

# Planning Rationale / Design Brief

## Applications for Site Plan Control and Draft Plan of Condominium

### Minto's Arcadia Stage 6



# Planning Rationale / Design Brief

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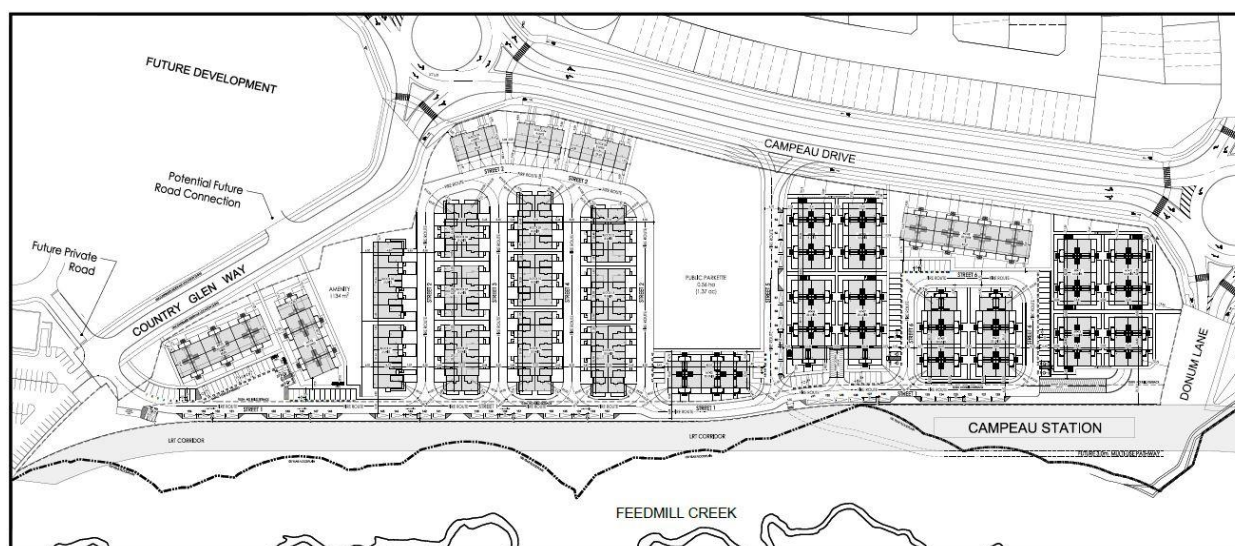
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## 1.0 INTRODUCTION

J.L. Richards & Associates Limited (JLR) has been retained by Minto Communities Inc. to provide a Planning Rationale / Design Brief in support of the applications for Site Plan Control and Draft Plan of Condominium (common elements) for 8415 Campeau Drive, which is found in Kanata West in the City of Ottawa's west end. This document will henceforth be called the "Design Brief".

The subject property fronts onto Campeau Drive and is east of Huntmar Drive, where direct access onto Highway 417 is provided. The subject property is also north of the planned LRT West extension alignment (Stage 3), between Country Glen Way and Donum Lane. The surrounding neighbourhood is being developed for a range of uses, including the remainder of the Arcadia residential subdivision and commercial development either planned or proposed immediately west of the subject property, across Country Glen Way.

A Design Brief is the required document for an application for Site Plan Control as per the City's Terms of Reference for a Design Brief. This Design Brief aims to demonstrate compliance with the recently approved zoning for the subject property. Council approved the rezoning of the subject property from "Development Reserve (DR) Zone" to "Residential Fourth Density Zone, Subzone 'Z', Urban Exception 2783 [R4Z(2783)]" and "Parks and Open Space (O1) Zone" when they enacted By-law no. 2022-175 on June 6<sup>th</sup>, 2022. The R4Z(2783) Zone permits a planned unit development of townhouses and stacked townhouses, as shown on **Figure 1** below. Full sized copies of the Site Plan, prepared by SRN Architects and dated July 13<sup>th</sup>, 2022, have been included in the site plan application and are filed separately.



**Figure 1:** Site Plan, prepared by SRN Architects and dated July 13<sup>th</sup>, 2022

The proposed planned unit development of 264 stacked townhouses and 104 townhouses, all proposed to be condominiums (common elements), are permitted and all proposed parking, driveways, and amenity spaces meet the requirements of the recently approved zoning. To

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maximize the site area for landscaping and outdoor amenity space, 156 of the 314 parking spaces proposed for the stacked townhouses are provided underground in a parking garage.

As requested by City Staff during the rezoning process, our client had agreed to rezone 0.56 ha of land in the central area of the site from 'DR' to 'O1' to satisfy the City's request for on-site parkland dedication in accordance with the City's Parks First Policy. The rezoning satisfactorily demonstrated our client's intent to dedicate the land for a public parkette, to be conveyed to the City of Ottawa. As requested by City Staff prior to Council's approval of the Zoning By-Law Amendment, the details of the park block, including design (facility fit plan), servicing, conveyance, and other requirements, are proposed to be confirmed through the Site Plan Control application and approval process in conjunction with City Staff. The park block is not proposed to be registered as a block through the subdivision process for Arcadia Stage 6.

Overall, the site-specific zoning by-law provisions, including the permission of the private roof-top amenity spaces, reflect our client's proposal to develop a modern compact built form that is increasingly common of residential greenfield development sites across the City of Ottawa. The proposed residential intensification is also appropriate based on the subject property's location next to a planned LRT Station (Campeau Station). The rezoning of the central area to be dedicated to the City as parkland further shows our clients commitment to create communities rather than simply a residential condominium. The proposed Site Plan Control application simply aims to implement what has been approved for the site through recent Zoning By-Law Amendment. The proposed application for Draft Plan of Condominium (common elements) will allow for the future development of separate multiple condominiums on-site across a planned unit development that conforms to the approved zoning.

The subject property is designated as Mixed-Use Centre by the City of Ottawa Official Plan, no. 2003, as amended. The Planning Rationale, prepared by J.L. Richards & Associates Limited (Ltd) and dated February 16<sup>th</sup>, 2022, submitted in support of the approved zoning sufficiently demonstrated conformity to the City's Official Plan, no. 2003 as amended, and was primarily used to justify a zoning change to permit residential intensification adjacent to a planned LRT Station (Campeau Station). This Design Brief only includes a review of the existing Official Plan policies, as required by the City's Terms of Reference for a Design Brief prepared for a Site Plan Control application. Prior to Council's approval, the Zoning By-Law Amendment was supported by Staff and was carried on consent by Planning Committee. In our opinion, the policies of the existing Official Plan were sufficiently reviewed for conformity during the rezoning process and the proposed development will conform with the approved zoning for the subject property.

The application for Zoning By-law Amendment was deemed complete prior to Council approval of the City's New Official Plan (2048) on November 24<sup>th</sup>, 2021 when they enacted by-law 2021-386. The City's New OP has not yet received ministerial approval. According to the City's Transition Policies, which provides guidance for planning applications submitted after Council's approval of the New OP, all Site Plan Control applications need only to be evaluated based on the approved zoning, as per Zoning By-Law 2008-250, as amended.

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This Site Plan Control application is accompanied by all the required plans and studies identified in the list received by our client following a pre-consultation meeting with City of Ottawa Staff for Zoning By-Law Amendment, Site Plan Control and Draft Plan Condominium, which took place on September 6<sup>th</sup>, 2021.

The list includes:

- An Full (Site) Servicing Study, prepared by J.L. Richards & Associates Limited, and including the Site Servicing Plan, a Site Servicing Study, a Grade Control and Drainage Plan, an Erosion and Sediment Control Plan/ Brief, and a Storm Water Management Report.
- Geotechnical Study/ Slope Stability Study, prepared by Paterson Group
- Transportation Impact Assessment, prepared by CGH Transportation Inc. ‘
- A Noise Study, prepared by J.L. Richards & Associates Limited
- A vibration and LRT proximity study, prepared by Paterson Group
- A Draft Plan of Condominium, Survey Plan (existing) and Draft Plan of Subdivision (M-Plan), prepared by Stantec.
- A Design Brief, prepared by J.L. Richards & Associates Limited
- A Landscape Plan, prepared by NAK Design Strategies
- A Survey Plan, prepared by Stantec
- An Architectural Building Elevation Drawings (Dimensioned), prepared by SRN Architects.
- Phase 1 Environmental Site Assessment, prepared by Paterson Group
- A Tree Conservation Report (TCR) and Environmental Impact Statement (EIS) prepared by Kilgour & Associates
- A Sight Lighting Plan and Certification Letter, prepared by J.L. Richards & Associates Limited.

This Design Brief has been prepared to demonstrate that the Site Plan prepared by SRN Architects and dated July 13<sup>th</sup>, 2022, conforms to the recently approved zoning. This design brief also speaks to the landscape plan prepared by NAK Design Strategies. Since the proposed design complies with all relevant zoning provisions established through by-law 2022-175 and found in Zoning By-Law 2008-250, the proposed development will meet the general intent and purpose of the Zoning By-Law. This Design Brief further speaks to the proposed layout, massing and built form, as well as the relevant policy criteria and applicable guidelines that the proposed development must adhere to, as required by the Terms of Reference of the Design Brief .

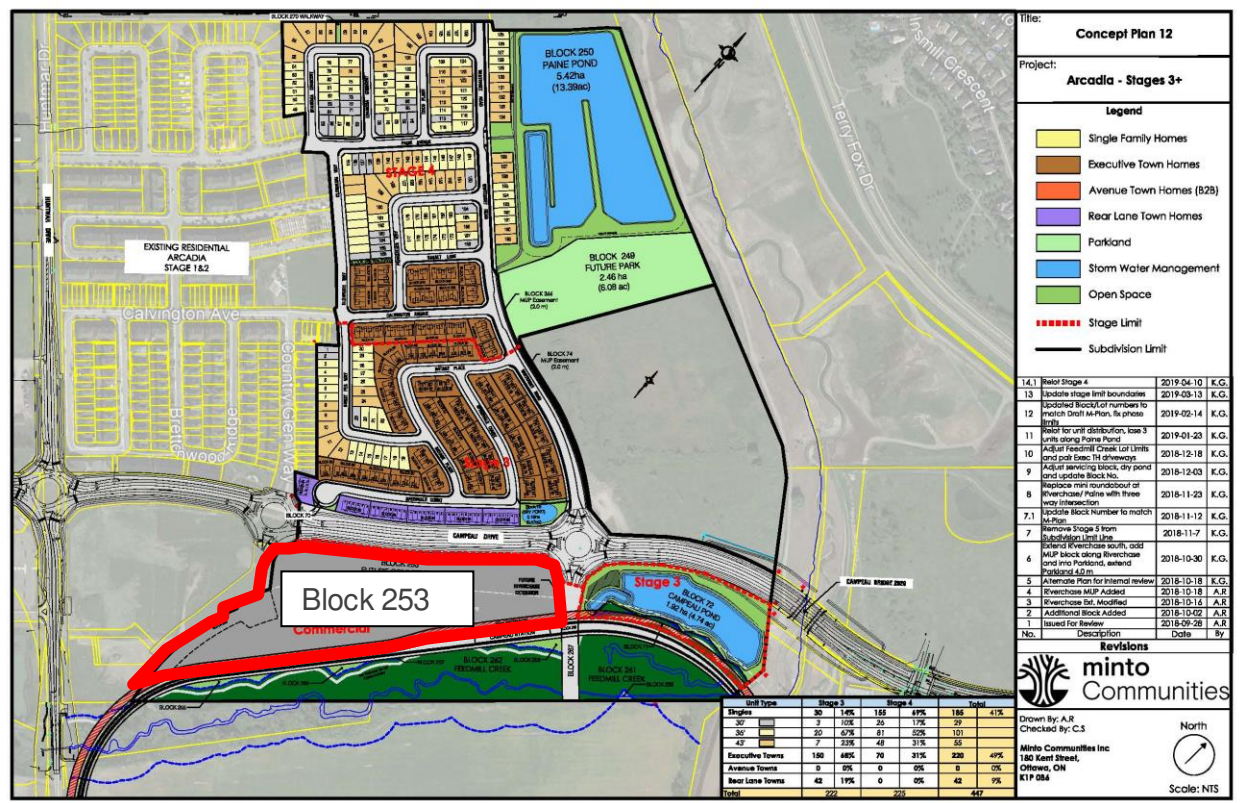
Our client will follow the City’s public consultation process for applications for Site Plan Control and Draft Plan of Condominium.

