

**Embassy of the Republic of Iraq  
215 McLeod Street  
Ottawa, ON**

**Zoning Bylaw Amendment and  
Site Plan Control Applications  
Planning Rationale**



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Existing Embassy Building, spring 2012



Proposed Embassy Building

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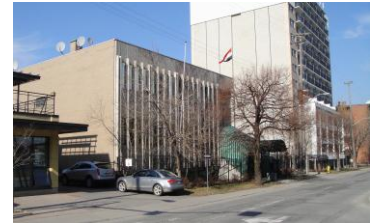
## 1. INTRODUCTION

The following planning rationale has been prepared by CIMA's Urban Planning and Sustainable Development office in Ottawa on behalf of the Embassy of the Republic of Iraq. This rationale supports the proposed zoning by-law amendment application and site plan control application for the construction of a new Embassy building to be located at 215 McLeod Street.

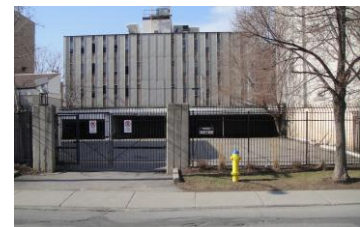
The context and rationale highlight how the development of a new embassy on this site complies with the Provincial Policy Statement (PPS), the City of Ottawa Official Plan (OP) and the intent of City of Ottawa's consolidated Zoning By-law (ZBL) as well as associated urban design guidelines for the Downtown, draft community design plan and build heritage.

### 1.1 BACKGROUND

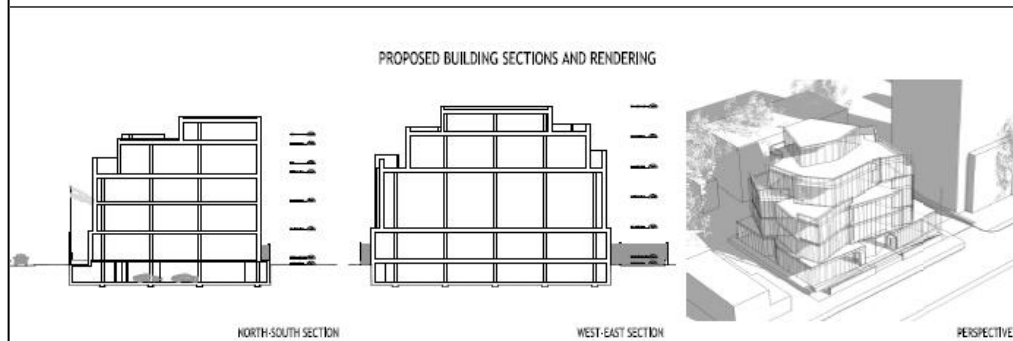
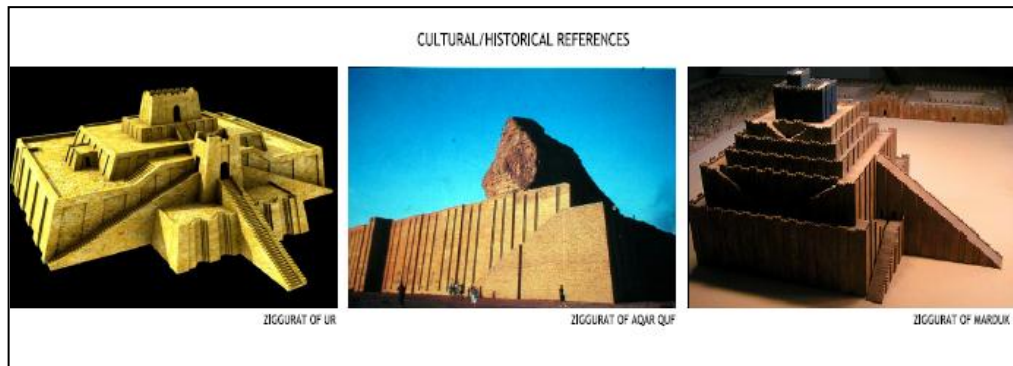
The subject site features an existing two storey concrete structure of approximately 554 sq metres for gross floor area (GFA), surrounded by a 3m-high wrought iron fence. The building was erected in 1957 and is serviced by a fenced parking lot along the east side. The parking lot is only accessible through a set of security gates and is used exclusively for diplomatic vehicles. The site presently has a one storey carport capable of housing six vehicles, located at the rear of the property.



The Government of the Republic of Iraq, through the current Iraqi Ambassador to Canada, is seeking to redevelop the site by having the existing building demolished and replacing it with a new modern embassy. The existing building has served as the embassy of Iraq since its construction in the late 1950's. The building has fallen into disrepair over the years due to major political changes in Iraq and was left vacant for a period during the early 2000's.



The Government of Iraq is now seeking to improve its diplomatic ties and intergovernmental services through the establishment of a larger and more modern embassy. The new Embassy building of approximately 2,619 sq. metres GFA will be a major improvement upon the former design. The proposed new building will incorporate historical Iraqi architectural elements and ornamentation. This new embassy will be designed to accommodate a total staff of approximately 30 personnel.





