

# URBAN DESIGN BRIEF

## MARKETPLACE WEST

Issued: April 25, 2024

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35 STOREY APARTMENT BUILDING

1034 MCGARRY TERRACE



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# PROJECT DESCRIPTION:

## PROJECT SUMMARY:

The proposed building is located at the corner of Marketplace and the McGarry Terrace extension. The building consists of a four-storey base podium, residential link and two towers. The podium is to provide ground floor commercial spaces and building ancillary spaces at the first floor as well as residential units at the remaining 3 floors of the base. Within the open spaces surrounding the base, it is proposed to provide a large open courtyard that are meant for public use facing Marketplace Ave. The open spaces account for approximately 25% of the overall site. On top of the base, the proposed building has 2 towers providing a total of 597 residential units and amenities. Below grade it is proposed to have 6 levels of underground parking providing approximately 653 spaces.

## PROJECT STATISTICS:

Gross Floor Area:	66,780m <sup>2</sup> (West Tower 38,510m <sup>2</sup> + East Tower 28,270m <sup>2</sup> )
Floor Plate Breakdown:	1 <sup>st</sup> Floor Retail-1120m <sup>2</sup> 2 <sup>nd</sup> -4 <sup>th</sup> Floors Residential Units- 1834m <sup>2</sup> (each) 5 <sup>th</sup> -9 <sup>th</sup> Floors Residential Units- 1524m <sup>2</sup> (each) 10 <sup>th</sup> Floor Residential Units- 1113m <sup>2</sup> 11 <sup>th</sup> Floor Residential Units- 1105m <sup>2</sup> 12 <sup>th</sup> Floor Residential Units- 1078m <sup>2</sup> 12 <sup>th</sup> -35 <sup>th</sup> FLRS WEST Residential Units- 560m <sup>2</sup> 12 <sup>th</sup> -26 <sup>th</sup> FLRS EAST Residential Units- 578m <sup>2</sup>
Total No. of Units:	Total Units- 597 Studio Units- 83 Units (13.9%) 1 Bed Units- 250 (41.9%) 2 Bed Units- 247 (41.4%) 3 Bed Units- 17 (2.8%)
Total No. of Parking Spaces:	Total Spaces- 653 Standard Stalls – 448 Compact Stalls- 178 BF Type A- 16 BF Type B- 11
Total No. of Bike Parking:	Total Spaces- 304
Building Height:	130m
Lot Coverage:	55.2% (1 <sup>st</sup> FLR Footprint 2267m <sup>2</sup> )



# DESIGN DIRECTIVES:

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## **RESPONSE TO DESIGN POLICIES- OFFICIAL PLAN**

### **MOBILITY:**

The concept plan for the proposal includes 653 underground parking spaces allocated between residents, visitors, and commercial units. Access to the parking garage is shared with the abutting development. The driveway entrance configuration ensures that pedestrians using Marketplace will have right of way over vehicles entering or exiting the site.

### **HOUSING:**

The proposal includes rental dwellings ranging from studio to three-bedroom units. Through the Canada Mortgage and Housing Corporation (CMHC) MLI Select Program the proposal is seeking preferential financing in exchange for providing below-market rent, barrier-free units, and achieving sustainability standards.

### **PARKS AND RECREATION FACILITIES:**

The proposal provides a large courtyard within the site along Marketplace Avenue. The courtyard will integrate with setbacks and sidewalks along Marketplace Avenue to create a larger functional space. Creating a variety of community amenities and gathering places.

### **URBAN DESIGN:**

The courtyard abutting Marketplace Avenue is situated to animate the streetscape and integrate the commercial spaces. This space is located along Marketplace Avenue makes it easily visible, accessible, and welcoming to residents and the public.

The proposed building heights and orientation are consistent with the direction of the Hub Town Centre designation. The proposal is set back from Neighbourhoods, providing a transition from the site to surrounding low-rise communities.

Building heights decrease incrementally from the site to the east and south. The four-storey podium provides a transition in materials and massing from the podium to the tower and linked section above the fourth floor.

There are private outdoor amenity areas located on the north side of the building at-grade and on the roof at the 12<sup>th</sup> floor. The courtyard in combination with the private amenity areas will be spaces designed to provide protection from heat, wind, extreme weather, noise, and air pollution. Extensive landscaping and tree planting is proposed at-grade through the Landscape Strategy.

The proposed towers provide sufficient spacing between each other and the tower to the north. The east tower is separated from the western Havens tower by approx. 21m; while this is less than the 23 m separation preferred by the Official Plan and noted in the Zoning By-law, the reduction is minimal.

# DESIGN DIRECTIVES:

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## **RESPONSE TO DESIGN POLICIES- OFFICIAL PLAN**

### **DRINKING WATER, WASTEWATER & STORMWATER:**

The Assessment of Adequacy of Public Services report prepared by McIntosh Perry examines the required water and sanitary servicing requirements of the proposed development and investigates existing and planned infrastructure that is accessible to the proposal.

### **SUBURBAN TRANSECT:**

High-rise buildings, considered between ten and 40 storeys, are permitted in Hub Town Centres in the Suburban Transect by the Official Plan.

The proposal introduces higher-density mixed-use development in proximity to rapid transit network. The proposal provides a range of dwelling sizes (from studio to three-bedroom units) and will provide rental housing to a broad range of demographics. The proposal will provide additional purpose-built rental housing to Barrhaven while also increasing the supply of apartment dwellings.

### **EVOLVING OVERLAY:**

The proposal further advances the urban built form along Marketplace Avenue and other neighboring streets in the Town Centre. The proposal frames the public realm with active frontages and providing trees and soft landscaping in a more efficient use of land.

### **TOWN CENTER HUB:**

The increased height and density proposed is consistent with the evolving environment. The proposed mixed-use development promotes efficient development in proximity to a major existing transit station and existing amenities. The proposed form will frame the public realm while introducing a publicly accessible courtyard. Building entrances have been located to provide direct access to sidewalks and the public courtyard. While vehicle parking, loading and service areas have been located away from public streets with the principal vehicle access shared with the neighboring development to minimize its impact on the public realm.

The proposal is located in an area by commercial retail shopping and within walking distance to an existing transit station. Which supports higher densities and overtime will evolve into dense communities that add value to community and social life. The community will benefit from the increased infill that the proposal will add.

# DESIGN DIRECTIVES:

**PREVIOUS CONSIDERATIONS:**

Stantec has prepared the following response to recommendations received from the Urban Design Review Panel (UDRP) on 19 June 2023 following an informal presentation by the project team on 2 June 2023. Panel comments have been listed in the left column of the table below, whereas the project team’s response is provided in the right column.

**PANEL RECOMMENDATIONS:**

**PROJECT TEAM RESPONSE:**

**KEY RECOMMENDATIONS:**

The Panel recommends revisiting all applicable city guidelines to ensure they are incorporated in the preliminary stages of the design process. Of particular note, the Panel recommends reviewing the City guidelines for:

- Building setbacks from property lines
- Building height
- Building floorplate size, and
- Separation between towers

The Panel recommends the height of the building be limited to a maximum of 30 storeys given its location.

The proposal has been revised to reduce the number of storeys. To ensure that there is variation in height between the two towers, it is proposed that the west tower be reduced to 35 storeys in height, whereas the east tower would be reduced to 26 storeys.

The Panel supports the height of the podium at 4-storeys, however, recommends further increasing the tower step-backs

Larger step-backs are provided along the street-facing (west and south) faces of the proposal.

The Panel appreciates the inclusion of the POPS on Marketplace Avenue; however, the Panel recommends creating a larger POPS that strives to be a central public gathering space for the community.

A Landscape Strategy has been prepared which includes design of the courtyard and abutting sidewalk space. The strategy proposes a continuation of the paving and street tree pattern of the development to the east while integrating seating, planting areas, and green groundcover areas. While the courtyard area remains unchanged, the space now includes improvements to publicly owned space within the Marketplace right-of-way, and provides a continuous landscape approach across the entire frontage of the site. Further grading investigation has removed most stairs and retaining walls previously proposed.

The Panel recommends the public realm treatment of the proposed development be a more comprehensive and robust design proposal that considers pedestrian connectivity through the site and fosters a central gathering space for the future growth in the area.

# DESIGN DIRECTIVES:

**PANEL RECOMMENDATIONS:**

**PROJECT TEAM RESPONSE:**

**SITE DESIGN & PUBLIC REALM:**

<p>The Panel appreciates the location of the POPS on the site</p> <ul style="list-style-type: none"> <li>• Consider including animated uses in the POPS including seating and benches.</li> <li>• Consider bolstering the POPS function and appearance as a forecourt for the greater community.</li> <li>• Consider enlarging the POPS along Marketplace Avenue.</li> </ul>	<p>A Landscape Strategy has been prepared which includes design of the courtyard and abutting sidewalk space. The strategy proposes additional seating and includes improvements to publicly owned space within the Marketplace right-of-way to provide a continuous landscape approach across the entire frontage of the site.</p>
<p>The Panel recommends increasing the setbacks further from the property line.</p>	<p>Setbacks along Marketplace Avenue have been increased where possible</p>
<p>The Panel suggests the applicant undertake a wind study to ensure that the POPS has four-season pedestrian comfort.</p>	<p>Acknowledged. A pedestrian-level wind study was identified as a requirement of the applications.</p>
<p>The Panel recommends giving greater consideration to the porosity of the site.</p> <ul style="list-style-type: none"> <li>• Consider incorporating a fully accessible connection across the north property-line.</li> <li>• Consider walkability and pedestrian experience traversing the north side of the building to adjacent sites.</li> </ul>	<p>Addition of a connection from the private driveway area to McGarry Terrace would not reduce walking times to or through the site in comparison to a similar path along McGarry Terrace and Marketplace. Additionally, a pathway along the north property line would traverse the communal amenity space intended for building residents. A block length of approximately 140 m will still allow for easy pedestrian circulation and connectivity considering the modified grid street network and comprehensive pedestrian infrastructure.</p>
<p>The Panel suggests an entrance to the tower from the courtyard would be beneficial and support the user’s interaction from the front terrace on Marketplace Avenue.</p>	<p>Further detail can be explored through Site Plan Control. At this time several commercial spaces have direct access to the courtyard and will provide permeable, active facades and passive surveillance.</p>
<p>The Panel recommends interchanging the amount of hardscape and softscape shown in the proposal, with a focus on introducing a stronger green element to the plaza space.</p>	<p>The Landscape Strategy provides larger greenspace areas</p>

# DESIGN DIRECTIVES:

## **PANEL RECOMMENDATIONS:**

### **SITE DESIGN & PUBLIC REALM(CONT.):**

The Panel recommends investigating a better resolution to the streetscape that allows for plantings and street trees. These public realm investments are important in creating the sense of a neighbourhood core and a downtown area.

The Panel recommends giving greater consideration to the Mainstreet condition on Marketplace Avenue.

- Consider access to amenities, retail, schools, etc.
- Consider the character of the streetscape and how the project contributes to establishing Marketplace Avenue as a Mainstreet.
- As proposed, the POPS reads as a semi-private space, not a public space. The Panel recommends designing the POPS in a way that is more inviting to the public and passers-by. Consider how to best locate and design the POPS to take advantage of the Mainstreet and optimize public use.
- The Panel has concerns regarding the survival of trees as proposed. Consider the viability of the POPS space having a greater connection to the street corner and allowing more space for the trees to thrive.

The Panel recommends designing a bigger POPS that would be more usable for local's leisure.

- Consider designing one large central greenspace with more soft-scaping and large tree canopies.
- Alternatively, consider two medium sized greenspaces with large tree canopies and a central walkway that could lead to commercial and residential entry points.
- The Panel strongly recommends reconsidering the small tree beds proposed for the POPS.

Investigate creating public space with more soft-scaping and identifying ways to ensure the trees thrive to maturity with full-sized canopies.

## **PROJECT TEAM RESPONSE:**

A Landscape Strategy has been prepared which includes design of the publicly accessible courtyard and abutting sidewalk space. The strategy proposes additional seating and includes improvements to publicly accessible space within the Marketplace right-of-way to provides a continuous landscape approach across the entire frontage of the site.

As per the Panel's recommendation, the Landscape Strategy proposed two medium-sized greenspaces in the courtyard with central and encircling walkway to provide access to commercial entrances.

Further details regarding tree planting will be addressed through Site Plan Control.



# DESIGN DIRECTIVES:

**PANEL RECOMMENDATIONS:**

**PROJECT TEAM RESPONSE:**

**SUSTAINABILITY:**

The Panel recommends greatly reducing the number of parking spaces provided.

The Panel appreciates the applicant’s exploration of affordable rental units and supports making the building as affordable as possible for residents.

The Panel has concerns with the excessive amount of parking spaces being provided.

The number of parking spaces has been reduced from 748 to 653 spaces. While the site is located close to many amenities and frequent transit, vehicle use remains high in Ottawa suburban areas and there remain challenges to rental uptake in the absence of vehicle parking. All parking has been located below grade and has the potential to be used for other purposes in the future should parking needs decrease.

Acknowledged.

**BUILT FORM & ARCHITECTURE:**

The Panel appreciates the applicant’s exploration of affordable rental units and supports making the building as affordable as possible for residents.

The project as revised proposed 580 residential units ranging in size from studio to three-bedrooms. The property is designated as a Town Centre Hub and a Protected Major Transit Station Area by the City Official Plan. The property is further designated as Mixed Use Neighbourhood by the Downtown Barrhaven Secondary Plan. The property, and surrounding area, are intended for high-density development within walking distance of a range of amenities and public transit options.

# DESIGN DIRECTIVES:

## PANEL RECOMMENDATIONS:

## PROJECT TEAM RESPONSE:

### BUILT FORM & ARCHITECTURE (cont.):

Continuation of previous comment:

The January 2023 CMHC Rental Market Report noted that the rental vacancy rate for the Ottawa-Gatineau region has fallen to 2.1%, with average rents increasing by 17% on new leases. In Nepean specifically, the rental vacancy rate is 1.7%. These rates are considered very constrained. A high quality of life can be provided in higher density neighbourhoods through the provision of high quality public realm and public spaces, providing reliable alternatives to private vehicles for mobility, and ensuring a range of amenities (retail, services, education, recreational, etc.). The proposed development will be supported by, and contribute to, the provision of these amenities.

The Panel has concerns with how tight the separation distances are between the towers.

