code of conduct and ethics guidelines that will ensure appropriate conduct and representation for transportation planning and/or transportation engineering works.

City of Ottawa 2017 TIA Guidelines TIA Screening

1. Description of Proposed Development

Municipal Address	6208 Renaud Road
Description of Location	South side of Renaud Rd between Mullin Priv & Glenlivet Ave
Land Use Classification	Residential - Low Rise
Development Size (units)	8 units
Development Size square metre (m ²)	
Number of Accesses and Locations	One existing to be relocated
Phase of Development	One
Buildout Year	TBD

If available, please attach a sketch of the development or site plan to this form.

2. Trip Generation Trigger

Considering the Development’s Land Use type and Size (as filled out in the previous section), please refer to the Trip Generation Trigger checks below.

Table notes:

1. Table 2, Table 3 & Table 4 TRANS Trip Generation Manual
2. Institute of Transportation Engineers (ITE) Trip Generation Manual 11.1 Ed.

Land Use Type	Minimum Development Size
Single-family homes	60 units
Multi-Use Family (Low-Rise) ¹	90 units
Multi-Use Family (High-Rise) ¹	150 units
Office ²	1,400 m ²
Industrial ²	7,000 m ²
Fast-food restaurant or coffee shop ²	110 m ²
Destination retail ²	1,800 m ²
Gas station or convenience market ²	90 m ²

If the proposed development size is equal to or greater than the sizes identified above, the Trip Generation Trigger is satisfied.

3. Location Triggers

	Yes	No
Does the development propose a new driveway to a boundary street that is designated as part of the Transit Priority Network, Rapid Transit network or Cross-Town Bikeways?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is the development in a Hub, a Protected Major Transit Station Area (PMTSA), or a Design Priority Area (DPA)? ²	<input type="checkbox"/>	<input checked="" type="checkbox"/>

If any of the above questions were answered with ‘Yes,’ the Location Trigger is satisfied.

4. Safety Triggers

	Yes	No
Are posted speed limits on a boundary street are 80 kilometers per hour (km/h) or greater?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Are there any horizontal/vertical curvatures on a boundary street limits sight lines at a proposed driveway?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is the proposed driveway within the area of influence of an adjacent traffic signal or roundabout (i.e. within 300 metre [m] of intersection in rural conditions, or within 150 m of intersection in urban/ suburban conditions)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is the proposed driveway within auxiliary lanes of an intersection?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Does the proposed driveway make use of an existing median break that serves an existing site?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

² Hubs are identified in Schedules B1 to B8 of the City of Ottawa Official Plan. PMTSAs are identified in Schedule C1 of the Official Plan. DPAs are identified in Schedule C7A and C7B of the Official. See Chapter 4 for a list of City of Ottawa Planning and Engineering documents that support the completion of TIA.

Transportation Impact Assessment Guidelines

	Yes	No
Is there is a documented history of traffic operations or safety concerns on the boundary streets within 500 m of the development?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Does the development include a drive-thru facility?	<input type="checkbox"/>	<input checked="" type="checkbox"/>







If any of the above questions were answered with ‘Yes,’ the Safety Trigger is satisfied.

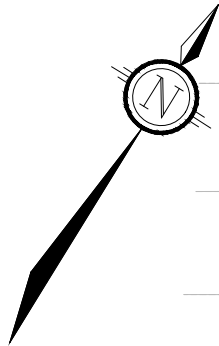
5. Summary

Results of Screening	Yes	No
Does the development satisfy the Trip Generation Trigger?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Does the development satisfy the Location Trigger?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Does the development satisfy the Safety Trigger?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

