

451 Smyth Road  
University of Ottawa  
Advanced Medical Research Centre (AMRC)  
**Planning Rationale**

Minor Zoning By-law Amendment & Site Plan Control Applications  
April 18, 2024



uOttawa



PARKIN  
ARCHITECTS LIMITED

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**April 18, 2024**

**Prepared For:**

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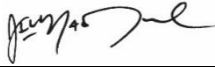
# SIGNATURES

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# 1 Introduction

WSP was retained by PCL Construction on behalf of the University of Ottawa (“uOttawa”), to prepare a Planning Rationale (the “Report”) in support of Minor Zoning By-law Amendment and Site Plan Control applications for the lands municipally known as 451 Smyth Road in the City of Ottawa (the “Site”). The requested Minor Zoning By-law Amendment and Site Plan Control applications (the “Applications”) would permit the development of a six-storey state-of-the-art medical research facility, the Advanced Medical Research Centre (AMRC), as an addition to the existing Roger Guindon Hall, which is also owned and operated by uOttawa and is part of the Ottawa Health Science Complex (OHSC).

The proposed AMRC development will showcase and elevate the uOttawa’s status as one of the top research universities in Canada. The proposed AMRC development at 451 Smyth Road will have a gross floor area of 13,726.17 m<sup>2</sup> (147,747.26 ft<sup>2</sup>), and contain the following uses:

- An Animal Care and Veterinary Services (ACVS);
- Innovation Hub and Labs; and
- A mechanical service penthouse with an atrium roof.

The site is currently being used as a parking lot, which services uOttawa and the greater OHSC. As part of the proposed AMRC development, 120 new parking spaces will be provided, as well as six (6) new service spaces for staff.

This Report is set up as follows:

- **Section 2** provides a description of the site location and community context;
- **Section 3** provides an explanation of the proposed development;
- **Section 4** outlines the policy and regulatory framework applicable to the site, and provides a planning rationale;
- **Section 5** summarizes the planning opinion;
- **Appendix A** contains the Site Plan;
- **Appendix B** contains the Landscape Plan;
- **Appendix C** contains the Building Elevations; and
- **Appendix D** contains the draft Zoning By-law Amendment text and Schedule.

A number of technical studies have been prepared in support of the application and submitted to the City; the supporting studies are available under separate cover.

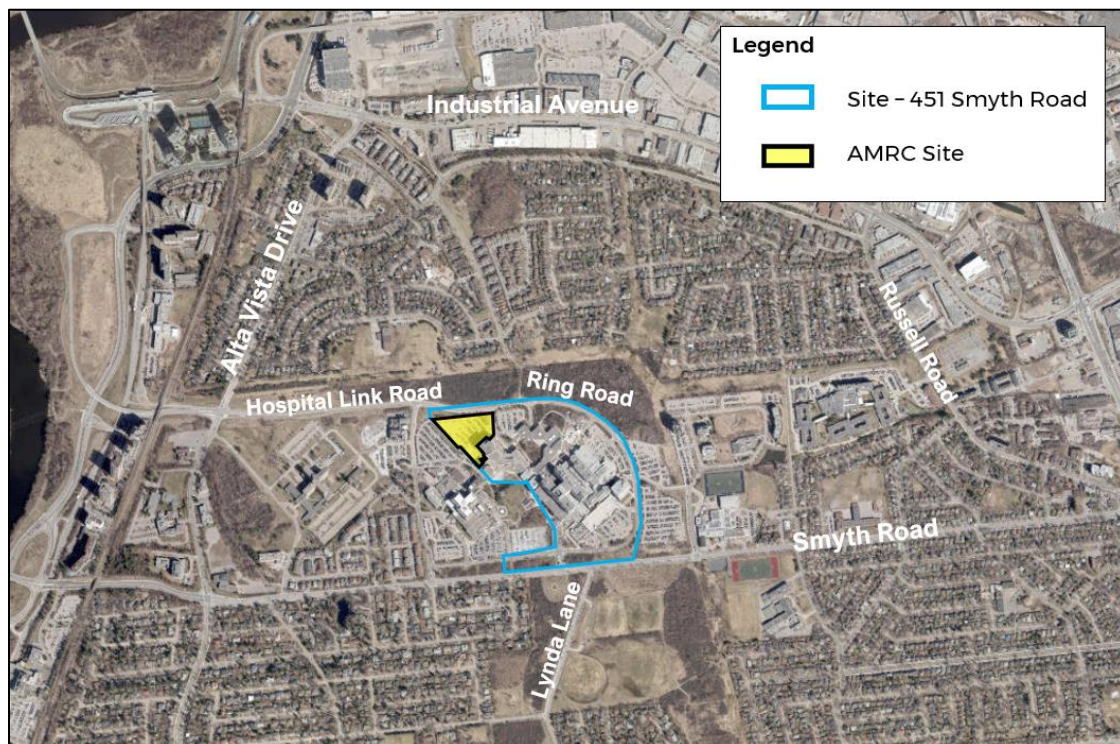
# 2 Site Location and Community Context

## 2.1 Site Location

The subject property (the “Site”) is municipally known as 451 Smyth Road in the City of Ottawa. It is legally described as ‘PIN 04258-0412, LT 29A & PT LTS 25, 26 & 27 & LTS 28, 29 & 30 & PT LTS 31 & 32, PL 405 , PT Terrace Road, PL 405 , closed by order GL40441 ; PT Hawthorne Avenue, PL 405 , (closed by order GL40441) ; PT LT 15, CON JG ; PART 1, 5R463, PARTS 1 & 2, 5R10411, PART 1, 5R4687 except PARTS 4 & 5, 5R10411, PART 3, 5R4687 & PART 3, 5R10411; T/W NS89571 ; S/T N723950; Together with an easement over PART LOT 15, CONCESSION JG, PARTS 1-23 PLAN 4R20355 as in OC2073628; City of Ottawa’ as per confirmation from uOttawa in their letter to City staff, dated November 29, 2023.

The site is part of uOttawa’s Alta Vista Campus within the Ottawa Hospital General Campus, which is located in the Alta Vista neighbourhood in Ward 18 (Alta Vista). The portion of the site being redeveloped (“AMRC Site”) is located at the northwestern limit of Ring Road and north of Smyth Road. The AMRC site is irregular in shape and has a frontage of approximately 180 metres (590.55 ft) along Ring Road, and an area of approximately 20.48 hectares (50.61 acres). **Figure 2-1** illustrates the site and AMRC site’s location and approximate boundaries. The property at 451 Smyth Road (shown in blue in **Figure 2-1**) is subject to the Minor Zoning By-law Amendment and Site Plan Control applications.

Figure 2-1: Site Location (GeoOttawa, 2024)



The AMRC site is located within the OHSC, which consists of a range of institutional uses, including the uOttawa Alta Vista Campus, The Ottawa Hospital (TOH), and the Children's Hospital of Eastern Ontario (CHEO). These institutions are independent from each other but collaborate frequently and work together to form a network of services and resources for healthcare and education in Ottawa and Eastern Ontario. Collectively, they form the OHSC.

The future AMRC building will be located adjacent to Roger Guindon Hall at 451 Smyth Road. The site is currently being used as a parking lot (Parking Lot 13) for Roger Guindon Hall and greater OHSC. As part of the development of 451 Smyth Road, the existing 361 parking spaces will be demolished.

The AMRC site is currently accessed from Ring Road, which is owned by the Ottawa Hospital. The greater OHSC is accessed from Smyth Road and Roger Guindon Avenue.

A site visit was undertaken by WSP on October 23, 2023. All photos in this Planning Rationale were taken by WSP unless otherwise specified.

## 2.2 Community Context

The site is located within the Alta Vista neighbourhood, which is primarily comprised of low to mid-rise residential, commercial, and institutional uses. There are a range of other institutional uses including other uOttawa facilities and buildings, the Riverview Alternative School, and the OMS Montessori School. The site is located south of the Ottawa Train Yards, which is a commercial area and business park located along Industrial Avenue that contains a variety of retail and grocery stores, restaurants, and services.

The surrounding community context, as well as nearby amenities and services are shown in **Figure 2-2**. Views of the site are shown in **Figure 2-3** (Site Photos – Pinpoint Map), and **Figure 2-4** through **Figure 2-11**.

Land uses adjacent to the site are as follows:

- **North:** Ring Road and a hydro corridor. Further to the north is a residential neighbourhood consisting of primarily one to two-storey detached dwellings, as well as Riverview Park and Riverview Alternative School.
- **South:** uOttawa RGN at 451 Smyth Road, Roger Neilson House at 399 Smyth Road, and the Children's Hospital of Eastern Ontario (CHEO) at 401 Smyth Road.
- **East:** A surface parking lot and the Ottawa Hospital Rehabilitation Centre.
- **West:** A surface parking lot, TransAlta OHSC Cogen Plant at 405 Smyth Road, and the Ronald McDonald House at 407 Smyth Road.



Figure 2-2: Surrounding Community Context (WSP, 2024)





Figure 2-3: Site Photos - Existing Site Conditions and Context - Pinpoint Map (WSP, 2024)



**Figure 2-4: Site looking southwest towards Roger Guindon Hall**



**Figure 2-5: Site looking northeast towards Roger Guindon Hall**



**Figure 2-6: Site looking east - existing loading areas servicing Roger Guindon Hall**



**Figure 2-7: Site looking north - existing loading areas servicing Roger Guindon Hall**



**Figure 2-8: Site looking east - Existing uOttawa parking lot**



**Figure 2-9: Site looking east from Ring Road**





**Figure 2-10: OC Transpo bus stop by Roger Guindon Hall**



**Figure 2-11: Site looking south towards Roger Guindon Hall**



## 2.2.1 Surrounding Development Activity

**Figure 2-12 identifies active and recently completed development applications in the vicinity of the site, and Table 2-1**

**Table 2-1** provides a summary of these development applications.

**Figure 2-12: Development Activity in Vicinity of the Site**



**Table 2-1: Development Activity in Vicinity of the Site**

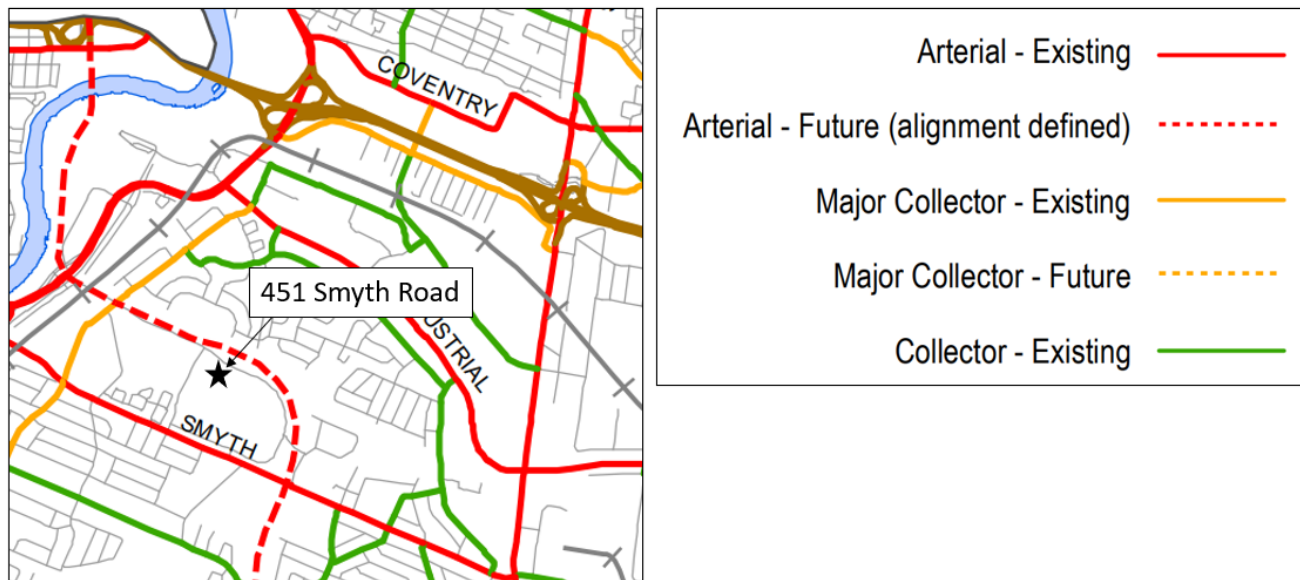
No.	Address	Land Use	Max. Height (Storeys)	No. of Units or GFA	Development Application Status
1	401 Smyth Road (IDoor4Care CHEO Expansion)	Institutional (Parking Garage)	7	32,292 m <sup>2</sup> (1,050 parking spaces)	SPC* (D07-12-22-0170) approved in October 2023. Parking lot is currently under construction.

\*Site Plan Control Application (SPC)

### 2.3 Transportation Network

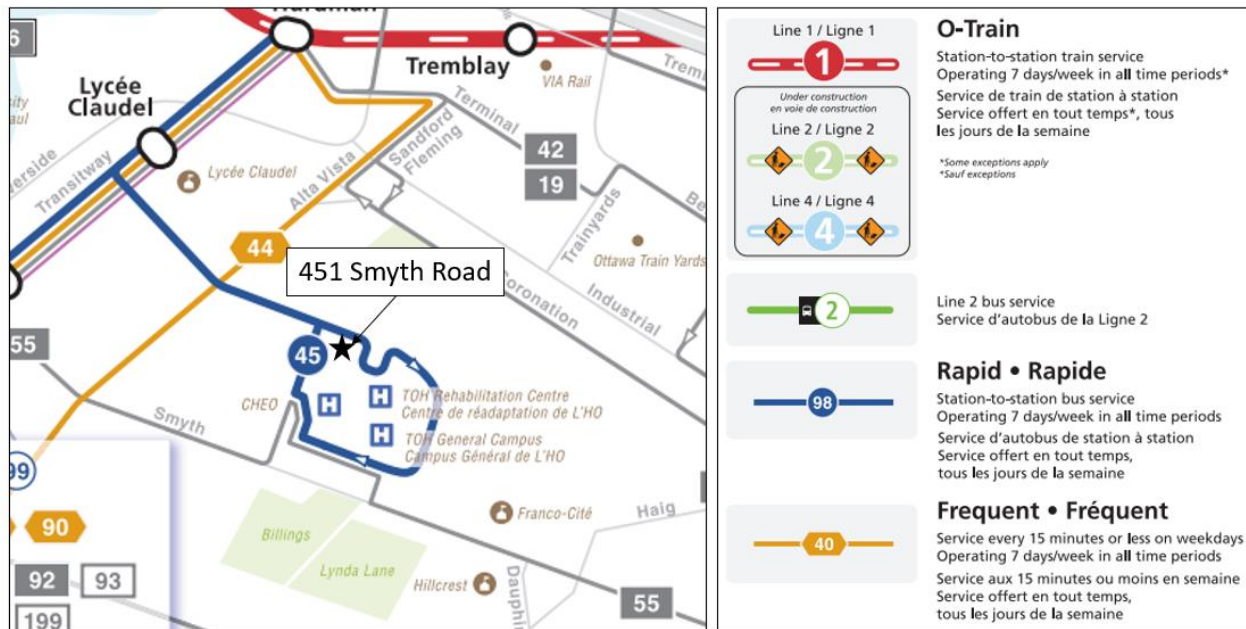
The road network surrounding the site as per Schedule C5 - Downtown Core Road Network to the City of Ottawa Official Plan (Adopted November 24, 2021 and approved by MMAH on November 4, 2022 with Modifications) is illustrated in **Figure 2-13**. Smyth Road is identified as an existing Arterial Road and provides vehicular and transit access to the greater Ottawa area. Ring Road is identified as an existing local road on Schedule C-5 (**Figure 2-13**). The planned Alta Vista Transportation Corridor (AVTC) is identified as a Future Arterial Road.

**Figure 2-13: Schedule C5 - Urban Road Network (Excerpt), (City of Ottawa Official Plan (Adopted November 24, 2021 and approved by MMAH on November 4, 2022 with Modifications))**



The site is also serviced by public transit, as shown in **Figure 2-14**. Ring Road is serviced by OC Transpo Bus Route 45: Hospital, which is a rapid bus service route that runs between Hurdman Station and Rideau B bus stop, with service every 15 minutes during peak hours. Additionally, Bus Route 55: Elmvale - Westgate runs along Smyth Road and services the greater TOH campus, with stops every 15 minutes. The site is also located in close proximity to one (1) bus rapid transit station, Lycée Claudel. The site is located more than 600 m from an O-Train rapid transit station.