Tower floorplates have been reduced to the extent feasible to increase tower separation distances and increase step-backs. Tower heights have also been reduced to reduce wind tunnelling. It should be noted that floorplate areas, separation distances, etc. are guidelines only, and not regulations or required. The revised proposed seeks to balance the best practices of the design guidelines with the economic and spatial constraints of the property.

The Panel has concerns with the sizes of the floorplates proposed.

- Consider reducing the floorplate sizes to help increase the separation distance between towers.
- The Panel recommends designing more slender towers to foster a better quality of life for those living in the units between the two towers and help mitigate privacy concerns.
- The Panel recommends reviewing the City of Ottawa Guidelines on tower separation.

The towers propose floorplates of 750 and 765 m<sup>2</sup> and are consistent with the design guidelines.

# DESIGN DIRECTIVES:

<b>PANEL RECOMMENDATIONS:</b>	<b>PROJECT TEAM RESPONSE:</b>
<p><b>BUILT FORM &amp; ARCHITECTURE(cont.):</b></p> <p>The Panel has concerns with the density of development being proposed.</p> <ul style="list-style-type: none"> <li>Is this concentration of units necessary in this area of the city?</li> </ul>	<p>See comment above. Barrhaven Downtown has been designated by the Official Plan, and supported by the Secondary Plan, to accommodate significant development and growth with the intent of developing into a compact, walkable, and vibrant mixed-use district.</p>
<p>The Panel recommends stepping the towers back considerably along the street frontages</p> <ul style="list-style-type: none"> <li>Consider a minimum 3 metre step-back above the 4-storey podium</li> <li>Consider additional step-backs up the tower.</li> </ul>	<p>Step-backs have been provided from Marketplace Avenue and McGarry Terrace. Tower floorplates have been reduced to the extent feasible to increase tower separation distances and increase step-backs.</p>
<p>The Panel supports the current proposed massing over the previous studies shown in the presentation.</p>	<p>Acknowledged.</p>
<p>The Panel has strong concerns regarding the size of the floorplates proposed.</p> <ul style="list-style-type: none"> <li>The Panel recommends reducing the floorplates to a maximum size of 750 square metres, as per the City of Ottawa guidelines</li> </ul>	<p>The towers propose floorplates of 750 and 765 m2 and are consistent with the design guidelines.</p>
<p>The Panel recommends investigating the possibility of a 35-metre separation between the two towers.</p> <ul style="list-style-type: none"> <li>Consider tower separation as a remedy to wind-tunnelling and improving the micro-climate of the POPS.</li> </ul>	<p>Tower floorplates have been reduced to the extent feasible to increase tower separation distances and increase step-backs. Tower heights have also been reduced to reduce wind tunnelling.</p>
<p>The Panel recommends staggering the heights of the towers to a more appropriate scale and transition from the surrounding context.</p> <ul style="list-style-type: none"> <li>Consider a 30-storey tower on the west side nearest to McGarry Terrace, and a shorter tower closer to 20-storeys on the east side</li> </ul>	<p>To ensure that there is variation in height between the two towers, it is proposed that the west tower be reduced to 35 storeys in height, whereas the east tower would be reduced to 26 storeys.</p>

# DESIGN DIRECTIVES:

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## ***PREVIOUS CONSIDERATIONS:***

As a response to the UDRP recommendations provided on June 2<sup>nd</sup>, 2023, and Phase 2 Pre-Consultation meeting Nov 10<sup>th</sup>, 2023 :

1. We have reduced the building height and number of storeys from 40 to 35 at the west tower and from 30 to 26 at the east tower. We have also reduced the number of units from 619 to 592.
2. The area of the tower floor plates have been reduced to less than 750 m<sup>2</sup> per tower.
3. The massing along Marketplace has been revised to create more stepping from the podium level to the tower as well as between the podium and the residential link. The central podium to the residential link currently provides a 3m setback between levels. The southeast corner of the west tower has been setback about 4.75m from the previous design. The southwest corner of the east tower has been setback an additional 2.1m from the previous design. The east tower set back to the property line has been increased to 11.65m. We have stepped back the east and west building facades by 1.5m allowing for additional stepping from the podium.
4. GJA Landscape Architect have been assisting in developing the publicly accessible courtyard into a more community driven gather space and creating more connectivity with the public realm. The new landscape strategy creates more open and accessible gather spaces, a larger green space in the core of the space. With this landscaping strategy we have also introduced a colonnade of city trees along Marketplace Ave & McGarry Terrace extension.
5. We have considered the accessibility of the tower from the courtyard and created an additional exit/entry point for the residents to directly access the courtyard.
6. The number of parking spaces have been reduced from 749 spaces to 653 spaces.
7. The overall building expression been updated by detailing the base podium levels matching the neighboring developments. The tower assembly was simplified by applying uniformity to the form and more glass elements to lighten the overall massing. Considerations were made on how to simplify the building massing at its penthouse levels as well. We kept some of the vertical elements that were previously provided but simplified the overall masses to cap the structure.

# SITE, CONTEXT & ANALYSIS:

## EXISTING SITE CONDITIONS- PROPERTY DESCRIPTION

The property is located north of the intersection of Marketplace Avenue and Sue Holloway Drive. While the property fronts on Marketplace Avenue, the property is addressed as 1034 McGarry Terrace due to it having approximately 20 m of frontage onto McGarry Terrace. The site is currently vacant and is being used for construction logistics related to the adjacent development.

The property is rectangular in shape with 70.4 m of frontage on Marketplace Avenue, 20 m of frontage on McGarry Terrace, and an area of 5,194 m<sup>2</sup> (1.28 acres). The property is legally described as *Part 1 on Plan 4R-31372 and Part 1 on Plan 4R-33239, also known as Part of Part 3 on Plan 5R-4730, subject to an easement over Part 63 on Plan 4R-34704, subject to an easement over Part 1 on Plan 4R-31372 in favour of the City of Ottawa as in OC2058616, Part of Lot 15 Concession 2 (Rideau Front, Geographic Township of Nepean, now City of Ottawa).*



Part 1 on 4R-31372 is a future extension of McGarry Terrace south across the property to the intersection of Marketplace Avenue and Sure Holloway Drive. Water, sanitary, and storm sewers were installed beneath the future right-of-way in 2018 to facilitate development north of the property. Part 1 remains under Kionas ownership with an easement in favour of the City for infrastructure access.

The site is located immediately west of Haven Towers (150 Marketplace Avenue), a high-rise mixed use development constructed by Kionas and nearing completion. Vehicular access to Haven Towers straddles the property line with the subject property and maintained through mutually beneficial reciprocal easements.

Aerial imagery of the site (orange) and surrounding context. Construction south, east, and north of the site includes high-rise residential and mixed use development.

# SITE, CONTEXT & ANALYSIS:

## EXISTING SITE CONDITIONS- IMAGES



Existing site from Southwest corner.



Existing site from Southeast corner.



Aerial of existing site.

# SITE, CONTEXT & ANALYSIS:

## SITE CONTEXT AND SURROUNDING AREAS



The site is located in Barrhaven Downtown, the core of the City's southwest suburban community that includes Barrhaven, Riverside South, and Barrhaven South.

# SITE, CONTEXT & ANALYSIS:

## LAND USES SURROUNDING THE SITE

### NORTH

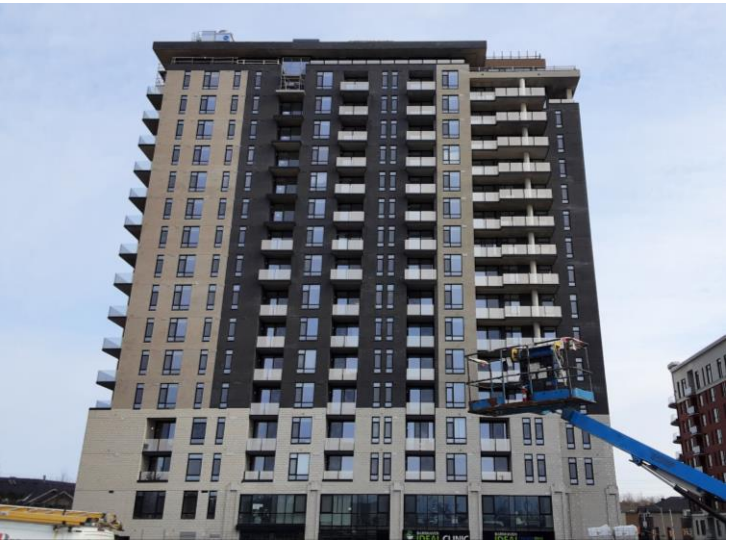
Howard Grant Terrace (1024 McGarry Terrace), an 18-story residential building, is located immediately north of the site and was constructed in 2019. McGarry Terrace currently ends at the northwest corner of the site in a temporary turnaround circle. North of Howard Grant Terrace is Dymon Storage (1000 McGarry Terrace), a five-storey self-storage warehouse building with at-grade retail at the corner of McGarry Terrace and Strandherd Drive. Further, separated by Strandherd Drive to the north, the neighbourhood transitions to stacked townhomes and detached dwellings.



Howard Grant Terrace from corner of Marketplace Ave & Sue Holloway.

### EAST

Haven Towers (150 Marketplace) is located immediately east of the site and is composed of two towers of 17 and 15-storeys above a seven-storey podium with at-grade retail facing Marketplace Avenue and Longfields Drive. Northeast of the site is The Court at Barrhaven, a three-storey independent living community operated by Atria Retirement. Further east, separated by Longfields Drive, is *École secondaire catholique Pierre-Savard* (1110 Longfields Drive), a French language school accommodating grades 7 to 12. The neighbourhood southeast of the corner of Longfields Drive and Clearbrook Drive is composed of stacked townhomes and townhouses and, separated by Strandherd Drive to the north, the neighbourhood transitions to stacked townhomes and townhomes.



Under construction Haven Towers on proposed site, looking east.



# SITE, CONTEXT & ANALYSIS:

## LAND USES SURROUNDING THE SITE

### SOUTH

A nine-storey residential building- the Bristol (125 Marketplace Avenue) and eight-storey retirement community- Waterford Grand (121 Marketplace Avenue) are located south of Marketplace Avenue opposite the site. Lindenshade Park is located South of Waterford Grand in a neighbourhood composed of four-storey low-rise apartment buildings and stacked townhomes.



The Bristol from corner of Marketplace Ave & Sue Holloway.

### WEST

The extension of McGarry Terrace to Sue Holloway Drive will bound the site to the west. West of the site, on the opposite site of the McGarry Terrace extension, is Chapman Mills Marketplace, a large-format shopping centre with extensive surface parking. A Wal-Mart Supercentre abuts McGarry Terrace with its principal entrance facing west. Chapman Mills Marketplace includes a range of retail and service uses. Marketplace Station is located 350 m west of the property.



Marketplace Ave looking towards Longfields Drive.

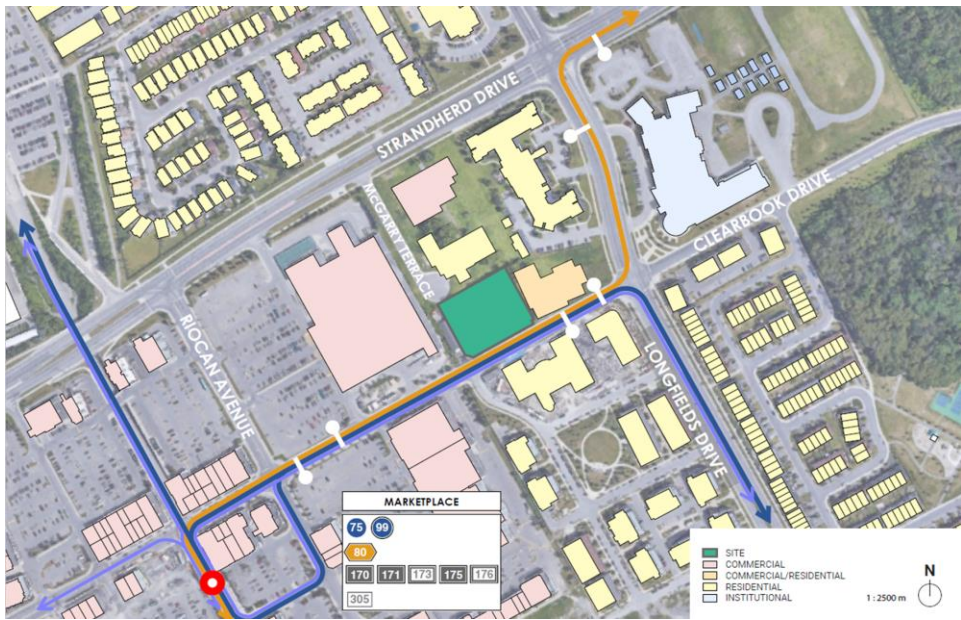
# SITE, CONTEXT & ANALYSIS:

## TRANSPORTATION AND MOBILITY

The property abuts Marketplace Avenue, a two-lane collector street with off-peak on street parking and a posted speed limit of 50 km/h.

Barrhaven Downtown is currently served by the City’s bus rapid transit network with several stations along the Southwest Transitway and the Chapman Mills Transitway. The area is also well-served by frequent, regular, and peak bus routes. The following bus routes listed have stops within 400 m (five-minute walk) from the property.

Routes 75 and 173 will connect to Baseline Station upon completion of the light rail Line 1 extension (target completion 2025). Route 99 will connect to Leitrim Station upon completion of the light rail Line 2 extension (target completion late 2023). An environmental assessment study is currently underway to extend Line 1 from Baseline Station to Barrhaven Town Centre Station as part of a third stage of light rail expansion.



