



August 21, 2012

City of Ottawa
110 Laurier Avenue West, 4th Floor
Ottawa, ON K1P 1J1

Attention: Wally Dubyk, C.E.T.
Project Manager, Infrastructure Approvals

**Re: OTT-00205359-A0 215 McLeod Street – Iraqi Embassy Expansion
Transportation Overview**

Dear Mr. Dubyk:

We are pleased to submit the following Transportation Overview in support of the site plan control application for the new Iraqi Embassy site at 215 McLeod Street in Ottawa, Ontario. The following is a brief description of the proposed development from a transportation perspective, a description of the existing transportation network, the anticipated community impacts, and a discussion of traffic demand management strategies for the site.

Proposed Development

A new building with a gross floor area (GFA) of 2,619 m² (28,190 sq. ft) is proposed to replace the existing Embassy building (554 m² GFA - 5,960 sq.ft). The above ground parking area for the site has been eliminated and replaced with underground parking. A total of twenty-five (25) secure valet underground parking spaces for staff have been provided, including one (1) parking space for the disabled. Bicycle parking spaces and separate change rooms with bathrooms and showers have been provided in the underground garage for cyclists. Staff access to the underground parking garage will be provided by the 10m easterly relocation of the existing gated two-way driveway on McLeod Street. Due to security concerns, public access to the existing and the proposed Embassy building is limited to pedestrian traffic. There is no visitor parking on both the existing site and on the proposed site plan due to security concerns.

Current zoning for the site is GM1[39]f(1.0). Details of zoning changes required for the site to accommodate the planned development are provided in the Planning Rationale prepared by CIMA+ submitted under a separate cover. Assuming that the 'office' land-use designation is appropriate for the Embassy, the City of Ottawa Zoning By-law 2008-250 requires a total of forty one (41). The total number of spaces is based on requirement for 2 spaces per 100 m² of GFA less a 10% reduction when all parking is provided underground (Section 102 Clause 6). A further reduction of six (6) parking spaces is allowed under Section 111 Clause (12) when change rooms and showers are provided for cyclists. In addition to vehicle parking, the Zoning By-law requires 1 bicycle parking space per 250 m² of floor area which equals a requirement of 11 bicycle parking spaces. In addition to providing 80 m² for separate shower and change room

facilities, the proposed site plan provides 11 secure spaces in the underground parking level. Embassy officials are committed to mitigating the sixteen (16) space shortfall in required parking by enhancing their existing Transportation Demand Management (TDM) strategies in consultation with City staff. A detailed discussion of TDM is included below in a separate section of this letter.

The proposed site plan features a paving stone pedestrian boulevard complete with benches along the front of the building. The entrance to the Embassy is raised. Steps and universal access ramps provide pedestrian access from the City sidewalks to the front doors of the building. It is anticipated that the construction of the site will be completed for an opening tentatively scheduled for the fall of 2014.

Existing Transportation Network

McLeod Street is a two-lane, two-way local road from the Queen Elizabeth Driveway to Elgin Street. West of Elgin Street, McLeod becomes a one-way two-lane local road, with an east-west orientation except when it connects the north and south legs of Metcalfe Street near the Museum of Nature. This section of McLeod Street becomes a two-lane one-way urban arterial without on-street parking. There is no posted speed limit along McLeod Street, and therefore subject to the 50 km/hr speed limit as set out in the Highway Traffic Act.

Concrete sidewalks are provided on both sides of McLeod Street and provide strong pedestrian links between the concrete sidewalks located on both sides of Elgin Street, O'Connor Avenue, and Metcalfe Street (north of McLeod). Pedestrians on the south side of McLeod Street are directed to the north side of the street at the unsignalized intersection of Metcalfe Street (south of McLeod) and McLeod Street. Pedestrians are directed along the north side of McLeod Street to the signalized intersection of Metcalfe Street (north of McLeod) and McLeod Street. Pedestrian crossings at the signalized intersections of Metcalfe Street (north of McLeod) and McLeod Street tie-into depressed and textured pedestrian ramps. Street lights are mounted on utility poles on the north side of the street.

The Transportation Master Plan does not identify any dedicated on-road or off-road cycling facilities, or multi-use pathways on McLeod Street. Gladstone Avenue, O'Connor Street and Metcalfe Street, north of Gladstone Avenue, have been identified as on-road cycling routes as part of the City-wide cycling network.

Regular bus service is provided by OC Transpo route #14 along Gladstone Avenue and by Route #5 along Elgin Street. Peak hour service is provided by Route #6 along Elgin Street. Bus stops #7669, #2374, #7672, #7670, #7671 on Gladstone Avenue and stops #2472, #2468 and #2473 on Elgin Street are located within the OC Transpo service guideline of 400m radius.

Approximately 180 on-street (free and paid) parking spots are provided by the City in the area bounded by Cartier Street and O'Connor Street, and by Argyle Street and Gladstone Avenue. A small private parking lot provides ten (10) paid spaces at the northwest corner of the Elgin Street and Gladstone Avenue intersection. According to the Museum of Nature web-site, when construction is complete on Lot 2 a total of 246 parking spots will be provided by the museum.

This represents a net increase of 150 parking spaces over what currently is provided in the area. Parking for cyclists is provided along the south side of McLeod Street, and the NCC will installed a BIXI bike station at the museum in Spring of 2012.

Community Impacts

The proposed new building will enhance the street by replacing the building (circa 1980) and perimeter security fence with a distinctive modern building with strong ties to the pedestrian network through a stone paved boulevard and park benches.