**Figure 2-14: OC Transpo Network Map (August 27, 2023)**





# 3 The Proposed Development

uOttawa is proposing to develop the Advanced Medical Research Centre (AMRC), a six-storey medical research facility within the OHSC at 451 Smyth Road. The proposed building will have a gross floor area (GFA) of approximately 13,726.17 m<sup>2</sup> (147,747.26 ft<sup>2</sup>) and a proposed building envelope of approximately 5,839.45 m<sup>2</sup> (62,855.32 ft<sup>2</sup>).

The proposed AMRC building has been designed to be LEED Gold certified, and will be equipped with the building systems necessary to support intensive wet lab requirements. The 6-storey building is proposed as follows:

- First storey (i.e., ground floor) will contain Animal Care and Veterinary Services (ACVS); and
- The second to sixth storeys will contain space for the Innovation Hub and Labs, including open collaboration spaces, and mechanical penthouse.

The proposed AMRC has been designed to have holistic integration with the surrounding OHSC Campus. The new AMRC building will have several connections to the existing Roger Guindon Hall (RGN), which is located southeast of the proposed building. These include: a service connection on the ground floor, a pedestrian link on the second storey.

An Urban Design Brief, prepared by Parkin Architects (dated April 18, 2024) provides additional information on the design intent and features of the redevelopment proposal and has been included with this submission under separate cover.

## High Economic Impact Project

As confirmed by City staff at a meeting held on September 14, 2023, this project qualifies as a **High Economic Impact Project (HEIP)**. HEIPs are large development projects that provide significant benefits to Ottawa including job creation and economic growth. The proposed development was identified as a HEIP, as it will provide a significant number of construction jobs with a peak of 250 workers on site, and an average of 150 workers. The proposed development will provide thousands of permanent Full-Time Equivalent (FTE) Jobs that will create new high-quality jobs in the government, academic, and community industries, and will also generate high impact healthcare research. The anticipated economic impact of the proposed development would be directly \$522 million in salaries and wages within Ontario, and indirectly, \$2 billion annually in the creation of more than 140 companies and 6,000 employees.

## Amenity Areas

An atrium (483 m<sup>2</sup> GFA) is proposed to span the second to sixth storeys of the AMRC building, and it is intended that the atrium will serve as the collective heart of the new facility. Amenities and public spaces will be located on the second storey and will include an accessible roof above the southwest canopy.

## **Parking, Vehicular Access, Circulation, and Loading**

One (1) new vehicular access location to the AMRC facility will be provided from the Ring Road. Existing access to the OHSC from Smyth Road and Ring Road will be maintained. As a result of the proposed building construction, the existing surface parking lot containing 361 parking spaces (OHSC Parking Lot 13) would be demolished. The proposed AMRC facility will include 120 new parking spaces as part of the proposed development. Six (6) new barrier-free parking spaces are proposed, as well as six (6) new service parking spaces for staff. In sum, upon redevelopment, a total of 1,863 parking spaces will be provided on-site to serve OHSC.

A redesign of the existing loading area that is located south of the new AMRC building is proposed which would also services the RGN building. Three (3) new loading bays are proposed to be located southwest of the single-storey link between the RGN and AMRC buildings, and will be accessed from the internal Ring Road.

Bicycle parking is proposed in three (3) locations throughout the AMRC site and will include bicycle spaces by the main AMRC building entrance and below the canopy by the AMRC parking entrance, for a total of 222 new bicycle parking spaces on the AMRC site. A bicycle path is proposed to the north of the site along Ring Road. **It is noted that as part of the AMRC redevelopment, the entire site at 451 Smyth Road will be brought into compliance with the bicycle parking requirements in the City's Zoning By-law.**

The proposed Site Plan, prepared by Parkin Architects Limited (April 18, 2024) is shown in **Figure 3-1** and is also available in a larger format in **Appendix A**. Elevations of the proposed development are shown in **Figures 3-2 to 3-7** and are also included in **Appendix B**. The Landscape Plan, prepared by WSP (April 18, 2024) is shown in **Figure 3-8** and is included in **Appendix C**.

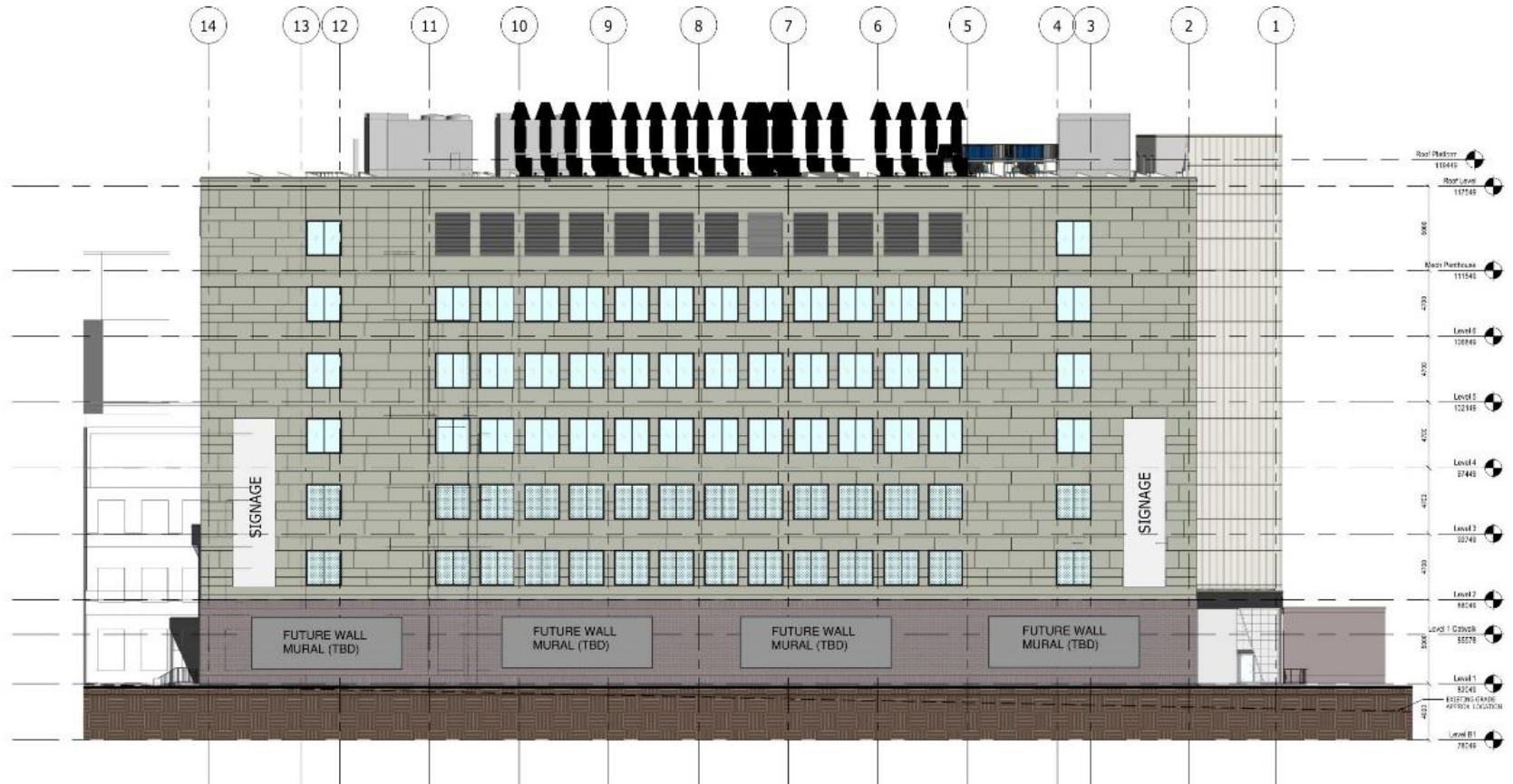
## **Servicing**

As outlined in the Servicing Report and Stormwater Management Report, both prepared by WSP (February 26, 2024), there are existing sanitary and storm sewers and watermain stubs that are available on site.

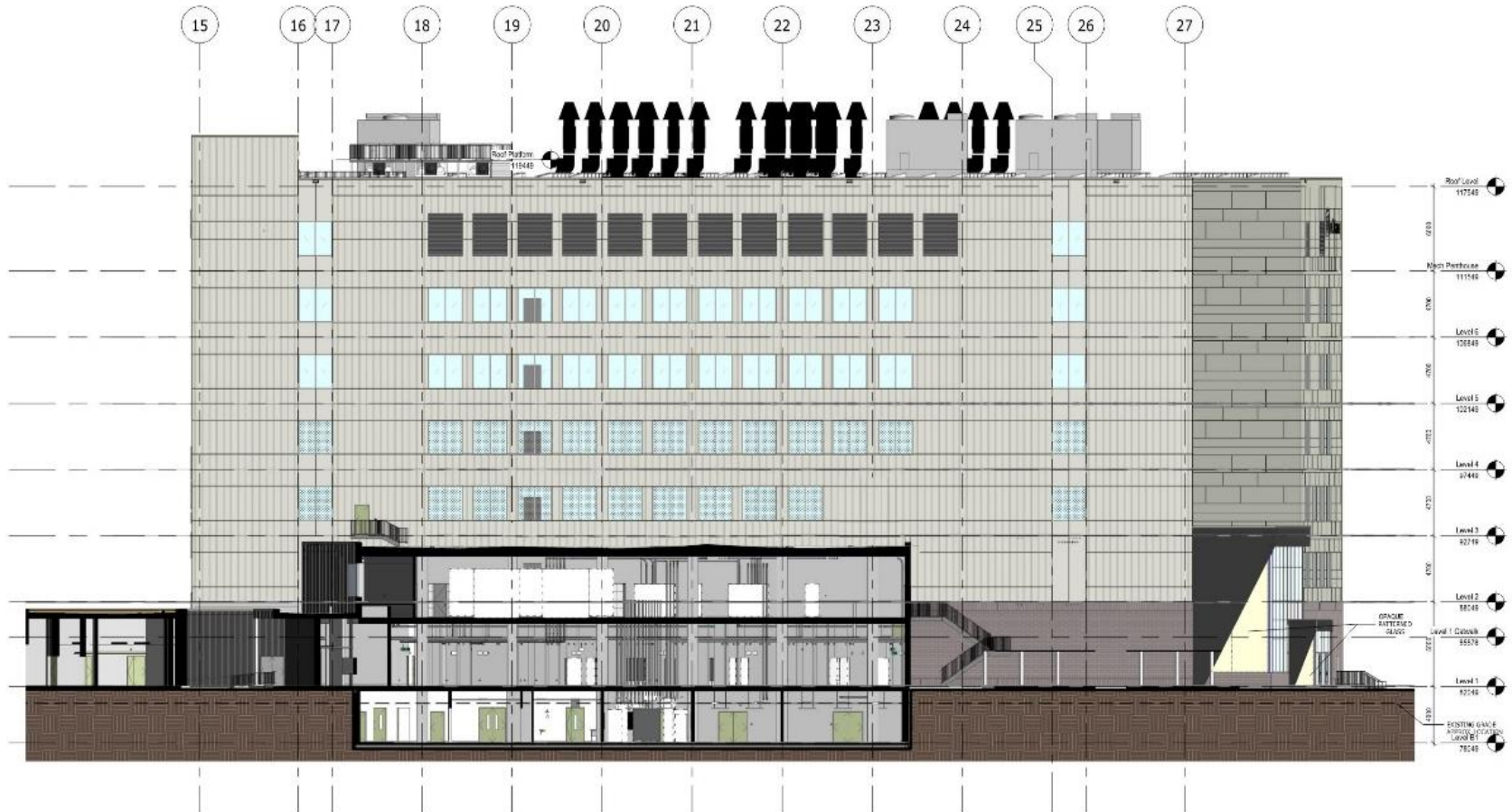




**Figure 3-2: Proposed Building Elevations (North) - 451 Smyth Road (Prepared by Parkin Architects Limited, dated April 18, 2024)**



**Figure 3-3: Proposed Building Elevations (South) - 451 Smyth Road (Prepared by Parkin Architects Limited, dated April 18, 2024)**





**Figure 3-4: Proposed Building Elevations (East) - 451 Smyth Road (Prepared by Parkin Architects Limited, dated April 18, 2024)**



**Figure 3-5: Proposed Building Elevations (West) - 451 Smyth Road (Prepared by Parkin Architects Limited, dated April 18, 2024)**

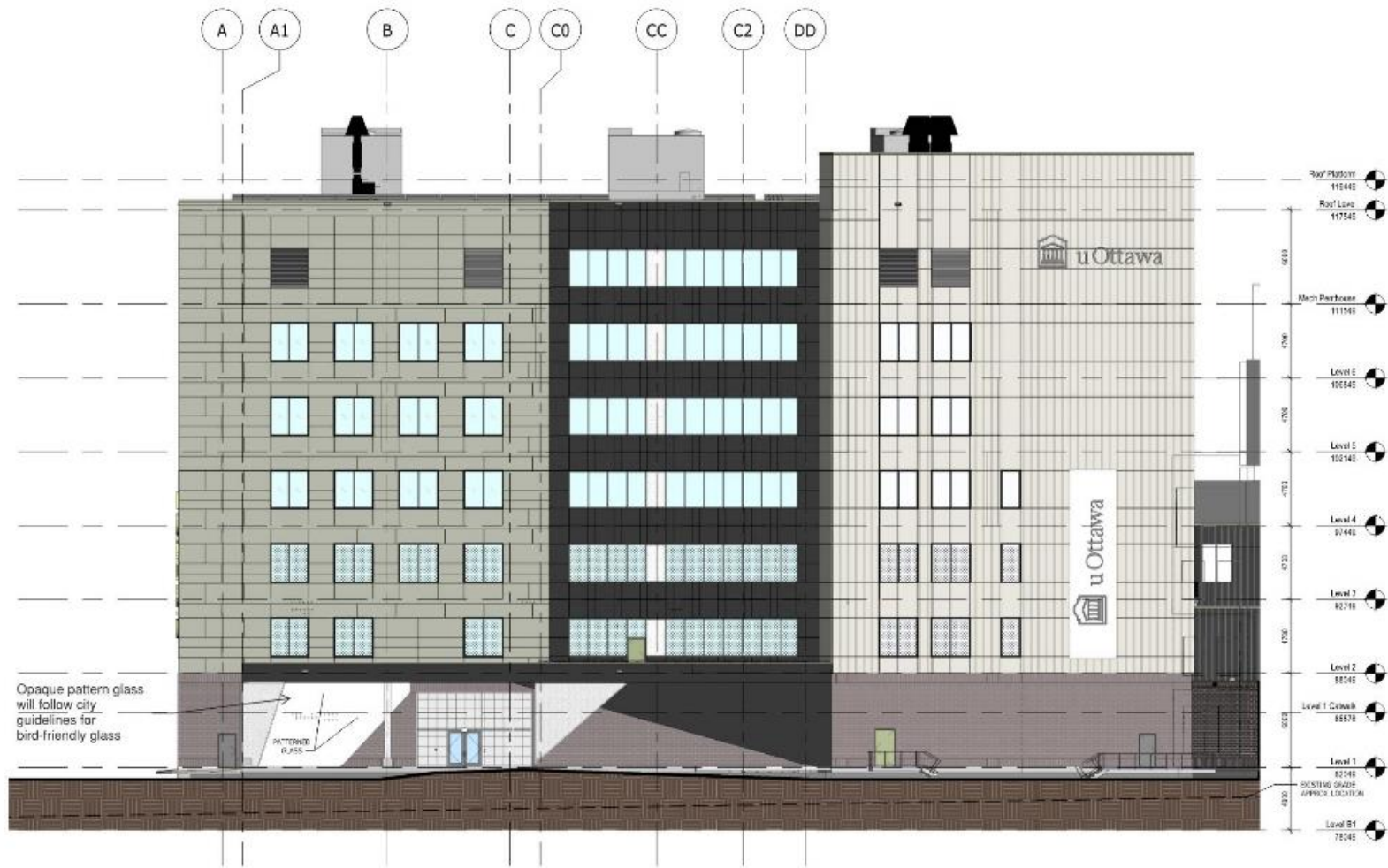
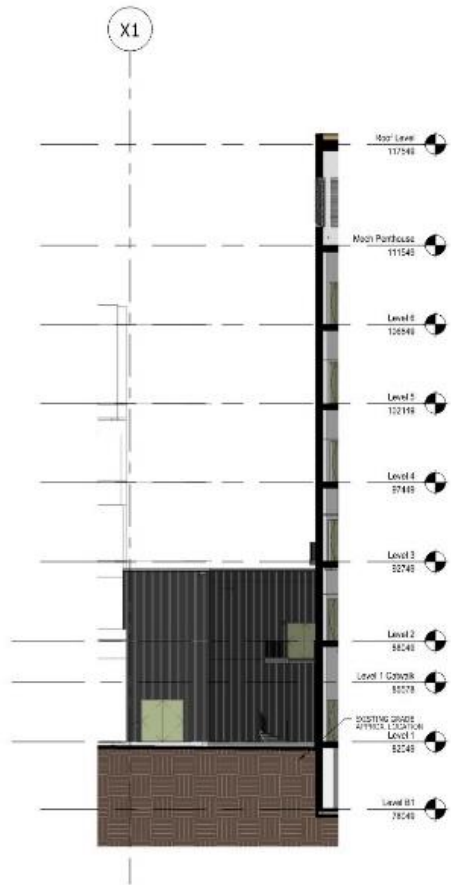


Figure 3-6: Proposed Building Elevations (Loading Docks) - 451 Smyth Road (Prepared by Parkin Architects Limited, dated April 18, 2024)



**Figure 3-7: Proposed Building Elevations (Link Elevation from Courtyard) - 451 Smyth Road (Prepared by Parkin Architects Limited, dated April 18, 2024)**







## 4 Policy and Regulatory Framework

This section describes the provincial and local policy framework that is relevant or applicable to the proposed development of the site including: the Provincial Policy Statement, 2020, City of Ottawa Official Plan (Adopted November 24, 2021 and approved by MMAH on November 4, 2022 with Modifications), and the City of Ottawa Zoning By-law 2008-250 (September 8, 2021 Consolidation).