**Aerial of site (green) and surrounding rapid (blue), frequent (orange), and regular (purple/grey) bus routes. Marketplace Stations is located less than 44 m from the site.**

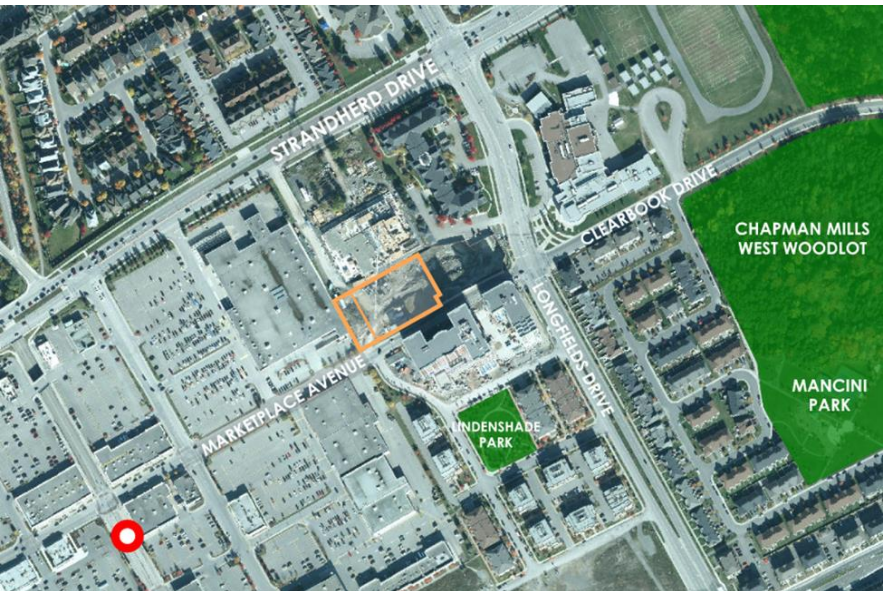
Route	Distance to Closest Stop*	Peak Weekday Frequency	Weekday Operating Range	Sunday Operating Range
75	350 m (5 minutes)	10 minutes	03:28 – 02:58	03:32 – 02:39
99	60 m (1 minute)	15 minutes	04:51 – 00:28	06:24 – 22:11
80	60 m (1 minute)	30 minutes	05:03 – 23:33	07:16 – 22:46
170	350 m (5 minutes)	30 minutes	05:30 – 22:57	05:56 – 22:56
171	350 m (5 minutes)	30 minutes	05:33 – 19:01	09:52 – 15:52
173	350 m (5 minutes)	30 minutes	06:18 – 19:01	none
175	60 m (1 minute)	60 minutes	06:36 – 22:41	19:50 – 19:50
176	60 m (1 minute)	60 minutes	05:56 – 18:57	none

\* Note: measured to closest decametre.

# SITE, CONTEXT & ANALYSIS:



Illustration of pedestrian mobility surrounding site.



The site is within a short walk of three different types of parks.

## TRANSPORTATION AND MOBILITY

The site and surrounding areas are connected by an interconnected network of sidewalks, trails, and pathways. Sidewalks are located along both sides of surrounding arterial, collector, and local streets including Marketplace Avenue, Sue Holloway Drive, Longfields Drive, and Clearbrook Drive. Sidewalks and pathways provide direct connections to commercial service and retail uses, schools, parks, and open space areas. Cycling lanes are located along Strandherd Drive and Longfields Drive and link to multiuse pathways (MUPs).

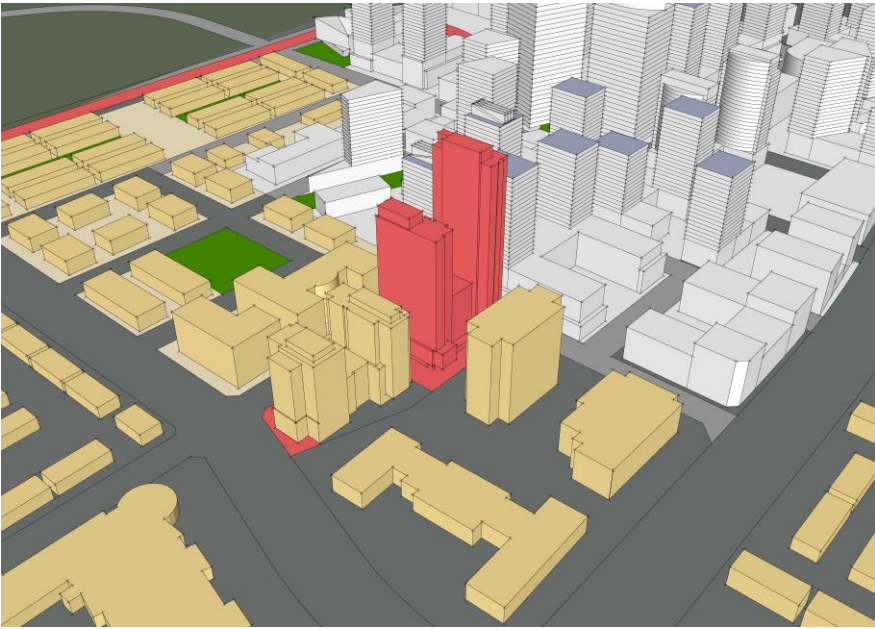
## PARKS & OPEN SPACE

A variety of parks and open spaces are located within walking distance of the property and provide a range of recreational opportunities. Lindenshade Park is an urban parkette located 120 m south of the property. The park has an area of 0.38 hectares and several areas for recreation and unstructured play. Mancini Park is a neighbourhood park located 450 m southwest of the property. The park has an area of 1.8 hectares and includes a splashpad, playground area, two full tennis courts and one half tennis court. Chapman Mills West Woodlot is a woodland park of low coniferous upland forest with minor wetland and hardwood elements straddling Clearbrook Drive 250 m east of the property. The park has a combined area of 8.8 hectares and includes passive recreation space including trails and pathways connecting to Mancini Park.

# SITE, CONTEXT & ANALYSIS:

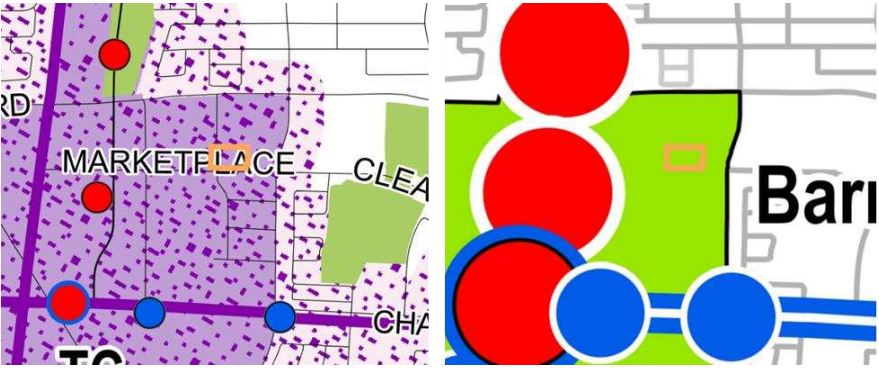
## FUTURE & CURRENT DEVELOPMENTS

Barrhaven Downtown is identified by the City's Official Plan as one of three Town Centre Hubs intended to become the most important and largest Hub of their suburban community with employment and more urban-type development.



Conceptual sketch showing existing (yellow) and potential future development (white) of Barrhaven Downtown based on the policies of the secondary plan.

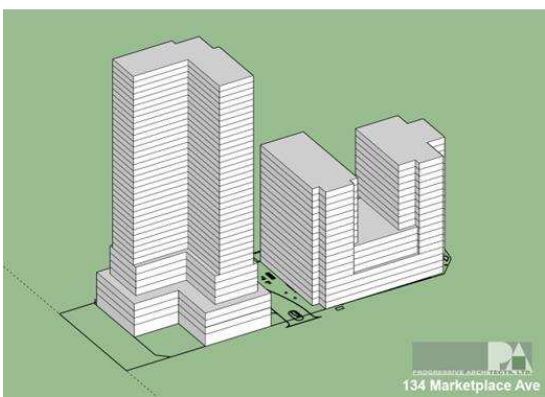
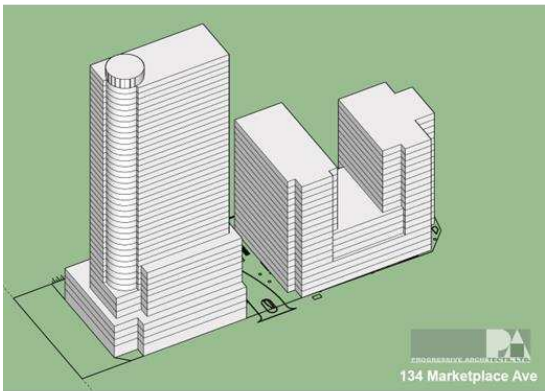
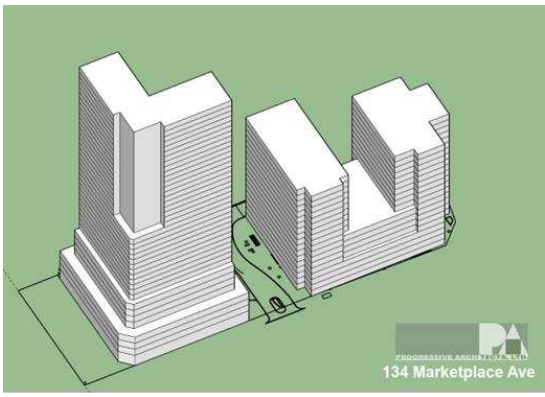
The property is within the Suburban Transect (Schedule A) and designated Town Centre Hub with an Evolving Overlay Schedule B6) by the Official Plan. The property is also located within a Protected Major Transit Station Area (PMTSA) (Annex C1) and the Airport Vicinity Development Zone (Annex C14). Generally, Hubs permit "a diversity of functions, higher density of development, greater degree of mixed uses and higher level of public transit connectivity than the areas abutting and surrounding the Hub".



The site is within the Suburban Transect and designated Hub (Town Centre) and Evolving Overlay (left). Barrhaven Downtown is a Protected Major Transit Station Corridor (PMTSA) (right).

# DESIGN RESEARCH:

## MASSING STUDY



## CONCEPT ONE



We started our design by examining the neighboring developments first from the development at the north which shows one tower with a large footprint over 16,500 ft<sup>2</sup> and a 5 storey base and second from the eastern tower development which shows two slimmer towers properly separated.

To respond to the needs of the investment we created first similar concept like the eastern towers which was submitted to planning for review.

## CONCEPT TWO



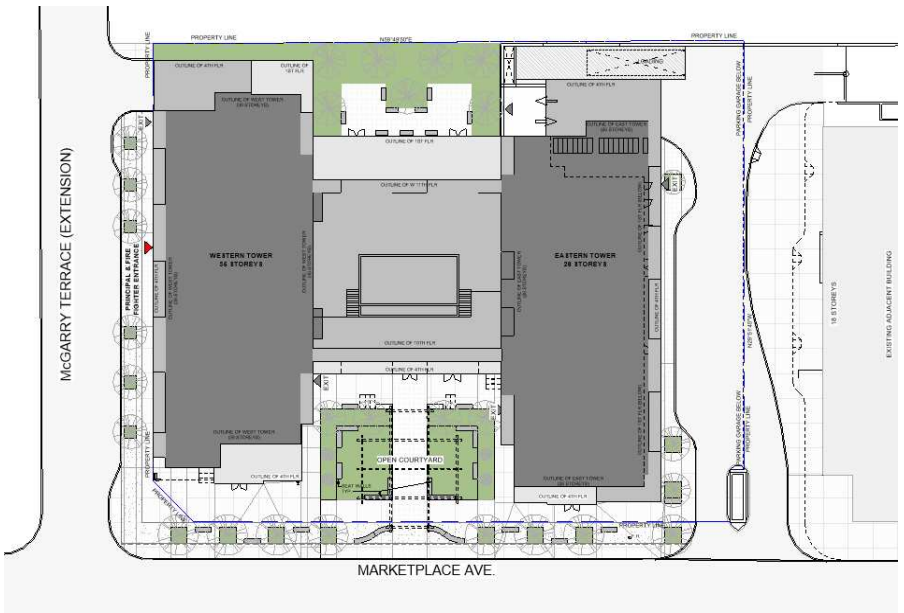
We created a base that provided many open spaces. Unlike the eastern towers and unlike the building on the north, we created a concept that blended and enriched the public realm. The centre of the base was drawn back reducing the overall footprint, providing a generous size courtyard along Marketplace.

# DESIGN RESEARCH:

## CURRENT DESIGN CONCEPT

### SITE

- The design offers two generous size courtyards. Marketplace Ave is provided with the larger of the two, creating a positive connection to the street and community. This courtyard will be provided as public space.
- Both sides of the front courtyard will be set back from the street between 6.6m to 9.2 m, creating a total public use area of 937 m<sup>2</sup>. The surrounding occupancy of the courtyard is intended to be used by shops, restaurants and other public use activities.
- At the back there will be a residential courtyard at an area of around 290 m<sup>2</sup> with a landscaped link to McGarry Terrace extension. This courtyard is suggested to be used by the residents of the development which will be connected to the first-floor amenities facing the rear courtyard.
- The exterior public area at the first-floor level totals more than 25% of the site.
- The design of the building and the site is intended to provide as many as possible publicly used spaces along the Marketplace and McGarry Terrace, as well as the two sides of the front courtyard to allow for a smooth public-private transition. The commercial space along the east provides a pedestrian arcade creating a strong pedestrian and commercial connection.
- The principal entrance of the building is proposed to face McGarry Terrace Extension and linked to the east building with a middle lounge.
- Parking garage access has been provided at the back of the building to maximize the pedestrian connections along Marketplace and McGarry Terrace. The intent is to also hide or screen all loading and building services. Drop off and pick up locations have also been provided along McGarry Terrace and the east driveway.



# DESIGN RESEARCH:

## CURRENT DESIGN CONCEPT:

### BUILDING DESIGN

- The proposed building consists of a base podium and two towers. The 4 storey base podium is meant to have a shape that provides openness to the outside and to allow strong connection between the interior and exterior spaces.
- The façade treatment of the podium base will be mainly of masonry cladding. The detailing and floor heights of the base podium will be in line with the façade articulation of the eastern towers under construction. The base masses will be broken vertically and horizontally with façade articulation down to human scale elements within rhythm while keeping most of the base walls transparent.
- In between the two towers, the proposal is to have a 6-storey link which will house residential units and amenities.
- The two towers, 35 and 26 storeys in height, are designed with small footprints. Each with a GFA less than 750 m<sup>2</sup>.



# DESIGN RESEARCH:

## ***CURRENT DESIGN CONCEPT:***

### ***BUILDING DESIGN***

- The separation distance between the two towers is 23m.
- The concept behind the two towers is to achieve the largest building perimeter which in return will provide plenty of exterior wall exposure and sunshine to the units.
- The detailing of the tower facades will be predominantly glass and metal panels to provide the feeling of light massing. Residential units will be provided with balconies that are to be surrounded with glass guardrails covering the balcony slab edges similar to the eastern development under construction.
- The 10<sup>th</sup> floor of the link portion will be dedicated to amenities including a party room, exercise room, golf simulator and an infinity outdoor pool at the roof. With a roof top terrace and an infinity pool on the 12<sup>th</sup> floor level.