Impact on the nearby signalized intersections is expected to be negligible based on the size of development and does not warrant a traffic study, as is outlined in the City of Ottawa Traffic Impact Assessment guidelines. The City has expressed concern that the proposed site does not meet the minimum parking requirements.

Chauffeurs transport some staff, dignitaries and guests to the site during regular business hours and during important events, reducing the demand for on-site parking. No changes are expected in the operations of the Embassy. Visitor parking demand is included in typical parking rates of an office land-use under zoning. There is no visitor parking provided by the existing Embassy, and there can be no visitor parking provided in the new site plan due to security concerns. Therefore, we expect that the parking demand of the site to be smaller than that of a typical 'office' land-use.

According to the NCC Museum project web page, on-street parking availability in the area is scarce during the summer tourist season, on weekends and on holidays because of the current lack of parking for patrons of the Museum of Nature. The museum expects that the opening of a second lot located on the west side of the property will improve on-street parking availability in the neighborhood. When completed, current plans for the new parking lot will add 150 spaces and bring the total supply of parking (on-street and in lots) in the neighborhood to 435 spaces. This increase in the supply of parking at the museum is expected to increase in parking availability in the area substantially. Visitors to the Museum will be able to park on-site increasing the availability of on-street parking for all motorists. It should be noted that embassy is closed to the public on weekends and on holidays when parking demand from the museum is high.

We presume that the parking demand for the Embassy will be lower than that required under zoning regulations due to the small staff complement (33 persons anticipated), to the use of chauffeurs to transport senior embassy staff and dignitaries to the site, and to an on-going commitment to Transportation Demand Management (TDM) from senior embassy officials. We expect that the new building will have a negligible effect on parking availability in the neighboring given since there will be little change to the staff complement and the nature of operation of the Embassy.

Transportation Demand Management

Transportation Demand Management (TDM) is a City of Ottawa strategic initiative aimed at reducing automobile dependency. TDM measures encourage people to change their travel

mode, timing or destination to reduce the need for transportation roadway infrastructure. It can include the more efficient and effective use of existing transportation infrastructure.

The planned development supports the City TDM initiatives by providing infrastructure that enhances pedestrian linkages to the downtown pedestrian network, transit and cycling networks, and by limiting on-site parking. The preliminary site plan provides park benches and stone pavers between the building and the sidewalk along the front of the site to promote the pedestrian appeal of the site. A sidewalk is provided along the east side of the building from the parking garage to universal access ramp at the front of the site. Benches have been provided in the paved boulevard in front of the building to encourage casual use of the boulevard by pedestrians. Bicycle parking spaces and change rooms with showers have been provided for staff in the underground parking level of the new building. The development has easy access to transit with at least seven (7) OC Transpo bus stops within walking distance of the site. Similarly, the proximity of the Embassy to city-wide cycling routes on Gladstone Avenue, O'Connor Street, and Metcalfe Street, north of Gladstone Avenue, provides cyclists with high quality transportation links to the Embassy.

Senior Embassy officials recognize that the parking shortfall is a concern to the City, and to the community. The following TDM measures will be in-place at opening to promote active modes of transportation, and to reduce parking demand:

- continue to chauffeur some officials, dignitaries and select staff;
- provide transportation to staff in case of home emergencies;
- promote carpooling and the City's online ride matching service;
- locate transit and cycling information boards in public and staff areas;
- provide Ecopass/Presto cards to staff at a reduced cost or through payroll deduction;
- provide staff with incentives (i.e. reduced rates, one month free, etc.) to use car sharing and/or bike sharing facilities (e.g. VRTUCAR, Capital BIXI); and,
- provide staff parking at or above market rates.

The Embassy will exercise a leadership role and implement strategic initiatives, aimed at reducing vehicular and parking demand by promoting sustainable modes of transportation. A staff member, reporting to senior Embassy officials, will be identified to promote the use of walking, cycling and transit within the organization. This staff member will be tasked to develop a Workplace Travel Plan (WTP), in consultation with the City of Ottawa, within 12 months following the opening of the new building.

Conclusions and Recommendations

We trust the transportation overview of the proposed development of 215 McLeod Street has examined the issues in sufficient detail to support the following conclusions:

- the impact on the nearby signalized intersections is expected to be negligible based on the size of development;

- the completion of a second parking lot at the Museum of Nature will increase the supply of parking by 150 spaces to a total of 435 spaces within 300m of the site;
- peak demand for parking for the museum occurs on week-ends and holidays, and does not correspond with peak weekday demand of the Embassy;
- the proposed site plan enhances links to existing walking, cycling and transit infrastructure in the downtown area;
- TDM initiatives will be provided upon completion to reduce parking demand, and to promote active modes of transportation; and,
- a comprehensive workplace travel plan (WTP) will be developed within 12 months of opening to address the parking deficit of sixteen (16) spaces under zoning.

Therefore, we recommend that the site plan applications for the proposed Iraqi Embassy at 215 McLeod Street be approved from a transportation perspective.

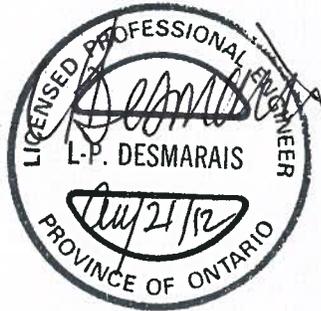
We trust this transportation overview has addressed the transportation issues for the proposed development. Should you have any questions or comments, do not hesitate to contact the undersigned.

Yours truly,

exp Services Inc.



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