## 2. SITE LOCATION AND NEIGHBOURHOOD CONTEXT

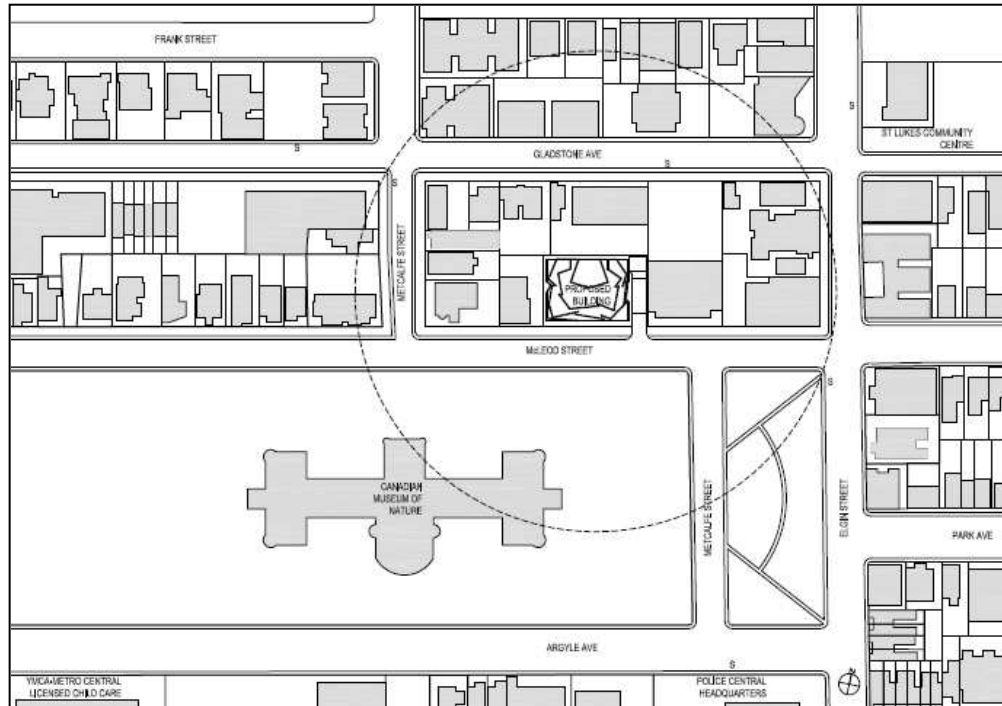
Citywide context:



The subject site is located at 215 McLeod Street in the south-eastern quarter of the Centretown neighbourhood. Centretown is generally defined as that area of the city located to the east of Bronson Avenue, west of the Rideau Canal, north of the 417 Queensway Freeway and south of Gloucester Street. Centretown is presently located within Somerset Ward.

The Centretown neighborhood is composed of a mix of residential and commercial uses. Housing types range from single family detached units to high-rise condominium and apartment buildings. Centretown also features two of Ottawa's main commercial avenues on Bank Street and Elgin Street. Statistics Canada reported that this neighbourhood had a local residential population of approximately 20,513 residents in 2006.

**Neighbourhood Context:**



The embassy site is located directly north across the street from the Canadian Museum of Nature and in close proximity to other embassy sites located along Metcalfe Street including the embassies of Hungary, Nigeria and Ukraine.

The subject site fronts directly onto the north side of McLeod Street and is located midblock between Metcalfe Street and Elgin Streets. This section of Metcalfe is a one-way street that serves as an important connection route for vehicles exiting the Queensway and traveling downtown via Metcalfe. The Embassy has direct access to bus transit with **3** bus stops located within **100m** of the site.

Immediately to the south of the site is the Canadian Museum of Nature which is a popular tourist attraction and draws visitors from across the city. To the east of the site is located a residential neighbourhood commonly referred to as the Golden Triangle in addition to the Rideau Canal. There is a wide range of existing mixed land uses within close proximity to the site including high density residential buildings, offices, restaurants, and recreational open space.



**Site Context:**



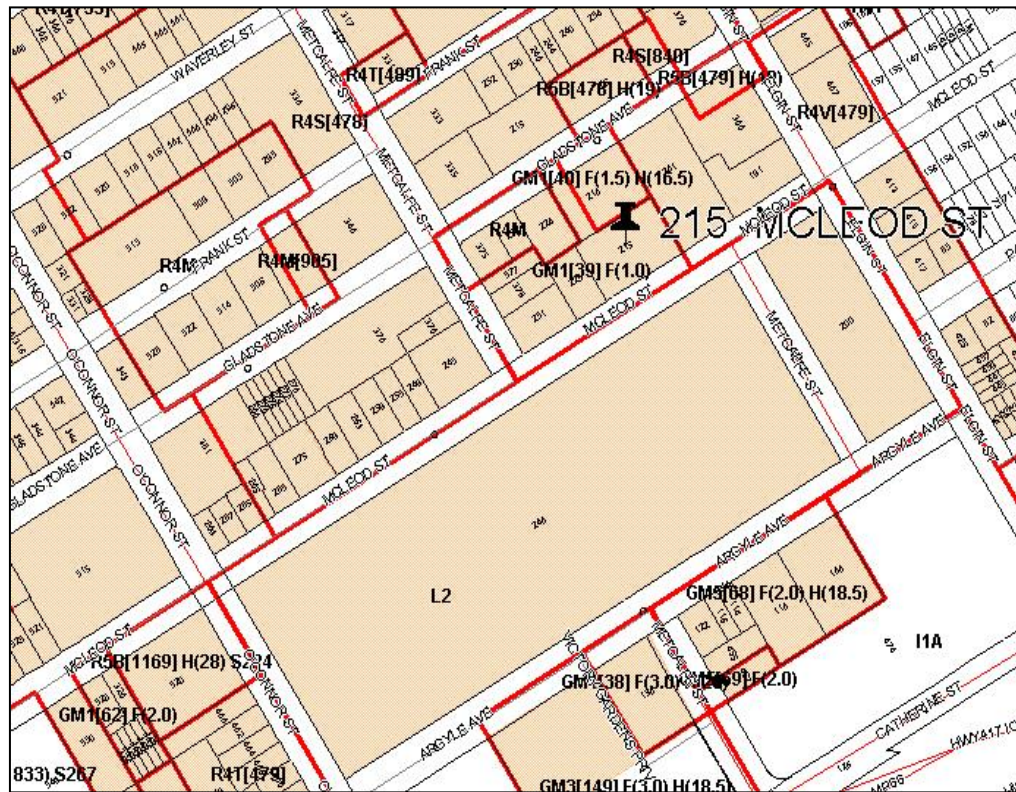
The majority of the buildings abutting the site are mid-rise or high-rise residential and commercial buildings. To the east is a twelve storey residential apartment building. A four storey commercial office complex is located immediately to the north of the site and fronts onto to Gladstone Street. A row of two storey town houses is located kitty-corner to the sites



rear yard. To the west of the site is located a two storey professional office building. The Museum of Nature is located to the south of the site and features an associated parking lot sited directly across from the Embassy.