### ***SUSTAINABILITY***

- The new concept provides a reduction in the parking count from 748 in the previous design to 653 in the current one.
- Energy efficiency will be achieved through proper building envelope design (airtightness), insulation and thermal values (reduce height loss), proper M&E systems (reduce energy consumption), and eco-friendly products.
- The design Proposes quite a few landscaped and amenity areas which will help reduce urban heat island effects.
- We will study the feasibility of using photo-voltaic cladding panels as an alternative energy source to help reduce electrical and heating costs.
- The residents will have access to community gardens with the rear courtyard which contributes to their overall health and wellbeing.





# DESIGN RESEARCH:

## MASSING – EXISTING CONTEXT



**SOUTHEAST AERIAL**



**SOUTHWEST AERIAL**

## MASSING – FUTURE CONTEXT



**SOUTHEAST AERIAL**



**SOUTHWEST AERIAL**

# DESIGN RESEARCH:

## **BUILT FORM TRANSITION BETWEEN PROPOSED AND SURROUNDING AREA**

The towers and link portions of the building are positioned above a four-storey podium that frames the abutting streets and encloses the proposed courtyard on three sides. A four-storey podium (approximately 16 metres in height) provides a 2:1 ratio of building height to distance from the façade of the mid-rise building on the south side of Marketplace Avenue. The building massing is similar to that of the Havens to the east and provides an urban and active courtyard space which mirrors a courtyard with vehicle drop off area on the south side of Marketplace. The towers have been set back from the street edge established by the podium and are differentiated by a change in materials as well as more pronounced building articulation along the south and west elevations. The west tower—at 35 storeys, has a floorplate of 744m<sup>2</sup>, whereas the east tower—at 26 storeys, has a floorplate of 736m<sup>2</sup>. Each tower terminates with a two-storey portion with significantly smaller floorplates to visually narrow their appearance and create a more defined silhouette.



# DESIGN RESEARCH:

## **BUILT FORM TRANSITION BETWEEN PROPOSED AND SURROUNDING AREA**

The ground floor of the proposed development will contain commercial/retail uses, with residential amenities located on the 1st and 10th floor, with a roof top terrace on the 12<sup>th</sup>. The remainder of the building will accommodate residential apartment units. Access to the development and underground parking will be via Marketplace Avenue.

The proposed towers provide sufficient separation between each other (23 m) and with the existing tower to the north (31.5 m). The east tower is separated from the westernmost Haven tower by 21m.

The two towers are 90 m from the closest low-rise residential buildings (being the four-storey apartments at 301 Sue Holloway Drive) and 120 m from the closest ground-oriented residential buildings (102 Lindenshade Drive). Both buildings are also within the Barrhaven Downtown Secondary Plan. The existing surrounding built form provides an incremental transition and reduction in heights from the proposed development to low-rise residential areas to the south, north, and east.



# DESIGN RESEARCH:

## **RESPONSE FOR ABUTTING PUBLIC REALM-BEYOND THE SITE BOUNDARY**

### **MARKETPLACE AVENUE**

Marketplace is a collector street and an active frontage street. The proposal provides a wide sidewalk, additional pedestrian space at the corner of Marketplace and McGarry, and contiguous frontage occupied by commercial spaces with entrances directly to the sidewalk. Regularly spaced street trees along the sidewalk to provide separation from vehicle traffic while also providing summer shade. Streetscape design will continue the approved design in front of the Havens to the east to provide a consistent pedestrian experience.



**Southeast corner of building on Marketplace Ave.**

### **MCGARRY TERRACE**

McGarry is a local street with a right-of-way width of 20 m which bounds the development to the west. The street intersects with Marketplace but will provide access to Howard Grant Terrace and the Dymon Storage facility. The proposed development will provide a contiguous four-storey façade along McGarry which includes the principal residential lobby entrance, one commercial space with an active entrance, and another commercial space along the Marketplace corner. An enclosed service bay is provided from the north part of the site to McGarry.



**Southwest corner of building on Marketplace Ave & McGarry Terrace.**

# DESIGN RESEARCH:

## **RESPONSE FOR ABUTTING PUBLIC REALM-BEYOND THE SITE BOUNDARY**

### **MARKETPLACE COURTYARD**

The courtyard will provide a break in the street edge along Marketplace and extend the public realm into the site. The Landscape Strategy envisions a barrier-free publicly accessible courtyard framed by commercial units and occupied by landscaped areas, seating, and a pergola extending to the street. The courtyard design continues along Marketplace to provide a consistent pedestrian experience that invites the public to use the courtyard.



**Marketplace Ave courtyard.**

### **DRIVEWAY ALLEY**

The driveway alley provides site access and servicing requirements with pedestrian comfort. The ground floor is inset from the building edge creating an arcade with commercial spaces. Loading spaces are provided for drop-off/loading.

The development has been designed in a manner which ensures a pedestrian scale along Marketplace Avenue via appropriate building setbacks, variations in building articulation and fenestration. The intention is to provide defined transitions from the public realm to the high-rise built form. The ground floor provides a greater floor to ceiling height for the retail and commercial uses and defines the buildings ground floor relationship to the public realm.



**Driveway alley access and commercial arcade.**

# DESIGN RESEARCH:

## **BIRD SAFE DESIGN**

To avoid monolithic, undistinguished expanses of glazing. The proposal is to use less reflective materials at the podium level. At the towers, to avoid large expanses of glazing we broke the vertical and horizontal spacing to glass mullions to avoid having the glass indistinguishable.

In order to incorporate visual interest and differentiation of finishes. At the podium levels as well as at the towers we introduced different building envelope materials such precast concrete, opaque metal panels, glass and other cladding materials to fragment any glass reflections and add interest.

In response to bird-safe glass requirements since the height of the podium is little more than 16 m the large parts of the glass, particularly the ones that are at the outward walls will be provided with bird-safe glass. As well where green roofs, rooftop gardens, terraces and courtyards are provided special attention will be paid to glass close to these areas towards using bird-safe glass.



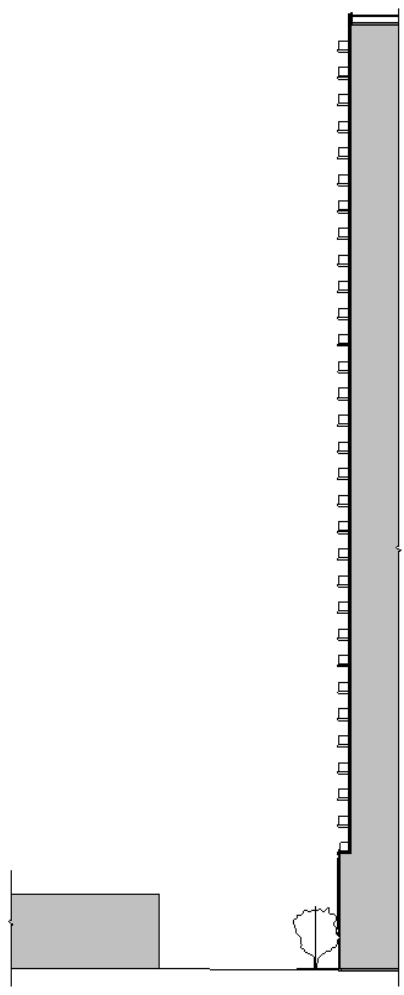
**Southeast corner of building on Marketplace Ave.**



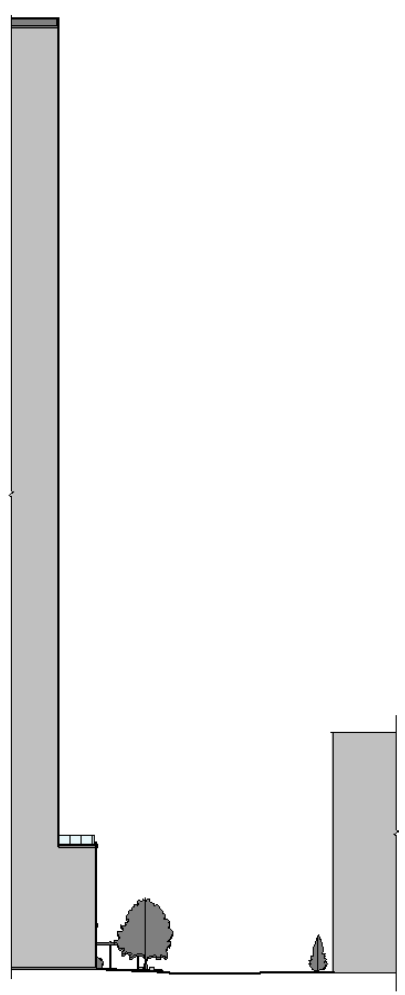
**Southeast Aerial of property.**

# DESIGN RESEARCH:

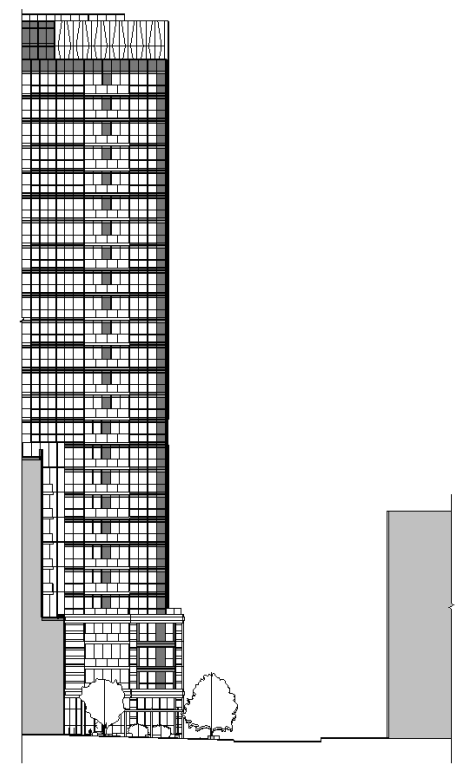
## STREET CROSS SECTIONS- BUILDING WALL TO BUILDING WALL



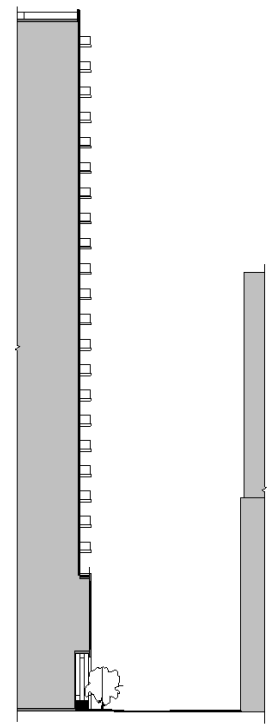
**WEST- MCGARRY TERRACE**



**SOUTH- MARKETPLACE**

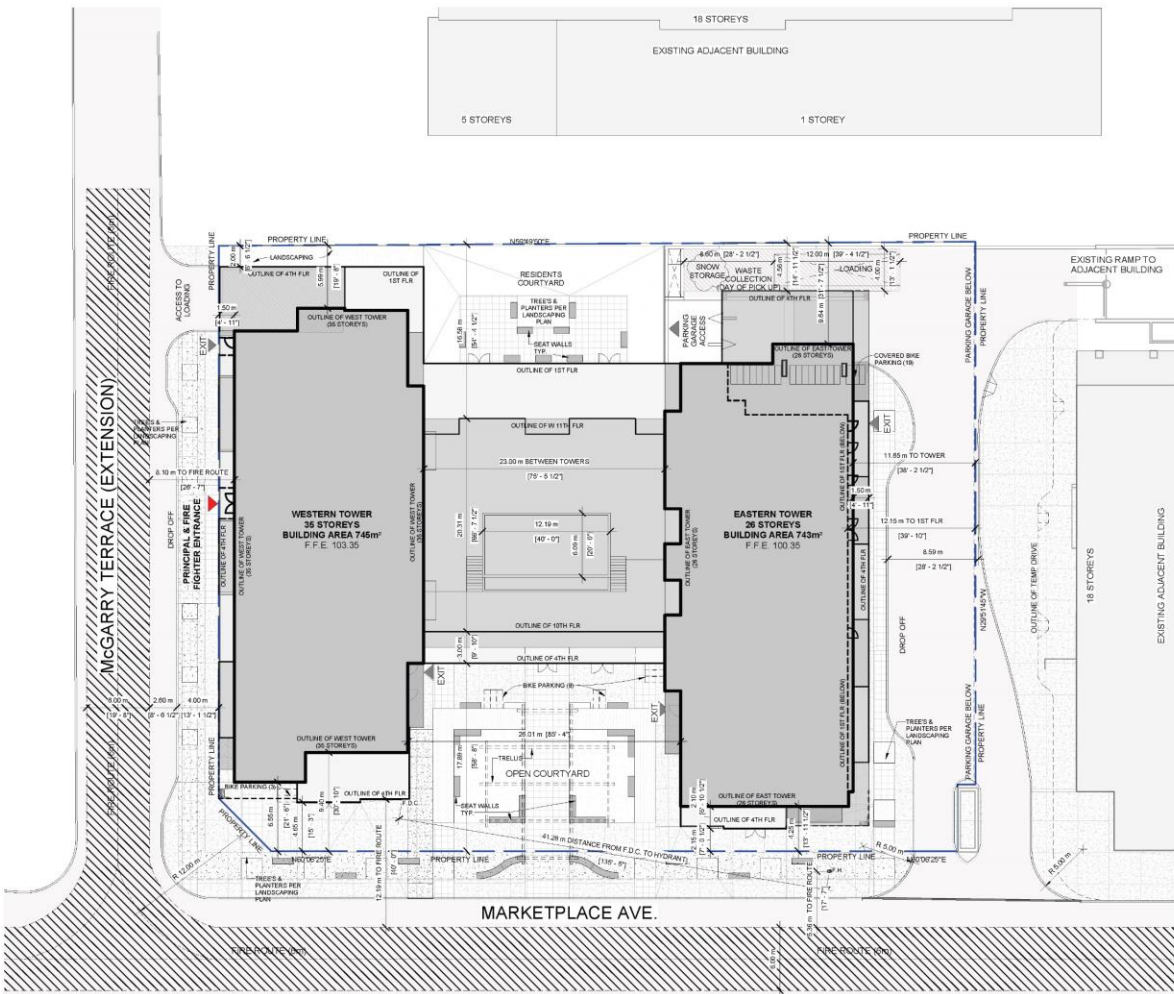


**SOUTH- MARKETPLACE COURTYARD**



**EAST- PRIVATE DRIVEWAY**

# APPENDIX:



1 ARCHITECTURAL SITE PLAN  
1: 200



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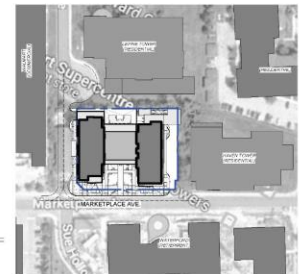