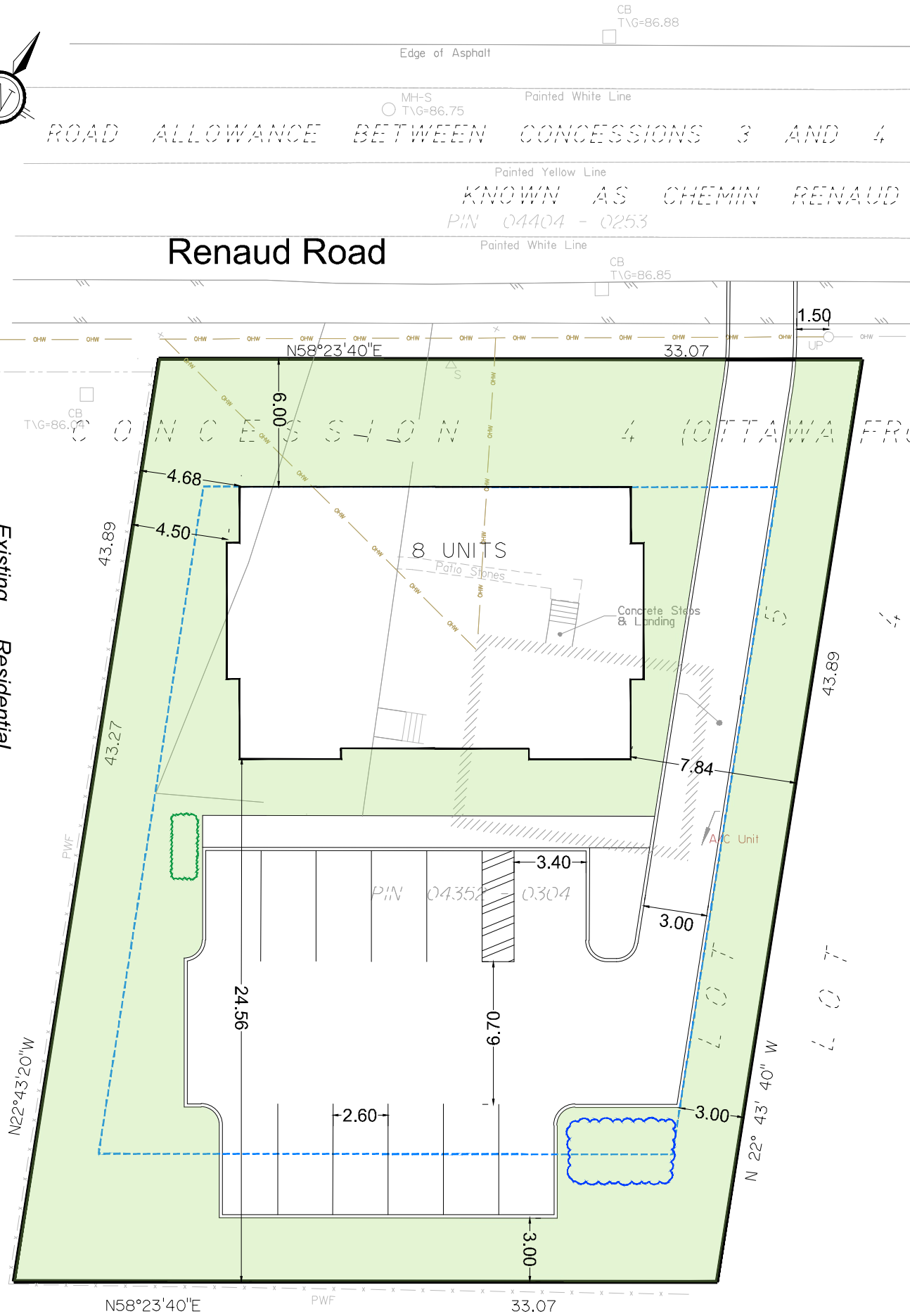
If none of the triggers are satisfied, the TIA Study is complete. If one or more of the triggers is satisfied, the TIA Study must continue into the next stage (Screening and Scoping).