### 3. CURRENT ZONING BY-LAW PROVISIONS

The subject site is currently zoned GM1[39] F(1.0) within the City of Ottawa's Zoning By-law 2008-250 as amended.



The GM zone is the “General Mixed-Use” Zone. The purpose of this zone is to:

- 1) Allow residential, commercial and institutional uses, or mixed use development in the **General Urban Area** and in the **Upper Town, Lowertown and Sandy Hill West Character Areas** of the **Central Area** designations of the Official Plan;
- 2) Limit commercial uses to individual occupancies or in groupings in well defined areas such that they do not affect the development of the designated Traditional and Arterial Mainstreets as viable mixed-use areas;
- 3) Permit uses that are often large and serve or draw from broader areas than the surrounding community and which may generate traffic, noise or other impacts

*provided the anticipated impacts are adequately mitigated or otherwise addressed;  
 and*

- 4) *Impose development standards that will ensure that the uses are compatible and complement surrounding land uses.*

**TABLE 187 – GM ZONE PROVISIONS** (adapted from the City of Ottawa Zoning By-law)

I ZONING MECHANISMS		II PROVISIONS	
(a) Minimum lot area		No minimum	
(b) Minimum lot width		No minimum	
(c) Minimum front yard and corner side yard setbacks		3 m	
(d) Minimum interior side yard setbacks	(i) for a non-residential or mixed-use building, from any portion of a lot line abutting a residential zone	5 m	
	(ii) for a residential use building	1. for a building equal or lower than 11 metres in height	1.2 m
		2. for a building higher than 11 metres in height	3 m
	(iii) all other cases	No minimum	
(e) Minimum rear yard setback	(i) abutting a street	3 m	
	(ii) from any portion of a rear lot line abutting a residential zone	7.5 m	
	(iii) for a residential use building	7.5 m	
	(iv) all other cases	No minimum	
(f) Maximum building height		18 m	
(g) Maximum floor space index		2, unless otherwise shown	
(h) Minimum width of landscaped area	(i) abutting a street	3 m	
	(ii) abutting a residential or institutional zone	3 m	
	(iii) other cases	No minimum	



Permitted uses within the GM zone allow for a range of residential and non-residential uses. Exception [39] prohibits all non-residential uses except artist studios and offices. The zoning is also modified by a F(1.0) indicating a maximum FSI of 1.0 for any future development. The permitted FSI for this

zoning is even further modified by the GM1 subzone which prescribes “no more than 50% of the permitted floor space index may be used.”

Although there is no specific defined use for “Embassy” under the current zoning by-law, an embassy is interpreted to fall under the definition of “Office” as it fulfills part (c) of the City’s formal definition of an office.

The City’s zoning bylaw defines “Office” to mean a place used by an agency, business or organization for:

- (a) the transaction of administrative, clerical, data processing or management business;
- (b) the practice of a profession other than a medical facility; or
- (c) the provision of government or social services and other similar services. (bureau)

#### 4. **PROPOSED EMBASSY DESIGN**

The Embassy proposes to use the building as follows: Source: Space Measurements calculated by Julian Jacobs Architects

**Basement:** (primary parking) **219 sq. m gross floor area**

Excluding common areas:

- Washrooms
- Mechanical/Electrical
- Stairs
- Elevator shaft
- Parking Lot

**First Floor:** (reception / office / visitor hall) **729 sq. m gross floor area**

Excluding common areas:

- Stairs
- Elevator shaft
- Washrooms
- Storage
- Electrical

**Second Floor:** (offices) **664 sq. m gross floor area**

Excluding common areas:

- Washrooms
- Elevator Shaft
- Stairs
- Storage



**Third Floor:** (Multipurpose Hall / Kitchenette) **639 sq. m gross floor area**

Excluding common areas:

- Washrooms
- Stairs
- Elevator Shaft

**Fourth Floor:** (Ambassador's Office / Meeting room) **368 sq. m gross floor area**

Excluding common areas:

- Washrooms
- Elevator Shaft
- Stairs
- Storage

**Total Area:** with common area exclusions removed: **2,619 sq. m gross floor area**

**Base Building Measurements**

Refer to Appendix A – Site Plan  
Appendix B – Site Elevations, and  
Appendix C – Floor Plates GFA.

**Setbacks**

Front Yard Setback: 0m  
Side Yard Setback: 0m  
Rear Yard Setback: 0m

The proposed base building is to be constructed atop a 1m podium base and is to be built out to the property line on the south front yard, west side yard, and north rear yard of the lot.