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project: **MARKETPLACE WEST**  
address: 1034 MCGARRY TERRACE, OTTAWA ON  
sheet no.: **ARCHITECTURAL SITE PLAN**

project no.: **22-0039**  
sheet no.: **A001**



2 KEYPLAN  
1: 1500

**CITY OF OTTAWA ZONING BYLAW 2008-250**

PROPOSED USE: 26-STOREY WEST TOWER, 36-STOREY EAST TOWER  
1034 MCGARRY AVE  
1034 MCGARRY AVE  
LOT DEPTH: 31.50M

MINIMUM PROVISION	REQUIRED	ESD/COD	COMPLIANT
LOT WIDTH (M)	NO MINIMUM	4.76M	✓
LOT WIDTH (M)	NO MINIMUM	72.34	✓
FRONT YARD SETBACK (M)	NO MINIMUM	0N, 3.05M	✓
REAR YARD SETBACK (M)	NO MINIMUM	3.05M	✓
REAR YARD SETBACK (M)	NO MINIMUM	0.15M	✓
FLOOR SPACE INDEX (M <sup>2</sup> )	NO MAXIMUM	17.0 FSI	✓
BUILDING HEIGHT (M)	25M	75.5M	✓
BUILDING HEIGHT (M)	57M	131M	X
GROSS FLOOR AREA (M <sup>2</sup> )	38,270M <sup>2</sup>	80,740M <sup>2</sup>	X
LANDSCAPED AREA (M <sup>2</sup> )	NO MINIMUM	1,050M <sup>2</sup>	✓

**PROVISIONS FOR HIGH-RISE BUILDING (SECTION 2.1.1.1.1)**

PROVISION	REQUIRED	ESD/COD	COMPLIANT	
MINIMUM FLOOR AREA (M <sup>2</sup> )	11.75M	5.1M	11.87M	✓

**PARKING PROVISIONS**

PROVISION	REQUIRED	ESD/COD	COMPLIANT
RESIDENTIAL PARKING SPACES (PER 100 RESIDENTIAL UNITS)	50	53	✓
OFFICE PARKING SPACES (PER 100 OFFICE SPACES)	14	14	✓
RETAIL PARKING SPACES (PER 100 RETAIL SPACES)	11	11	✓
VEHICLE PARKING SPACES	432 SPACES	463 SPACES	✓

**VEHICLE PARKING**

PROVISION	REQUIRED	ESD/COD	COMPLIANT
VEHICLE PARKING SPACES (PER 100 RESIDENTIAL UNITS)	50	53	✓
VEHICLE PARKING SPACES (PER 100 OFFICE SPACES)	14	14	✓
VEHICLE PARKING SPACES (PER 100 RETAIL SPACES)	11	11	✓
TOTAL VEHICLE PARKING SPACES	304 SPACES	304 SPACES	✓

**VEHICLE PARKING**

PROVISION	REQUIRED	ESD/COD	COMPLIANT
VEHICLE PARKING SPACES (PER 100 RESIDENTIAL UNITS)	50	53	✓
VEHICLE PARKING SPACES (PER 100 OFFICE SPACES)	14	14	✓
VEHICLE PARKING SPACES (PER 100 RETAIL SPACES)	11	11	✓
TOTAL VEHICLE PARKING SPACES	304 SPACES	304 SPACES	✓

**OTHER PROVISIONS**

PROVISION	REQUIRED	ESD/COD	COMPLIANT
VEHICLE PARKING SPACES (PER 100 RESIDENTIAL UNITS)	50	53	✓
VEHICLE PARKING SPACES (PER 100 OFFICE SPACES)	14	14	✓
VEHICLE PARKING SPACES (PER 100 RETAIL SPACES)	11	11	✓
TOTAL VEHICLE PARKING SPACES	304 SPACES	304 SPACES	✓

**UNIT TYPE COUNT**

UNIT TYPE	2ND-4TH FLR	WEST TOWER	EAST TOWER	TOTAL
1 BED	37	46	-	83 (13.3%)
2 BED	103	72	76	251 (41.3%)
3 BED	79	95	74	248 (41.3%)
3 BED	16	1	-	17 (2.9%)
<b>TOTAL</b>	<b>234</b>	<b>214</b>	<b>149</b>	<b>597 UNITS</b>

**BUILDABLE AREA**

LEVELS	PER FLR	# OF FLRS	TOTAL
P1 PARKING	3033 m <sup>2</sup>	1 FLRS	3033 m <sup>2</sup>
P2-P6 PARKING	4728 m <sup>2</sup>	5 FLRS	23640 m <sup>2</sup>
P7-LOWER LEVEL	2007 m <sup>2</sup>	1 FLRS	2007 m <sup>2</sup>
<b>TOTAL BELOW GRADE</b>			<b>29680 m<sup>2</sup></b>
1ST FLR	2289 m <sup>2</sup>	1 FLR	2289 m <sup>2</sup>
2ND-4TH FLR	2287 m <sup>2</sup>	3 FLRS	6861 m <sup>2</sup>
5TH-9TH FLR	1962 m <sup>2</sup>	5 FLRS	9790 m <sup>2</sup>
10TH-11TH FLR	1969 m <sup>2</sup>	2 FLRS	3938 m <sup>2</sup>
<b>TOWERS</b>			
12TH-36TH WEST	745 m <sup>2</sup>	24 FLRS	17880 m <sup>2</sup>
12TH-36TH EAST	743 m <sup>2</sup>	16 FLRS	11888 m <sup>2</sup>
<b>TOTAL ABOVE GRADE</b>			<b>51738 m<sup>2</sup></b>

**VEHICULAR PARKING SPITAL TYPE**

STANDARD	48 SPACES
P1 PARKING	12 SPACES
P2-P6 PARKING (61 PER LEVEL)	266 SPACE PER FLR
<b>TOTAL PARKING</b>	<b>463 TOTAL</b>

**BIKE PARKING COUNT**

GROUND FLR	30 SURFACE SPACES
P1 PARKING	12 SPACES
P2-P6 PARKING (61 PER LEVEL)	266 SPACE PER FLR
<b>TOTAL PARKING</b>	<b>304 TOTAL</b>



APPENDIX:

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***LANDSCAPE PLAN***

APPENDIX:

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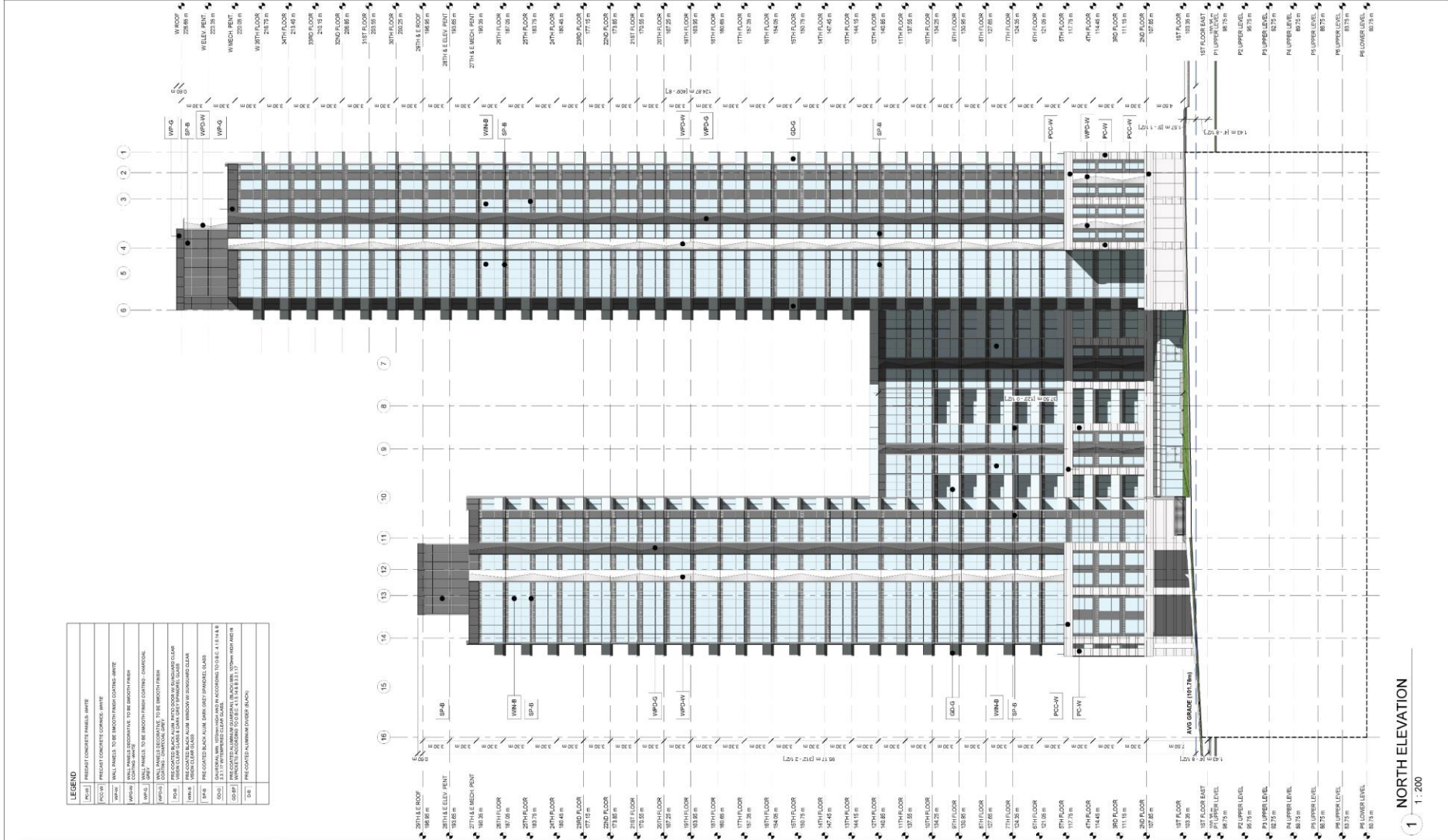
***PLAN OF SUBDIVISION***

APPENDIX:

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***GRADING AND DRAINAGE PLAN***

# APPENDIX:



**LEGEND**

PC-2	PRECAST CONCRETE FINISH, WHITE
PC-3	PRECAST CONCRETE, CONCRETE, WHITE
PC-4	WALL PANELS TO BE SMOOTH FINISH COATING, WHITE
PC-5	WALL PANELS TO BE SMOOTH FINISH COATING, WHITE
PC-6	WALL PANELS TO BE SMOOTH FINISH COATING, WHITE
PC-7	WALL PANELS TO BE SMOOTH FINISH COATING, WHITE
PC-8	WALL PANELS TO BE SMOOTH FINISH COATING, WHITE
PC-9	WALL PANELS TO BE SMOOTH FINISH COATING, WHITE
PC-10	WALL PANELS TO BE SMOOTH FINISH COATING, WHITE
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PC-27	WALL PANELS TO BE SMOOTH FINISH COATING, WHITE
PC-28	WALL PANELS TO BE SMOOTH FINISH COATING, WHITE
PC-29	WALL PANELS TO BE SMOOTH FINISH COATING, WHITE
PC-30	WALL PANELS TO BE SMOOTH FINISH COATING, WHITE

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**ON-TARIO ASSOCIATION OF ARCHITECTS**  
 SANDA A. KISHAN  
 ARCHITECT

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project: **MARKETPLACE WEST**  
 address: 1034 MCGARRY TERRACE, OTTAWA ON  
 sheet name: **NORTH ELEVATION**

project no.: **22-0039**  
 sheet no.: **A401**



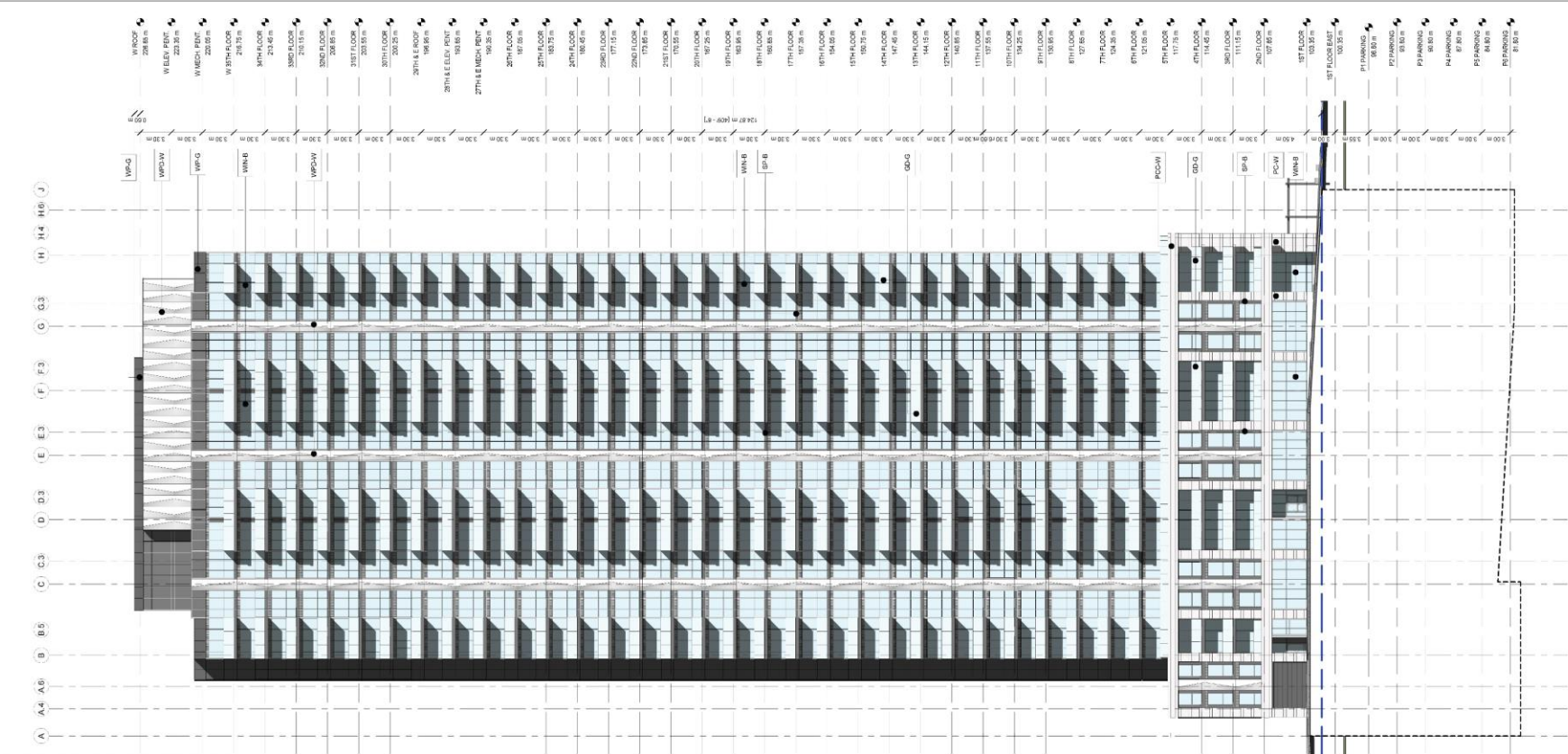








# APPENDIX:



**LEGEND**

FAÇADE	PRECAST CONCRETE PANELS, ANNEZ
POUR	PRECAST CONCRETE CORNER, ANNEZ
W1000	WALL PANELS TO BE BRUSHED FINISH CONTING ANNEZ
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W1024	WALL PANELS TO BE BRUSHED FINISH CONTING ANNEZ
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W1030	WALL PANELS TO BE BRUSHED FINISH CONTING ANNEZ
W1031	WALL PANELS TO BE BRUSHED FINISH CONTING ANNEZ
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1 WEST ELEVATION- WEST TOWER  
1:200