In our opinion, the proposed development represents good land use planning which meets the urban design objectives of the City. The proposed development is an appropriate level of intensification adjacent to a rapid transit station and will provide a range of modern living options which will add to the City’s housing stock.

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## 1.1 Background & Applications

Arcadia Stage 6 is part of a multi-phase subdivision which has been ongoing since 2012. The Draft Plan of Subdivision was submitted for Stages 1-4, and 6. Stages 1 & 2 are complete and Stages 3 & 4, approved in 2019 year, are nearly built-out. As a part of Stage 4, a significant park block north of Campeau and east of Winterset, will be constructed. An application for Draft Plan of Subdivision and Zoning By-Law Amendment for Stage 5 lands is pending with the City of Ottawa. The Draft Plan of Subdivision (D07-16-22-0004) for Arcadia 6 lands, shown as Block 253, was recently extended to May 3<sup>rd</sup>, 2024. All conditions are currently being reviewed and will be satisfied prior to the registration of the plan. The plan will be registered prior to the approval of the proposed application for Site Plan Control. **Figure 2** below shows all stages of the Arcadia Subdivision.



The plan of subdivision for Stage 6 still needs to be registered with the City of Ottawa. The Draft M-Plan submitted with this application includes two (2) blocks: one (1) for the entire planned unit development condominium block as proposed and one (1) for the one-foot reserve. The one-foot reserve will be lifted through by-law to Council sometime after the registration of the plan. As previously mentioned, the required park block will be dedicated to the City of Ottawa through the Site Plan Control application and approval process. All park details and requirements, such as the facility fit plan, servicing and easements, will also be confirmed through this process and not the subdivision process.

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There are reserves and easements over the east part of the lands in favour of the City. The intent of these easements is to provide a temporary turning circle at the existing end of Donum Lane. However, we do not anticipate that Donum Lane will be extended south, until it is required to be constructed under the future LRT alignment, to provide access to the lands to the south. There is no need for the road construction at this time, hence no requirement for the turning circle. The developable lands to the south are owned by others and represent the last parcel on north of Highway 417. Subject to discussions with the City's engineer and traffic operations, our client will be seeking to remove these parts on the existing survey plan through the subdivision process, prior to registration of the plan. As mentioned, there will also be a need to lift the 0.3 metre reserves.

The Design Brief is required for the application for Site Plan Control and meets the requirements of the City's Terms of Reference for Design Briefs. Though it is true that the subject property is found within a Design Priority Area within the framework of the City's existing Official Plan, a submission to the Urban Design Review Panel (UDRP) is not required for the proposed development given the current proposed height of four (4) storeys.

This application for Site Plan Control is being submitted concurrently with an application for Draft Plan of Condominium.

## 1.2 Subject Property Location

The legal description of the Subject Site is Part of Lot 3, Concession 1, former Township of March, Part 1 and 2 on Reference Plan 4R-26467, in the former City of Kanata, now the City of Ottawa. The lands are currently known as Block 253 of the Arcadia Draft Plan of Subdivision. The Draft Reference Plan is shown as **Figure 3** below. This will be updated prior to site plan approval to accurately reflect the registration of the subdivision.

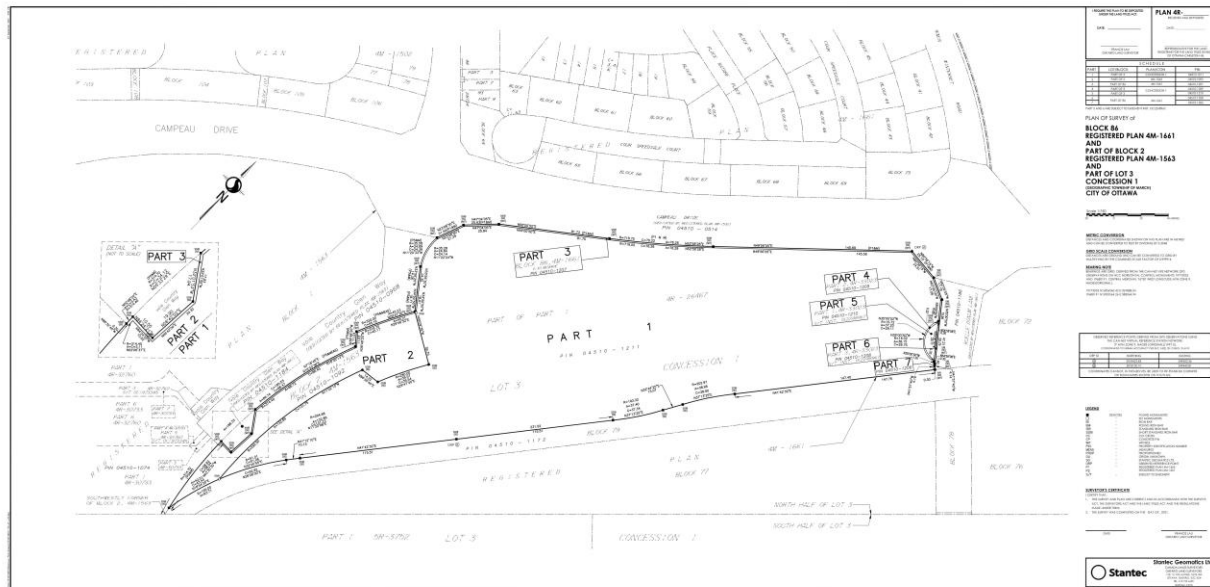


Figure 3: Draft R-Plan

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The subject property is approximately 5.5 ha (55,000 m<sup>2</sup>) in size. The subject property currently consists of undeveloped urban lands designated Mixed Use Centre.

The subject property is not within the Carp River Restoration Policy Area Overlay, nor the City's Floodplain overlay for the Carp River or Feedmill Creek. The City's LRT corridor provides separation from between the subject property and the Feedmill Creek.

**Figure 4** shows the location of the subject property.



**Figure 4:** Subject Property Location

## 1.3 Existing Conditions

Historically, the subject site has primarily been used for agriculture up until the 1990s. Today, the site is vacant and underdeveloped, mostly flat and containing minimal tree foliage. The immediate properties around the site were also used primarily for agricultural purposes but are now mostly developed lands, consisting of the first four (4) Stages of the Arcadia Subdivision. Planning applications for Stage 5 of the subdivision are currently being reviewed by City of Ottawa Staff, as noted earlier.



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The subject property is the remaining parcel of the larger subdivision development, which has been through various approval phases since 2012. As a result of previous development, this parcel has planned services provided by previous stages which will be connected to. The property is located close to both the Carp River, travelling north-south, to the East and Feedmill Creek, running east-west to the south of the property. The lands for roadway network extensions, including Campeau Drive, Country Glen Way and Donum Lane, were dedicated and conveyed to the City of Ottawa during early phases of development. **Figure 5** shows Campeau Drive opposite the site.



**Figure 5:** The existing road conditions and public realm of Campeau Drive

### 1.4 Subject Property Context

The subject property is located within the Kanata West Community and the northern part of the Kanata West Secondary Plan, within the framework of Official Plan 2003, as amended. Surrounding the property are a several other uses, including the existing Arcadia residential neighbourhoods, nearby employment lands, commercial facilities and natural amenities. These include: the Tanger Outlet Mall, Palladium Auto Park, the Canadian Tire Centre, Kanata Centrum, the Carp River Restoration Lands, and the South March Highlands Conservation Forest. Access to these amenities and many other employment uses and communities services and facilities can be accessed through the nearby Highway 417 interchange at both Terry Fox Drive and Palladium Drive. The site also has access to existing and planned multi-use pathways and a future LRT connection at Campeau Station. Two current bus routes, route 62 and 162, serve the area, along Huntmar Drive. The proposed development includes a connection (via easement) to the multi-use pathways proposed along the northside of the LRT route.

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**Figure 6** includes two site context photos showing the existing multi-use pathway along the north side of Campeau Drive, as well as the earlier phases of the Arcadia development.



**Figure 6:** Stage 1 Townhomes and Stage 3 rear lane townhomes along Campeau Drive

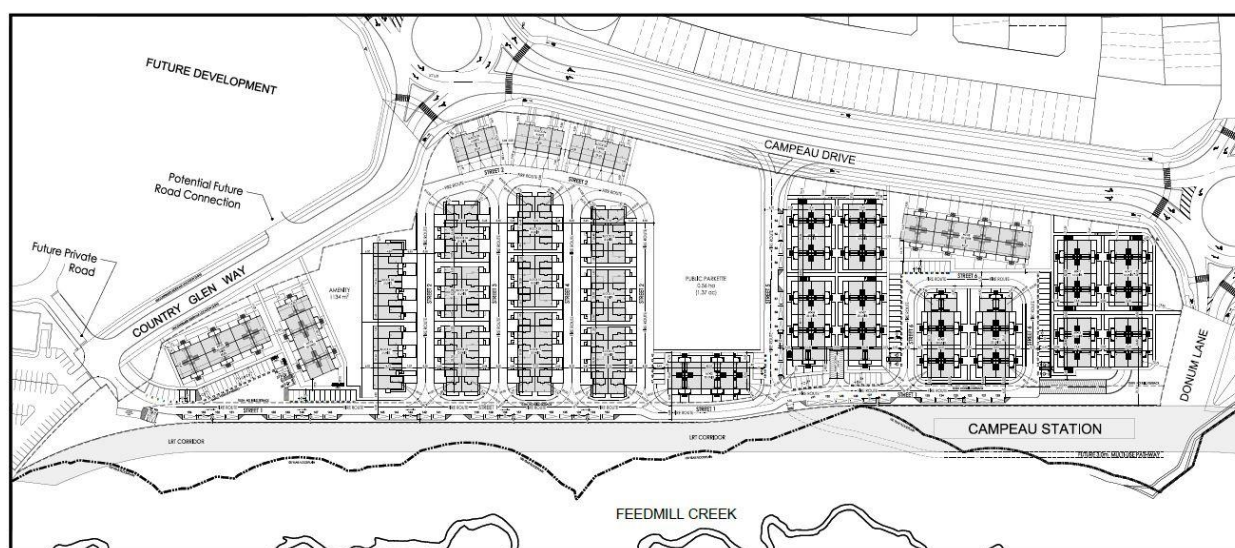
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## 2.0 PROPOSED DEVELOPMENT

### 2.1 Proposed Development Overview

A Design Brief is required for this application for Site Plan Control and Draft Plan of Condominium. The following sections of this report satisfy the City's requirements for a Design Brief.. The approved performance standards, which were first proposed in the Planning Rationale, prepared by J.L. Richards & Associates Limited (Ltd.) and dated February 16<sup>th</sup>, 2022, were established to allow for the planned unit development of townhouses and stacked townhouses.

The proposed development is shown as **Figure 7** below:



**Figure 7:** Site Plan, prepared by SRN Architects and dated July 13<sup>th</sup>, 2022

The proposed development consists of the following:

- 264 stacked townhouse dwellings;
- 104 townhouse dwellings;
- 156 regular and visitor underground parking spaces and 156 regular and visitor surface parking spaces for the stacked dwellings;
- 208 total parking for the townhouse dwellings, including one (1) on the driveway and one (1) in the parking garage;
- Three (3) Type A and three (3) Type B barrier free parking stalls for public use.
- 132 bike parking stalls for the stacked dwellings;
- Over 1000 sqm. of private outdoor amenity area off Country Glen Way;
- Private rooftop amenity areas for the stacked dwellings;
- 1.82 ha of landscaped area (36% coverage), excluding the park block; and
- A park block of 0.56 ha, to be developed by Minto Communities and dedicated to the City of Ottawa.