**Landscaping**

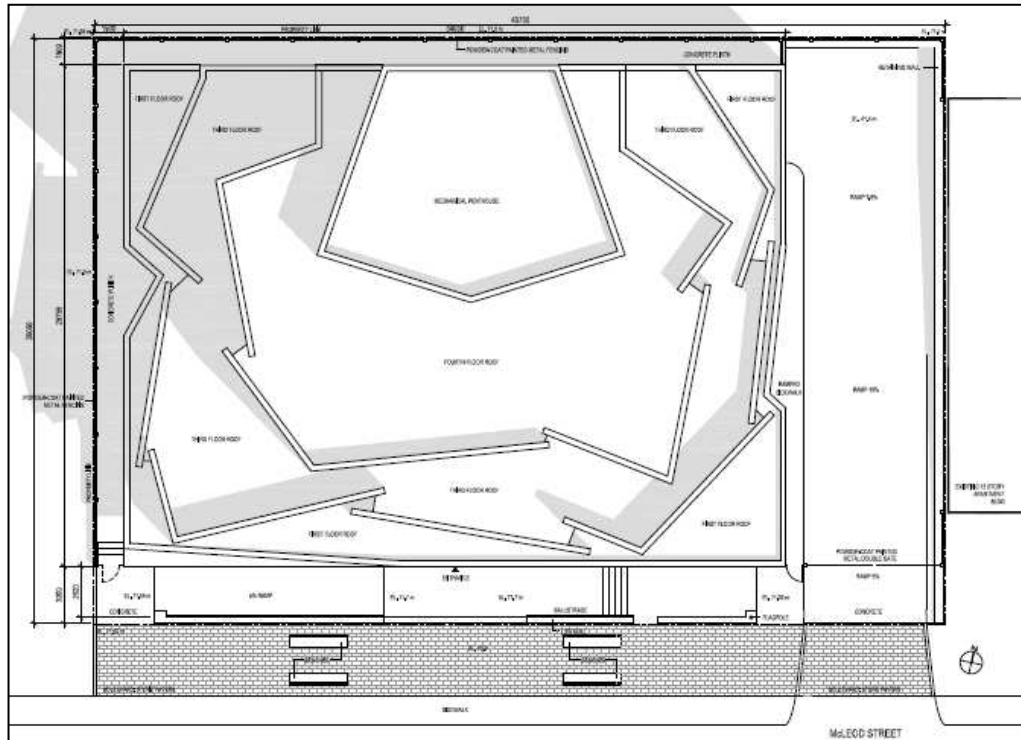
All existing landscape features (trees, shrubs) will be removed from the site in order to accommodate the proposed embassy building and parking garage access ramp. Refer to Appendix A for a detailed Tree Conservation report of the trees to be removed.

**Building Height and Massing**

The proposed design will have a Top of Parapet Height of 19.5m and an FSI of **1.99**.

Site specific amendments are required to allow for this type of building form on the subject site and are deemed minor in nature. These site specific amendments have been applied for in order to conform to the intent of the general intent of the GM1 sub-zone.





Conceptual Site Plan

## 5. REQUIRED ZONING AMENDMENTS

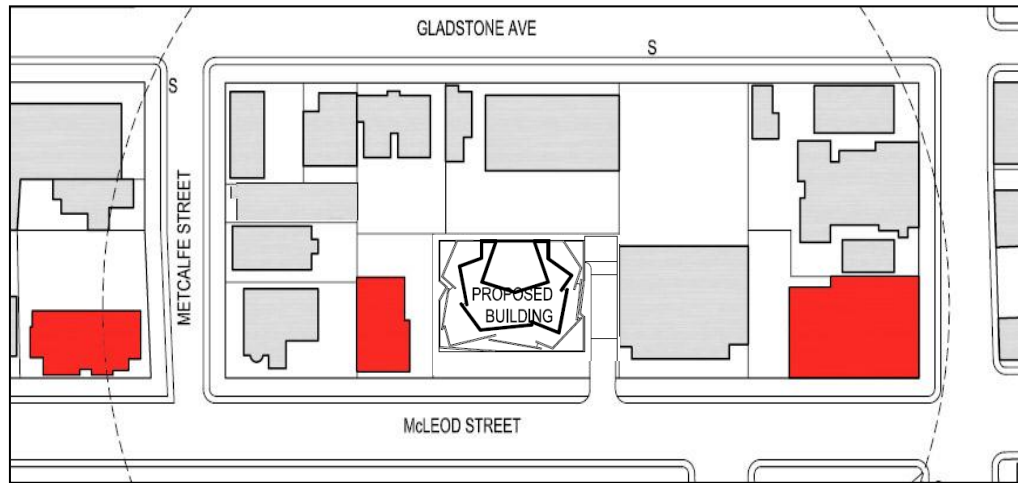
Site specific zoning bylaw amendments have been applied for to permit minor variances to several provisions of the bylaw. Under these amendments, relief is sought for setback, landscaping, height, FSI, parking space, loading space, and heritage overlay provisions.

### 5.1 RELIEF FROM MINIMUM YARD REQUIREMENTS

The proposed embassy will have a 0m setback from the front yard, west side yard and north rear yard property lines.

#### Front Yard Setback

The GM1 zoning for this site prescribes a minimum front yard setback of 3m. However, within close proximity to the subject site there are three neighbouring buildings, one on the east side and two on the west side (shown below in red), built right up to, or intruding into their respective front yards. The proposed design would be in keeping with the existing street wall established by these adjacent buildings along this section of McLeod Street.



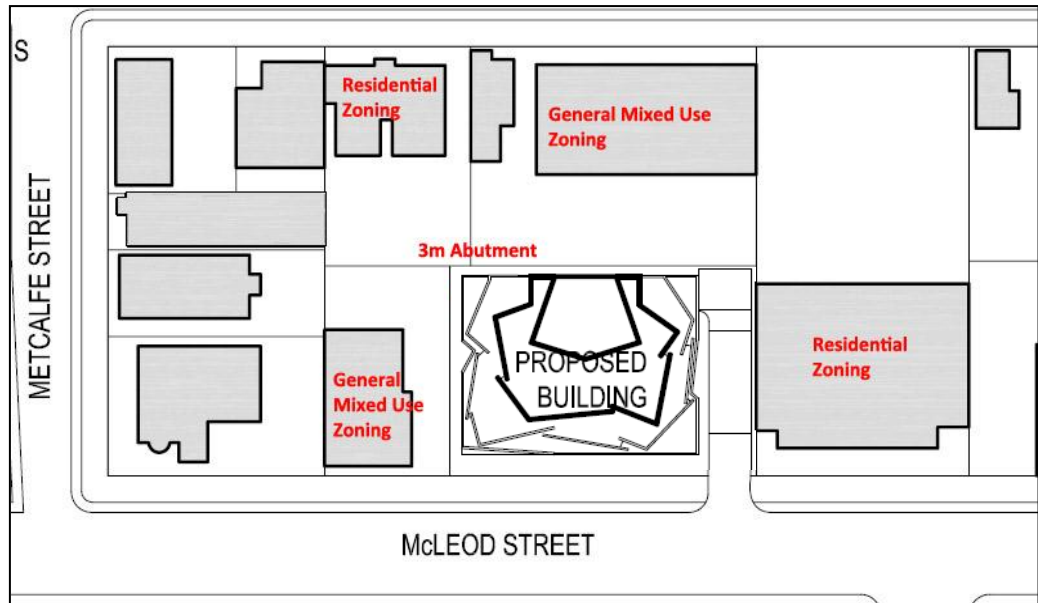
In addition, the front yard encroachment is a wheelchair accessible ramp which, according to Table 65 of the zoning bylaws General Provisions, allows a front yard protrusion with no limit for both residential and non-residential buildings. Refer to Appendix A for details of the proposed ramp design.