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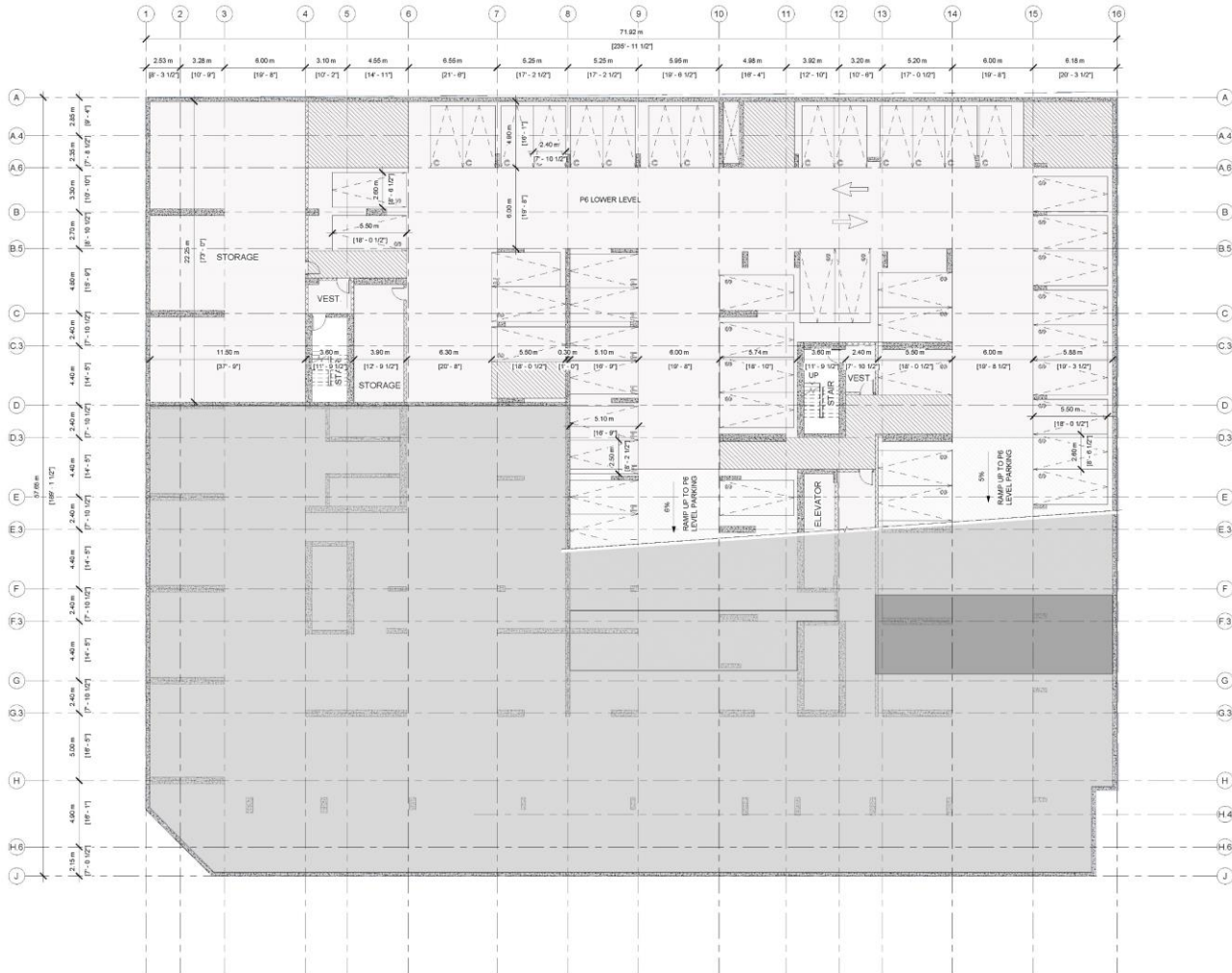
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**MARKETPLACE WEST**  
 address: 1034 MCGARRY TERRACE, OTTAWA ON  
 sheet name:  
**WEST ELEVATION- WEST TOWER**

project no.:  
**22-0039**  
 sheet no.:  
**A404(W)**



# APPENDIX:



P6 LOWER LEVEL PARKING	
XRSOLAR PARKING	
STANDARD	35 SPACES
COMPACT	35 SPACES
TOTAL PARKING	40 TOTAL

1 P6 LOWER LEVEL PARKING PLAN  
1 : 150



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SAAD A. KHALAF  
LICENSED ARCHITECT

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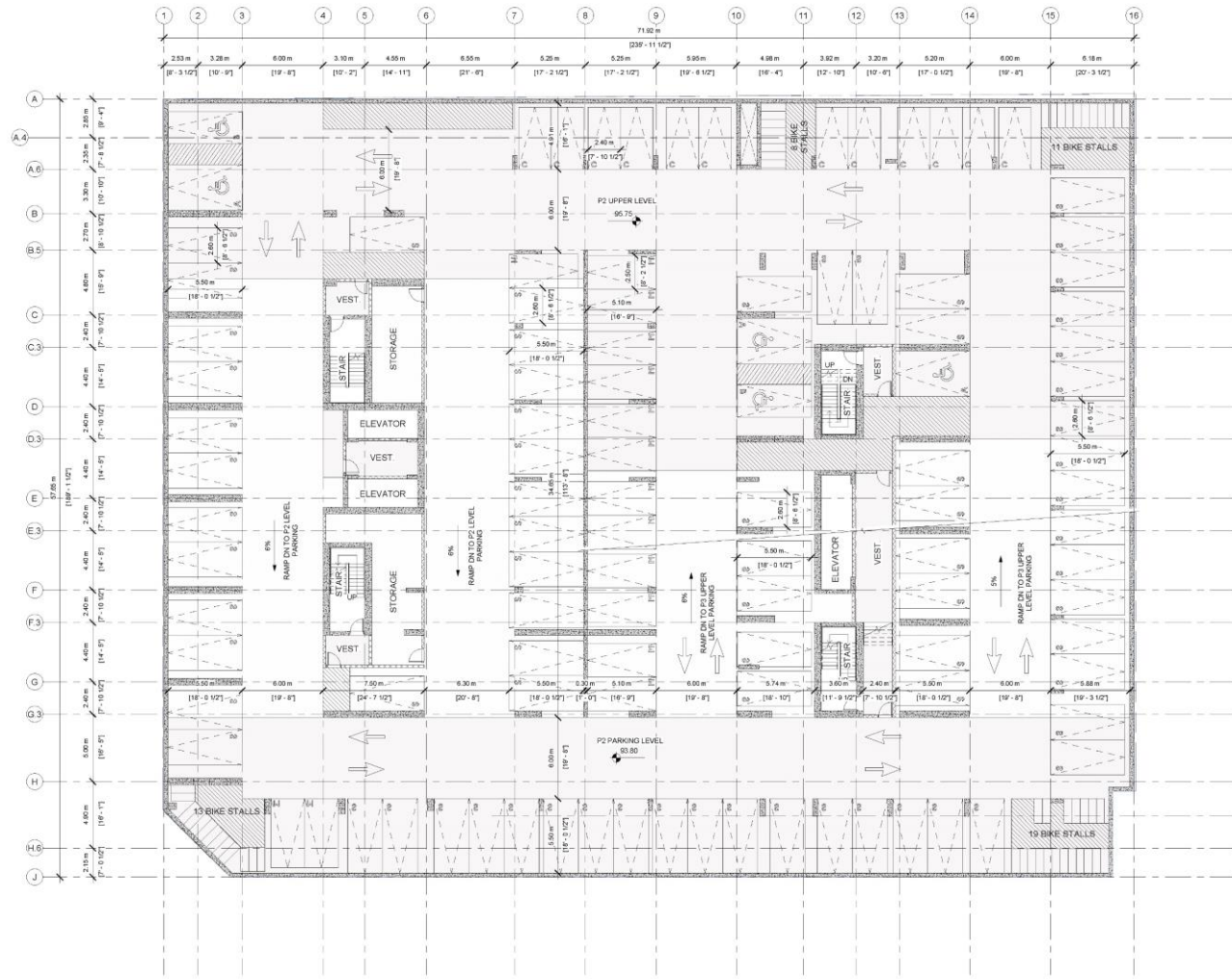
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address: 1034 MCGARRY TERRACE, OTTAWA ON

project no.: **22-0039**

sheet no.: **A107**

sheet name: **P6 LOWER LEVEL PARKING PLAN**

# APPENDIX:



P2-P6 PARKING LEVELS (PER LEVEL)	
<b>VEHICULAR PARKING:</b>	
STANDARD	24 SPACES
COMPACT	27 SPACES
TYPE 'B' BARRIER FREE	3 SPACES
TYPE 'W' BARRIER FREE	2 SPACES
<b>TOTAL PARKING (PER FLD.)</b>	<b>56 TOTAL</b>
<b>BIKE PARKING:</b>	
<b>TOTAL PARKING</b>	<b>51 TOTAL</b>

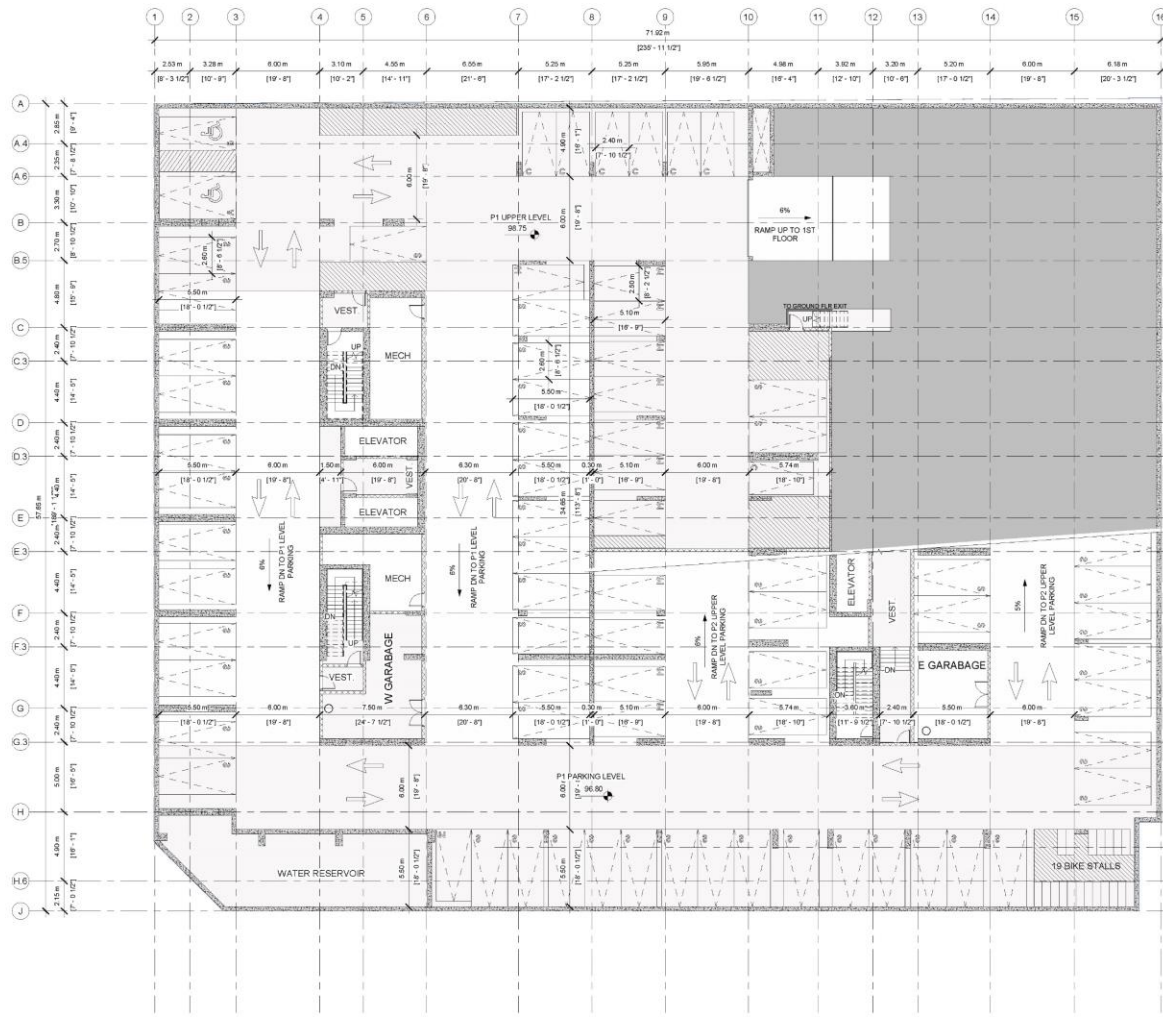
TOTAL PARKING P2-P6 LEVELS	
<b>ACCUMULATIVE VEHICULAR PARKING OF P2-P6 LEVELS:</b>	
STANDARD	370 SPACES
COMPACT	18 SPACES
TYPE 'B' BARRIER FREE	10 SPACES
TYPE 'W' BARRIER FREE	10 SPACES
<b>TOTAL PARKING (2 LEVELS)</b>	<b>390 TOTAL</b>
<b>ACCUMULATIVE BIKE PARKING OF P2-P6 LEVELS:</b>	
<b>TOTAL PARKING (2 LEVELS)</b>	<b>350 TOTAL</b>