### 4.1 Provincial Policy Statement, 2020

The Provincial Policy Statement, 2020 (PPS) was issued by the Province of Ontario under Section 3 of the Planning Act in May 2020, replacing the previous 2014 PPS. The PPS provides policy direction on matters of provincial interest including quality of the natural and built environment and public health and safety. All land use planning decisions shall be consistent with the policies of the PPS.

**Part IV: Vision for Ontario's Land Use Planning System** identifies that the Province depends on strong planning for strong, sustainable resilient communities for people of all ages, along with healthy environments and strong, competitive economies. Strong economies are linked with strong communities and clean and healthy environments, therefore long-term prosperity along with human and environmental health and social well-being should take precedence over short-term considerations.

**Section 1.1 Managing and Directing Land Use to Achieve Efficient and Resilient Development and Land Use Patterns** includes policies to sustain healthy, liveable, and safe communities.

Policy 1.1.1 (b) states that healthy, liveable and safe communities are sustained by accommodating an employment (including industrial and commercial) and institutional uses in order to meet the long-term needs in the City.

Further, Policy 1.1.1 (e) states that healthy, liveable and safe communities are sustained by promoting the integration of land use planning, growth management, transit supportive development intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments and standards to minimize land consumption and servicing costs. Policy 1.1.1 (g) states that healthy, liveable and safe communities are sustained by ensuring that necessary infrastructure and public service facilities are or will be available to meet current and project needs.

Policy 1.3.1 directs that planning authorities promote economic development by providing an appropriate range and mix of jobs to meet long-term needs.

Policy 1.1.3.2 (a) states that land use patterns within settlement areas shall be based on densities and a mix of land uses which:

- a) "efficiently use land and resources;

- b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;
- c) minimize negative impacts to air quality and climate change; [...]
- e) support active transportation; and
- f) are transit supportive, where transit is planned, exists or may be developed.”

Policy 1.1.3.4 states that appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety.

**Section 1.3 Employment** includes policies on the provision of lands for economic development. Specifically, Policy 1.3.1 directs planning authorities to “promote economic development and competitiveness by:

- a) “providing for an appropriate mix and range of employment, institutional, and broader mixed uses to meet long-term needs;
- b) providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses;
- c) facilitating the conditions for economic investment by identifying strategic sites for investment, monitoring the availability and suitability of employment sites, including market-ready sites, and seeking to address potential barriers to investment.”

**Section 1.6 Infrastructure and Public Service Facilities** includes policies on the provision of compatibility between new infrastructure and facilities with existing land uses. **Public Service Facilities is defined as including “land, buildings and structures for the provision of [...] educational programs”** amongst others.

**Section 1.6.8 Transportation and Infrastructure** speaks to the coordination between existing and planned transportation as well as adjacent land uses and infrastructure. Specifically, Policy 1.6.8.3 states that new development proposed on adjacent lands to existing or planned corridors and transportation facilities should be compatible with, and supportive of, the long-term purposes of the corridor and should be designed to avoid, mitigate or minimize negative impacts on and from the corridor and transportation facilities.

**Section 1.8 Energy Conservation, Air Quality and Climate Change** includes policies on supporting land use and development that consider energy conservation, air quality and climate change. Policy 1.8.1 directs planning authorities to “support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and preparing for the impacts of a changing climate through land use and development patterns which:

- b) promote the use of active transportation and transit in and between residential, employment (including commercial and industrial) and institutional uses and other areas [...].”

The proposed AMRC development is consistent with the PPS as it provides a necessary Public Service Facility (i.e., post-secondary educational institution) contributing to employment uses and opportunities and meets the PPS's overall vision for intensification and redevelopment, especially within proximity to public transportation.

In addition, the proposed AMRC development is consistent with the policies of Section 1.1 of the PPS that speak to creating and sustaining complete, healthy communities through the promotion of integrating land use and growth management that is transit supportive as it is located within close proximity to public transit options, as shown on **Figure 2-14**. The proposed development contributes to the overall complete community as it will be well integrated into the existing community, supporting the current and future growth for the area. The proposed AMRC building is consistent with Policy 1.3.1 as it will provide a public service facility that promotes economic development through providing a mix of employment and institutional opportunities to meet long-term needs.

Further, the proposed AMRC development is consistent with Section 1.6 of the PPS that speaks to infrastructure and public service facilities that are compatible with existing land uses. It is also consistent with Policy 1.8.1(b) through the promotion of the use of active transportation and transit, as the proposed development includes bicycle parking and will be well-connected to surrounding residential uses via public transit and the proposed bike path north of the site.

The AMRC building will support the current and future growth needs for the area by providing a new development that is well integrated into the existing community, transit supportive, and offers new employment and educational opportunities as an expansion to uOttawa's Alta Vista Campus. Therefore, the proposed AMRC development meets the general intent of the PPS.

## **4.2 City of Ottawa Official Plan (Adopted November 24, 2021 and Approved by MMAH on November 4, 2022 with Modifications)**

The City of Ottawa Official Plan (the "OP") was approved by Ottawa City Council on October 27, 2021 and adopted on November 24, 2021. It subsequently was approved by the Ontario Ministry of Municipal Affairs and Housing (MMAH) with modifications on November 4, 2022. The OP sets the vision for how the city will grow and develop to 2046. The OP introduces significant policy changes, moving from traditional land use planning with a focus on permitted land uses to context and transect-based planning with an emphasis on form and function.

### **Strategic Directions**

Section 2 of the OP, Strategic Directions, outlines the broad policies that will govern the growth and development of Ottawa over the next 25 years.

**Section 2.1 - The Big Policy Moves** outlines five broad policy directions to shape Ottawa as a liveable, mid-sized city.



1. Achieve, by the end of the planning period, more growth by intensification than by greenfield development.
2. By 2046, the majority of trips in the city will be made by sustainable transportation.
3. Improve our sophistication in urban and community design and put this knowledge to the service of good urbanism at all scales, from the largest to the very small.
4. Embed environmental, climate and health resiliency and energy into the framework of our planning policies.
5. Embed economic development into the framework of our planning policies.

**Section 2.2 – Cross Cutting Issues** identifies six (6) policy direction categories that span a range of topics related to achieving the City of Ottawa’s goal of becoming a livable City. The relevant cross-cutting issues are described as follows.

**Section 2.2.2 – Economic Development** is intended to enhance the City’s quality of life to attract skilled workforces and businesses. The direction supports the City’s knowledge-based economy and health needs by accommodating growth of post-secondary institutions and hospitals. Integrating economic activities with residential land uses is supported as part of this strategic direction to help support this goal. The City supports the development of a new Civic Hospital campus and encourages the development to existing hospitals and post-secondary educational institutions to meet the needs of a healthy growing city and economy.

**Section 2.2.3 – Energy and Climate Change** promotes the development of a compact urban form with a mix of land uses and housing options to ensure both energy efficient and sustainable patterns of development are created. The City also supports sustainable site and building design as part of development. Sustainable and resilient design measures, such as supporting the adaptive reuse and retrofit of existing buildings, are to be applied.

**Section 2.2.4 – Healthy and Inclusive Communities** is intended to support healthy and inclusive communities through the development of walkable 15-minute neighbourhoods that feature a range of housing options, supporting services and amenities. 15-minute neighbourhoods support cultural expression and community identity as well as ensuring access to goods and services within peoples’ communities using active transportation modes.

**The proposed development supports the Strategic Directions of the OP by providing a growth opportunity for a post-secondary institution in Ottawa, and new employment opportunities within the urban area. The proposed development contributes to the development of healthy, inclusive 15-minute neighbourhoods by adding employment and educational opportunities that are serviced by public transit, and within walking distance of surrounding residential neighbourhoods and amenities such as facilities and services.**

**These broad policy directions inform and generally capture the intent of the OP policies, which are described in further detail in the following sections.**

## **Growth Management Framework**

**Section 3 – Growth Management Framework** of the OP contains the City’s strategy for managing different types of growth in Ottawa, which ensures that there are sufficient development opportunities and a range of choices of where to locate growth and how to design it. The Growth Management Framework supports growth that increases sustainable mode shares and uses existing infrastructure efficiently while reducing greenhouse gas emissions.

Growth is to be concentrated within the urban area with most of the employment growth to occur in the built-up area through intensification. The relevant policy intent of the Growth Management Framework are as follows:

- To provide a transportation network that prioritizes sustainable modes over private vehicles, based on the opportunities for mode shifts presented by each transect area context. **The proposed AMRC development is part of the greater OHSC campus at 451 Smyth, which is planned in close proximity to public transit options and is connected to surrounding pedestrian and bicycle network.**
- To reduce greenhouse gas emissions in the development and building sectors and in the transportation network. **The AMRC building has been designed to pursue LEED-defined credits associated with the LEED Gold certification.**
- To establish a growth management framework that maintains a greater amount of population and employment inside the Greenbelt than outside the Greenbelt. **The site is located inside the Greenbelt.**

Section 3.5 contains policies on meeting employment needs in the City. The City’s employment is predominantly made of jobs in the public administration, health and education sectors. The proposed development will provide the area with significant employment and educational opportunities in the health services network. The proposed development would support the growth of the City by providing employment and educational opportunities for the community and student population.

**The proposed development conforms with the Growth Management Framework policies by providing employment opportunities and educational opportunities for the greater community.**

## **City-Wide Policies**

**Section 4.3 Large-scale Institutions and Facilities** of the OP contains policies which address large-scale institutions and facilities, including major health care facilities and universities. **As confirmed at the Phase 1 pre-consultation meeting held with City staff on August 23, 2023, the proposed development is considered to be a post-secondary educational institution, and is therefore is subject to these policies.** The intent of Section 4.3 is to provide policies to encourage the development of large-scale institutions and facilities be seamlessly integrated into the surrounding context and existing urban fabric, as well as ensuring they are located within 400 metres walking distance to a rapid transit station or are located along a Mainstreet Corridor with frequent street transit.

Section 4.3.1 contains policies for ensuring large-scale institutions and facilities are within 400 metres walking distance of rapid transit or along a Mainstreet Corridor with frequent street transit. Policy 4.3.1.1 states that although Large-scale institutions and facilities are not shown on the schedules of the OP, they are permitted within the urban area by amendment to the Zoning By-law within Hub, Mainstreet, Neighbourhood or Special District designations. The Large-scale facility should be within an approximate 400 metre walking distance of a rapid transit station or along a Corridor with frequent street transit. **The site is located along Ring Road, which is serviced by OC Transpo rapid bus route 45 and is also within 400 metre walking distance of Smyth Road, a Mainstreet Corridor serviced by OC Transpo bus route 55.**

Further, Policy 4.3.1.2 directs the development of facilities with a sub-regional catchment area, such as major employers, major public facilities like hospitals or stadia, or **post-secondary institutions**, to be located within a 400 metre walking distance of a rapid transit station. Policy 4.3.1.2 further states that the City will work with proponents to provide safe and convenient access by sustainable transportation modes, and where possible, negotiate agreements to provide direct connections.

Section 4.3.2 contains policies for designing large-scale institutions and facilities within the context of the existing urban fabric. Policy 4.3.2.1 directs that any new or expansion of existing large-scale institutions or facilities shall be evaluated on the following under b):

- i. "Providing public parks and privately-owned public spaces, tree planting and enhanced landscaping;
- ii. Large buildings are recognized as priority locations in support of their rooftop photovoltaic electricity potential to generate local renewable energy while reducing greenhouse gas emissions;

Further, Policy 4.3.2.1(c) states that the expansion of large-scale institutions shall be co-located that is transit supportive. Policy 4.3.2.1 (d) also states that such sites:

"[...] shall be designed in a way that makes pedestrian access the most convenient option from the surrounding neighbourhood, transit stations/stops and from existing public streets. New development shall integrate into and extend throughout its site a street grid pattern from the existing surrounding pattern of public streets and sidewalks. Sites shall be organized to create walkable blocks that support permeability through the site. Where appropriate, the street network may include private internal streets and pathways to provide more connections. All private and new public streets are required to have sidewalks on both sides, and a right of way width that accommodates cycling and trees. Any private street shall have the look and feel of a public street and be equally accessible to the public, other than in exceptional circumstances where fully demonstrated security requirements may entail access control".

In addition, Policy 4.3.2.1(g) directs that service and loading areas should be internalized for facilities over four (4) storeys.

**Section 4.6 Urban Design** includes policies on Urban Design that are to be applied within all land use designations. This Section is intended to help direct developments in the City including buildings, landscapes, pathways, parks and streets to ensure they are well-connected and build on the objective of creating 15-minute communities. The goal of the Urban Design policies contained in this section are to:

- “Protect views and enhance Scenic Routes including those associated with national symbols
- Ensure capital investments enhance the City's streets, sidewalks and other public spaces supporting a healthy lifestyle
- Encourage innovative design practices and technologies in site planning and building design
- Ensure effective site planning that supports the objectives of Corridors, Hubs, Neighbourhoods and the character of our villages and rural landscapes
- Enable the sensitive integration of new development of Low-rise, Mid-rise and High-rise buildings to ensure Ottawa meets its intensification targets while considering liveability for all.”

Policy 4.6.3 directs capital investments to enhance the City's streets, sidewalks, and other public spaces supporting a healthy lifestyle. Policy 4.6.3.1 states that developments and capital projects shall enhance the public realm, through methods including, curb extensions, curbside boulevards that widen pedestrian walkways, trees, landscaping, and street furniture. Enhancement methods like these, will improve the safety of streets and enhance the user experience through providing more pedestrian spaces, creating opportunities for relaxation and social interaction, and providing buffers from traffic.

In addition, Policy 4.6.4(3) also states that in the development of large-scale institutions and facilities, the installation of photovoltaic panels on roof structures, alternative rooftop designs that promote climate and energy resiliency including, green roofs, rooftop gardens or greenhouses are encouraged.

Policy 4.6.5(3) states that development shall minimize conflict between vehicles and pedestrians and that servicing, loading areas, mechanical equipment, and utilities shall be internal to the building in order to improve the public realm. Additionally, Policy 4.6.5(4) states that developments shall be designed to demonstrate universal accessibility, in accordance with the City's Accessibility Design Standards to ensure the built environment can address the needs of diverse users, providing a healthy, equitable and inclusive environment.