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The stacked townhouse dwellings will not exceed the maximum building height of four (4) storeys (16 m), and the townhouse dwellings will not exceed the maximum height of three (3) storeys (12 m). As well, the proposed stacked townhouses would be permitted to have rooftop terraces, which are furthermore not subject to the provisions set out in Table 55(8) of the Zoning By-law.

The proposed plan complements the existing built form of the first four stages of the subdivision, while continuing to provide a range of dwelling unit types, layouts and sizes for new residents, including young families. The proposed form of development is encouraged adjacent to higher order transit and conforms to numerous higher-level policies and guidelines stemming from the Provincial Policy Statement (PPS) 2020, City of Ottawa's Official Plan (2003, as amended), the Transit Oriented Development guidelines and follows the City's Transportation Master Plan.

The Subject Property will be serviced by municipal water, sanitary and storm sewers, which were previously installed to service this part of the subdivision. They were all installed during the previous phases of development. Municipal water, sanitary sewer and stormwater sewer services are available in Campeau Drive, Paine Avenue, Clonrush Way and Calvington Avenue to service the Subject Site. Existing stormwater ponds will serve the entire subdivision and no stormwater pond is required on-site. In accordance with the City's parkland requirements, the park block will not be used to storage stormwater runoff from the proposed development.

### 2.2 Design Approach

This section of the Report provides a thorough understanding of the design approach for the proposed Arcadia Stage 6 subdivision, based on the previously approved Zoning By-law Amendment. It looks to enhance the application by explaining the design approach and various design interventions, all of which comply with the provisions of Zoning By-Law 2008-250, as amended.

The following sections provide information on general layout and function of the site; the typologies of dwelling units; the urban design approach to the units; parkland dedication; and compatibility with the surrounding area and amenities.

#### 2.2.1 Dwelling Types and Products

The planned subdivision provides a mix of townhouse types, with a range of product types, which creates sufficient variation and visual interest. The development proposes a total of 104 traditional townhouses and 264 stacked dwelling units for a total of 368 units.

This includes three types that are "traditional" townhouses, which are only divided vertically. The Avenue Townhouses, Rear Lane Townhouses and Executive Townhouses range between 2-3 storeys and do not exceed the maximum building height of 12m. **Figures 8** and **9** below show examples of the traditional townhouse styles that are three (3) storeys in height.



Figure 8: Avenue Townhomes



Figure 9: Rear Lane Townhomes Elevation

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The stacked townhouse dwellings, also known as Metro Townhouses (**Figure 10**), do not exceed the maximum building height of 4 storeys tall (16 m).



**Figure 10:** Metro Townhouse Rendering (stacked dwellings)

Furthermore, there are three unique styles of the Metro Townhouse, presented as **Figure 11** below. ‘

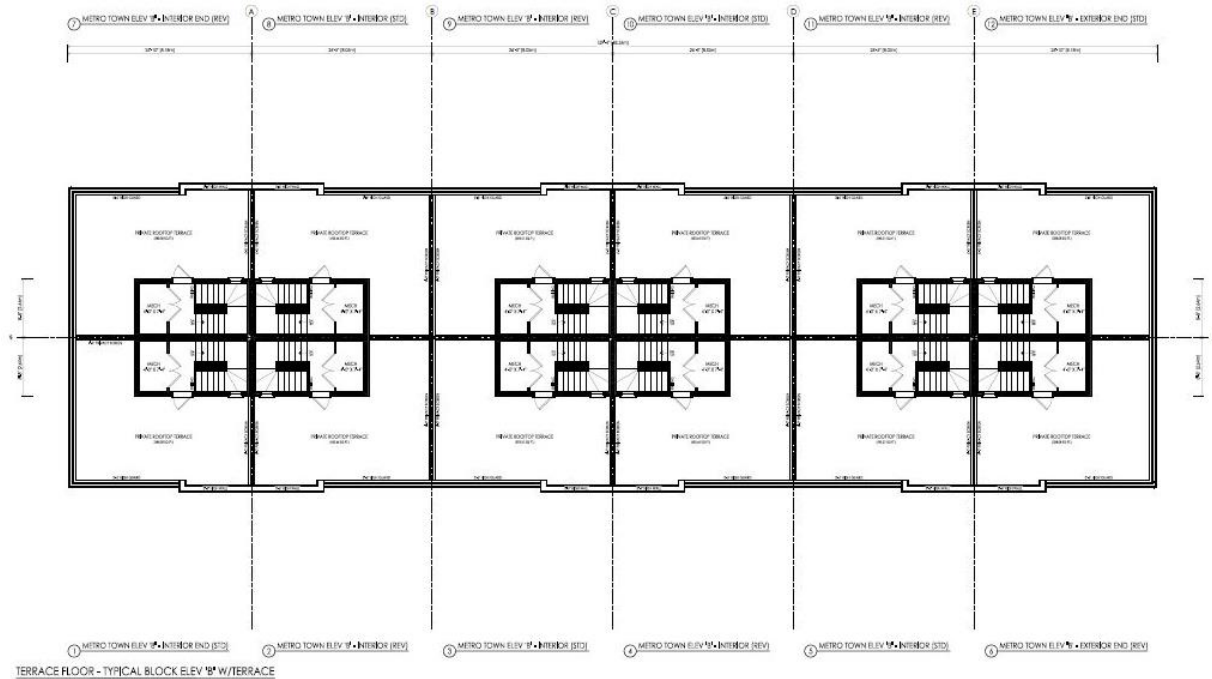


**Figure 11:** Metro Townhouse Elevations (stacked dwellings)

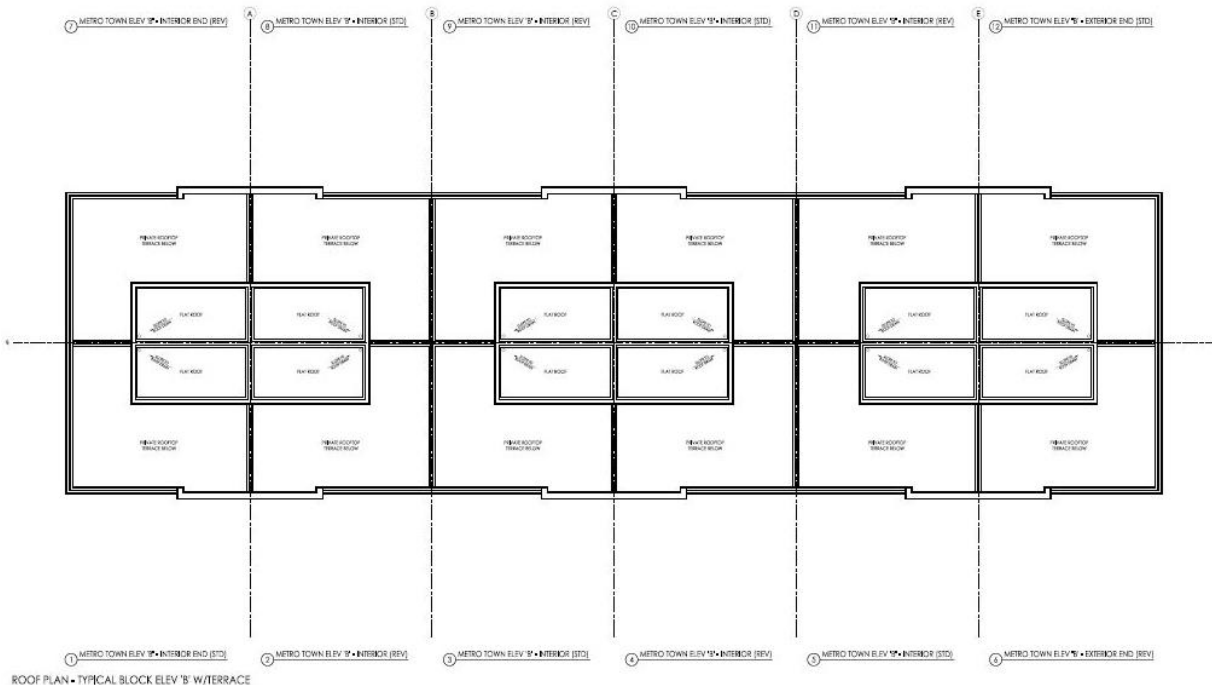
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Floor Plans also vary depending on the model type, but will ensure a variety in bedroom layouts, styles and sizes. These plans have been included in the Site Plan Control application package.

For the Metro Townhouses, there are private rooftop amenity spaces proposed, as shown on **Figure 12 (A5)** and **Figure 13 (A6)** below, prepared by SRN Architects and dated July 6<sup>th</sup>, 2022.



**Figure 12:** Roof Terrace Plan for Metro Townhouses (A5), prepared by SRN Architects, dated July 6<sup>th</sup>



**Figure 13:** Roof Terrace Plan for Metro Townhouses (A6), prepared by SRN Architects, dated July 6<sup>th</sup>

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The range of townhouse types and styles provided creates some variation in built-form and provides a good balance and mix of residential dwelling units, all permitted by the Residential Fourth Density, Subzone 'Z' (R4Z) Zone.

### 2.2.2 Urban Design Approach

The following section demonstrates how the proposed development resembles good urban design that complements the evolving character of the surrounding area and is considered an appropriate form of intensification adjacent to a planned rapid transit station.

#### *Land Use and Building Heights*

The development of a stand-alone residential neighbourhood is permitted by the underlying zoning which was recently approved by City of Ottawa Council when they enacted by-law no. 2022-175. During the rezoning process, Staff supported the proposed planned unit development consisting of only stacked townhouse dwellings and townhouse dwellings. As discussed in the Planning Rationale prepared by J.L. Richards & Associates Limited and dated February 16<sup>th</sup>, the residential intensification of the site for the proposed use is appropriate given the proximity to the planned Campeau LRT Station and the surrounding commercial uses.

**Figure 14** on the next page shows the Kanata West Secondary Plan, which prescribes maximum building heights. As shown below, a six (6) storey building is the maximum allowable building height for most of the easterly portion of the subject property, while the westerly portion of the subject property falls within "Area A", which prescribes a maximum building height of 15 storeys.