**Table 65 - Permitted Projections into Required Yards** (adapted from the City of Ottawa Zoning By-law)

I Feature	Maximum Size and Extent of Projection	
	II For Residential Use Buildings	III For All Other Buildings
(5) Fire escapes, open stairways, stoop, landing, steps and ramps	(a) Wheelchair ramps - no limit	

Rear Yard Setback

The GM1 zoning for this site prescribes a minimum rear yard setback of 7.5m for any portion of the lot line abutting a residential zone and a 0m setback for any other zone. The subject site only abuts a residential zone for an approximate 3m length along the north western edge of its rear property line. The proposed design thus conforms to the intent of the zoning bylaw for the majority of its rear yard setback requirements. Relief is therefore sought for the minor length (3m) of the rear property line abutting the residential zone.



## 5.2 RELIEF FROM MINIMUM LANDSCAPING REQUIREMENTS

### Landscaping for Areas Abutting a Residential Zone

The GM1 zone for this site prescribes a minimum width of landscaping of 7.5m for any portion of a lot line abutting a residential zone. The proposed design has no landscaping features along the previously mentioned 3m length of its rear property line or anywhere along the west property line which abuts a residential zone. However, this design is consistent with the existing conditions of the surrounding properties. Landscaping is completely absent between the property lines of the adjacent surrounding properties on the west, north and east side of the subject site. Only hardscape surfaces, such as driveways and parking lots are present on these adjacent properties. The embassy design proposes similar hardscape surfaces between the new base building and the residential zones to the east and northwest.



Rear of existing Embassy Building, Spring 2012



**5.3 REQUEST FOR TREE REMOVAL ON SITE AND CITY RIGHT OF WAY**

The proposed design would eliminate the existing trees onsite. A Tree Conservation Report has been undertaken in support of the removal of these trees and is included in Appendix **A**. Trees and shrubs located within the City ROW are to be removed during the construction of the new embassy building.



#### 5.4 RELIEF FROM MAXIMUM HEIGHT AND FSI REQUIREMENTS

The GM1 F(1.0) zone requirements for this site prescribes a maximum height of 18.0m and a maximum FSI of 0.5. The proposed design calls for a Top of Parapet height of 19.5m and an FSI of 1.99 (See Appendix B). The proposed building height and massing is in keeping with the surrounding context of the site and the City’s planning vision, as identified within the draft of the “Ottawa Centretown – Community Design Plan for the Heart of the Centretown”. Directly adjacent to the site is a twelve storey apartment building and a four storey office complex. The proposed design calls for a minor variance in the height requirement by an additional 1.5 metres. In addition, the draft Community Design Plan illustrates that a mid-rise building, similar in mass and height to the proposed design, would be appropriate for the subject site.



Source: Chapter 7 of the Draft Centretown Community Design Plan

#### 5.5 RELIEF FROM HERITAGE OVERLAY

The subject site lies within the Centretown Heritage Conservation District and is thus subject to the provisions of the heritage overlay. Relief from the heritage overlay is thus sought in order to allow the demolition of the existing embassy building and the construction of the proposed new design. Concurrent with the Embassy’s zoning amendment application and site plan application is an associated application to the City of Ottawa’s Heritage Planning Committee seeking approval for the demolition of the existing two storey Embassy building. A supporting Cultural Heritage Impact Statement has been undertaken to rationalize the demolition of the existing Embassy building. (See Appendix



B). This Cultural Heritage Impact Statement indicates that the existing Embassy building is a “Grade IV” heritage structure, identifying that the existing building has little or no heritage significance.

## 5.6 RELIEF FROM ONSITE PARKING REQUIREMENTS

### City Parking Requirement

The City of Ottawa considers an embassy site to be an “Office Use” with regards to the land use and parking provisions in the Zoning By-law. The By-law provision for “Office Use” in Area B is 2 parking spaces per 100 sq. metres of G.F.A. The proposed new embassy has a G.F.A. of 2619 sq. metres therefore 52 parking spaces would be required.

As per Section 101(6) of the Zoning By-law, if all parking spaces are located below grade in the same building as the land use, the By-law permits required parking be reduced by 10%. The Embassy is proposing all parking to be underground and as such, the parking requirement is reduced by 10% to 47 required spaces.

As per Section 111(12) of the Zoning By-law, the parking requirement may be reduced by an additional one parking space for every 13 square metres of gross floor area provided as shower rooms, change rooms, locker rooms and other similar facilities intended for the use of the bicyclists in conjunction with required or provided bicycle parking. The Embassy has included 80.3 sq. metres of change room facilities which equates to a reduction of 6 parking spaces.

Accounting for reductions in the required parking from Sections 101(6) and 111(12) of the Zoning By-law changes the required number of parking spaces from 52 to **41**.