As per Section 11.1(1), the entire City of Ottawa is designated as a Site Plan Control Area to be implemented by the City's Site Plan Control By-law. Site Plan Control may be applied to all commercial, institutional, industrial, and multiple residential developments in the city. The site is therefore subject to Site Plan Control.

The Site is not within a Design Priority Area on Schedule C7-A – Design Priority Areas.



The AMRC development conforms with the City-Wide Policies for large-scale institutions and facilities as it proposes a new institutional building on an underutilized part of the OHSC (i.e., surface parking lot). The AMRC building will be sited and designed in a manner that will be integrated with the surrounding OHSC and neighbourhoods.

While the AMRC site is located internal to the greater OHSC, the site at 451 Smyth Road has frontage on a Mainstreet Corridor that is serviced by public transit. Internal pedestrian sidewalk connections will be provided throughout the AMRC site and will link to other pathways in the OHSC, and a bicycle path is located along Ring Road to the north. 150 new bicycle parking spaces are proposed throughout the site.

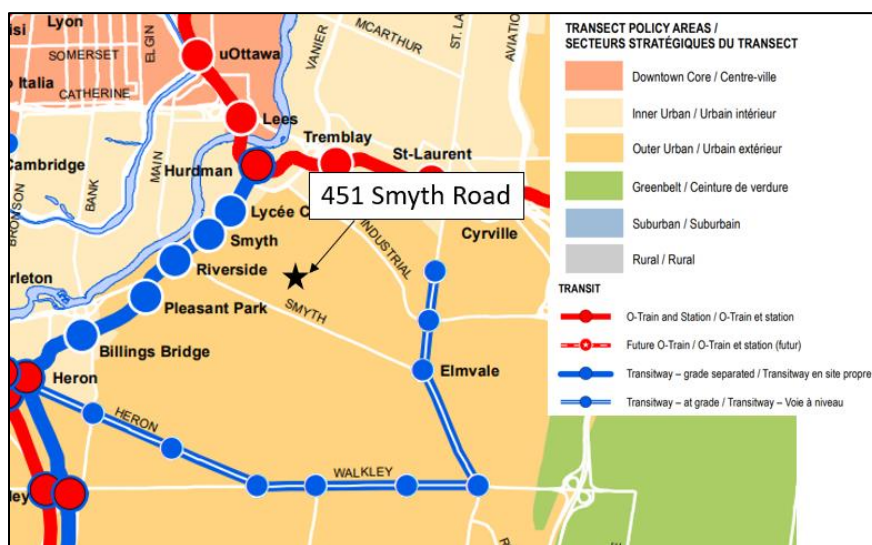
Landscaping is proposed around the perimeter of the AMRC building and surface parking areas. In accordance with Policy 4.3.2.1(g), all service and loading areas will be located internal to the site and away from the pedestrian and parking areas.

An Urban Design Brief, prepared by Parkin Architects (dated April 18, 2024) is also included with the Minor Zoning By-law Amendment and Site Plan Control applications under separate cover.

### Transect Policy Area

The site is located within the urban boundary and is part of the **Outer Urban Transect** Policy Area, as illustrated in **Figure 4-1**, which provides an excerpt from Schedule A – Transect Policy Areas. The OP states that the Outer Urban Transect represents the classic suburban model, characterized by the separation of land uses, stand-alone buildings, generous setbacks and low-rise building forms. A common planning challenge for this area is to provide more active mobility and public transit options to improve connectivity.

**Figure 4-1: Schedule A - Transect Policy Areas (Excerpt), City of Ottawa Official Plan (Adopted November 24, 2021 and approved by MMAH on November 4, 2022 with Modifications)**



The goals of the Outer Urban Transect is to recognize the context of the built form and site design, enhance mobility options and connectivity and provide direction for development in Neighbourhoods located within the Outer Urban Transect.

Policy 5.3.1.2 states that the Outer Urban Transect is generally characterized by low- to mid-density development with low-rise development to be located within Neighbourhoods (5.3.1.2(a)).

Policy 5.3.2 states that the Outer Urban Transect should enhance mobility options and street connectivity. Further, Policy 5.3.2.1 states that the transportation network for the Outer Urban Transect shall:

- “Acknowledge the existing reality of automobile-dependent built form that characterizes the Outer Urban Transect while taking opportunities as they arise to improve the convenience and level of service for walking, cycling and public transit modes;”

Policy 5.3.4.1(c) states that lands designated as Neighbourhood in the Outer Urban Transect are intended to be developed as low to mid-rise with a required maximum building height of three (3) storeys.

**The proposed development conforms to the Outer Urban Transect policies as its built form positively contributes to the existing neighbourhood. The provision of bicycle parking, and access to public transit will encourage alternative forms of transportation to and from the site.**

## **Land Use Designations**

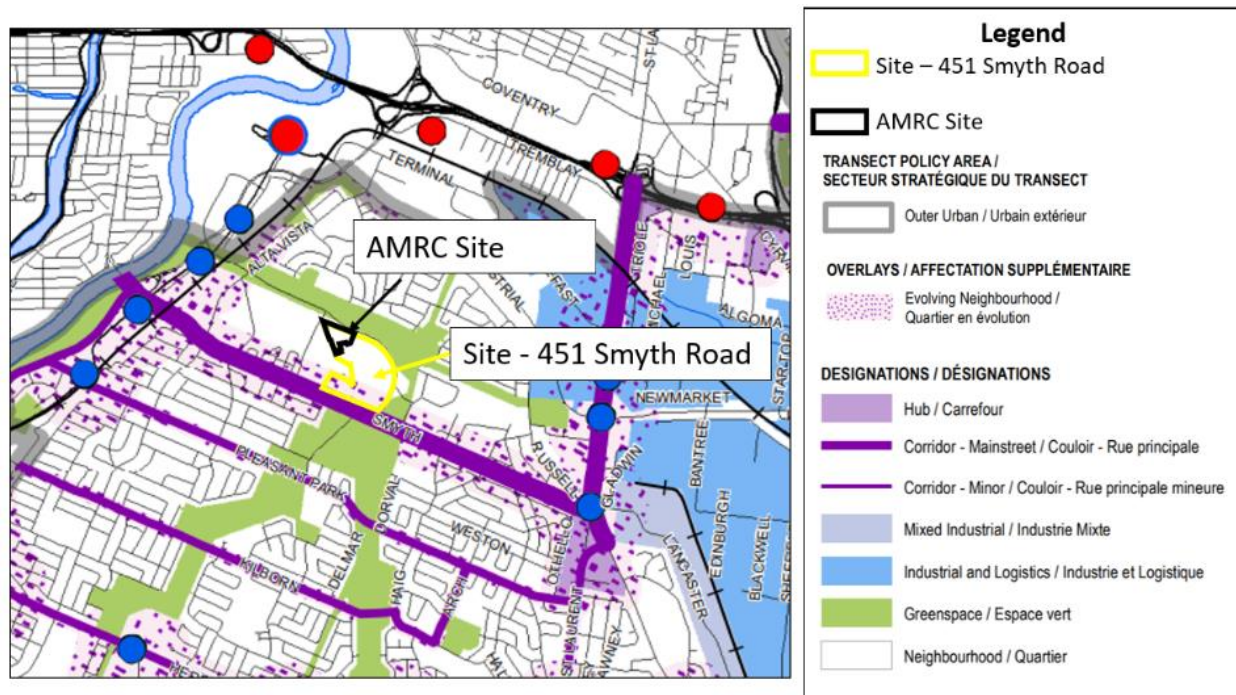
In the OP, as per Schedule B3 – Outer Urban Transect (**Figure 4-2**), the following land use designations apply:

- The entire site (i.e., 451 Smyth Road) is located along the Smyth Road **Mainstreet Corridor**;
- A portion of the entire site is part of the **Evolving Neighbourhood Overlay**; and
- The AMRC site and building area are designated **Neighbourhood**.

### **Neighbourhood**

The Neighbourhood designation permits a mix of building forms and densities, encourages the development of 15-minute communities and ensures that neighbourhoods are the cornerstone of liveability in the City. As per Policy 6.3.1.2, the OP supports development of buildings exceeding low-rise form, where existing zoning for greater building heights exists or in areas already characterized by taller buildings. **The proposed AMRC development is adjacent to existing six (6) to eight (8)-storey buildings within the OHSC.**

**Figure 4-2: Schedule B3 – Outer Urban Transect (Excerpt) (City of Ottawa New Official Plan (Adopted November 24, 2021 and approved by MMAH on November 4, 2022 with Modifications))**



The Neighbourhood designation permits a range of residential and non-residential built forms, including limited large-scale non-residential uses including large-scale institutions and facilities as per Policy 6.3.1(4)(e). As confirmed in the Phase 1 pre-consultation meeting with City staff held on August 23, 2023, the proposed development would be considered a post-secondary educational institution.

Further, Policy 6.3.3(1) supports large-scale non-residential land use provided it conforms with specific criteria, including the following:

- The use is suitable to be located in the Neighbourhood designation and does not otherwise belong within a Hub or Corridor designation or within the Mixed Industrial designation [...];
- The use can be well-integrated within the Neighbourhood and contributes to the goals of the designation;
- The use is located on a rapid transit network or along a street where frequent transit service is already provided; and
- A site design is provided in accordance with transect and overlay policies.

**The proposed AMRC development conforms to the Neighbourhood policies in the OP. The building height of the new AMRC building is in keeping with the existing 6-8 storey building heights within the TOH campus.**

### Mainstreet Corridors

The entire site at 451 Smyth Road is also located along the Smyth Road Mainstreet Corridor in Schedule B1 (**Figure 4-2**). Corridors will generally permit residential uses and **non-residential uses** that integrate with a dense, mixed-use urban environment (Policy 6.2.1.3). Policy 6.2.1.1(a)(i), states that the Corridor designation applies to any lot abutting the Corridor, subject to, “Generally, a maximum depth of [...] In the case of Mainstreet Corridors, a maximum depth of 220 metres from the centreline of the street identified as a Mainstreet Corridor”. Further, Policy 6.2.1.1(a)(iii) states that, “Where part of a lot lies beyond the maximum depths specified in Policies i) and ii), that part of the lot is excluded from the Corridor designation”.

As shown in **Figure 4-3**, the AMRC site and building area is located approximately 334.9 metres from Smyth Road. As such, given that the depth of the proposed AMRC building is 334.9 metres from Smyth Road, it is our professional opinion that this part of the lot is excluded from the Mainstreet Corridor designation.

**Figure 4-3: Distance of AMRC Site to Smyth Road to demonstrate applicability of Policy 6.2.1.1(a)**



### Evolving Neighbourhood Overlay

As shown in Schedule B1 (**Figure 4-2**), the southernmost portion of the AMRC site is part of the Evolving Neighbourhood Overlay. Section 5.6.1.1, Policy 1) states that the Evolving Neighbourhood Overlay will generally apply to lands 150 m from the boundary of a Mainstreet designation.



The proposed AMRC building conforms to the Neighbourhood designation policies that permit range of residential and non-residential uses and encourage the development of 15-minute communities. The proposed building height of six (6) storeys is in keeping with surrounding building heights in the OHSC, which range from six (6) to eight (8) storeys. The proposed development conforms to the policies of the OP.

The proposed development conforms to the policies of the Official Plan policies including Strategic Directions, Growth Management, Transect Areas, Compatibility, and Land Designation policies.

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#### 4.2.1 Road Classification and Rights-of-Way

As per Section 4.1.7 of the OP, the City may acquire land for rights-of-way or the widening of rights-of-way through conditions of approval for a plan of subdivision, severance (severed and retained parcels), site plan or a plan of condominium.

Table 1 - Road Right-of-Way Protection of the OP sets forth the right-of-way (ROW) widths that the City may acquire for roads. Smyth Road is classified as an arterial road as per the OP, and has an existing right-of-way protection of 30 metres.

#### 4.3 Integrated Environmental Review

This section provides an overview of the technical studies that were completed in support of the Minor Zoning By-law Amendment and Site Plan Control applications for the development of the site. This section also provides a summary of the potential environmental concerns raised and the impact on design, where applicable, and a high-level assessment of relevant policies from the City of Ottawa Official Plan (Adopted November 24, 2021 and Approved by MMAH on November 4, 2022 with Modifications).

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##### 4.3.1 Phase One Environmental Site Assessment (October 12, 2023)

Paterson Group Inc. was commissioned by PCL Construction to conduct a Phase One Environmental Site Assessment ("Phase I ESA") for part of 451 Smyth Road, in the City of Ottawa, Ontario. The objective of this Phase I ESA was to research the past and current use of the development site (the "Site"), as well as neighbouring properties within a 250 m study area (the "Study Area") and to identify any environmental concerns with potential site impacts.

The Phase I ESA reported that the site was historically used for agricultural purposes until it was first developed with an asphalt parking lot in the early 1980's. Since then, the site has remained as a parking lot. No potentially contaminating activities were identified with respect to the historical use of the site.

The surrounding properties within the Study Area were similarly used for agricultural purposes until the development of the University of Ottawa Faculty of Medicine campus, the Ottawa Hospital campus, and the Children's Hospital of Eastern Ontario campus in the 1980's. A natural gas-fuelled power generation facility was identified approximately 75 metres to the

west at 405 Smyth Road. Given its separation distance and hydraulically down-gradient orientation with respect to the anticipated groundwater flow direction to the west, the power generation facility to the west is not considered to pose an environmental concern to the Study Area.

No potentially contaminating activities were identified with respect to the current use of the Study Area. The surrounding lands around the site and within the Study Area remain occupied by buildings and structures associated with the Ottawa Hospital.

Policy 10.1.6 of the Official Plan states that developments shall only occur on lands where environmental conditions are suitable for the proposed use and are in accordance with provincial legislation and regulations. Further, Policy 10.1.6.2 states that phase one environmental site assessment reports must include “the City of Ottawa Historical Land Use Inventory reports and all other required environmental information sources that are reasonably and publicly available through public regulatory bodies and industry standard private environmental databases”. **The Site and the Study Area were investigated for any environmental impacts associated with the proposed development, and identified no potentially contaminating activities within the Site and Study Area that would impact the proposed development.**

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#### 4.3.2 Geotechnical Investigation (November 1, 2023)

Paterson Group Inc. was commissioned by PCL Construction to conduct a geotechnical investigation in support of the proposed AMRC at 451 Smyth Road.

The objectives of the Geotechnical Investigation were to:

- Determine the subsurface and groundwater conditions by means of boreholes and existing soils information.
- Provide geotechnical recommendations pertaining to design of the proposed development including construction considerations which may affect the design.

The key findings of the Geotechnical Investigation are summarized:

- A material testing and observation services program is required to be completed.
- Review preliminary and detailed grading, servicing, and structural plan(s) from a geotechnical perspective.
- Review of the geotechnical aspects of the excavation contractor’s shoring design, prior to construction, if applicable.
- Review of architectural plans pertaining to foundation and underfloor drainage systems and waterproofing details for elevator shafts.
- Review the bedrock stabilization and excavation requirements at the time of construction.

- Review and inspection of the installation of the foundation and underfloor drainage systems and elevator waterproofing.
- Observation of all bearing surfaces prior to the placement of concrete.
- Sampling and testing of the concrete and fill materials.
- Periodic observation of the condition of unsupported excavation side slopes in excess of 3 metres in height, if applicable.
- Observation of all subgrades prior to backfilling.
- Field density tests to determine the level of compaction achieved.
- Sampling and testing of the bituminous concrete including mix design reviews.

It is recommended that a report confirming that the construction has been conducted in general accordance with its recommendations could be issued upon the completion of a satisfactory materials testing and observation program by the Geotechnical Consultant. Additionally, all excess soil must be handled as per Ontario Regulation 406/19: On-Site and Excess Soil Management.

**Policy 4.9.1 states that in the quality and quantity of surface water features and groundwater features should be protected, improved and restored in new developments. The site was investigated to determine the subsurface and groundwater conditions to ensure the proposed development would be suitable for the site.**

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### 4.3.3 Site Servicing Report (April 18, 2024)

WSP was retained by PCL Construction to provide servicing, grading and stormwater management design services in support of the proposed AMRC development. The sanitary, storm and water services will be provided by the private utilities located along Ring Road, which abuts the site.