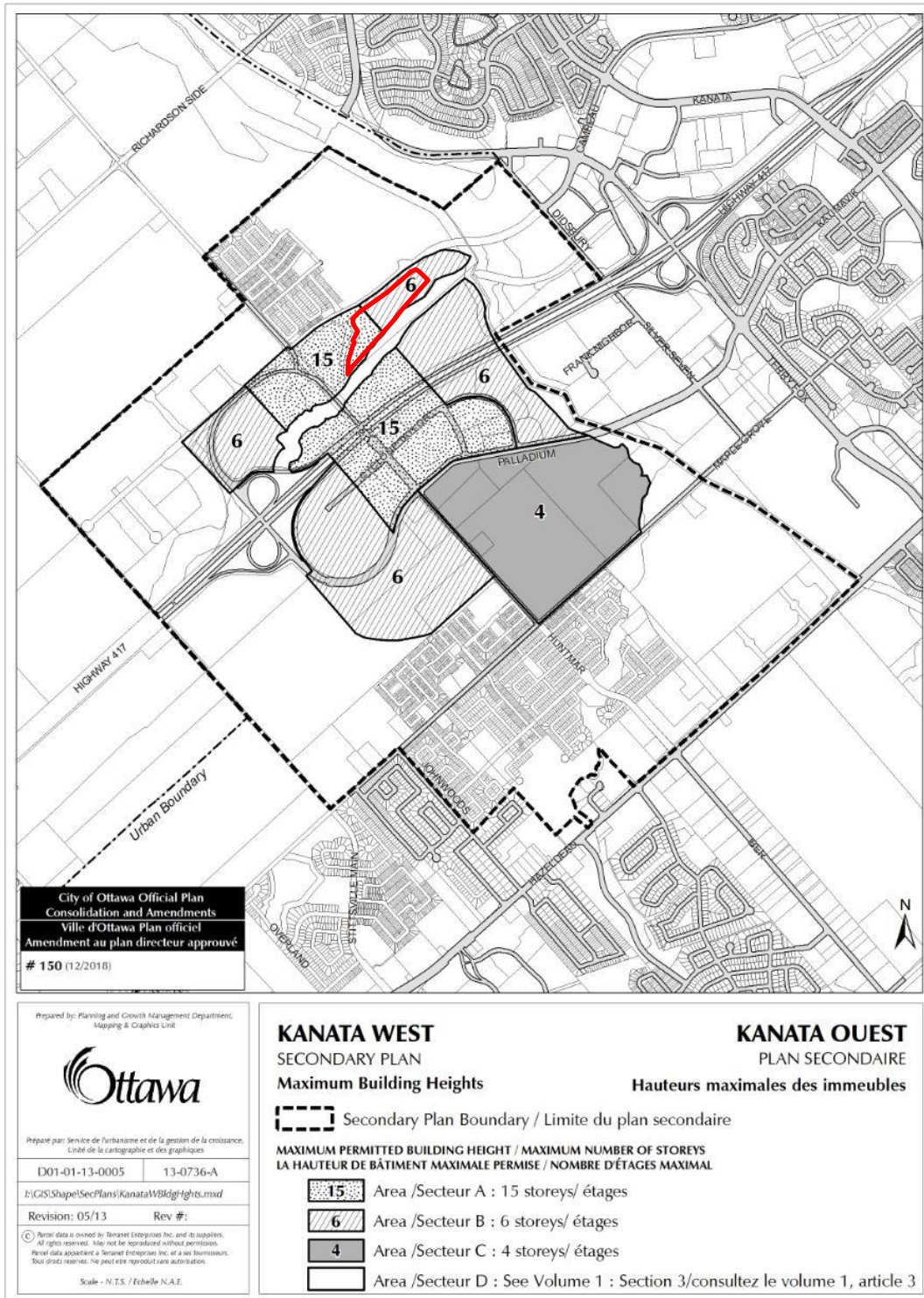


Figure 14: Kanata West Secondary Plan

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Given the maximum building height allowed by the Kanata North Secondary Plan, higher density residential development was contemplated and is still an option for the subject property but is not being proposed at this time.

We understand that the allowable heights of the Kanata West Secondary Plan have been readopted within the framework of the City's New Official Plan (2021) as an area specific policy in Volume 2C and that mid-to-high rise development would still be considered "allowable" based on the proposed policy.

### *Overall Massing, Layout and Orientation*

During the rezoning process, City staff offered several massing approaches and building layouts for our client's consideration. Our client and the City reached an agreement on the proposed layout and design during the rezoning process to permit the development as proposed in this application for Site Plan Control and Draft Plan of Condominium.

The approved Zoning for the site prescribes setbacks that support an urban layout in developing communities across the City. **Figure 15** below shows the overall massing approach to the site from all four (4) directions.



**Figure 15:** Massing Approach (Four Sides)

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The residential units oriented towards Campeau Drive create a staggered frontage that complement the existing development to the north and create an urban village feeling east of an evolving commercial area and south of the existing phases of the Arcadia subdivision. As well, the townhouses fronting County Glen Way provide a similar feel to the development across Campeau Drive. These units activate the public realm along these two streets and create a walkable environment for future residents. The proposed layout provides an urban treatment that better defines the street corridor, maintaining priority views and vistas, to and from the Feedmill creek and the future LRT corridor.

The public parkette provided along Campeau Drive and the outdoor amenity space proposed along Country Glen Way both provide “gateways” to the subject property, which are well-placed and will naturally soften the streetscape with plantings and open space. These areas are strategically designed in between buildings to ensure the active use of the lands along all edges of each space. There is a large central public parkette block proposed. Our client agreed to rezone this area to O1 to show their intention to dedicate this park block through the Site Plan Control application and approval process.

The design and layout of the park block will be refined during the Site Plan Control application and approval process, where the City’s requirements for design and programming will be met. We expect that the City’s normal process for consultative design will be undertaken concurrently with the site plan so that the community can understand the entirety of the proposed development. The overall objective of designing this park and the outdoor amenity space is to ensure that the park space is welcoming for surrounding residents and provide for a true activation of this portion of the neighbourhood.

**Figure 16** below shows a preliminary layout for the private outdoor amenity space proposed along Country Glen Way.

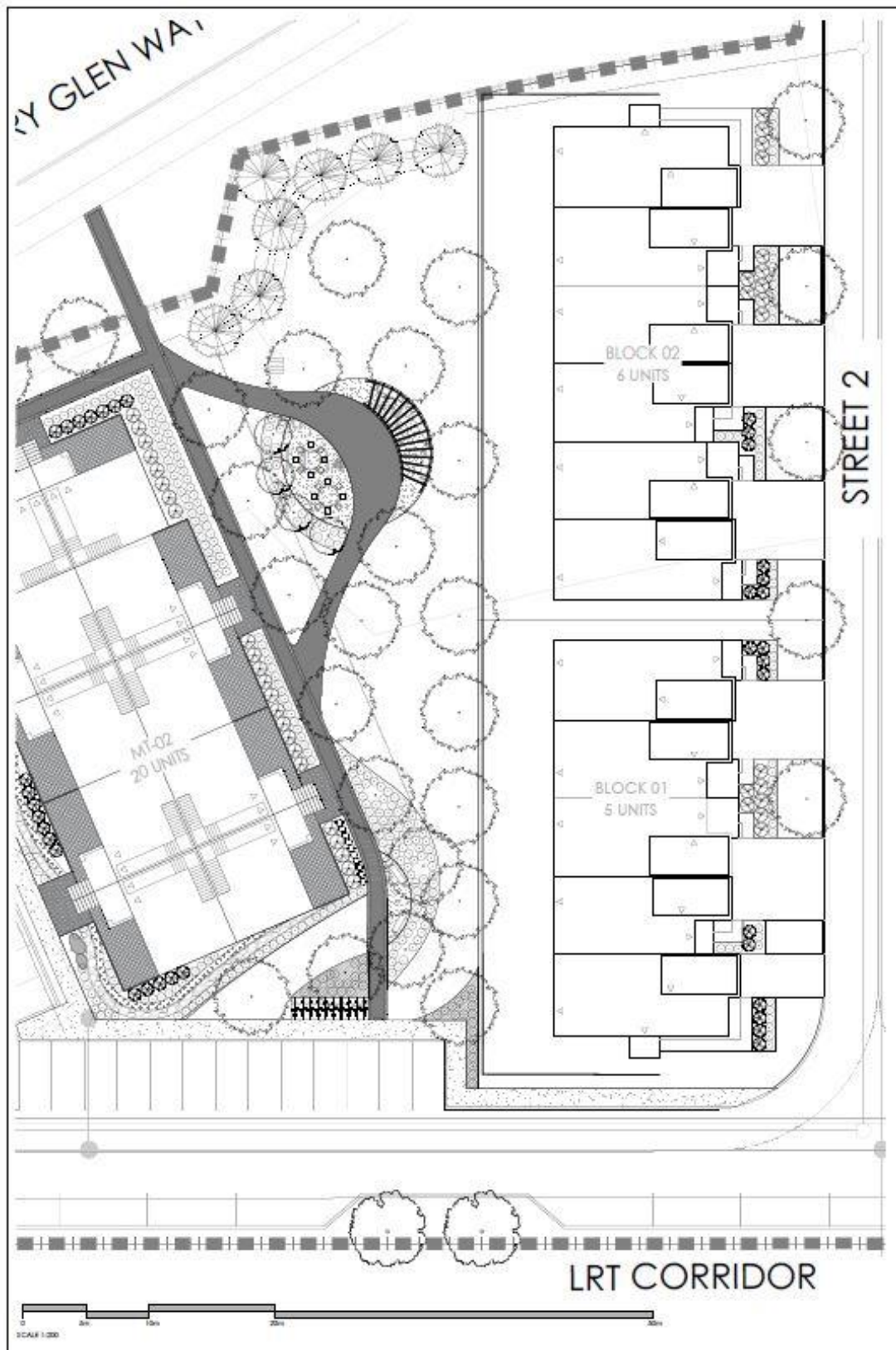


Figure 16: Private Outdoor Amenity Space

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The proposed development conforms to the zoning while complementing the built form found on the North Side of Campeau Drive. Street facing units, along with the proposed amenity space and required park block, break up the wall of residential uses and create a welcoming entrance to the site that is complimentary of and blends into the public realm along Country Glen Way.

The stacked townhouse dwellings located on the eastern half of the development are not perfectly parallel to Campeau Drive but help to continue the built form along the edge of Campeau Drive, while providing a variety of units and an unique design approach to the site. This strikes a balance between the development found along north side of Campeau Drive and the proposed development on the south side on Campeau Drive. It establishes a built form that is both urban and appropriate for the subject lands. Some variation in building orientation along Campeau Drive should be encouraged – and the proposed conceptual layout accomplishes this and more.

The overall layout of the Site Plan complies with the zoning requirements for the site, as will be further discussed in Section 3.0 of this Report.

### *On-site Circulation, Parking and Connections to the LRT (Campeau Station)*

The proposed driveway accesses and on-site circulation are supported by the Transportation Impact Assessment (TIA), prepared by CGH Transportation Inc. There are two (2) driveway entrances proposed: one (1) providing access to Campeau Drive and the other one (1) connecting to Country Glen Way. All parking, loading facilities, garbage enclosures etc. will be screened from public view along Campeau Drive and Country Glen Way in accordance with the current approaches found in the Zoning By-law. These details will be refined during the Site Plan Control application and approval process.

The proposed development will correspond with future LRT plans and accommodate broader community access to the Campeau LRT Station through pedestrian connections proposed along Country Glen Way and Donum Lane. The City's multi-use pathway location on city-owned land, immediately north of the LRT alignment, will provide a seamless east-west connection through which the city that will guarantee pedestrian and cycling circulation. Easements in favour of the City, as identified as a requirement by the City during the rezoning process, are proposed by our client to provide connection to these lands. These easements are proposed along the rear lot line abutting the LRT corridor. The details of these access easements will be determined through the Site Plan application and approvals process.

In terms of parking, there are 314 parking stalls proposed for the 264 stacked townhouse units. A mix of surface parking (156 spaces) and below-ground parking (156 spaces) are proposed. Each townhouse dwelling will be equipped with a parking garage and a driveway, meaning that each townhouse will have two (2) parking spaces. Based on the total townhouse dwellings proposed (104), 208 parking spaces will be provided for all townhouse dwellings. There is parallel parking proposed along the main private ways that provide access to the site, being Street 1 and Street 5. These are to be allocated as visitor parking. The site-specific by provisions allow for a driveway width of 6.7 m, excluding the parallel parking spaces. Most of these spaces will be separated from

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the lot line shared with the LRT corridor with a landscape buffer of 1.5 m. There are no public roads proposed and the Draft Plan of Condominium for common elements will address the ownership and maintenance of all private roads, services and maintenance of facilities (e.g. garbage, snow removal, landscape maintenance).

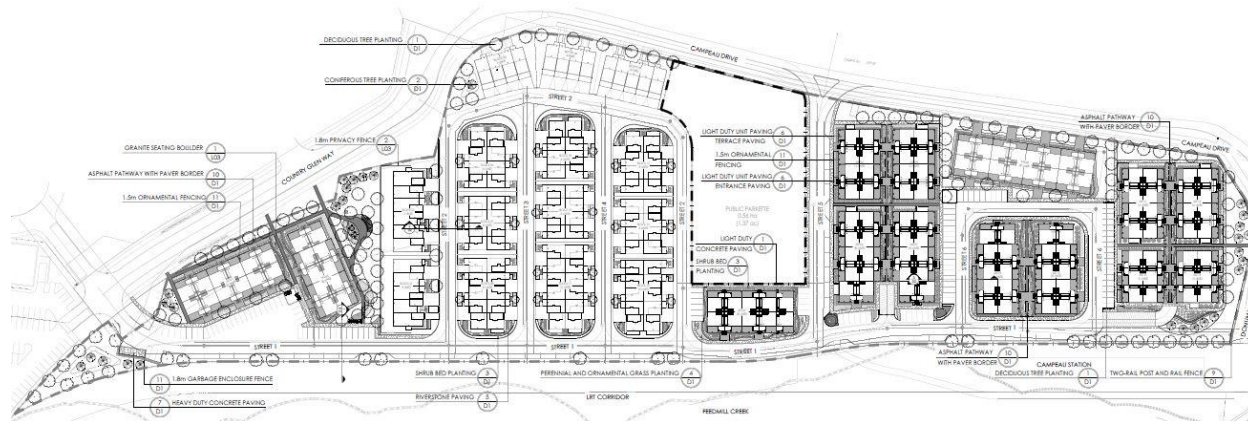
While we have seen preliminary drawings for the layout of the Campeau (LRT) Station, detailed design associated with Phases 4 and 5 of the EA has not been undertaken. Given the level of uncertainty, as demonstrated during the rezoning process, it is not considered feasible to propose the integration of the LRT station into the proposed development at this time. We understand that Donum Lane will eventually be used as the primary access point for the LRT station when the plans for the LRT are eventually solidified, but a temporary turning circle is currently unnecessary.