1 P2- P6 TYPICAL PARKING PLANS  
1 : 150



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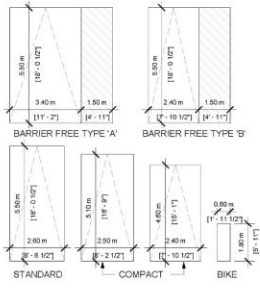


**P1 PARKING LEVEL**

<b>VEHICLE PARKING:</b>	
STANDARD	53 SPACES
COMPACT	28 SPACES
TYPE 'A' BARRIER FREE	1 SPACE
TYPE 'B' BARRIER FREE	1 SPACE
<b>TOTAL PARKING</b>	<b>75 TOTAL</b>

<b>BIKE PARKING:</b>	
<b>TOTAL PARKING</b>	<b>18 TOTAL</b>

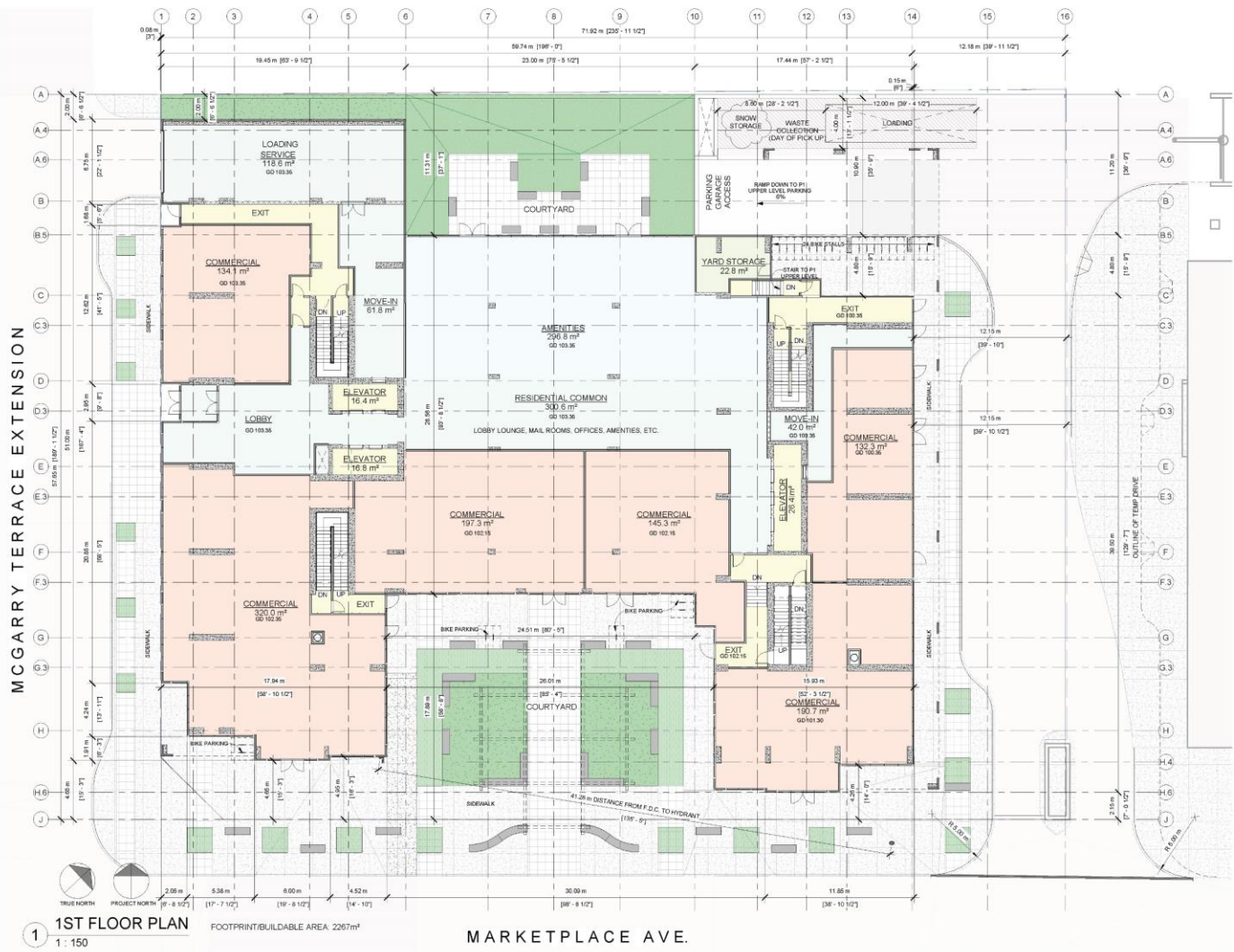


**1 P1 PARKING PLAN**  
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# APPENDIX:



1ST FLOOR BIKE	
BIKE PARKING:	
TOTAL PARKING:	39 TOTAL

PLAN KEY	
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<span style="display:inline-block; width:10px; height:10px; border:1px solid black; background-color:lightblue;"></span>	SERVICE
<span style="display:inline-block; width:10px; height:10px; border:1px solid black; background-color:lightgreen;"></span>	YARD STORAGE

1ST FLR GFA	
COMMERCIAL	6 1119.7 m <sup>2</sup>
	1119.7 m <sup>2</sup> (TOTAL GFA)

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**BARA A. KHANM**  
 ARCHITECT

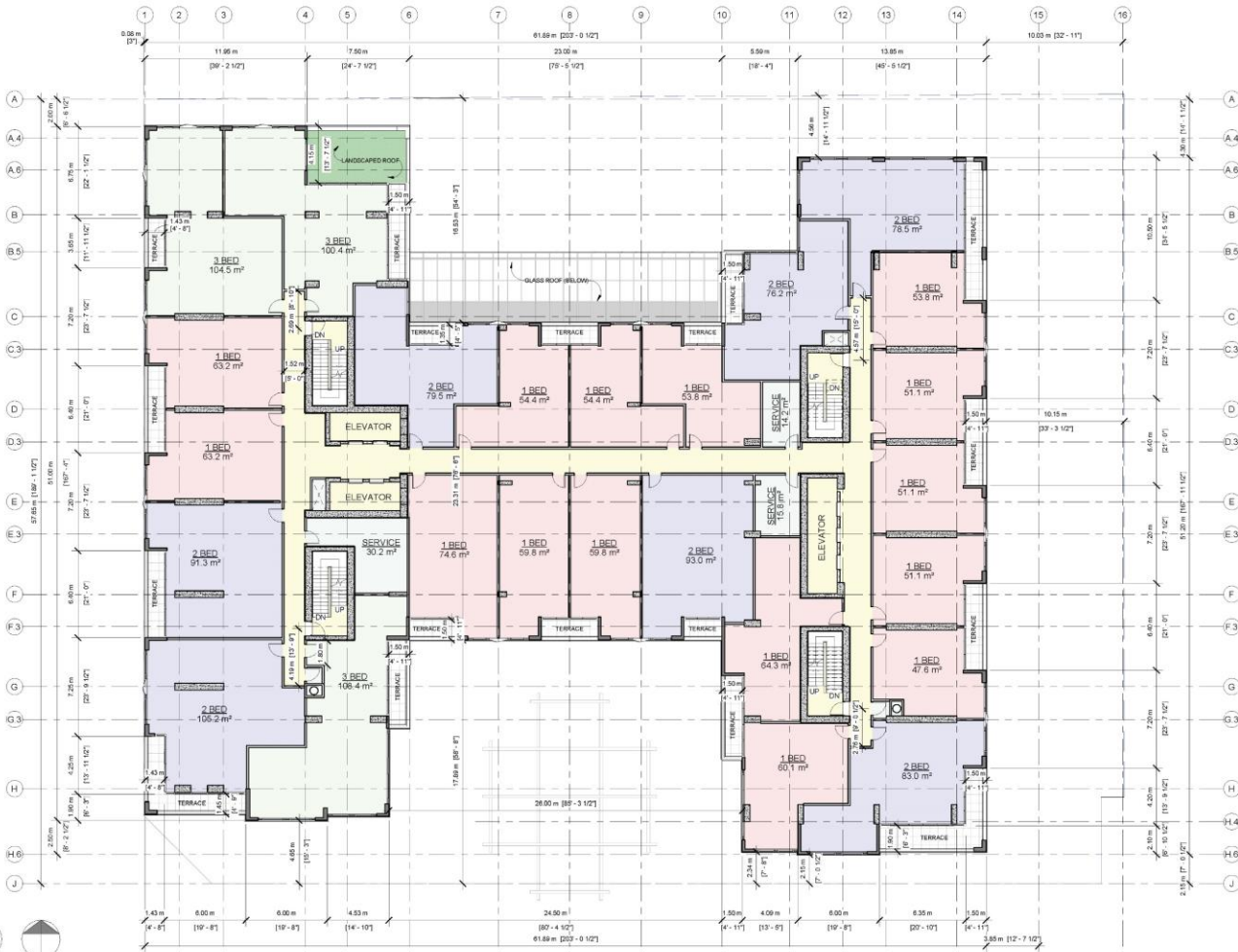
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MARKETPLACE WEST	15
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sheet no.: A201

# APPENDIX:



- PLAN KEY**
- 1 BED
  - 2 BED
  - 3 BED
  - CIRCULATION
  - SERVICE
  - SERVICE E

2ND-4TH FLR GFA		
1 BED	15	952.0 m²
2 BED	7	606.9 m²
3 BED	3	313.3 m²
<b>UNITS:</b>	<b>25</b>	<b>1762.2 m² (TOTAL GFA)</b>

**2ND FLOOR PLAN**  
 1 : 150

FOOTPRINT/BUILDABLE AREA: 2261m² (PER FLR)

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THE CONTRACTOR SHALL CHECK AND VERIFY ALL DIMENSIONS, CONDITIONS AND SERVICES AND CONDITIONS TO THE CONTRACT BEFORE PROCEEDING WITH THE WORK.

THIS DRAWING IS VALID ONLY FOR THE PROJECT AND SHALL BE VOID FOR CONSTRUCTION IN ANY OTHER PROJECT OR FOR THE COMPANY NAME.

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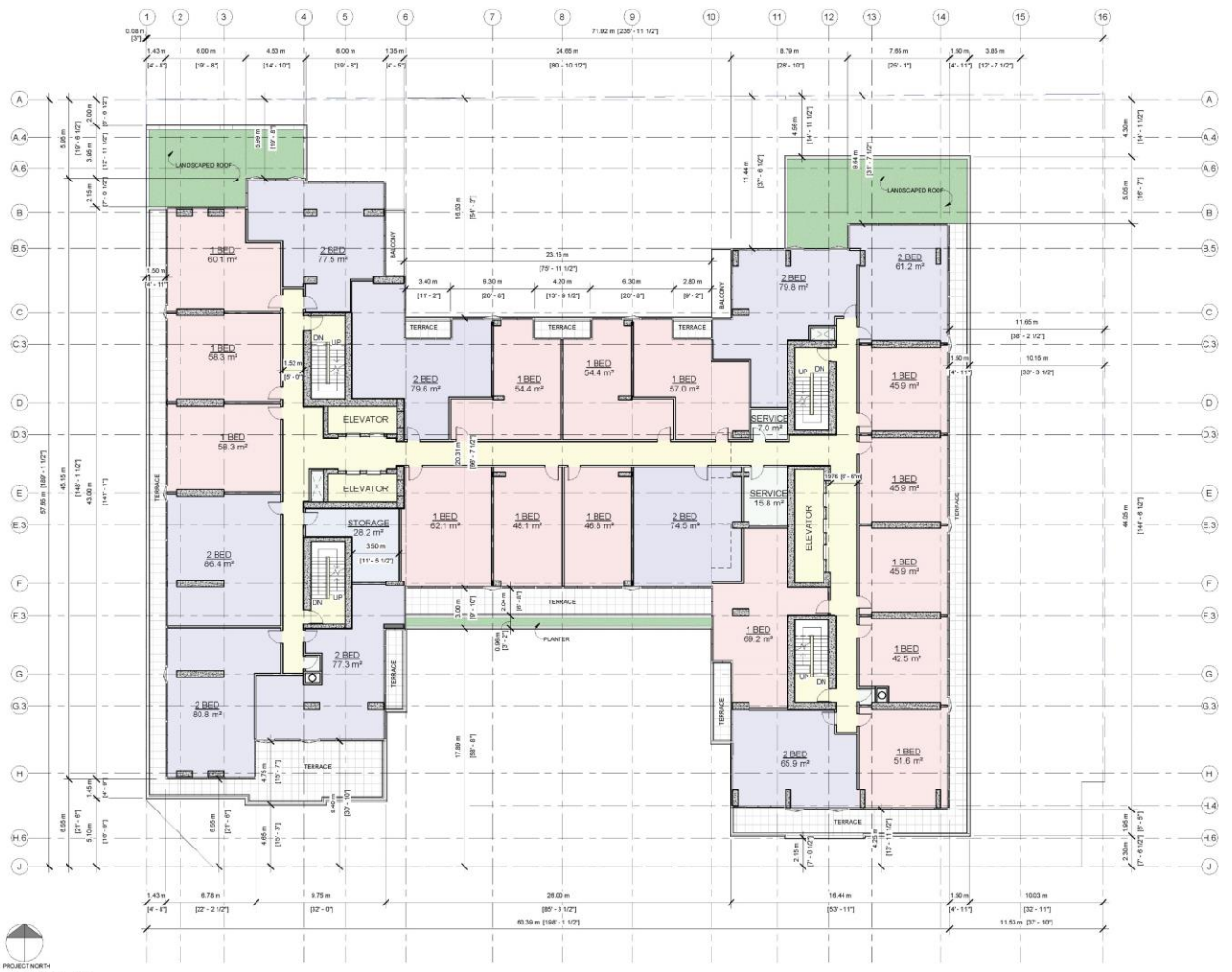
SAAD A KHANLAF  
 OFFICE