### Embassy Parking Supply

Due to the security protocols of the Iraqi Embassy, a limited number of onsite parking spaces will be provided for diplomats, VIPS, and a limited number of staff. No visitor parking will be provided on site due to no requirement for the provision visitor parking in the Zoning By-law and for security requirements. The basement parking garage will be only accessible to the diplomatic VIP vehicles. A total of **10** underground parking spaces will be provided. The rationale to support this parking allotment is as follows.

- Due to the security protocols of the Embassy, the onsite parking for VIP vehicles will be serviced by valet attendants 24 hours a day. The provision of a valet service means up to 15 additional approved vehicles can be parked onsite in tandem within the garage and thus increases the number of usable parking spaces to a total of **25**.
- It is the City of Ottawa's goal to support land uses which can utilize alternative modes of transportation. There are three bus stops within 100 metres of the Embassy. The subject site area is serviced by bus transit routes along Bank Street and Elgin Street. Bank street, is designated within the City's Official Plan as a "Transit Priority Corridor". The majority of the Embassy's staff will be required to take transit or park their personal vehicles off-site, as the on-site parking is exclusively reserved for the diplomatic VIP vehicles for security reason.
- There are 180 on-street (free and paid) parking spots provided by the City in the area bounded by Cartier Street and O'Connor Street, and by Argyle Street and Gladstone Avenue, where visitors or embassy staff can park their vehicles.
- A small private parking lot provides ten (10) paid spaces at the northwest corner of the Elgin Street and Gladstone Avenue intersection.
- According to the Museum of Nature website, when construction is complete on Lot 2 a total of 246 parking spots will be provided by the museum. This represents a net increase of 150 parking spaces over what currently is provided in the area.
- The Embassy will provide a bicycle rack onsite exclusively for staff wishing to cycle to work, in support to the City of Ottawa's alternative forms of Transportation initiatives and policies.
- Due to the security protocols of the Embassy, the onsite parking for VIP vehicles will be serviced by valet attendants 24 hours a day. The provision of a valet service means additional approved vehicles can be parked onsite in tandem within the garage and thus increases the number of usable parking spaces to a total of 14.
- No on-site visitor parking is required under the zoning by-law. In addition, given the unique security requirements for an Embassy site, no on-site visitor parking will be provided.

### **Required Relief from Parking Requirement**

The proposed development is seeking relief from the required 41 parking spaces to 25 parking spaces. Of the 25 spaces on-site, 10 meet the performance requirements of the Zoning By-law and 15 are to be provided as tandem valet parking spaces. The proposed development will require a relief of 16 parking spaces overall from the city's Parking By-law. This relief is supported by the City of Ottawa's focus on intensification in Centretown, the availability of transit, off-site parking, and a small number of employees working at the embassy compared to the use of a typical office building. A letter from the Ambassador has been attached to confirm that the Embassy site will employ only 30 full-time staff.

#### **5.7 BIKE PARKING REQUIREMENT**

The proposed new embassy has a G.F.A. of 2619 sq. metres. The Zoning By-law requires 1 space per 250 square meters of floor area which equals a requirement of 11 spaces. 11 bicycle parking spaces have been provided in the underground parking area to meet this requirement.

#### **5.8 RELIEF FROM LOADING SPACE REQUIREMENT**

The security protocols of the Embassy prohibit the loading vehicles from aggressing the building's parking garage. The parking garage will be designed to allow only vehicles with a clearance of 3 metres or less to enter the building. All material handling drop off and pick requirements will be accommodated at the bottom of the garage parking ramp and transported by hand or trolley into and out of the garage. Relief is therefore required from the zoning by-law requirement of a loading space within the parking garage. The elimination of this loading space from within the garage will yield sufficient floor space to create one (1) of the ten onsite vehicle parking stalls.

### **6. PROVINCIAL POLICY STATEMENT (PPS)**

The Provincial Policy Statement provides policy direction on matters of provincial interest related to land use planning and development. The Planning Act requires that planning matters "be consistent with" the provincial Policy Statement.

The proposed zoning amendments are consistent with the PPS as the proposed embassy addresses the following relevant sections:

**6.1 PART IV: VISION FOR ONTARIO'S LAND USE PLANNING SYSTEM**

- The Provincial Policy Statement focuses growth within settlement areas. It recognizes that the wise management of development may involve directing, promoting or sustaining growth. Land use must be carefully managed to accommodate appropriate development to meet the full range of current and future needs, while achieving efficient development patterns. “

**Consistency:** The provision for an embassy office on the site would assist in sustaining growth by providing for the establishment of government and diplomatic services in the neighbourhood which will address the current and future needs of residents to have local access to this type of use.

- “Efficient development patterns optimize the use of land, resources and public investment in infrastructure and public service facilities. These land use patterns promote a mix of housing, employment, parks and open spaces, and transportation choices that facilitate pedestrian mobility and other modes of travel.”

**Consistency:** The provision for an embassy office on the site addresses the optimization of an efficient land use which will promote and sustain the existing residents and offer staff and embassy visitors the opportunity to use adjacent public transit.

- “Strong, liveable and healthy communities enhance social well-being and are economically and environmentally sound.”

**Consistency:** The provision for an embassy office on the site supports strong, liveable and healthy communities which would enhance the social well-being of residents, particularly new immigrants arriving to the Country.

**6.2 BUILDING STRONG COMMUNITIES (PPS)**

**6.2.1 1.1 Managing and Directing Land Use to Achieve Efficient Development and Land Use Patterns (PPS)**

**1.1.1** Healthy, liveable and safe communities are sustained by:



b) Accommodating an appropriate range and mix of residential, employment (including industrial, commercial and institutional uses), recreational and open space uses to meet long-term needs;

And

g) Ensuring that necessary *infrastructure* and *public service facilities* are or will be available to meet current and projected needs.”