The proposed development does meet the City's requirement to have a no-building setback of 10 m from the LRT alignment. The building setback of greater than 10 m will ensure safety and noise and vibration mitigation from the elevated LRT, which will subsequently provide a high quality of life for future residents. The proposed LRT corridor, as currently proposed, is shown on the Site Plan prepared by SRN Architects and dated July 13<sup>th</sup>, 2022. The LRT proximity study prepared by Paterson speaks to the site's relationship with the LRT alignment including the grading and elevation.

There were also no comments received from the LRT team at the City of Ottawa, so no further action, other than an update to the LRT proximity and noise and vibration studies, have been required for this application for Site Plan Control.

### *Greening, Open Space & Sustainability*

A Landscape Plan, shown as **Figure 16** below, has been developed by NAK for this Site Plan Control application and is dated July 15<sup>th</sup>, 2022. A full-sized copy is provided as apart of this Site Plan Control application package.



**Figure 17:** Master Landscape Plan, prepared by NAK design strategies and dated July 15<sup>th</sup>, 2022.

## Planning Rationale / Design Brief

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The proposed development consists of many green and open spaces, including aggregated outdoor amenity areas and potential landscaping areas along Campeau Drive that will create a comfortable pedestrian environment. A mix of deciduous and coniferous plantings are proposed. Please refer to the Landscape Plan for more information and details on planting.

The Landscape Plan also shows the required park block. The design of the public parkette will be further refined through the Site Plan Control application and approval process.

The site is presently used as a staging area and has very limited vegetation cover and nothing that has been identified as significant for retention. Connections to existing sidewalks and multi-use pathways are proposed and strengthened through the proposed development.

There is a total of 1.82 ha of landscaped area proposed, excluding the park block. This represents 36% of the net lot area (without park block). The proposed landscaped area meets the definition of Zoning By-Law 2008-250 and will include both soft (e.g. sodded) and hard (e.g. concrete) landscaping areas. In terms of sustainability, the proposed development will integrate various sustainability features, which includes landscaped open spaces, tree plantings and opportunities for roof-top terraces and gardens. Energy efficient building design, including construction methods, will be employed.

Generally, reducing the total parking required for stacked townhouse dwellings, locating residential use next to a planned rapid transit station and greening the site will offset carbon emission and enhance the local natural environment. On-site lighting and building materials will adhere to sustainability standards.

### **2.2.3 Parkland Dedication**

The parkland dedication and all related parkland requirements, including servicing, the facility fit plan, programming and easements etc. will be determined during the review of the Site Plan Control application and approval process. The park area was rezoned from “DR” to “O1”, which demonstrates the intent that this central area will be developed as a park.

During the rezoning process, Staff requested that parkland dedication and all requirements related to the Park block, including required easements, be determined through the Site Plan Control application and approval process. A complete facility fit plan and securities will be provided to the City to fulfill its requirements.

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## 3.0 ZONING COMPLIANCE

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This section of the Design Brief demonstrates zoning compliance with the recently approved zoning for the subject property and the applicable provisions of Zoning By-Law 2008-250, as amended. The Zoning By-Law Amendment was approved by City of Ottawa Council when they enacted by-law no. 2022-175 on June 8<sup>th</sup>, 2022.

J.L. Richards & Associates had prepared a Planning Rationale in support of the approved Zoning By-law Amendment, final version dated February 16<sup>th</sup>, 2022. This Planning Rationale included a review of the City of Ottawa Comprehensive Zoning By-law and concluded that the proposed rezoning would conform to both the general intent and purpose of Zoning By-Law 2008-250, as well as the policies of Official Plan, 2003, as amended.

The Zoning By-Law Amendment rezoned the subject property from “Development Reserve (DR)” to “Residential Fourth Density, Urban Exception 2783[R4 2783]”, to permit a planned unit development consisting of townhouses and stacked townhouses. As reflected in Staff’s recommendation to Planning Committee, which was later approved by Council, the intent of the rezoning was to allow for the future development of the whole lot as a planned unit development notwithstanding the potential to develop multiple condominiums on-site.

As such, the planned unit development provisions were reviewed for compliance during the rezoning process. The Zoning By-Law Amendment only sought one exemption from the planning unit development provisions as reflected in the site-specific zoning exception enacted through By-law 2022-175. The whole lot is to be treated as one lot for zoning purposes and the area to be developed will ultimately be reflected as one block on the plan of subdivision once registered.

The proposed development will comply with all relevant Planned Unit Development provisions contained in Zoning By-Law 250-2008

The approved by-law includes provisions that prescribe the following:

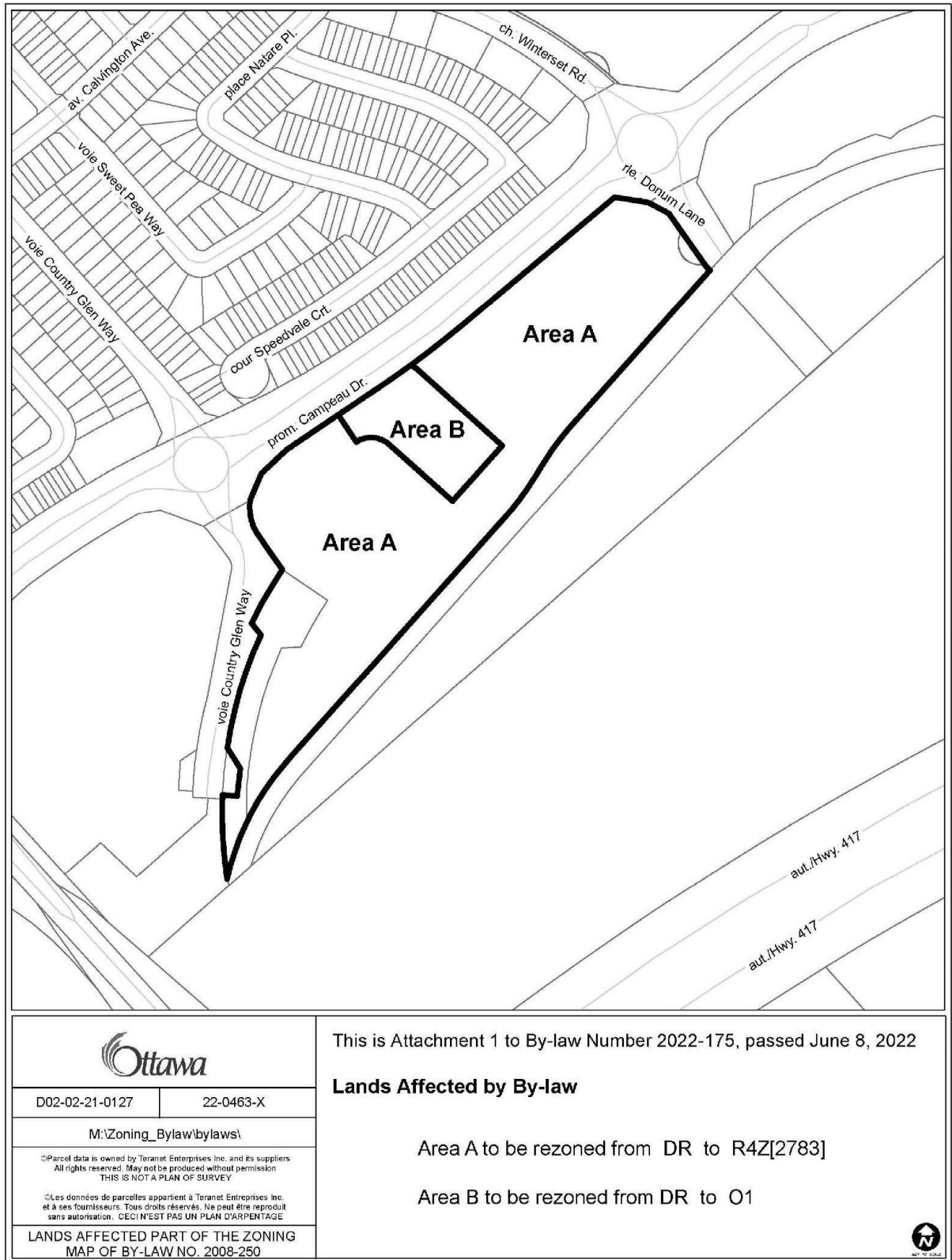
- maximum building heights for both dwelling types;
- building setbacks from the park block for both dwelling types;
- for stacked townhouses, a parking rate of 0.9 regular spaces per dwelling unit and 0.1 visitor parking spaces per dwelling unit;
- the permission of rooftop terraces and exemption from the various restrictive provisions of Zoning By-Law 2008-250;
- a reduce driveway width for private ways with parallel parking spaces in a planned unit development; and
- other site-specific provisions to guide interpretation of front yards and amenity space location (corner side yard).

The details of the Zoning By-law Amendment can be found in **Appendix A**.



# Planning Rationale / Design Brief

Figure 18 below shows the approved Zoning Schedule.



**Figure 18: Zoning Schedule**

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The purpose of the R4 zone is to:

*“allow a wide mix of residential building forms ranging from detached to low rise apartment dwellings, in some cases limited to four units, and in no case more than four storeys, in areas designated as General Urban Area in the Official Plan”.*

The permitted uses in the R4 Zone included a variety of residential uses from low-rise detached dwellings to stacked townhouse dwellings, as proposed on the site.

The purpose of the O1 zone is to:

*permit parks, open space and related and compatible uses to locate in areas designated as General Urban Area, General Rural Area, Major Open Space, Mixed Use Centre, Village, Greenbelt Rural and Central Area as well as in Major Recreational Pathway areas and along River Corridors as identified in the Official Plan, and*

The proposed development meets the general purpose and intent of the R4 Zone.

The following Site-Specific zoning provisions, stemming from Urban Exception 2783, apply to the development of the subject property:

- **The lot line that abuts Campeau Drive is considered the front lot line.**

*The proposed Site Plan shows Campeau Drive as the front lot line. The dwellings proposed along Campeau exceed the minimum required front yard setback of 3 m.*

- **The side yard along Country Glen Way is considered to extend from Campeau Drive to the southern extent of Country Glen Way.**

*The proposed Site Plan shows the corner side yard along Country Glen where the dwellings proposed exceed the minimum required corner side yard setback of 3 m.*

- **Notwithstanding Table 131(5)(b), visitor parking may be provided as parallel parking on a private way with a minimum width of 6.7m.**

*The proposed Site Plan shows the private driveway (private way) running parallel to the rear lot line with a minimum width of 6.7m therefore providing parallel visitor parking spaces which, comply with the By-law provision noted above.*

- **Townhouse Dwellings are subject to the following**

- 1. Maximum building height is 12m**

*The proposed Site Plan and Architectural Drawing show the townhouse dwellings which do not exceed the maximum building height of 12m.*

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### 2. Minimum building setback from the O1 zone is 1.2 metres.