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project:  
**MARKETPLACE WEST**  
 address: 1034 MCGARRRY TERRACE, OTTAWA ON  
 sheet name:  
**2ND FLR PLAN**

project no.:  
**22-0039**  
 sheet no.:  
**A202**

# APPENDIX:



- PLAN KEY**
- 1 BED
  - 2 BED
  - CIRCULATION
  - SERVICE
  - SERVICE E
  - STORAGE

5TH-6TH FLR LEVELS		
1 BED	15	800.4 m <sup>2</sup>
2 BED	9	683.0 m <sup>2</sup>
<b>UNITS:</b>	<b>24</b>	<b>1,483.3 m<sup>2</sup> (TOTAL GFA)</b>

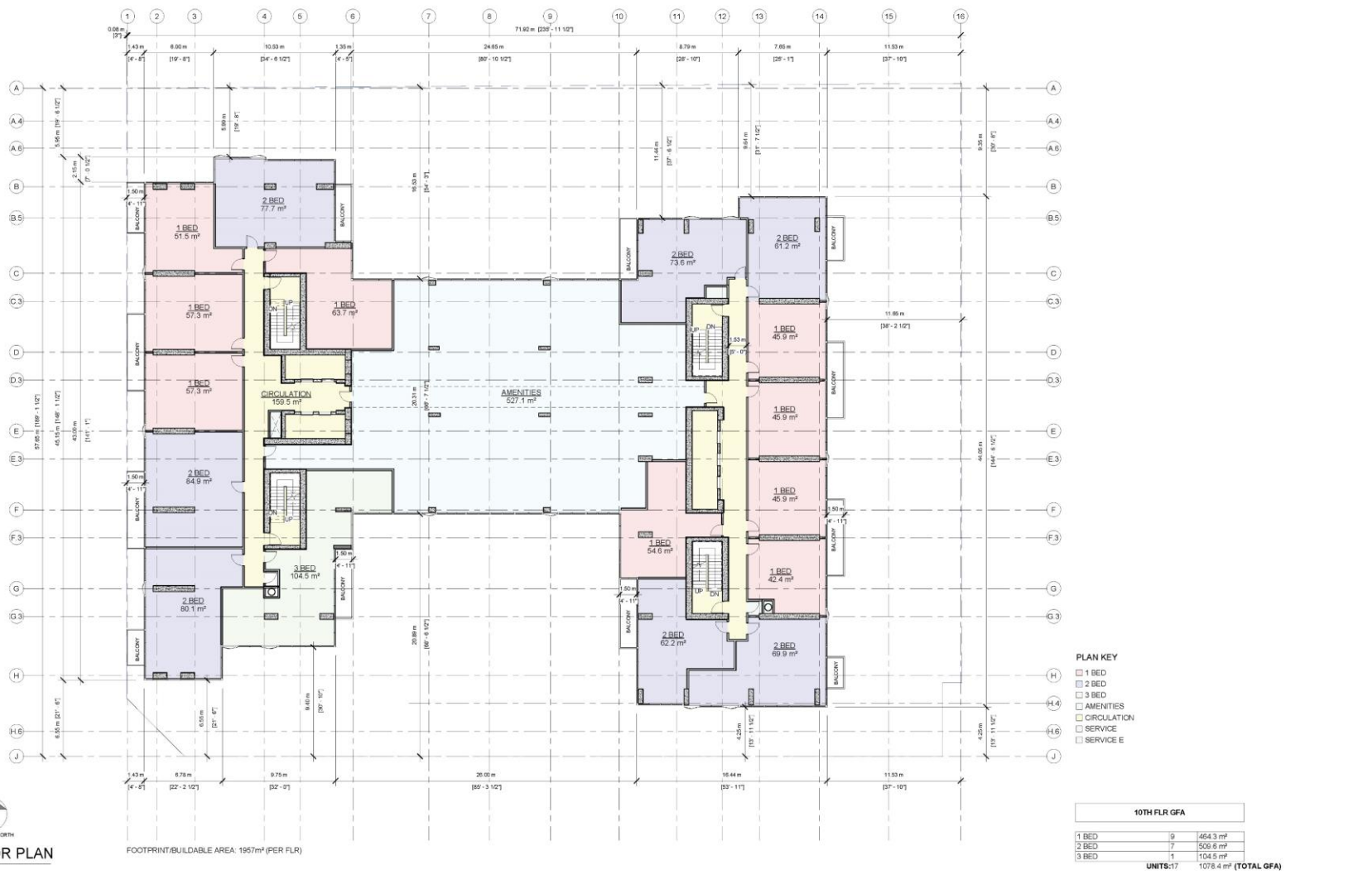
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**5TH FLOOR PLAN**  
 1 : 150

FOOTPRINT/BUILDABLE AREA: 1942m<sup>2</sup> (PER FLR)

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# APPENDIX:



**1 10TH FLOOR PLAN**  
1:150

FOOTPRINT/BUILDABLE AREA: 1957m<sup>2</sup> (PER FLR)

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SARAH A. KHULAF  
LICENSED ARCHITECT

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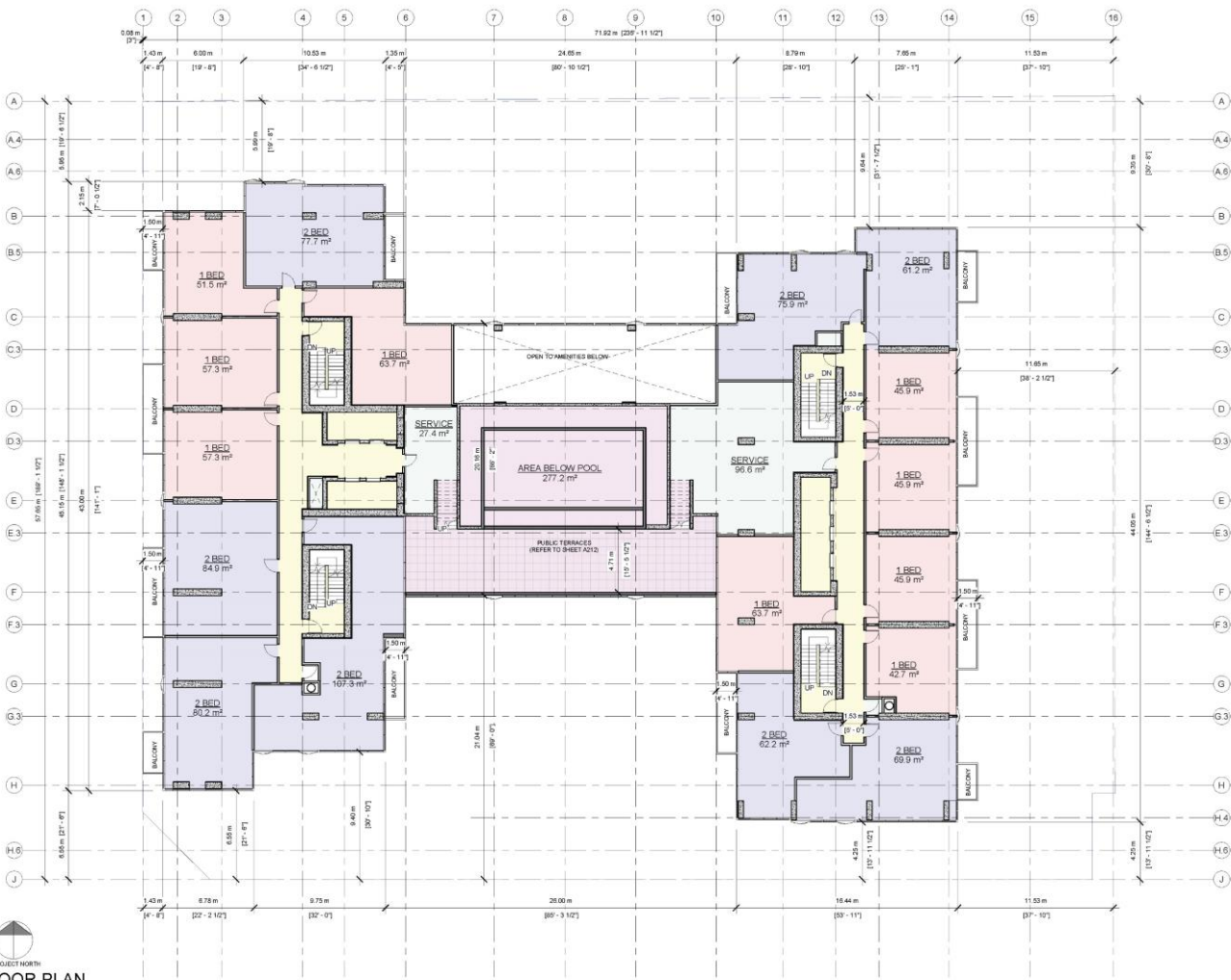
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address: 1034 MCGARRY TERRACE, OTTAWA ON

project no.: **22-0039**

sheet name: **10TH FLR PLAN**

sheet no.: **A210**

# APPENDIX:



- PLAN KEY**
- 1 BED
  - 2 BED
  - AREA BELOW POOL
  - CIRCULATION
  - SERVICE
  - SERVICE E

11TH FLR GFA		
1 BED	9	473.6 m <sup>2</sup>
2 BED	8	519.3 m <sup>2</sup>
<b>UNITS: 17</b>		<b>1093.1 m<sup>2</sup> (TOTAL GFA)</b>

**1 11TH FLOOR PLAN**
  
 1 : 150

FOOTPRINT/BUILDABLE AREA: 1957m<sup>2</sup> (PER FLR)

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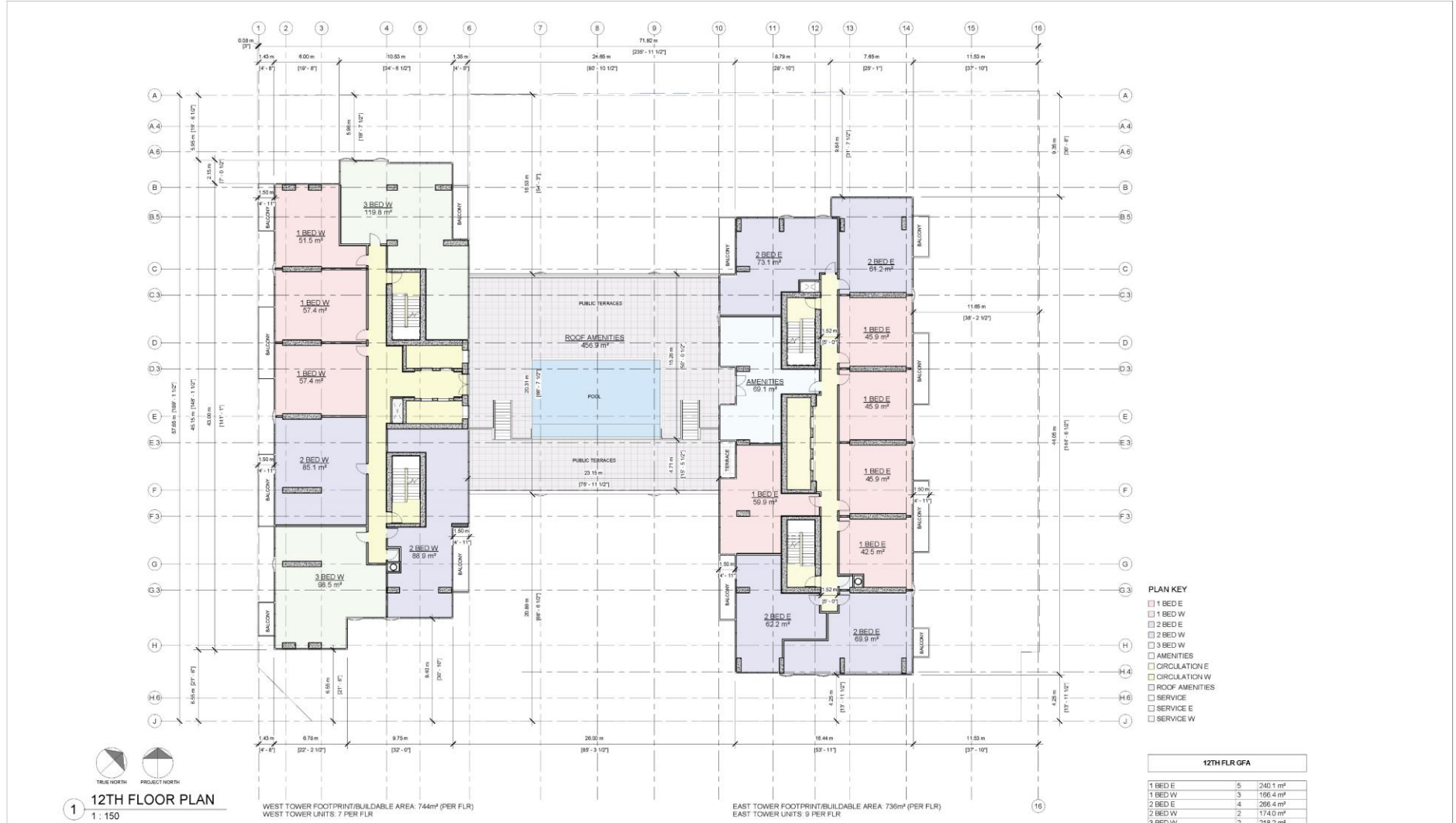
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 SAAD A. KHALAF  
 ARCHITECT

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 sheet name: **11TH FLR PLAN**

project no.: **22-0039**  
 sheet no.: **A211**

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13TH FLOOR PLAN  
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APPENDIX:

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***WIND ANALYSIS***

APPENDIX:

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***SHADOW ANALYSIS***

# APPENDIX:

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## ***HIGH-PERFORMANCE DEVELOPMENT STANDARDS CHECKLIST***

APPENDIX:

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***HERITAGE IMPACT STATEMENT***



APPENDIX:

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***UDRP SUBMISSION- JUNE 2023***

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**Saad Khalaf, OAA, NCARB, PMP**  
**President**

Progressive Architects has assumed all information received from the owner and third parties in the preparation of the Report to be correct. Progressive Architects has exercised a customary level of judgment or due diligence in the use of such information, Progressive Architects assumes no responsibility for the consequences of any error or omission contained therein.