#### **Water Distribution**

The water services for the proposed development are proposed to connect to the existing 300mm watermain on Smyth Road and the existing 152mm diameter private watermain along the access road west of the new AMRC building. Two (2) new 150mm service mains are proposed to service the AMRC building from the 152mm watermain on the private access road and from the 305mm watermain on Ring Road. There are no off-site improvements to watermains, feeder mains, pumping stations, or other water infrastructure required to maintain existing conditions and services to adjacent developments.

#### **Water Supply for Firefighting**

The proposed development will be protected with a supervised automatic fire protection sprinkler system additionally, a two-hour fire wall is proposed between the existing RGN building and the new AMRC building. The proposed development will be adequately serviced

by fire flow protection through one (1) existing municipal fire hydrant and one proposed relocated fire hydrant.

### **Wastewater Disposal**

A review of the wastewater disposal options for the proposed development was undertaken in accordance with the City of Ottawa's Sewer Design Guidelines. A review of the soil conditions evaluated in the Geotechnical Investigation, prepared by Paterson Group Inc. (November 1, 2023), was undertaken, including the recommendations to provide bedding and backfill, utilization of conventional sewer materials, and dewatering.

The proposed sanitary sewer network for the site will consist of a 200mm sanitary service and three 1200mm maintenance holes. The proposed development will have no impact on existing pumping stations and will not require new pumping facilities. The review determined there are no environmental constraints that will impact the sanitary servicing design's ability to preserve the physical condition of watercourses, vegetation, or soil cover, or to manage water quantity or quality. Additionally, no sanitary forcemains or sanitary pumping stations are proposed on the AMRC development site.

### **Site Storm Servicing**

The Ottawa Health Sciences Centre (OHSC) campus has a dual drainage system that currently does not meet City of Ottawa criteria. As such, the proposed AMRC development will be consistent with the recommendations of the Ottawa Health Sciences Centre Stormwater Master Plan, reducing the run-off from the site to the existing storm sewer.

The stormwater system was designed following the principles of dual drainage, with accommodation for both major and minor flow. The detailed design for this Site will maintain the existing 600mm storm sewer connection to the 900mm storm sewer located on Ring Road. The drainage system consists of a series of manholes, catchbasins and storm sewers leading to the outlet manhole at the north-west of the site. All drainage areas on the site are collected in the site piped drainage system. The AMRC building will be provided with piped storm services for roof drainage, connected to the storm sewer that flows into the sewer in an uncontrolled capacity, ensuring an unobstructed flow for these areas.

### **Sediment and Erosion Control**

Various construction techniques designed to reduce unnecessary construction sediment loadings will be used in the development of the AMRC building, including;

- Silt sacks will remain on open surface structures such as manholes and catchbasins until these structures are commissioned and put into use;
- Installation of silt fence, where applicable, around the perimeter of the proposed work area.
- The installation of straw bales within existing drainage features surround the site; and
- Bulkhead barriers will be installed in the outlet pipes.



During construction of services, any trench dewatering using pumps will be fitted with a “filter sock.” Thus, any pumped groundwater will be filtered prior to release to the existing surface runoff. The contractor will inspect and maintain the filter sock as needed including sediment removal and disposal.

All catchbasins, and to a lesser degree, manholes, convey surface water to sewers. Consequently, until the surrounding surface has been completed, these structures will be covered to prevent sediment from entering the minor storm sewer system. These measures will stay in place and be maintained during construction and build-out until it is appropriate to remove them.

During construction of any development both imported and native soils are placed in stockpiles. Mitigative measures and proper management to prevent these materials entering the sewer system are needed. During construction of the deeper watermains and sewers, imported granular bedding materials are temporarily stockpiled on site.

**Policy 4.7.1 states that adequate, cost-effective drinking water, wastewater and stormwater infrastructure shall be provided in developments that can help meet the City’s growth targets in urban areas. The proposed development provides various techniques and measures to ensure that storm and water services will be provided in a manner that will not disrupt the existing servicing infrastructure and will be able to support the growth that will occur due to the proposed development.**

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#### **4.3.4 Stormwater Management Report (April 18, 2024)**

WSP was retained by PCL Construction to prepare a Stormwater Management (SWM) Report in support of the proposed development at 451 Smyth Road. The SWM Report examines potential water quality and quantity impacts of the proposed AMRC development.

The water quality was assessed and recommendations were provided to enable the proposed development, determined a treatment unit will be included near the on the underground cistern to meet MOE Enhanced treatment standards (80% TSS removal).

The water quantity to enable to proposed development was also assessed, determining that runoff will be controlled by ponding surface storage on the parking lot, in an underground chamber and rooftop storage on the building. Flow from the parking area will be controlled with an ICD, and roof drainage will be controlled with adjustable roof drains.

**Policy 4.7.1.4(b) states that in order to support a range of intensification developments through policies and programs, and mechanisms for on-site stormwater management to efficiently and adequately support developments and intensification in existing neighbourhoods. The proposed development conforms to Policy 4.7.1.4(b) as there will be significant mechanisms incorporated in the design of the proposed development that will support intensification including a water quality treatment unit, surface ponding storage, underground storage and roof drainage systems.**

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### 4.3.5 Transportation Impact Assessment (TIA) Strategy Report (April 18, 2024)

A Transportation Impact Assessment (TIA) Report was prepared by WSP in support of the Minor Zoning Bylaw Amendment and Site Plan Control applications for the proposed AMRC development. The TIA Report examined existing conditions along the study area roadways, intersections, driveways, pedestrian and cycling facilities, transit facilities, as well as existing study area traffic management measures, peak hour travel demands and collision history.

The site was also assessed based on future changes to the study area's transportation network, which identified two potential future roadway upgrades in the vicinity of the proposed development site within the study horizon years. The potential future roadway upgrades include:

- The Alta Vista Transportation Corridor (AVTC): a proposed arterial roadway that is proposed to connect from Highway 417 at the Lees Avenue / Nicholas Street interchange south to Walkley Road opposite Conroy Road; and
- A Ten-Year Road Rehabilitation Plan for the Ottawa Hospital General Campus was prepared by Morrison Hershfield in 2022, which outlines a rehabilitation plan where improvements are required. Improvements in the vicinity of the AMRC includes roadway and sidewalk improvements along the Ottawa Rehabilitation Centre access road; sidewalk improvements have been designated as Priority A (2023-2026) and roadway improvements have been designated as Priority B (2023-2032).

The TIA Report determined the peak hours for traffic were weekday AM being 7:30 a.m. to 8:30 a.m., and weekday PM being 3:30 p.m. to 4:30 p.m.

Based on the ITE trip generation rates for vehicle trips and Ottawa conversion factor to person trips, the proposed AMRC development is projected to generate 275 and 262 vehicle trips during the weekday AM and PM peak hours, respectively. Additionally, the assessment determined the projected person trips generated by the proposed AMRC development would be 352 and 335 person trips during the weekday AM and PM peak hours, respectively.

The TIA Report determined based on the results of the traffic operations analysis, the transportation network surrounding the proposed AMRC development for uOttawa located at 451 Smyth Road can accommodate the development without adverse impacts to future traffic operations during the 2026 built-out year and 2031 planning horizon.

**Policy 4.1.2 includes policies on the promotion of 15-minutes neighbourhoods. Further, Policy 4.1.2.6 states that new developments should provide connections to existing or planning network of public sidewalks, pathways or bicycle facilities.**

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### 4.3.6 Noise Impact Assessment (April 18, 2024)

WSP was retained by uOttawa to prepare a Noise Impact Assessment in support of the Minor Zoning By-law Amendment and Site Plan Control applications for the proposed AMRC development. The Study addresses the impacts of stationary sources associated with the

proposed development, as well as the transportation sources associated with the nearby future arterial road and current transit priority corridor.

The Noise Impact Assessment assessed each noise source type at the points of reception and their compliance with NPC-300 requirements. The assessment determined the following recommendations:

- The development will require central air conditioning system as an alternate means of open window and the development includes central air condition system.
- The preliminary acoustical performance requirements for exterior façade elements (i.e. exterior walls, and windows) for the development, including:
  - Exterior wall: Exterior wall providing an STC-45 or better. The selected wall assembly achieves STC-51 or better.
  - Exterior window: Exterior window glazing into sensitive spaces providing a STC 32 or better. A fully sealed 12mm single glazed window or double glazed window consisting of 4mm pane separated by 16mm air space can achieve STC-32 or better.
- Noise control for stationary sources include:
  - Generator will be an outdoor packaged units with specification of 75 dBA at 7 meters. No additional control will be required.
  - All vents to mechanical rooms include 6" acoustic louvers.

**Policy 10.2.1.1 states that all developments “shall comply with the City’s Environmental Noise Control Guidelines which are based on the applicable Provincial noise guidelines [currently NPC – 300, MOEE 2013].” The proposed development conforms to this Policy as the Noise Impact Assessment was assessed based on the NPC-300 requirements, and includes the assessment of each noise source type.**

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#### **4.3.7 Tree Conservation Report (TCR) (April 18, 2024)**

WSP was retained by PCL Construction to prepare a Tree Conservation Report (TCR) in support of the Minor Zoning By-law Amendment and Site Plan Control applications for proposed AMRC development.

The TCR evaluated the associated impacts that the proposed development would have on existing trees located within the AMRC development site and on adjacent lands where construction may disturb the critical root zone of existing trees per the requirements of by-law no. 2020-340. As a result of construction of the proposed development, seventy-four (74) trees are proposed to be removed and eighteen (18) trees on adjacent lands will be preserved. Prior to construction, tree protection fencing as outlined in the City of Ottawa’s Tree Protection Specification (rev. March 2021), will be installed around the critical root zone of trees on adjacent lands and remain in place until construction works are complete.

Policy 4.8.2.3(a) states that in the development of new infrastructure on public or private lands, the preservation and consideration for mature and healthy trees, and the provision of adequate soil should be respected as recommended by a Landscape Architect. Further, Policy 4.8.2.3(d) states that the preservation of existing trees takes priority over replacement and compensation planting when considering the impacts of a development. The proposed development conforms with this Policy as the development will ensure that seventeen (18) trees are preserved on adjacent lands and will take the necessary precautions to protect these trees in the construction process. Additionally, an extensive Tree Conservation Report and Landscape Plan was prepared by WSP's Landscape Architects that provides details and notes on the preservation of existing trees and the provision of soil in the construction of the proposed development.

#### 4.4 Parkland Requirements

As confirmed by City Parks and Facilities Planning staff in the Revised Feedback Form received on October 26, 2023, cash-in-lieu of parkland / parkland dedication will be required in accordance with the City Parkland Dedication By-law No. 2022-280.

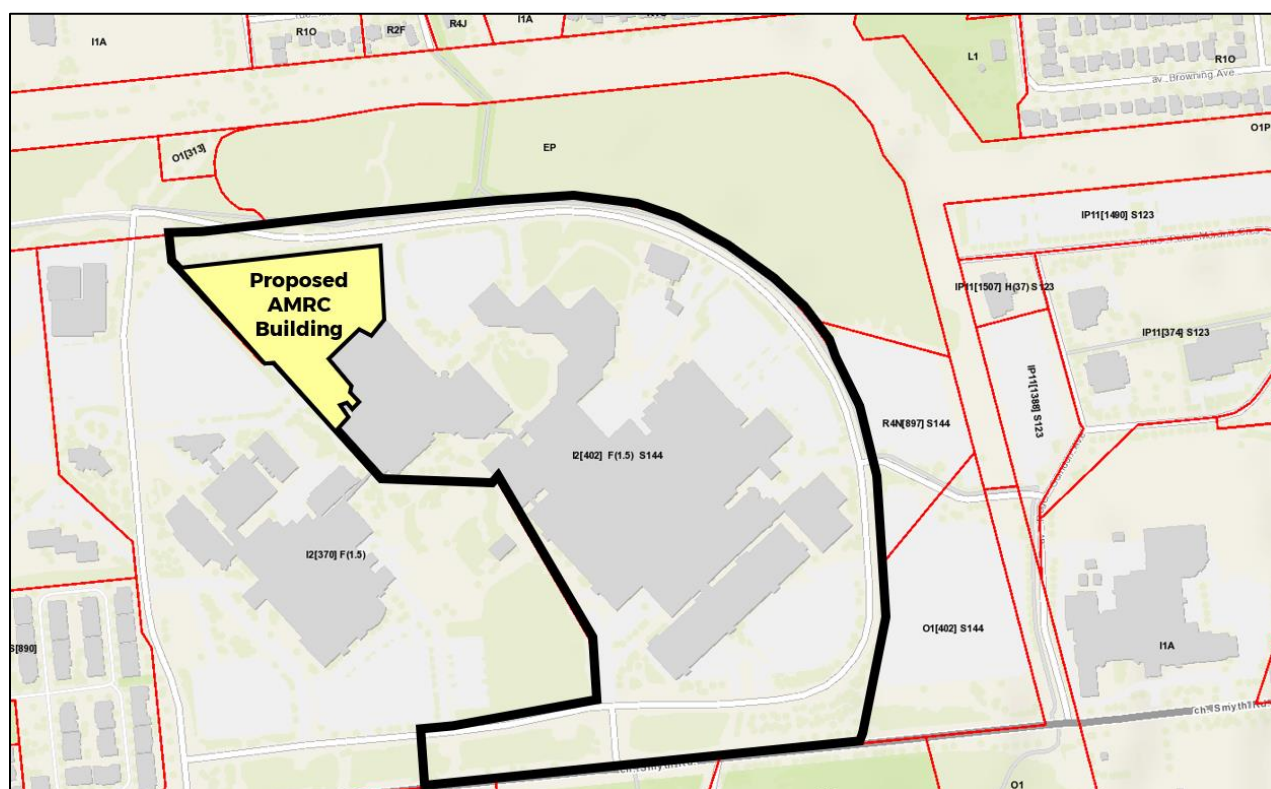
#### 4.5 City of Ottawa Comprehensive Zoning By-law 2008-250 (Consolidation September 8, 2021)

Under the City of Ottawa Comprehensive Zoning By-law 2008-250 (Website Consolidation September 8, 2021), the site is zoned **Major Institutional Zone, Urban Exception 402, Floor Space Index 1.5, Schedule 144 (I2[402] F(1.5) S144)**, as shown in **Figure 4-4**.

Properties immediately adjacent to the site are zoned Environmental Protection Zone (EP), Parks and Open Space Zone (OP), Parks and Open Space Zone, Urban Exception 313 (O1[313]) to the north, Residential Fourth Density Zone, Urban Exception 897, Schedule 144 (R4N[897] S144) and Parks and Open Space Zone, Urban Exception 402, Schedule 144 (O1[402] S144) to the east. Properties to the south are zoned Parks and Open Space Zone (O1), Parks and Open Space Zone, Subzone C (O1C) and Residential First Density Zone, subzone GG (R1GG) and Major Institutional Zone, Urban Exception 370, Floor Space Index 1.5, (I2[370] F(1.5)) to the west.



Figure 4-4: Site Zoning (geoOttawa, 2024)



#### 4.5.1 Zoning Provisions

The general purpose of the I2 Zone is to:

- ensure that major institutional uses such as hospitals, colleges and universities are located at appropriate locations within areas designated as General Urban Area, Central Area and Mixed Use Centre in the Official Plan;
- ensure that these large scale, high traffic generating institutions locate only on large parcels of land, with direct access to an arterial road and near rapid transit stations;
- impose regulations which ensure that the size and intensity of these uses is compatible with adjacent uses; and
- permit minor institutional uses and provide for a range of ancillary service uses.

The I2 Zone permits a number of non-residential uses, including a post secondary educational institution, community health and resource centre, emergency service, hospital, parking garage, and parking lot. The proposed AMRC will include Animal Care and Veterinary Services, an Innovation Hub, and Faculty of Medicine Labs associated with uOttawa, and **can collectively be considered a post-secondary educational institution.**

The City's Zoning By-law defines "**post-secondary educational institution**" as a "university which means a place of higher education, which has a body of teachers and students on the premises, and that offers instruction at the undergraduate level, post-graduate level, or both, and which is empowered by law to grant a degree upon the successful completion of a prescribed course of study."

**At the Phase 1 pre-application consultation meeting with City staff on August 23, 2023**, City staff confirmed that the proposed AMRC facility will function as a post-secondary education institution. The City further confirmed in the initial Feedback Form dated August 25, 2023 that adding "research and development centre" as a proposed use for the site will not be required.