**Consistency:** The application to amend the zoning bylaw to allow the construction of a new embassy office for the subject site addresses the need to accommodate the current and projected long term needs for government services of citizens and new immigrants to Canada.

“1.1.3.2” (PPS) Land use patterns within *settlement areas* shall be based on:

a. densities and a mix of land uses which:

1. efficiently use land and resources;
2. are appropriate for, and efficiently use, the *infrastructure* and *public service facilities* which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;”

**Consistency:** The provision for an embassy office on the subject site is an efficient land use and re-use of the existing surrounding infrastructure. It is appropriate for the area and will be complimented by the availability of bus transit routes along Bank and Elgin Streets.

### 6.3 1.6.5 TRANSPORTATION SYSTEMS

1.6.5.4 A land use pattern, density and mix of uses should be promoted that minimize the length and number of vehicle trips and support the development of viable choices and plans for public transit and other alternative transportation modes, including commuter rail and bus. “

**Consistency:** The provision for an embassy office on the site is supportive of a land use pattern, density and mix of uses along McLeod for both local and city-wide residents needs. It allows for residents to use public transit or walk to the site thereby minimizing the length and number of vehicle trips. The location



supports the choices and plans for public transit and other alternative modes such as walking and cycling.

## 7. OFFICIAL PLAN POLICIES AND ZONING RATIONALE

### 7.1 OFFICIAL PLAN GUIDING PRINCIPALS

Section 1.3 of the Official Plan identifies **guiding principles and accompanying objectives** which are stated to be equally important and must be balanced when making decisions. The proposed zoning amendment is consistent with the guiding principles and objectives in the following manner:

#### A Caring and Inclusive City

- **Access to the Basics** – All people including local and citywide residents will have access to the embassy services located at the subject site.
- **New Immigrants to Canada** – newly arrived immigrants from Iraq will have access to governmental services that can respond to their needs.

#### A Creative City Rich in Heritage, Unique in Identity

- **Vibrant Local Arts and Heritage** – The proposed design of the building will follow the advice of the City of Ottawa's Urban Design Panel. Its architecture incorporates the heritage qualities of the Iraqi people. It will support community vitality and will complement the surrounding historic architecture.

#### A Green and Environmentally-Sensitive City

- **Development in Harmony with the Environment** – the re-use of the existing embassy site is deemed an appropriate use within the current urban boundary and avoids outward sprawl.
- **A Focus on Walking, Cycling and Transit** – The limited quantity of parking available for the embassy favors walking, cycling and public transit over the use of private motor vehicles, thereby facilitating the use of modes of transportation that are socially accessible, environmentally healthy and economically feasible.

#### A City of Distinct, Livable Communities

- **A Sense of Community** – The Centretown neighborhood has developed into a unique community with its own look and character. The proposed embassy will support and enhance this sense of community among residents.
- **Complete Communities** – the continued use of an embassy office on the subject site supports and will sustain the Centretown community, adding to the existing range of services and facilities accessible by walking, cycling and transit.

#### A Responsible and Responsive City

- The provision of an embassy office responds to the need of such facilities to be located near residents and responsive to their needs.

### **A Healthy and Active City**

- **Accessibility** – the proposed amendment will enable Citizens and new immigrants to have access to important governmental services.

The proposed zoning amendment is consistent and complies with the policy direction identified within this section of the Official Plan regarding the development of **General Urban Areas** with mixed uses.

## **7.2 CENTRETOWN SECONDARY PLAN**

Volume 2(a) of the Official Plan addresses secondary plan goals, objectives and policies. The proposed embassy is consistent with the following goals, objectives, and policies of Sections 3.3.1, 3.3.2, and 3.4.2 of the Centretown Secondary Plan.

### **Goals 3.3.1**

- a. To maintain and enhance the residential character of Centretown while allowing for a moderate increase in population.
- b. To accommodate persons of all age groups, income levels, cultural backgrounds, lifestyles and household sizes wishing to live in Centretown with good quality, affordable housing

**Consistency:** The proposed embassy will enhance the residential character of the Centretown neighborhood through its improved architectural design that incorporates historical Arabic motifs and ornamentation. It also improves accessibility to important government services for new Iraqi-Canadians and their families.

### **Objectives 3.3.2**

- d. To encourage innovative building and site development which will contribute to the physical environment of the community and enhance the desirability of Centretown as a place to live.

**Consistency:** The proposed embassy features a unique and innovative architectural design that will enhance the community's physical environment and complement surrounding buildings.

- k. To allow for the improvement of the public transportation system and decrease reliance of the private automobile.

**Consistency:** The proposed embassy limits private automobile use to only diplomatic VIP's. The site is located in close proximity to several bus stops and major bus routes, thus encouraging embassy staff and visitors to walk, cycle, or take public transit.

**Land Use Policy 3.4.2(c)**

The embassy site at 215 McLeod Street is classified as a **Medium Profile Residential Area** under Schedule H of the Centretown Secondary Plan. However, as stated by the policy, uses other than residential are not to be excluded if considered compatible with the neighborhood. As there are several other embassy buildings located within close proximity to 215 McLeod and the history of the site serving as an embassy for several decades, the continued use as an embassy would be considered compatible with the neighborhood.

**7.3 DOWNTOWN OTTAWA URBAN DESIGN STRATEGY 20/20**

The Downtown Urban Design Strategy identifies 215 McLeod Street as within section 37, the Metcalfe Gateway and Canadian Museum of Nature Block Restoration. The proposed embassy's location and height are in no way obscuring the preserved view plane from the Museum of Nature to Parliament Hill. Although there are no specific design guidelines that affect the proposed embassy, the strategy does call for an enhanced level of architectural and design surrounding the Museum. The proposed embassy will complement the newly renovated Museum of Nature and provide a unique and high quality architectural design that is consistent with the intent of the Downtown Urban Design Strategy.