*These dwellings exceed the minimum setback from the O1 zone with a setback of 1.75 m.*

- **Stacked Dwellings are subject to the following**

#### 1. Maximum building height is 16m

*The proposed Site Plan and Architectural Drawing show the stacked townhouse dwellings which do not exceed the maximum building height of 16m.*

#### 2. Private rooftop terraces are permitted, and the provisions set out in Table 55(8) of the Zoning By-Law do not apply

*As stated in the Planning Rationale submitted to the City in support of the rezoning application, rooftop terraces are proposed for the Metro Towns. Proposed roof plans are provided in this Site Plan Control application. The Site Plan complies with this provision and is not subject to the regulations set out in Table 55(8).*

#### 3. Minimum required parking rate is 0.9 space per dwelling unit

*The proposed regular parking for the stacked dwellings exceeds the rate of 0.9 spaces per dwelling unit. This requirement will be met through the proposal of above ground parking, provided on the private roads, and the underground parking for the residents.*

#### 4. Minimum required visitor parking rate is 0.1 space per dwelling unit

*The proposed visitor parking for the stacked dwellings exceeds the rate of 0.1 spaces per dwelling unit. Again, these spaces will be provided through above ground parking and in the underground parking garage for the residents.*

#### 5. Minimum building setback from the O1 zone is 3.0 metres

*These dwellings comply, as they are setback at least 4.6 m from the O1 zone.*

**Table 1** below demonstrates compliance with the approved Zoning Schedule and all relevant R4 Zoning provisions and those stemming from Urban Exception 2783.

<b>Table 1 – Planned Unit Development: required versus proposed</b>			
<b>R4Z – for planned unit development</b>	<b>Required</b>	<b>Proposed</b>	<b>Compliance</b>
<b>Permitted Use</b>			
Townhouse	-	✓	✓
Stacked Dwelling (Stacked Townhouse)	-	✓	✓
<b>R4Z Zone Provisions</b>			
Minimum Lot Area (sqm)	1,400	1,400+	✓
Minimum Frontage (m)	18	18+	✓
Minimum Front Yard Setback (m) <sup>1</sup>	3	4.84	✓
Minimum Corner Side Yard (m) <sup>1</sup> (East)	3	5.63	✓

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Minimum Corner Side Yard (m) <sup>1</sup> (West)	3	3	✓
Rear Yard Setback (m) <sup>2</sup>	6	12.93	✓
Minimum Landscape Area Coverage	30%	36.7%	✓
Maximum Building Height for Stacked Dwelling (m)	16	Less than 16	✓
Maximum Building Height for Townhouse (m)	12	Less than 12	✓

1. Footnote 5: The minimum setback between the vehicular entrance to a private garage or carport and an existing or planned sidewalk is 6.2 m. No portion of a private garage or carport shall be located more than 2.5 m closer to a street lot line than the closer of:

- (i) a building front wall or side wall, or
- (ii) a covered porch or veranda that is at least 2.5 m wide (By-law 2020-288)

2. Footnote 6: Where located outside of Area A of Schedule 342, the minimum required rear yard setback is 6 metres. Notwithstanding the foregoing, where the rear lot line abuts the interior side lot line of an abutting lot, the minimum required rear yard setback is equal to the minimum required interior side yard setback of the abutting lot along each point of the shared lot line. (By-law 2010-354) (By-law 2013-320) (By-law 2020-288)

The proposed development conforms with all other provisions including, planned unit development provisions, residential provisions, parking, amenity areas and any remaining general provisions of the Zoning By-law 2008-250.

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## 4.0 APPLICABLE POLICIES AND DESIGN GUIDELINES

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### 4.1 Planning Act

This application for Site Plan Control will meet the requirements prescribed under Section 41 of Ontario's *Planning Act*, R.S.O. 1990, c. P.13.

The following requirements for Site Plan Control prescribed under Section 41 of the *Act* are being met through this submission:

#### Section 41

*(4) No person shall undertake any development in an area designated under subsection (2) unless the council of the municipality or, where a referral has been made under subsection (12), the Tribunal has approved one or both, as the council may determine, of the following:*

*1. Plans showing the location of all buildings and structures to be erected and showing the location of all facilities and works to be provided in conjunction therewith and of all facilities and works required under clause (7) (a), including facilities designed to have regard for accessibility for persons with disabilities.*

**The following plans and studies are being submitted to provide information required in accordance with the above and clause (7) (a):**

- **A complete Architectural Design Package prepared by SRN Architects**
- **A Tree Conservation Report (TCR) and EIS prepared by Kilgour & Associates Limited.**
- **A Landscape Plan prepared by NAK Design Strategies**
- **A Functional Servicing Report, prepared by J.L. Richards & Associates Limited (Civil)**
- **A Planning Rationale / Design Brief prepared by J.L. Richards & Associates Limited (Planning)**

*2. Drawings showing plan, elevation and cross-section views for each building to be erected, except a building to be used for residential purposes containing fewer than 25 dwelling units, which drawings are sufficient to display,*

*(a) the massing and conceptual design of the proposed building;*

**The Architectural Design Package prepared by SRN Architects and submitted as a part of the application for Site Plan Control includes a Site Plan. NAK Design has developed massing / context perspectives. This Design Brief speaks to the massing and conceptual design of the proposed development.**

*(b) the relationship of the proposed building to adjacent buildings, streets, and exterior areas to which members of the public have access; N/A*

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The architectural and landscape architectural drawings and plans submitted as a part of this Application convey the relationship of the proposed buildings to adjacent buildings, streets and exterior areas to which members of the public have access, including the treatment and connections proposed adjacent to the LRT and the required public parkette. The details of these connections will be confirmed during the Site Plan Application and Approval process, as required by Staff. The Design Brief speaks to the surrounding context and the relationship with adjacent properties and public assets (existing / proposed).

*(c) the provision of interior walkways, stairs, elevators and escalators to which members of the public have access from streets, open spaces and interior walkways in adjacent buildings; N/A*

**The architectural design package prepared by SRN Architects, and the landscape plan prepared by NAK Design Strategies provide details in accordance with the clause noted above. These elements are further discussed within the Design Brief prepared by JLR.**

*(d) matters relating to exterior design, including without limitation the character, scale, appearance and design features of buildings, and their sustainable design, but only to the extent that it is a matter of exterior design, if an official plan and a by-law passed under subsection (2) that both contain provisions relating to such matters are in effect in the municipality;*

**This Design Brief covers all matters relating to exterior design, in accordance with the designs prepared by others for this application.**

*(e) the sustainable design elements on any adjoining highway under a municipality's jurisdiction, including without limitation trees, shrubs, hedges, plantings or other ground cover, permeable paving materials, street furniture, curb ramps, waste and recycling containers and bicycle parking facilities, if an official plan and a by-law passed under subsection (2) that both contain provisions relating to such matters are in effect in the municipality;*

**All plans include information as required above. Stage 6 is almost entirely devoid of natural cover, as per the combined EIS & TCR. The TCR confirmed that there are no significant trees. The Landscape Plan shows all proposed exterior sustainable design elements on-site. The Feedmill Creek is shown to the south of the LRT alignment.**

*(f) facilities designed to have regard for accessibility for persons with disabilities. R.S.O. 1990, c. P.13, s. 41 (4); 2002, c. 9, s. 56 (1); 2006, c. 23, s. 16 (3, 4); 2009, c. 33, Sched. 21, s. 10 (9); 2016, c. 25, Sched. 4, s. 5; 2017, c. 23, Sched. 5, s. 80. Almost same a #1\*

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**All facilities will be designed to have regard for accessibility for persons with disabilities, as shown on in the plans contained in the Architectural Design Package prepared by SRN Architects. AODA requirements for Type A and Type B accessible parking will be met.**

### 4.2 Provincial Policy Statement, 2020

The approved Zoning By-law Amendment, which permits the proposed development as described earlier, is consistent with the vision and policy direction from the Provincial Policy Statement 2020 (PPS) as issued under Section 3 of the *Planning Act*.

The policies of the PPS support building strong, healthy communities, wise use and management of resources, and protecting public health and safety. The proposed redevelopment is consistent with the policies of the PPS 2020, as first demonstrated through the Planning Rationale that was written in support of the Amendment to the City's Zoning By-Law 2008-250, which were furthermore approved by Council when they enacted by-law no.(s) 2022-175 on June 8<sup>th</sup>, 2022.

The proposed site plan is an appropriate form of greenfield development, one that represents an efficient use of land, existing services and infrastructure, and provides a range of unit types, including connections to a range of transportation options and community amenities in the surrounding context.

### 4.3 City of Ottawa Official Plan (2003, as amended)

J.L. Richards & Associates Limited (Ltd) prepared a Planning Rationale in support of the approved Zoning By-Law Amendment, final version dated February 16<sup>th</sup>, 2022. This report, which was informed by several discussions between the City, J.L. Richards and our client prior to Council's approval of the Zoning By-law Amendment, sufficiently reviewed and evaluated the policies of the City's existing Official Plan, 2003.

The Subject Property is located within the urban boundary (urban area) of the City of Ottawa and is part of the settlement area for the City. Schedule 'B' of the Official Plan shows the subject property designated as Mixed-Use Centre, which is further considered a design priority area in the context of the Official Plan policies. The proposed development is not required to attend the Urban Design Review Panel because it does not exceed four (4) storeys in building height. Emphasis on the quality of the building design has been made a priority and this is shown through the Site Plan and Architectural Drawings.

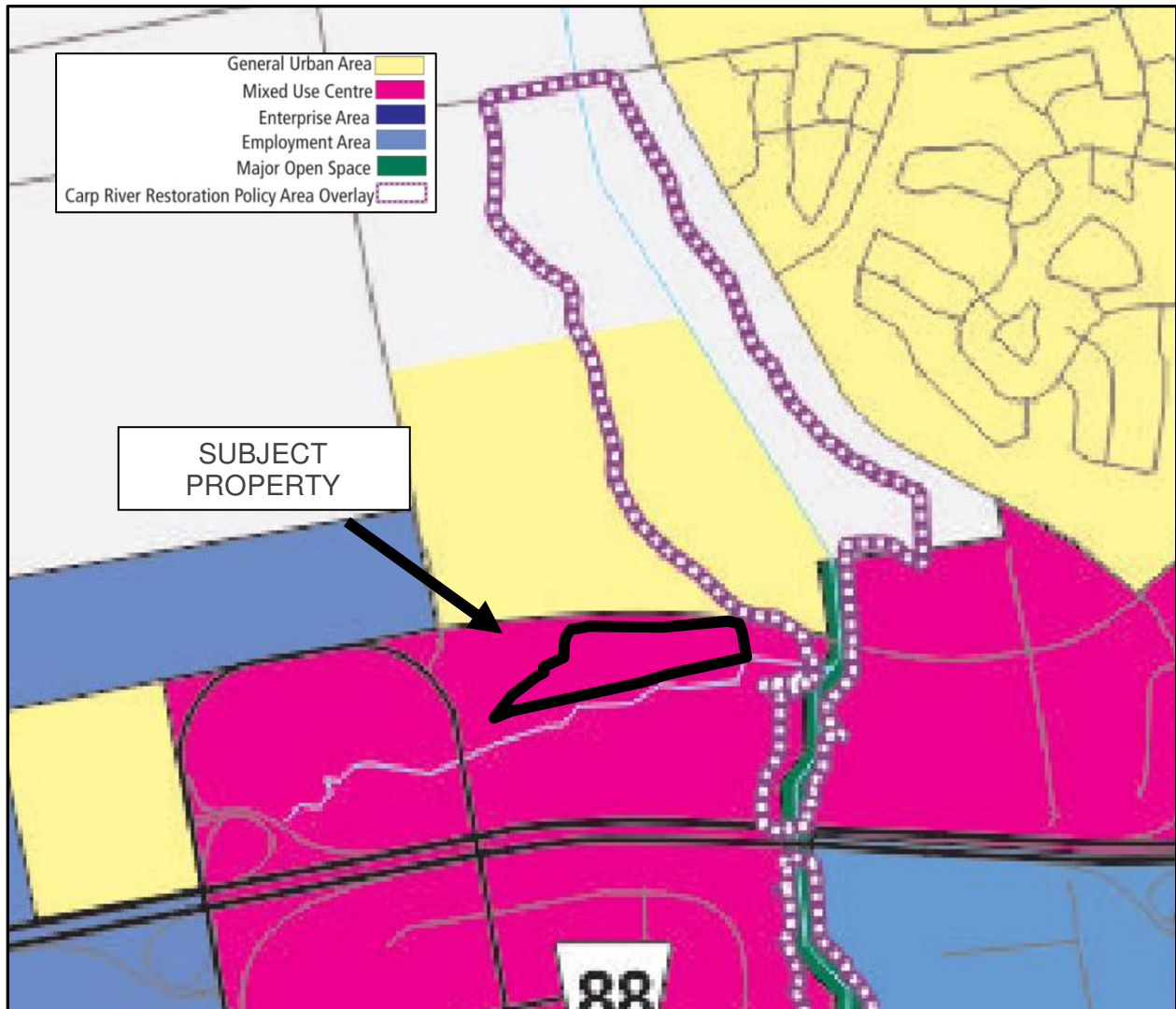
The development of these lands has been considered for quite some time now since the development of the Kanata West Concept Plan. In the existing Official Plan policy framework, these lands had been designated as a Mixed-Use Centre which could accommodate several uses, including residential, and have a higher density on the lands. The recently approved zoning conforms to the goal of this area as a variety of land uses are allowed, including stand-alone residential use, providing a range of building heights in the built form, and contributing to the range

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of housing supply in an area. The proposed development therefore conforms to Official Plan, 2003, as amended

**Figure 19** shows Schedule 'B'.



**Figure 19:** Existing City of Ottawa Official Plan Schedule B

The application for Zoning By-law Amendment was deemed complete prior to Council approval of the City's New Official Plan (2021) on November 24<sup>th</sup>, 2021 when they enacted by-law 2021-386. The City's New OP has not yet received ministerial approval. According to the City's Transition Policies, which provides guidance for planning applications submitted after Council's approval of the New OP, all Site Plan Control applications need only to be evaluated based on the approved zoning, as per Zoning By-Law 2008-250, as amended.