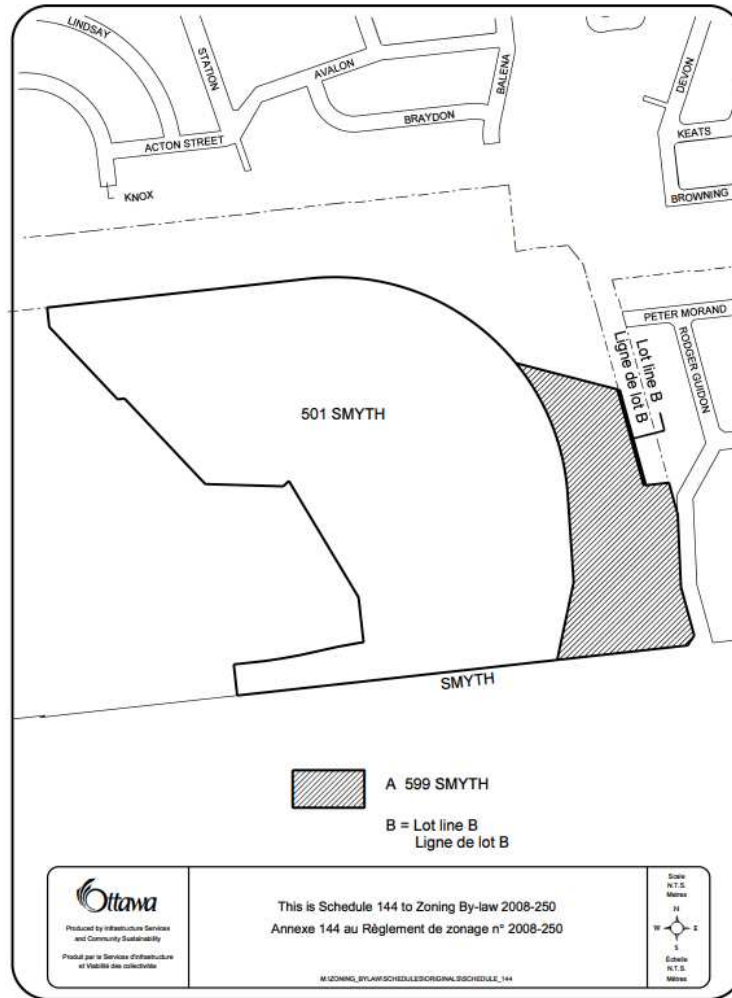
In addition, **Urban Exception 402 applies to the Site** and includes the following provisions:

- Minimum front yard setback is 46 metres;
- No parking is permitted within the front yard setback; and
- Parking spaces provided within Area A on Schedule 144 may be used to fulfill parking requirements for development occurring subsequent to February 29, 2004 at 501 Smyth Road.

**Schedule 144 applies to the Site** and illustrates the properties addressed 501 Smyth Road (i.e., 451 Smyth Road), as shown in **Figure 4-5**. It is noted that 599 Smyth Road is owned by the City.

**Table 4-1** provides a detailed zoning compliance analysis of how the proposed AMRC development at 451 Smyth Road meets the Zoning By-law provisions for the I2 Zone. The information in **Table 4-1** is based on the Site Plan, prepared by Parkin Architects (dated April 18, 2024).

**Figure 4-5: City of Ottawa Zoning By-law 2008-250 - Schedule 144**



**Table 4-1: Zoning Compliance for Major Institutional Zone (I2) – 451 Smyth Road**

Zoning Provision	Requirement -	Calculation (if applicable)	Compliance (Yes or No)
<b>Zone Requirement - I2 Zone (Table 171)</b>			
<b>Minimum lot width - Sec. 171, Table 171(1)</b>	No minimum	N/A	Yes - 257.13 m
<b>Minimum lot area - Sec. 171, Table 171(2)</b>	No minimum	N/A	Yes - Lot area (451 Smyth Road): 204,834.211 m <sup>2</sup>
<b>Minimum front yard setback -</b>	46 m	N/A	Yes - 364.54 m

Zoning Provision	Requirement -	Calculation (if applicable)	Compliance (Yes or No)
<b>Part 15 – Exceptions, Urban Exception 402</b>			
<b>Minimum rear yard setback- Sec. 171, Table 171(4)</b>	7.5 m	N/A	Yes - 117.79 m
<b>Minimum interior side yard setback - Sec. 171, Table 171(5)</b>	7.5 m	N/A	Yes - 34.79 m
<b>Minimum corner yard setback- Sec. 171, Table 171(6)</b>	7.5 m	N/A	Yes - 103.7 m
<b>Maximum building height - Sec. 171, Table 171(7)</b>	Within 12 m of an R1, R2, or R3 Zone: 15 m  <b>Other cases: no maximum</b>	N/A	Yes - 39.1 m (6 storeys + mechanical penthouse & atrium roof)
<b>Minimum width of landscaped area along all lot lines - Sec. 171, Table 171(8)</b>	3 m	N/A	<b>No - Landscaped area/buffer is not provided along the rear lot line of 451 Smyth Road as Ring Road and internal roads cross the rear property line of 451 Smyth Road.</b>
<b>Maximum floor space index - FSI 1.5</b>	1.5	FSI (Entire site at 451 Smyth):  Site area: 204,834.21 m <sup>2</sup>  <b>Total GFA =</b> 13,726.17 m <sup>2</sup> (Proposed AMRC at 451 Smyth) + 128,023 m <sup>2</sup> (Existing RGN, TOH and Oasis buildings on site) <b>= 141,749.17 m<sup>2</sup></b>  FSI = Total GFA / Lot Area	Yes - Total FSI (Proposed AMRC + existing buildings including RGN, TOH, Oasis): 0.69



Zoning Provision	Requirement -	Calculation (if applicable)	Compliance (Yes or No)
		= 141,749.17 m <sup>2</sup> / 204,834.21 m <sup>2</sup> = <b>0.69</b>	

#### 4.5.2 Provisions for Accessory Uses, Building or Structures

The Zoning By-law contains provisions for accessory uses, building or structures, as set out in **Table 4-2**. There is a new generator proposed to the west of AMRC building as shown on the Site Plan, which is an accessory structure.

**Table 4-2: Provisions for Accessory Uses, Building or Structures**

Zoning Provision	Requirement (IV - All other zones)	Calculation (if applicable)	Compliance (Yes or No)
<b>Minimum required setback from a front lot line, Sec. 55, Table 55(1)</b>	Same as required for principal building - 46 m	N/A	Yes - 365 m
<b>Minimum Required Setback from a Corner Side Lot Line or a Rear Lot Line abutting a street, Sec. 55, Table 55(2)</b>	0 m	N/A	N/A
<b>Minimum Required Setback from an Interior Side Lot Line or Rear Lot Line not abutting a street, Sec. 55, Table 55(3)</b>	(e) Other accessory buildings or structures, or situations not otherwise specified above. (ii) Not abutting a residential zone - 0 m.	N/A	Yes - 4.387 m (New generator)
<b>Minimum Required Distance from Any Other Building Located on the same lot, except for a hot tub, Sec. 55, Table 55(4)</b>	0 m	N/A	N/A - No minimum distance required.
<b>Maximum Permitted Height, Sec. 55, Table 55(5)</b>	6 m	N/A	Yes - 4.5 m

Zoning Provision	Requirement (IV - All other zones)	Calculation (if applicable)	Compliance (Yes or No)
<b>Maximum Permitted Size, Sec. 55, Table 55(6)</b>	No restriction	N/A	Yes - 3.5m X 12 m
<b>Maximum Number of Accessory Buildings Permitted on a lot, Sec. 55, Table 55(7)</b>	No restriction	N/A	Yes - One (1) accessory structure (the new generator) is proposed.

### 4.5.3 Permitted Projections Above the Height Limit and Permitted Projections into Required Yards Provisions

The Zoning By-law contains provisions for permitted projections above the height limit and permitted projections into required yards, as set out in **Table 4-3**. There are no proposed projections into required yards for the proposed AMRC development at 451 Smyth Road.

**Table 4-3: Provisions for Permitted Projections Above Height Limit / into Required Yards**

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
<b>Permitted projections above the height limit - Sec. 64</b>	Except in the case of buildings or structures located within the area shown on Schedules 11 to 88 (Central Area Height Schedules), the maximum height limits do not apply to the structures listed below or to any other similar structures that may require a height in excess of maximum height limits in order to serve their intended purpose, unless otherwise specified in the by-law and provided these structures are erected only to such height or area as is necessary to accomplish the purpose they are to serve and that is necessary to operate effectively and safely:	N/A	Yes - Mechanical room above Level 6 = 6 m high + Atrium = 3.6 m Elevator overrun = currently unknown  There is no maximum height limit.

<b>Zoning Provision</b>	<b>Requirement</b>	<b>Calculation (if applicable)</b>	<b>Compliance (Yes or No)</b>
	<ul style="list-style-type: none"> <li>- Mechanical and service equipment penthouse, elevator or stairway penthouse</li> </ul>		
<b>Chimney, chimney box and fireplace box, Sec. 65, Table 65(1)</b>	1 m, but not closer than 0.6 m to a lot line	N/A	N/A
<b>Eaves, eaves-troughs and gutters, Sec. 65, Table 65(2)</b>	1 m, but not closer than 0.3 m to a lot line	N/A	N/A
<b>Ornamental elements such as sills, belt courses, cornices, parapets and pilasters, Sec. 65, Table 65(3)</b>	0.6 m, but not closer than 0.6 m to a lot line	N/A	N/A
<b>Canopies and awnings, Sec. 65, Table 65(4)</b>	(b) All other buildings including a low-rise apartment dwelling and mid-high-rise apartment dwelling: <ul style="list-style-type: none"> <li>i) a distance equal to ½ the depth of a front, rear or corner side yard but not closer than 0.6 m to a lot line, and</li> <li>ii) 1.8 m into an interior side yard, but not closer than 0.6 m to a side lot line</li> </ul>	N/A	N/A – Proposed entrance canopies will not project into required yards.
<b>Fire escapes, open stairways, stoop, landing, steps and ramps</b>	(b) Other features: <ul style="list-style-type: none"> <li>i) where at or below the floor level of the first floor:</li> </ul>	N/A	N/A

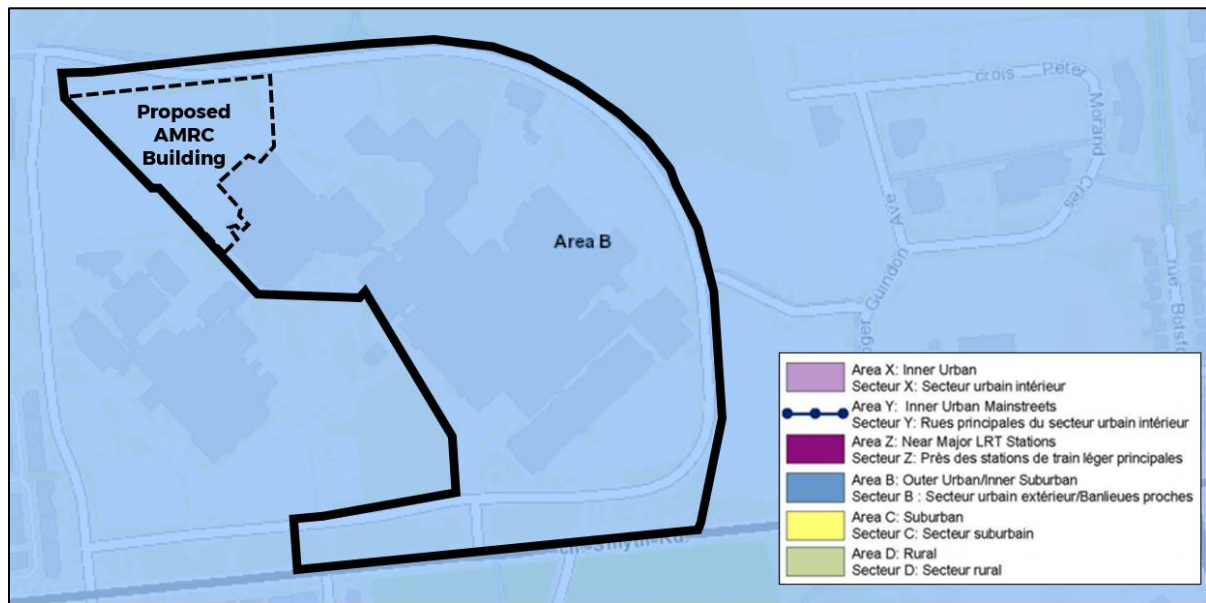
Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
<b>- Sec. 65, Table 65(5)</b>	<p>1. in the case of the interior side yard or rear yard: no limit, and</p> <p>ii) other cases: 1. In the case of any yard: 1.5m, but not closer than 1 m to a lot line</p>		
<b>Covered or uncovered balcony, porch, deck, platform, and verandah, with a maximum of two enclosed sides, excluding those covered by canopies and awnings, Sec. 65, Table 65(6)</b>	<p>a) uncovered, unenclosed features such as decks or platforms where the walking surface is not higher than 0.6 m above adjacent grade: (i) in the interior side yard and rear yard: no limit (ii) in the front yard and corner side yard – the greater of 2m or 50% of the required front yard or corner side yard, but no closer than 1m to a property line</p> <p>c) In all other cases: 2 metres, but no closer than 1 metre from any lot line.</p>	N/A	N/A
<b>Bay window where window faces a lot line, Sec. 65, Table 65(7)</b>	1 m, but not closer than 1.2 m from a lot line	N/A	N/A
<b>Air conditioner condenser, heat pump or similar equipment, Sec. 65, Table 65(8)</b>	<p>III) For All Other Buildings (i.e., non-residential)</p> <p>(b) Other cases – no restriction.</p>	N/A	<p>N/A – No requirement.</p> <p>Interior yard does not abut a residential use.</p>
<b>Exit stairs – Sec. 161(13)(m)</b>	Exit stairs providing required egress under the Building Code may project a maximum	N/A	Yes

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
	of 2.2 metres into the required rear yard.		

#### 4.5.4 Parking Provisions

The Zoning By-law contains provisions for parking, as set out in **Table 4-4**. The site is within Area B on Zoning By-law Schedule 1A, as illustrated in **Figure 4-6**.