**7.4 BUILDING LIVEABLE COMMUNITIES**

Section 2.5. of the Official Plan promotes and defines livable communities as communities having appropriate housing at a price people can afford. It is built around greenspaces and has places to shop, socialize and play nearby. Residents know where to find the local library, health services, schools and other community facilities. Many of these are within walking or cycling distance, and form a core for the community.

The OP proposes to create more livable communities by focusing more on community design and by engaging in collaborative community building, particularly in and around the Mixed-Use Centres and that have a great potential for growth.

Section 2.5.1 identifies compatible development to mean development that, although it is not necessarily the same as or similar to existing buildings in the vicinity, nonetheless enhances an established community and coexists with existing development without causing undue adverse impact on surrounding properties. It 'fits well' within its physical context and 'works well' among those functions that surround it. Generally speaking, the more a new development

can incorporate the common characteristics of its setting in its design, the more compatible it will be. Nevertheless, a development can be designed to fit and work well in a certain existing context without being ‘the same as’ the existing development.

The proposed zoning amendment and site plan applications to allow a new embassy building to be constructed on the subject site are deemed consistent and compatible with the associated livable communities and policy direction identified above. The new embassy building will enhance and promote Centretown as a sustainable and livable community.

## **7.5 COMPATIBILITY**

Section 4.11 of the OP addresses policies on compatibility. At the city-wide scale, issues of compatibility are addressed in the Official Plan through the appropriate designation of land and associated policies that direct where and how certain categories of land use should be permitted to develop. Locational policies are identified in this section of the OP in order to direct uses that have the potential to generate negative impacts to appropriate locations, most typically at the periphery of residential neighbourhoods.

The OP states that to achieve the Plan’s strategic directions for managing growth, the zoning in many areas of the city, particularly areas adjacent to major roads, at the periphery of neighbourhoods and proximate to transit stations, may allow for more intensive development than has occurred in the past. In addition, an amendment to the zoning by-law may be needed to change the permitted use of the land and increase the height or density permitted. In these circumstances, the compatibility of the proposed development must be considered.

When evaluating compatibility of development applications, the City will have regard for the policies of the site’s land use designation and all applicable Community Design Plans, Secondary Plans, or site specific policies, as well as the Design Objectives and Principles in Section 2.5.1.

As previously discussed the subject site is located along the southeastern edge of the Centretown community along that portion of McLeod Street which is used as a direct access route to Metcalfe Street from the Queensway. The proposed embassy building is compatible with the envisioned mix and scale of uses that the Official Plan has identified for this portion of McLeod Street within

the Centretown Secondary Plan and the Draft Community Design Plan for Centretown.

The proposed embassy building will comply and be consistent with the guidelines identified in Section 4.11.2 over the following:

- a) There is sufficient capacity on McLeod Street to accommodate traffic which would be associated with the use of the site as an embassy office.
- b) Vehicular Access to and from the site is deemed appropriate to support the proposed embassy office use.
- c) Parking on site is limited to 10 stalls due to the security protocols of the embassy. As previously discussed offsite parking can be addressed along Gladstone and Metcalfe streets with limited spillover on adjacent areas. The application complies with the policy with which City Council should consider opportunities to reduce parking requirements and promote increased usage of walking, cycling and transit.
- d) The height and massing of the proposed building has regard for and is deemed compatible with the existing neighbourhood.
- e) The use of the site as an embassy office has regard for the pattern of the surrounding Community. Other diplomatic and embassy buildings are within walking distance of the site
- f) The proposed use of the site as an embassy office has regard for and respects the privacy of adjacent residential units.
- g) Loading, service and any outdoor storage associated with the embassy on the subject site will be located away from residences where possible.
- h) The potential for light spill over or glare from lighting sources onto adjacent areas is deemed to be minimal and will be avoided and marginalized where possible. The use of external lighting is marginal and dictated by the limited operating hours during the week as the embassy will be primarily in operation during day time hours.
- i) The use of the site as an embassy office is designed to minimize the potential for any adverse effects on adjacent uses.



- j) Sunlight exposure and shadowing effects are constrained to the existing building design and site location.
- k) The effects of wind, snow drifting and temperature on adjacent buildings are influenced by the location of the building on the lot.
- l) The embassy office will provide governmental services to the community and will be provided at a scale that is appropriate for the needs and character of the area.

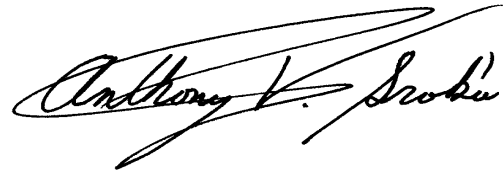
## 8. CONCLUSION

The proposed zoning amendments and site plan control application to allow the construction of the proposed Embassy office at 215 McLeod Street complies with the policy and land use guideline intent of the following:

- Consistent with the **Provincial Policy Statement** directives on building strong communities and managing and directing land use to achieve efficient development and land use patterns.
- Consistent and complies with the **Official Plan “General Urban Area”** designation and associated polices,
- Consistent with the **Official Plan’s Guiding Principles and Objectives**,
- Complies with the **Official Plan’s Building Liveable Communities** strategic approach and polices.
- Consistent with the **Centretown Secondary Plan**
- Consistent with **Draft Ottawa Centretown Community Design Plan** for the Heart of Centretown
- Consistent with **The Downtown Ottawa Urban Design Strategy 20/20**
- Is deemed a compatible land use to locate an Embassy Office at 215 McLeod Street in accordance with Official Plan policies.

The zoning amendment and associated site plan control application to construct the proposed embassy on the *subject property* within a General Urban Area constitutes a desirable use which will assist in sustaining the current and future residential population's service needs. It is my collective professional opinion that this zoning amendment and site plan control application is an appropriate use for the subject site, complies with and is consistent with the policy direction identified in the Official Plan, is in the public interest and represents good land use planning. I support this application and recommend that these Zoning By-law amendments be approved accordingly.

Submitted by:



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