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Notwithstanding this approach, we have reviewed the policies of the new Official Plan and are satisfied that there are no policies that would be more restrictive that would adversely affect this application for Site Plan Control.

### 4.3.1 Designing Ottawa (2.5.1 of the OP)

Section 2.5.1 of Official Plan no. 2003, as amended, provides direction on patterns, locations of land uses and issues related to urban design to be sensitive to and compatible with existing communities. The Official Plan requires that all development applications be evaluated in the context of Section 2.5.1.

**Table 2** offers a response to how the proposed redevelopment addresses the design objectives:

<b>Table 2 – Conformity with the policies of Section 2.5.1</b>	
<b>Design Objectives</b>	<b>How the Proposed Development Meets the Objectives</b>
1. To enhance the sense of community by creating and maintaining places with their own distinct identity.	The proposed development will establish a highly dense residential hub next to a planned LRT station; complement the existing and planned pedestrian and cycling network; and bring people closer to jobs, in one of the most diverse employment areas of the City. This will encourage a diverse community of residents, which will ultimately create a sense of place and community that has its own identity. The separation between the proposed housing and LRT alignment will reduce noise and vibration impacts, thereby creating a comfortable urban environment for future residents. A detailed noise study will be submitted in the future at the time of Site Plan approval.
2. To define quality public and private spaces through development.	<p>The emphasis of the proposed development is to formalize connections to the existing public realm (multi-use pathway) along Campeau Drive and the LRT corridor. A large central public parkette is a placemaking move that will be greened and provide opportunities for recreation and leisure for residents and transit riders, as will be determined through the Site Plan Control application and approval process with City Staff. A complete facility fit plan will be provided to the City as a part of this process.</p> <p>The planned internal road network will provide easy access to Campeau Drive and the Country Glen Way, which will ultimately connect future residents and transit riders to businesses on adjacent lands to the west.</p>

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	<p>All proposed residential blocks will be connected by sidewalks (pedestrian infrastructure) and open space / outdoor amenity areas that will be useable and enhance social interaction within and among households. The proposed public/ private spaces will add to the existing greenspace network and strengthen the public realm along both existing and planned roads abutting the site.</p> <p>North-south and east-west pedestrian connections are provided throughout the subject property to create a walkable neighbourhood. A multi-use pathway is proposed within City owned land along the southerly property line, abutting the LRT alignment. This pathway will span the entire width of the subject property, connecting both County Glen Way and the proposed Winterset Road extension (by others). Connections this multi-use pathway is proposed through the proposed development.</p>
<p>3. To create places that are safe, accessible and are easy to get to and move through.</p>	<p>The proposed development will separate all public-private spaces to avoid any potential conflicts with the future operations of the LRT. The proposed development will consist of strong Crime Prevention through Environmental Design (CPTED) principles, such as appropriate lighting and landscaping that does not block sightlines or viewsheds from existing sidewalks and multi-use pathways.</p> <p>The proposed building orientation will allow pedestrians, cyclists and vehicles to view the proposed LRT alignment and station from Campeau Drive.</p> <p>Development will be street facing and spaced appropriately in accordance with the planned unit development provisions of the Zoning By-law.</p> <p>The proposed development is divided up into shorter/ smaller residential building blocks. Pedestrian walkways are proposed between stacked townhouse blocks to create a comfortable and safe pedestrian environment that allows for movement between buildings and spaces. Bump-outs at key locations within the proposed driveway entrances, drive aisles and parking lots will shorten pedestrian crossing</p>

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	<p>distances and therefore increase safety standards of the proposed development.</p>
<p>4. To ensure that new development respects the character of existing areas.</p>	<p>The proposed development has been designed to complement the existing built-form found across Campeau Drive, but also to anticipate the future commercial development to the west, which will create a different feel than what currently exists. While all proposed buildings generally orient towards Campeau Drive, the staggering of building footprints resembles a mixed-use village. This will blend nicely with any future commercial development and complement the existing built form along the north side of Campeau Drive.</p> <p>The intersection of Country Glen Way and Campeau Drive will be defined by an urban treatment, consisting of townhouses that orient better to the roundabout. The internal block pattern provides shorter, more urban residential blocks.</p> <p>The subject property is currently bounded by road and vacant land. The R4Z setbacks applied through the proposed development reflect a more compact urban built-form that reflects the development pattern opposite Campeau, while strengthening the relationship to streetscape and public realm.</p> <p>Separation between uses and land use compatibility is not really a concern for the proposed development, as the site is surrounded by road on three sides and appropriately separated from future commercial development to the west.</p> <p>Nearby commercial development and proximity to transit will certainly establish a truly mixed-used centre in the immediate surrounding area.</p>
<p>5. To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.</p>	<p>The proposal considers long-term adaptability and housing diversity by proposing a well-balanced range of dense residential uses on the subject property, ranging in types and styles. The proposed development is characterized by variety and choice, as reflect by the balance and mix of dwelling unit types and sizes proposed. The proposed dwelling unit types and sizes offer some flexibility for young families and the</p>

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	<p>evolving workforce, including more overall floor space to accommodate Work-At-Home (WAH) scenarios.</p>
<p>6. To understand and respect natural processes and features in development design.</p>	<p>The proposed development will result in the removal of some trees, none of which have been identified as significant. Tree planting throughout the subject property will result in a significant contribution to the tree canopy. A Tree Conservation Report (TCR) and Landscape Plan has been submitted and provides detailed information on tree planting and the design of the proposed outdoor amenity area. Please see the Landscape Plan prepared by NAK for more.</p>
<p>7. To maximize energy efficiency and promote sustainable design to reduce the resource consumption, energy use and carbon footprint of the built environment. <b>(Sustainability Statement).</b></p>	<p>The proposed development will include energy efficiency design and greening.</p> <p>The following interior and exterior energy efficiency features and measures are being considered for integration:</p> <ul style="list-style-type: none"> <li>• Timed interior lighting</li> <li>• Smart Thermostats</li> <li>• Smart Showerheads (Low Flow)</li> <li>• LED interior lighting</li> <li>• Low VOC paint</li> <li>• Energy Star appliance</li> <li>• Triple Glazed Windows, as required by the Noise Study and not applicable to the entire site.</li> <li>• EV Charging Stations (underground garage)</li> <li>• Sensor Lighting (underground garage)</li> <li>• Low water gardening (potential)</li> </ul> <p>The proposed development offers the minimum requirement for bike parking and is found near commercial, and employment uses, rapid transit and multi-use pathways which provides the opportunity to reduce automobile dependency and promote sustainability</p>

### 4.3.2 Urban Design and Compatibility Guidelines

Section 4.11 of Official Plan no. 2003, as amended, provides direction on how to assess development applications to ensure their integration into, and compatibility with, existing or planned community character and in the overall context of the OP. The policies require

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consideration for views, design, massing, and amenity space, among others, as key factors for assessing the relationship between new and existing development.

As this application for Site Plan Control is for a greenfield development, the compatibility with existing built form is less of a factor as subject property is essentially landlocked by road and the LRT corridor. This is an evolving neighbourhood with a mix of uses and the proposed development aims to be complimentary.

The focus of this Site Plan Control application is to demonstrate how the proposed planned unit development of townhouses and stacked townhouses complies with the recently approved Zoning and resembles a suitable form of intensification adjacent to a rapid transit station. The Site Plan conforms to the broader intent of policies found in the Official Plan(s) and more specific provisions that are found in the Zoning By-law Amendment.

### **4.4 Additional Guidelines and Policies**

#### **4.4.1 Kanata West Concept Plan**

The Kanata West Concept Plan was first development from a Regional Official Plan Amendment #9 (ROPA 9), which was enacted on October 11, 2000, and expanded Ottawa's urban area by approximately 725 hectares (1791 acres). This expansion was aimed primarily at addressing the need for an adequate supply of land to meet the anticipated growth of the high technology sector.

The original vision for the subject property, shown in the Demonstration Plan, was that the site would develop as "High Profile Employment" and "Intensive Employment Area". However, following the implementation of the Kanata West Concept Plan several policy changes inevitably changed the land use context for the subject property. These changes coincided with the need for more residential land, which gave rise to the development of the Arcadia Subdivision.

As a result of this change in policy direction, the Kanata West Concept Plan was therefore not evaluated in the context of the proposed development. The Plan proposed a land use plan that is now considered outdated and substantially different than what currently in the community today, making the guidelines irrelevant to review in the context of this residential development. The guidelines were intended for high-profile and intensive employment use only, not a residential development.

Rather, the most relevant design guidelines in the context of the proposed development are the City's Transit Oriented Development (TOD) guidelines. These are reviewed following the City's policy criteria.

#### **4.4.2 Transit-Oriented Development Guidelines (2007)**

The subject property is within a 600 m walking distance from a planned LRT Station as per the City of Ottawa's Transportation Master Plan (TMP) – Ultimate Network. The proposed development is then subject to the City's Transit-Oriented Development Guidelines (2007) which provides guidance for the proper development of these strategically located properties. The

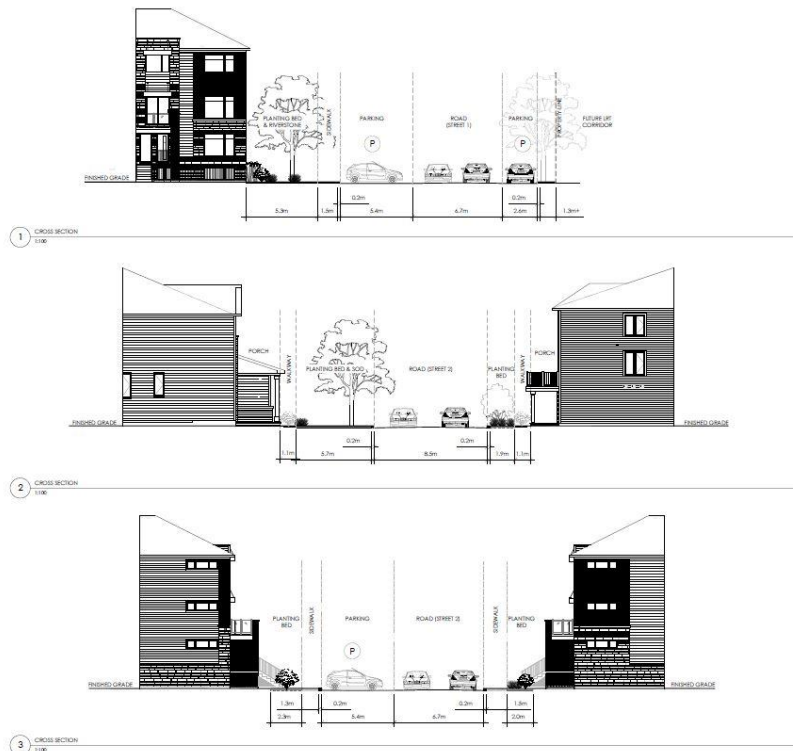
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guidelines address six elements of urban design including: land use, layout, built form, pedestrians and cyclists, vehicles and parking, and streetscape and environment.