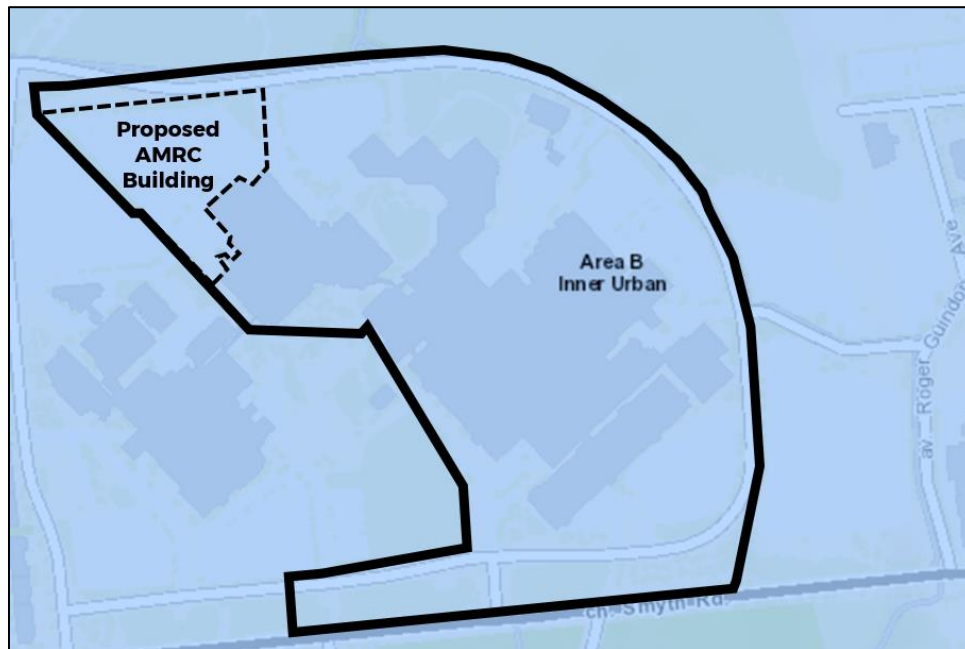
**Figure 4-6: Schedule 1A – Areas for Minimum Parking Space Requirements, City of Ottawa Zoning By-law (via GeoOttawa, 2024)**



For the purposes of applying the provisions of Section 103 - Maximum Limit on Number of Parking Spaces Near Rapid Transit Stations, the site is located in Area B: Inner Urban on Schedule 1 of the Zoning By-law, as illustrated in **Figure 4-6**. Applicable provisions are identified in **Table 4-4**. **The site is not within 300 m of an existing rapid transit station in Schedule 2A or 2B.**



**Figure 4-7: Schedule 1 – Boundaries of the Central, Inner Urban, Suburban and Rural Areas (Excerpt), City of Ottawa Zoning By-law (via geoOttawa, 2024)**



**Table 4-4: Parking and Loading Space Provisions**

<b>Zoning Provision</b>	<b>Requirement</b>	<b>Calculation (if applicable)</b>	<b>Compliance (Yes or No)</b>
<b>Schedule 1A</b>	The Site is designated as Area B “Inner Urban” in Schedule 1A of City of Ottawa Zoning By-law No. 2008-250.	N/A	N/A
<b>Schedule 2A and 2B</b>	The Subject Site is not within 300 metres of a rapid transit station in Schedule 2A or Schedule 2B.	N/A	N/A

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
<b>Minimum Parking Space Rates – Sec. 101, Table 101</b>	<p><u>Area B (Schedule 1A):</u></p> <p><b>Proposed Use (AMRC):</b> Post Secondary Educational Institution: 0.75 per 100 m<sup>2</sup> of gross floor area</p> <p><b>Other existing uses on-site (451 Smyth Road):</b>  Hospital: 1.4 per 100 m<sup>2</sup> of gross floor area  Daycare: 2 per 100 m<sup>2</sup> of gross floor area</p>	<p><b>451 Smyth Road (All uses – proposed AMRC and existing):</b></p> <p>GFA: Proposed AMRC = 13,726.17 m<sup>2</sup> Existing RGN = 27,387 m<sup>2</sup> Existing TOH = 124,985 m<sup>2</sup> Existing Oasis = 809.9 m<sup>2</sup></p> <p><b>Required Parking:</b></p> <p><u>Proposed AMRC only:</u> = 0.75 * (13,726.17/100) = 0.75 * 137.26 = 102.94 = 103 (rounded)</p> <p><u>Proposed AMRC + Existing Uses:</u></p> <p>Proposed AMRC + Existing RGN: = 0.75 * ((13,726.17 + 27,387)/100) = 0.75 * (411.1317) = 308</p> <p>Existing TOH: = 1.4 * (99,988/100) = 1,399</p> <p>Existing Oasis: = 2 * (648/100) = 13</p>	<p>Yes – The existing parking lot with 361 parking spaces will be demolished. As part of the AMRC redevelopment, 126 additional new spaces will be provided. See below:</p> <p><b>Proposed Parking (Proposed AMRC building only):</b></p> <p>= 120 (new parking spaces) + 6 (new service spaces) = 126 spaces</p> <p><b>Total Proposed Parking (451 Smyth Road):</b> = 126 + (2,098 (existing) – 361 (removed from RGN Parking Lot H)) = 126 + 1,737 = 1,863 parking spaces</p>

Zoning Provision		Requirement	Calculation (if applicable)	Compliance (Yes or No)
			<p><b>Total Required Parking - Existing and Proposed AMRC (451 Smyth Road):</b>            = 308 + 1,399 + 13            = 1,720</p> <p><b>Additional notes:</b>            Existing Parking at 451 Smyth Road:            = 1,737 spaces</p> <p>Parking spaces to be removed from the existing RGN Parking Lot H (361 existing parking spaces):            = 361 spaces</p>	
<b>Urban Exception 402</b>		No parking is permitted within the front yard setback.	N/A	Yes - No parking is proposed to be located within the front yard setback as part of the proposed AMRC development. It is noted the front yard setback is taken from Smyth Road.
<b>Minimum visitor parking spaces - Sec. 102, Table 102</b>		None required for the proposed use.	N/A	While none are required, visitors may use any of the parking spaces serving for the proposed building.
<b>Dimension requirements for a motor vehicle parking space - Sec. 106(1)</b>	(a) Width	Minimum width of 2.6 m; maximum width of 3.1 m.	N/A	Yes - Parking spaces will be 5.2 m x 2.6 m.
	(b) Length	5.2 m	N/A	

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
<b>Minimum Driveway Width to Parking Lot - Sec. 107(1)</b>	(a) (i) three metres for a single traffic lane;	N/A	Yes - 3 m.
<b>Minimum Required Aisle Width - Table 107</b>	6.7 m for angle of parking that is 90 degrees	N/A	Yes - 6.7 m.
<b>Accessible Parking Spaces (Part C - Parking required for persons with disabilities, Section 111, By-law no. 2017-301)</b>	<b>AMRC site only:</b> For a public parking area that contains 100-199 parking spaces, 2 accessible parking spaces are required.	N/A	Yes - 6 accessible parking spaces will be provided for the proposed AMRC building.
<b>City of Ottawa Accessibility Design Standards - Section 3.1.3(f)</b>	Minimum width of 3400 mm for "Type A" wide van accessible spaces	N/A	Yes - 6 "Type A" wide van accessible spots will be provided.
	Minimum width of 2400 mm for "Type B" standard parking spaces		
<b>City of Ottawa Accessibility Design Standards - Section 3.1.3(g)</b>	Provide an access aisle adjacent and parallel to each accessible parking space: <ul style="list-style-type: none"> <li>i. 1500 mm wide (minimum;</li> <li>ii. extend the full length of the space;</li> <li>iii. clearly indicated by high colour contrast diagonal pavement markings;</li> </ul>	N/A	Yes - 1.5 m access aisle is provided for accessible parking spaces.

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
	iv. where two accessible parking spaces are provided adjacent to each other, they may share an access aisle; v. connect with adjacent accessible path of travel and centre curb ramp on access aisle		
<b>Outdoor loading and refuse collection areas within a parking lot - Sec. 110(3)</b>	Requirements: - 9 m from a lot line abutting a public street - 3 m from any other lot line Screened from view by an opaque screen with a min. height of 2 m	N/A	Yes - 18.864 m from interior lot line (existing waste collection area). A 2 m high opaque screen is provided.
<b>Landscaping Provisions for Parking Lots - Sec. 110</b>	Minimum 15% of the parking lot must be provided as perimeter or interior landscape area comprised of:  a) a landscaped buffer must be provided between the perimeter of the parking lot and a lot	15% of parking lot area: = 0.15 x (24,625.76 m <sup>2</sup> ) = 3,693.86 m <sup>2</sup>	Yes - 17% of the parking lot area will be landscaped - 4,186.38 m <sup>2</sup>



Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
	<p>line in accordance with Table 110. A driveway may cross the landscaped buffer;</p> <p>and</p> <p>b) in addition to the landscaped buffer, interior landscaping may be provided including various landscaped is, landscaped medians, pedestrian pathways or public plazas to meet the minimum 15% requirement.</p>		
<p><b>Minimum required width of a landscaped buffer of a parking lot – Sec. 110, Table 110</b></p>	<p>For a parking lot containing 100 or more parking spaces:</p> <p>(a) abutting a street: 3 m</p> <p>(b) not abutting a street: 3 m</p>	<p>N/A</p>	<p><b>No – 1.045 m landscape buffer between new uOttawa parking lot and internal access street.</b></p>
<p><b>Minimum number of bicycle parking spaces – Sec. 111(2), Table 111A</b></p>	<p><b>Proposed Use (AMRC):</b></p> <p>(e) post secondary educational institution</p> <p>1 per 250 m<sup>2</sup> of gross floor area</p>	<p><b>451 Smyth Road (All uses – proposed AMRC and existing):</b></p> <p>GFA: Proposed AMRC: = 13,726.17 m<sup>2</sup></p> <p><b>Required Bicycle Parking:</b></p>	<p>Yes – 222 new bicycle parking spaces are proposed for the AMRC building.</p> <p>467 existing bicycle parking spaces serve the existing RGN, TOH and Oasis buildings. 20 spaces will be</p>

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
	<p><b>Other existing uses on-site (451 Smyth Road):</b></p> <p>(e) daycare: 1 per 250 m<sup>2</sup> of gross floor area</p> <p>(g) hospital: 1 per 1,000 m<sup>2</sup> of gross floor area</p>	<p>GFA: Proposed AMRC = 13,726.17 m<sup>2</sup> Existing RGN = 27,387 m<sup>2</sup> Existing TOH = 124,985 m<sup>2</sup> Existing Oasis = 809.9 m<sup>2</sup></p> <p><b>Proposed AMRC only:</b> = 1 * (13,726.17/250) = 54.90 = 55 (rounded)</p> <p>Proposed AMRC + Existing RGN: = 1 * ((13,726.17 + 27,387)/250) = 164.45 = 164 (rounded)</p> <p>Existing TOH: = 1 * (99,988/1000) = 99.98 spots = 100 (rounded)</p> <p>Existing Oasis: = 1 * (648/250) = 2.59 = 3 (rounded)</p> <p>Total Required Bicycle Parking (451 Smyth Road): = 164 + 100 + 3 = 267 bicycle parking spaces</p>	<p>removed as result of construction works.</p> <p>A total of 699 bicycle spaces will be provided across the entire 451 Smyth Road site.</p>

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
		<b>Additional Notes:</b> Existing Bicycle Parking serving the existing RGN, TOH and Oasis buildings = 497	
<b>Bicycle parking space location - Sec. 111 (3)</b>	Must be located on the same lot as the use or building for which it is provided	N/A	Yes - Bicycle parking spaces are proposed on the same lot.
<b>Bicycle parking space location - Sec. 111 (4)</b>	Must be located in order to provide convenient access to main entrances or well-used areas	N/A	Yes - Bicycle parking spaces are proposed to be located to building entrances.
<b>Bicycle parking spaces location in landscaped area - Sec. 111 (7)</b>	Max. of 50% or 15 spaces, whichever is greater	36/222 spots in landscaped area = 16.2% =26% (rounded)	Yes - 16% of bicycle parking spaces are located in landscaped area.
<b>Minimum bicycle parking space dimensions - Sec. 111 (8), Table 111B</b>	(a) Horizontal: Width: 0.6 m Length: 1.8 m	N/A	Yes - 1.8 m X 0.6 m
<b>Minimum bicycle parking aisle width - Sec. 111 (9)</b>	A bicycle parking space must have access from aisle having a min. width of 1.5 m	N/A	Yes - 1.5 m
<b>Minimum number of vehicle loading spaces required - Sec. 113, Table 113A</b>	a) Post secondary educational institution: 25,000 m <sup>2</sup> and over: 3 loading spaces	<b>Total GFA (451 Smyth Road):</b> = 13,726.17 m <sup>2</sup> (Proposed AMRC at 451 Smyth) + 128,023 m <sup>2</sup> (Existing RGN, TOH and Oasis buildings on site)  = 141,749.17 m <sup>2</sup>	Yes - Three (3) new loading docks (bays) are proposed at the proposed AMRC, and two (2) loading existing docks at RGN building noted on the Site Plan.
<b>Minimum width of driveway accessing loading space - Sec. 113, Table 113B(a)</b>	(i) - Single traffic lane - 3.5 m (ii) Double traffic lane - 6 m		Yes - 7.077 m

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
<b>Minimum width of aisle accessing loading space, by angle of loading space - Sec. 113, Table 113B(b)</b>	For a standard space: (i) 45 degrees or less: 5 m (ii) Between 45 degrees and 60 degrees: 6.3 m (iii) 60 degrees to 90 degrees: 9 m	N/A	Yes - 5.169 m
<b>Minimum width of loading space - Sec. 113, Table 113B(c)</b>	Standard Space: 3 m	N/A	Yes - 4.3 m
<b>Minimum length of loading space - Sec. 113, Table 113B(b)</b>	(ii) Other cases: 7 m	N/A	Yes - 9.75 m
<b>Minimum landscaped area of any parking lot - Sec. 110, provision (1)</b>	Except in the case of an industrial zone, a minimum of 15% of the area of any parking lot, whether a principal or an accessory use, must be provided as perimeter or interior landscaped area.		Yes - 22% of the parking lot is landscaped.

**4.5.5 Summary of Minor Zoning By-law Amendment**

In order to facilitate the proposed development, a Minor Zoning By-law Amendment to the to the City of Ottawa Zoning By-law 2008-250 is required. The entire property at 451 Smyth Road, including the AMRC development site is currently zoned Major Institutional Zone, Urban Exception 402, Floor Space Index 1.5, Schedule 144 (I2[402] F(1.5) S144). The Minor Zoning By-law Amendment proposes to modify Urban Exception 402 to grant relief from zoning requirements related to landscape buffers.

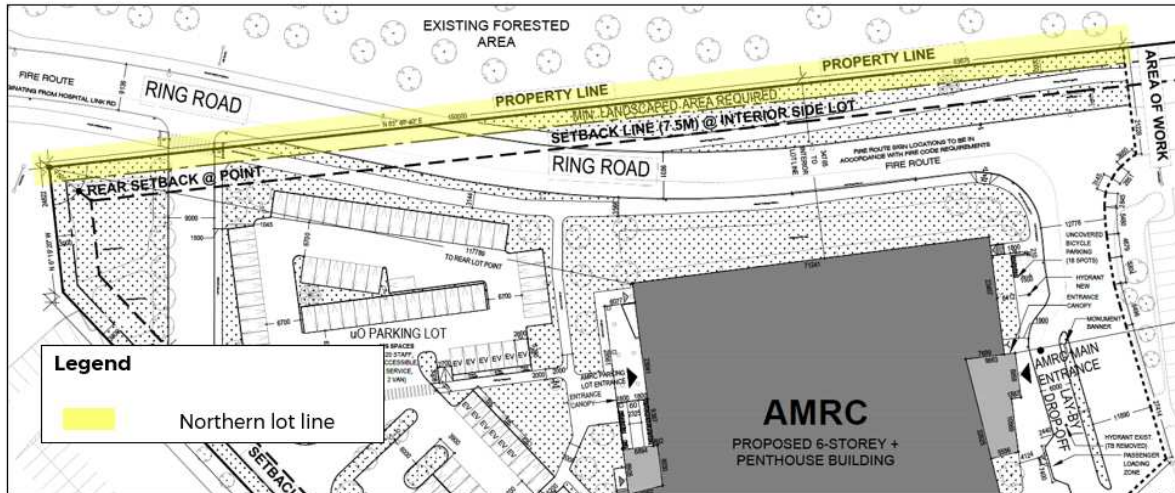
The amendments to Urban Exception 402 are required because the proposed development at 451 Smyth Road does not meet all the requirements of the I2 Zone in the Zoning By-law.

The requested amendments to Urban Exception 402, firstly include relief from the requirement for a 3 metre landscaped area along all lot lines per Sec. 171, Table 171(8) of the Zoning By-law. The requested reduction in the landscape area to 0 metres along the northern

lot line is requested to address existing conditions on-site, as Ring Road and the existing sidewalks on both sides of Ring Road cross the northern property line of 451 Smyth Road, as shown in the Site Plan (**Figure 4-8**).

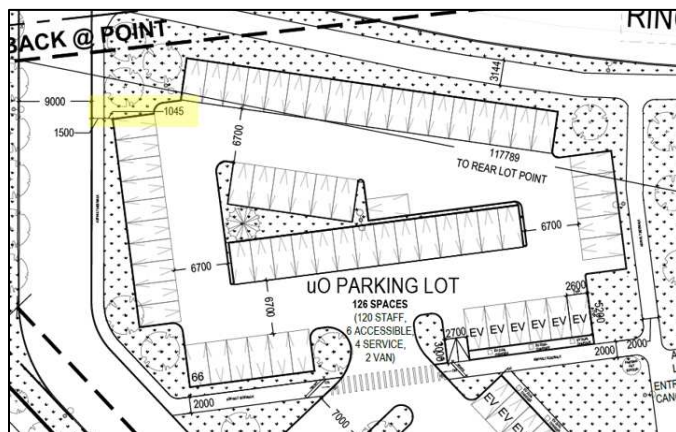
Additionally, the proposed parking layout for the site is designed to meet uOttawa’s program requirements, which are to facilitate access to health and research services in consideration of the overall Ottawa Health Science Complex (OHSC) operations.