LEGEND

-  Subject Property
(Rezone from DR to R4ZZ)
-  Proposed R4ZZ Setbacks
-  Landscaping and Tree Retention
to be determined at detailed design.
-  Waste Management to be determined
at detailed design.
-  Bicycle parking to be determined
at detailed design.
-  Building Footprint and layout to be
determined at detailed design.



CB
TVG=86.64



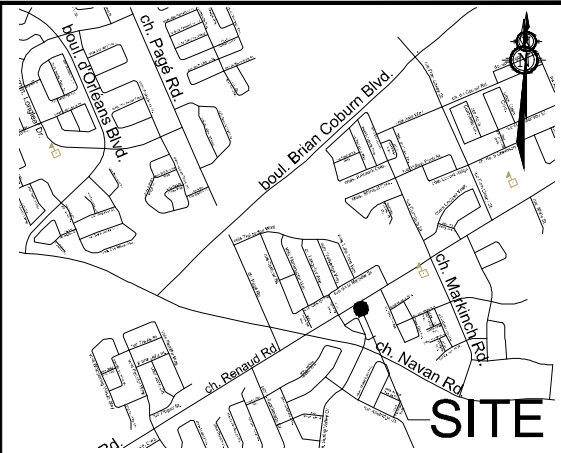
PART 2 PLAN 4R-28739
PIN 04352 - 2295
BY [Signature]

City of Ottawa Zoning By-law - Proposed R4ZZ Zone 6208 Renaud Road									
Subzone		Minimum Lot Width	Minimum Lot Area	Maximum Building Height	Minimum Front Yard Setback	Minimum Corner Side Yard Setbacks	Minimum Rear Yard Setback	Minimum Interior Yard Setback	End Notes
R4ZZ	Apartment dwelling low rise, Stacked	18 m	450 m ²	11 m	6 m	4.5 m	Varies 6, 8	Varies 8	5, 6, 8
	Provided	33.07 m	1,450 m ²	TBD	6 m	N/A	3 m+	3 m+	
5	The minimum setback between the vehicle entrance to a private garage or carport and an existing or planned sidewalk is 6.2m . No portion of a private garage or carport shall be located more than 2.5m closer to a street lot line than the closer of: (i) a building front wall or side wall, or (ii) a covered porch veranda that is at least 2.5m wide.								
6	Interior Yard Setback: For any part of a building located within 21m of a front lot line the minimum required interior yard setback shall be as follows: Where the building wall is equal to or less than 11m in height 1.5 m Where the building wall is greater than 11m in height 3m In all other circumstances the minimum required interior yard is 6m Rear Yard Setback: Where located within Area A of Schedule 342, See Part V - Section 144 for the minimum required rear yard setback. Where located outside of Area of Schedule 342, the minimum required rear yard setback is 6m. Notwithstanding the foregoing, where the rear lot line abuts the interior side lot line of an abutting lot, the minimum required rear yard setback is equal to the minimum required interior side yard setback of the abutting lot along each point of the shared lot line.								
8	Access to a lot by means of rear lane is permitted, provided the rear lane is a minimum of 8.5m wide. Where access is via the rear lane, the minimum rear yard setback may be reduced to 1.0m, and in no case may the width of the garage, carport or driveway exceed 50% of the width of the rear lot line.								

Existing Residential



CONTEXT PLAN
N.T.S.



KEY PLAN
N.T.S.

CONCEPT PLAN

ZONING BY-LAW AMENDMENT

6208 RENAUD ROAD

PART OF LOT 5,
CONCESSION 4 (OTTAWA FRONT)
Geographic Township of Gloucester
CITY OF OTTAWA

TTM (262615) Holding Inc
Marissa and Mathieu Brisebois

1 : 250



No.	REVISION	DATE	BY
2.	GENERAL REVISION	JAN 02/24	JJ
1.	PREPARED FOR DISCUSSION	NOV 16/23	JJ

NOVATECH
Engineers, Planners & Landscape Architects
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ISSUED
JANUARY, 2024
PROJECT No.
122075
DRAWING No.
122075-CP3