These guidelines have been reviewed for adherence. The proposed development is transit supportive through the increased level of residential density and the emphasis placed on improving connections to sidewalks and nearby multi-use pathways. Resulting from the recently approved zoning for the site, the proposed development provides less parking and meets the bike parking requirements of the Zoning By-Law. The underground parking garages will provide minimal disruption to the pedestrian environment created above ground and maximize areas on-site for landscaping and open space.

## 4.4.1 Urban Design Guidelines for Greenfield Neighbourhoods (2007)

Ottawa City Council approved the Urban Design Guidelines for Greenfield Neighbourhoods in Fall 2007. These Guidelines outline key expectations for new neighbourhoods being developed within the Urban Area of the City of Ottawa. Key elements of these guidelines include direction on: subdivision design, including structuring layout; street design; residential building and site design; and design of greenspaces, and utilities and amenities. **Figure 19** below shows the typical private road cross sections proposed within the Arcadia Stage 6 development, which have been included in the Landscape Design package prepared by NAK Design Strategies.



**Figure 20: Private Road Cross Sections (proposed)**

The proposed site plan is consistent with the direction provided in the Urban Design Guidelines for Greenfield Neighbourhoods.

### 5.0 CONCLUSION

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This Design Brief has been prepared to support the Site Plan Application for Stage 6 in the Arcadia Subdivision at 8415 Campeau Drive in Kanata West in light of the approved Zoning. It is in our professional opinion that the proposed greenfield development is appropriate and will comply with the recently amended zoning for the site and all other provisions of Zoning By-Law 2008-250.

The following is true of the proposed Application and development:

- The new development will see the construction of 368 residential dwelling units, with a range of townhouse typologies and provide access to many local amenities through a transit-supportive community and sustainable development.
- The Site Plan is consistent with the Provincial Policy Statement, 2020 (PPS) as the proposed development is located within a designated Settlement Area, is compatible with its surroundings and is consistent with the matters of provincial interest identified in the PPS.
- The application also adheres to the provisions of the Official Plan which has identified these lands as Mixed-Use Centre (design priority area). This proposal conforms to the intent of these designations in that transit-supportive land uses, such as denser residential development forms, are encouraged.
- The proposed development conforms to the recently approved zoning for the subject property and is supported by various supporting studies and plans.
- Finally, the proposed development meets the intent of all design guidelines and policy criteria in which apply to the site.

Based upon the above, this Site Plan Application represents good land use planning and urban design. The proposal is consistent with existing built form of the previous stages of the Arcadia subdivision, while developing a built form that is modern and supportive of the adjacent LRT station.

This report has been prepared for the exclusive use of Minto Communities, for the stated purpose, for the named facility. Its discussions and conclusions are summary in nature and cannot be properly used, interpreted or extended to other purposes without a detailed understanding and discussions with the client as to its mandated purpose, scope and limitations. This report was prepared for the sole benefit and use of Minto Communities and may not be used or relied on by any other party without the express written consent of J.L. Richards & Associates Limited.

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# Planning Rationale / Design Brief

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J.L. RICHARDS & ASSOCIATES LIMITED

Prepared by:



Connor Joy, BURPI  
Intern Planner

Prepared by:



Eric Forhan, MScPI  
Planner

Reviewed by:



Timothy F. Chadder, MCIP, RPP  
Associate  
Chief Planner



**APPENDIX A**  
**By-Law No. 2022-175**  
**(Zoning By-Law Amendment)**

BY-LAW NO. 2022 - 175

A by-law of the City of Ottawa to amend By-law No. 2008-250 to change the zoning of the lands known municipally as 8415 Campeau Drive.

The Council of the City of Ottawa, pursuant to Section 34 of the *Planning Act*, R.S.O.1990, enacts as follows:

1. The Zoning Map of By-law No. 2008-250, entitled the “City of Ottawa Zoning By-law” is amended by rezoning the lands shown on Attachment 1 as follows:

- a) Area A from DR to R4Z[2783]; and,
- b) Area B from DR to O1.

2. Section 239 – Urban Exceptions of the said By-law No. 2008-250 is amended by adding the following exception:

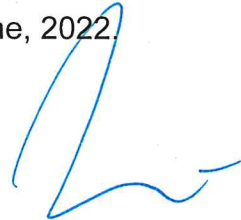
I Exception Number	II Applicable Zone	Exception Provisions		
		III Additional Land Uses Permitted	IV Land Uses Prohibited	V Provisions
2783	R4Z[2783]			<p>-The lot line that abuts Campeau Drive is considered the front lot line.</p> <p>-The side yard along Country Glen Way is considered to extend from Campeau Drive to the southern extent of Country Glen Way.</p> <p>-Notwithstanding Table 131(5)(b), visitor parking may be provided as parallel parking on a private way with a minimum width of 6.7 m.</p> <p>-Townhouse Dwellings are subject to the following:</p> <ul style="list-style-type: none"> <li>i. Maximum building height is 12 m.</li> <li>ii. Minimum building setback from the O1 zone is 1.2 metres.</li> </ul>

				<p>-Stacked Dwellings are subject to the following:</p> <ul style="list-style-type: none"><li>i. Maximum building height is 16 metres.</li><li>ii. Private rooftop terraces are permitted, and the provisions set out in Table 55(8) of the Zoning By-law do not apply.</li><li>iii. Minimum required parking rate is 0.9 space per dwelling unit.</li><li>iv. Minimum required visitor parking rate is 0.1 space per dwelling unit.</li><li>v. Minimum building setback from the O1 zone is 3 m.</li></ul>
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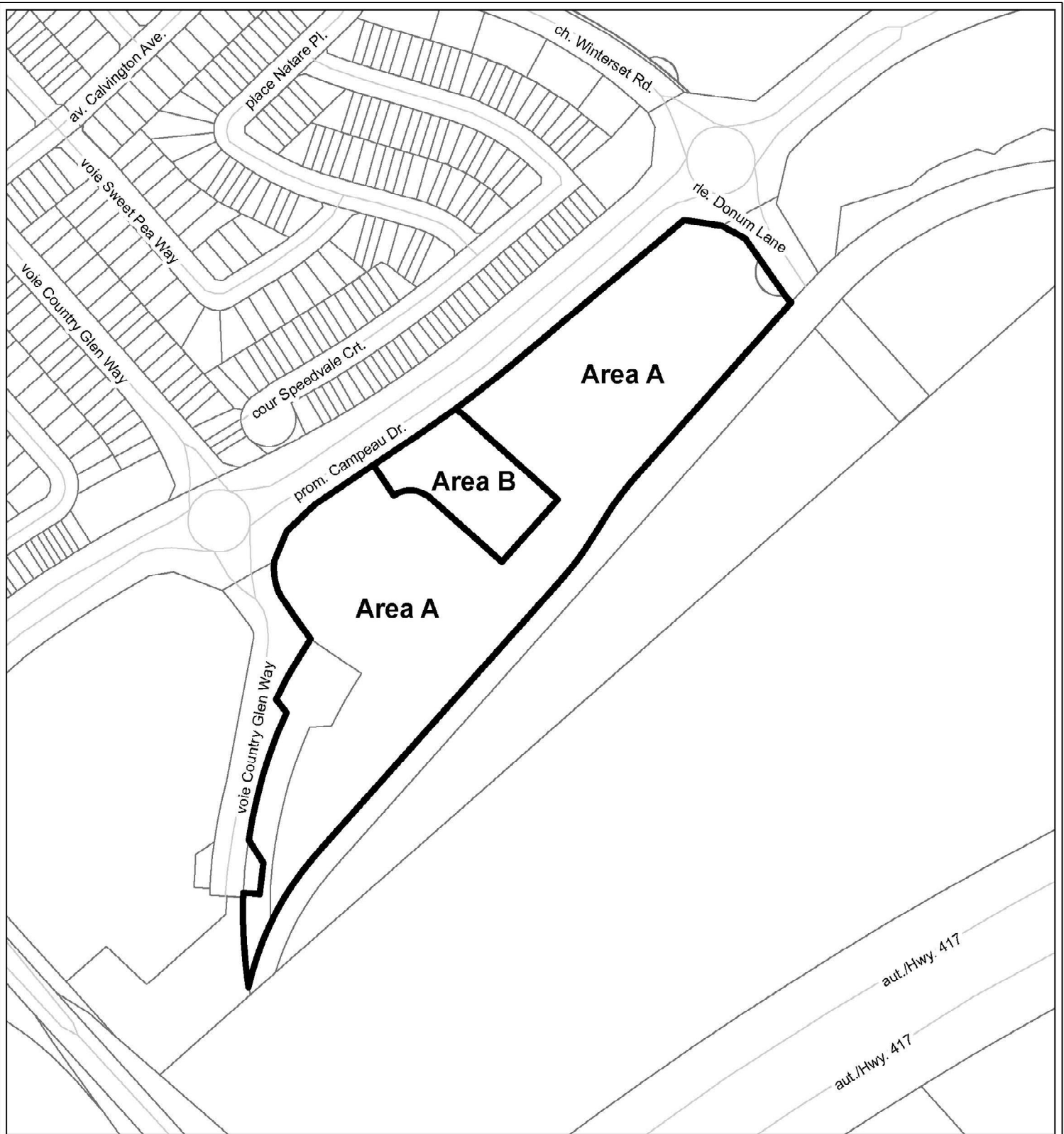
ENACTED AND PASSED this 8<sup>th</sup> day of June, 2022.



DEPUTY CITY CLERK



MAYOR



D02-02-21-0127

22-0463-X

M:\Zoning\_Bylaw\bylaws\

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LANDS AFFECTED PART OF THE ZONING  
MAP OF BY-LAW NO. 2008-250

This is Attachment 1 to By-law Number 2022-175, passed June 8, 2022

**Lands Affected by By-law**

Area A to be rezoned from DR to R4Z[2783]

Area B to be rezoned from DR to O1







[www.jlrichards.ca](http://www.jlrichards.ca)

**Ottawa**

864 Lady Ellen Place  
Ottawa ON Canada  
K1Z 5M2  
Tel: 613 728-3571

[ottawa@jlrichards.ca](mailto:ottawa@jlrichards.ca)

**Kingston**

203-863 Princess Street  
Kingston ON Canada  
K7L 5N4  
Tel: 613 544-1424

[kingston@jlrichards.ca](mailto:kingston@jlrichards.ca)

**Sudbury**

314 Countryside Drive  
Sudbury ON Canada  
P3E 6G2  
Tel: 705 522-8174

[sudbury@jlrichards.ca](mailto:sudbury@jlrichards.ca)

**Timmins**

201-150 Algonquin Blvd.  
East  
Timmins ON Canada  
P4N 1A7  
Tel: 705 360-1899

[timmins@jlrichards.ca](mailto:timmins@jlrichards.ca)

**North Bay**

200-175 Progress Road  
North Bay ON Canada  
P1A 0B8  
Tel: 705 495-7597

[northbay@jlrichards.ca](mailto:northbay@jlrichards.ca)

**Hawkesbury**

326 Bertha Street  
Hawkesbury ON Canada  
K6A 2A8  
Tel: 613 632-0287

[hawkesbury@jlrichards.ca](mailto:hawkesbury@jlrichards.ca)

**Guelph**

107-450 Speedvale Ave.  
West Guelph ON Canada  
N1H 7Y6  
Tel: 519 763-0713

[guelph@jlrichards.ca](mailto:guelph@jlrichards.ca)