**Figure 4-8: Northern lot line - 451 Smyth Road**



Additionally, a reduction in the minimum required width of a landscaped buffer for a parking lot is being requested. Per Sec. 110, Table 110, for a parking lot containing 100 or more parking spaces, a 3 metre landscaped buffer is required for parking lots abutting a street. A 3 metre landscaped buffer is also required for parking lots which do not abut street. As shown in the Site Plan (**Figure 4-9**), the landscaped buffer at the western edge of the new uOttawa parking lot is proposed to be 1.045 metres. As such, a reduction from 3 metres to 1 metre is requested for the landscaped buffer around the parking lot.

**Figure 4-9: Proposed AMRC Parking Lot - 451 Smyth Road**





The existing parent zone for the site (I2) will be maintained, as it currently permits the post-secondary educational institution use. Permitted uses in the I2 Zone are set out in Section 171 of the Zoning By-law which includes a post secondary educational institution.

The proposed Zoning By-law Amendment would seek to facilitate the proposed development through modification of the performance standards under Urban Exception 402 to add the following site-specific provisions:

- No requirement for a 3 m landscape buffer along the rear lot line; and
- Permit a reduction in the minimum required width of a landscaped buffer of a parking lot abutting a street from 3 m to 1 m.

In addition to the above-noted amendments, the existing provisions under Urban Exception 402 that currently apply to the site will be maintained:

- Minimum front yard setback is 46 metres;
- No parking is permitted within the front yard setback; and
- Parking spaces provided within Area A on Schedule 144 may be used to fulfill parking requirements for development occurring subsequent to February 29, 2004 at 501 Smyth Road.

The draft Zoning By-law Amendment text and Schedule are included as **Appendix D** to this Planning Rationale.

## 5 Summary of Opinion

Based on our review of the applicable land use planning policy framework, area context, and the supporting application materials, it is the professional opinion of WSP that the proposed Advanced Medical Research Centre at the Ottawa development at 451 Smyth Road represents good land use planning, and is appropriate for the site for the following reasons:

- The proposed development is consistent with the 2020 Provincial Policy Statement;
- The proposed development is permitted in the applicable land use designations and conforms to the strategic directions and policies of the City's Official Plan;
- The proposed development complies with the general intent and purpose of the Zoning By-law.

In conclusion, the proposed Minor Zoning By-law Amendment and Site Plan Control application being sought to support the proposed development at 451 Smyth Road represents good planning, and the development is in the public interest.

Please feel free to contact Nadia De Santi at [Nadia.De-Santi@wsp.com](mailto:Nadia.De-Santi@wsp.com) or at (613) 690-1114 or Jill MacDonald at [Jill.MacDonald@wsp.com](mailto:Jill.MacDonald@wsp.com) or (613) 690-3936 if you have any questions or require additional information.

Yours truly,



Nadia De Santi, MCIP, RPP  
Practice Lead



Jill MacDonald, MCIP, RPP  
Senior Planner



Grace Maxner  
Planner

# A Site Plan

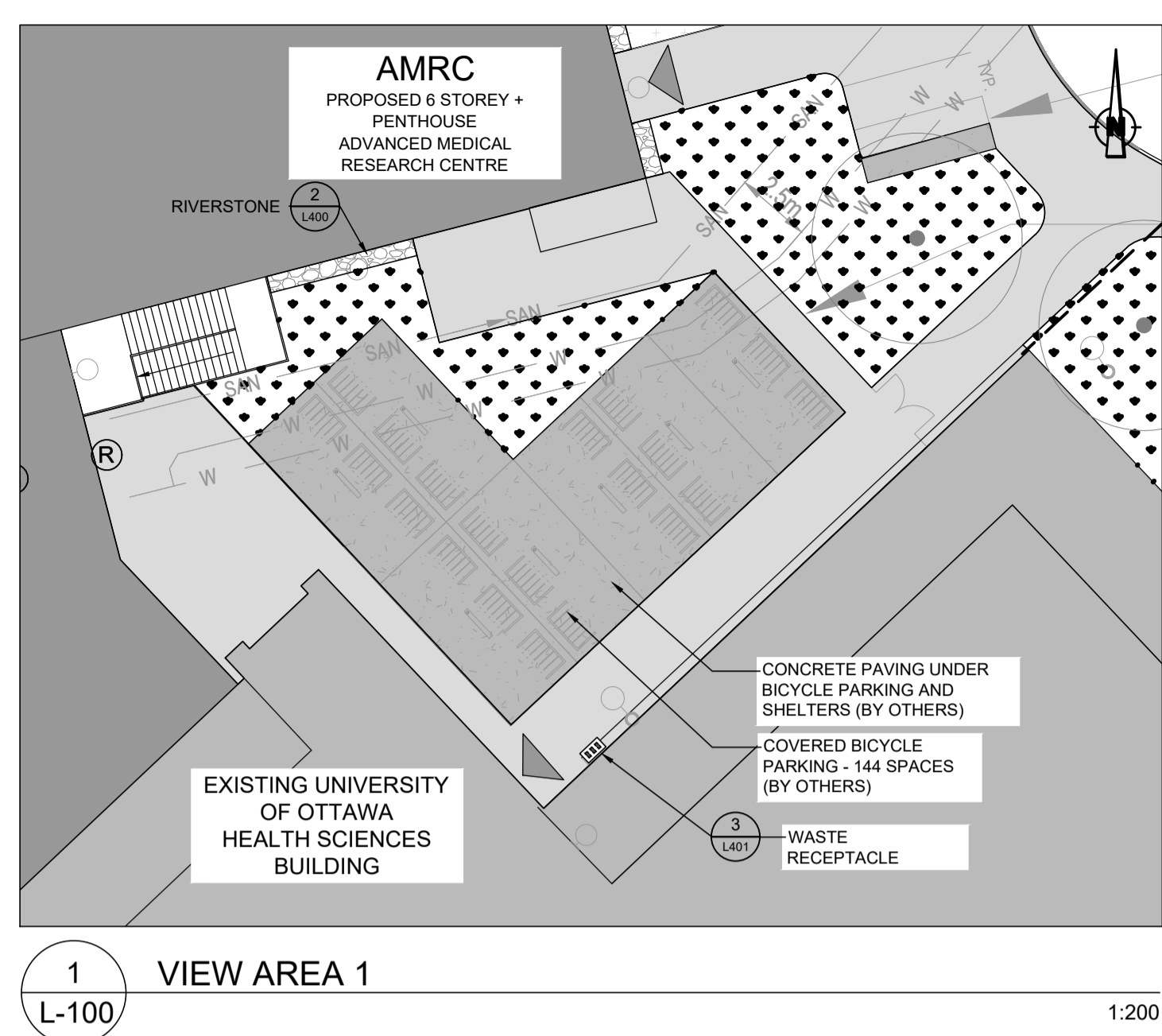






# B Landscape Plan





1 VIEW AREA 1  
L-100



- LEGEND**
- PROPERTY LINE
  - SETBACK LIMIT (7.5m @ INTERIOR SIDE LOT)
  - LIMIT OF EXCAVATION
  - IDENTIFICATION NUMBER FOR INVENTORIED TREE WITHIN APPLICANT PROPERTY. REFER TO TREE CONSERVATION REPORT
- PLANTING**
- PROPOSED TREES - DETAIL 1/L401
  - EXISTING TREES TO REMAIN - REFER TO TREE CONSERVATION REPORT
  - TREE PROTECTION FENCE - REFER TO TREE CONSERVATION REPORT
  - LOW-MAINTENANCE SEEDING/WILDOLOWER MIX - REFER TO ARCHITECTURE SITE PLAN FOR LIMIT OF LANDSCAPE AREA
  - SODDING - REFER TO ARCHITECTURE SITE PLAN FOR LIMIT OF LANDSCAPE AREA
  - SHRUB AND GRASSES MASS PLANTING BED - DETAIL 3/L401 AND 4/L401
- PAVING**
- RIVERSTONE MAINTENANCE STRIP - DETAIL 2/L400
  - ASPHALT PAVING - REFER TO ARCHITECTURE SITE PLAN FOR LAYOUT
  - CAST IN PLACE CONCRETE PAVING - REFER TO ARCHITECTURE SITE PLAN FOR LAYOUT
- SITE FURNITURE**
- WASTE RECEPTACLE - DETAIL 3/L400
  - BENCH WITH ACCESSIBLE SPACE - C.I.P. CONCRETE PAD - DETAILS 1/L400 AND 5/L400
  - BIKE SHELTER WITH BIKE RACKS - REFER TO ARCHITECTURE

NO	DATE	DESCRIPTION	BY
6	24-04-18	ISSUED FOR ZBLA AND SPC	TM
5	24-04-10	ISSUED FOR PHASE 3 RE-SUBMISSION	TM
4	24-02-26	ISSUED FOR PHASE 3 RE-SUBMISSION	TM
3	23-12-01	ISSUED FOR SPA	TM
2	23-11-06	ISSUED FOR PRICING PROPOSAL	JC
1	23-10-12	ISSUED FOR CITY PRE-CONSULTATION	JC

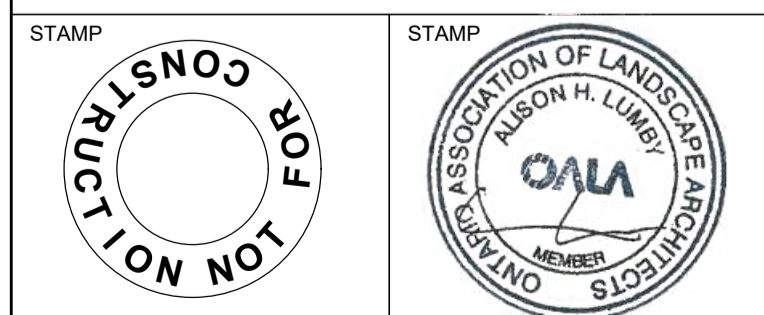


CLIENTS:  
**UNIVERSITY OF OTTAWA  
PCL CONSTRUCTION**

PROJECT TITLE:  
**ADVANCED MEDICAL  
RESEARCH CENTRE**

SITE ADDRESS:  
451 SMYTH ROAD, OTTAWA, ONTARIO

DRAWING TITLE:  
**LANDSCAPE PLAN**



DESIGNED JC	DRAWN JC	CHECKED AL
SCALE 1:500	DATE NOVEMBER 2023	
PROJECT NUMBER CA0009956.0165-CA	DWG. NUMBER L-100	



# C Building Elevations









# Draft Zoning By-law Amendment and Schedule



**DRAFT BY-LAW NO. 2024-XX**

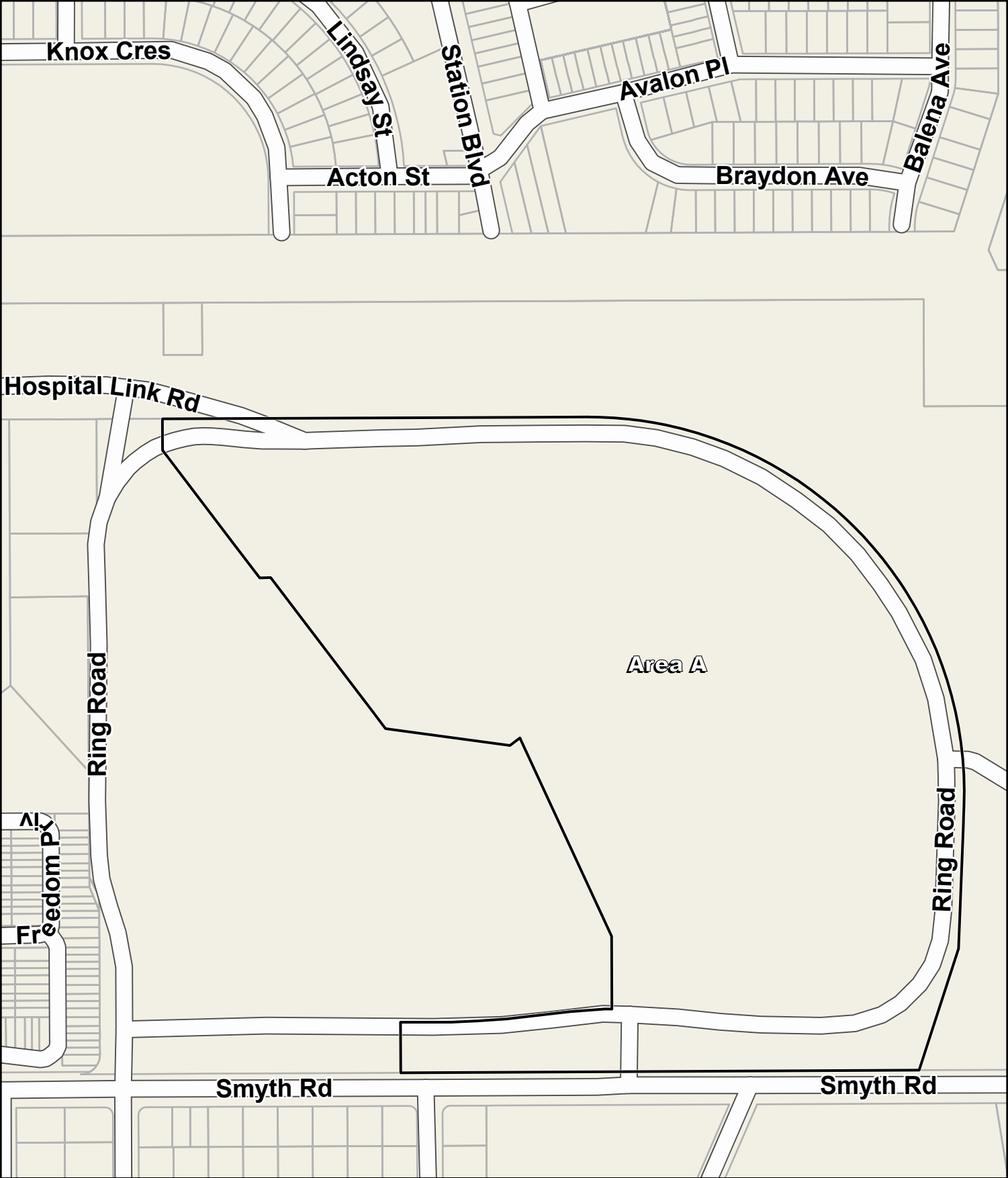
A by-law of the City of Ottawa to amend By-law No. 2008-250 to change the zoning of the lands legally described as Part of Lots 30, 31 and 32, Part of Terrance Road (closed by Judge’s Order INST.GL40441), Registered Plan 405, Part of Lot 15 Junction Gore, Geographic Township of Gloucester, Being Part of the Northerly and Westerly limits of PIN 04258-0412, City of Ottawa. The Council of the City of Ottawa, pursuant to Section 34 of the Planning Act, R.S.O. 1990, enacts as follows:

The Zoning Map of By-law No. 2008-250, entitled “City of Ottawa Zoning By-law” is amended by amending the lands on Attachment 1 to this by-law as follows:

1. To amend Urban Exception 402, with the following provisions shown in **bold**.

I Exception Number	II Applicable Zone	Exception Provisions		
		III Additional Land Uses Permitted	IV Land Uses Prohibited	V Provisions
402 [By-law No. 2024-XX]	I2 [402] F(1.5) S 144  O1[402] S 144	<ul style="list-style-type: none"> <li>• Parking lot and a right-of-way providing access to the parking lot</li> </ul>		<ul style="list-style-type: none"> <li>• <b>No requirement for a 3 m landscape buffer along the rear lot line;</b></li> <li>• <b>Minimum width of landscaped buffer of a parking lot abutting a street: 1 m;</b></li> <li>• Minimum front yard setback is 46 metres;</li> <li>• No parking is permitted within the front yard setback; and</li> <li>• Parking spaces provided within Area A on Schedule 144 may be used to fulfill parking requirements for development occurring subsequent to February 29, 2004 at 501 Smyth Road.</li> </ul>

ENACTED AND PASSED this \_\_\_\_\_ day of \_\_\_\_\_, 2024



This is "Attachment 1" to  
Zoning By-law 2023-XXX

Prepared by WSP  
April 2024

- Rezone Area A from I2[402] F(1.5) S144 to I2[XXXX] F(1.5) S144.

Source: GeoOttawa; Open Ottawa

**DRAFT**

N



1:4000