

**Planning Rationale in Support of an Application for
Site Plan Control**

**2012 Ogilvie Road
City of Ottawa**

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May 17, 2016



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1.0 Introduction

Holzman Consultants Inc. has been retained by RioTrin Shoppers City East Inc. (the “Owner”) to prepare a planning rationale in support of an application for Site Plan Amendment for the Shoppers City East commercial development at 2012 Ogilvie Road in the Beacon Hill - Cyrville ward of the City of Ottawa (the “Subject Property”). The requested amendment is to permit a revision to the Site Plan Agreement previously approved in July 2014 and as amended in April 2016.

This report provides a description of the existing conditions, the approved site plan and the proposed revision to the site plan and contains a review of the applicable land use planning policies including the Provincial Policy Statement (PPS), the Official Plan (OP) policies, the Transit-Oriented Development (TOD) Plan policies, Urban Design Guidelines and Zoning By-law provisions. The summary and conclusions indicate that the proposed revision is supported by the PPS, OP, TOD Plan and Zoning By-law.

2.0 Site Overview



Exhibit 'A' – Aerial Photo (Subject Property outlined in red).

The Subject Property consists of a parcel of land located on the south side of Ogilvie Road, east of Blair Place. The Subject Property is the location of the former Shoppers City East Shopping Centre, which is currently being demolished.



Exhibit 'B1' – Site Photo (Ogilvie Road frontage)

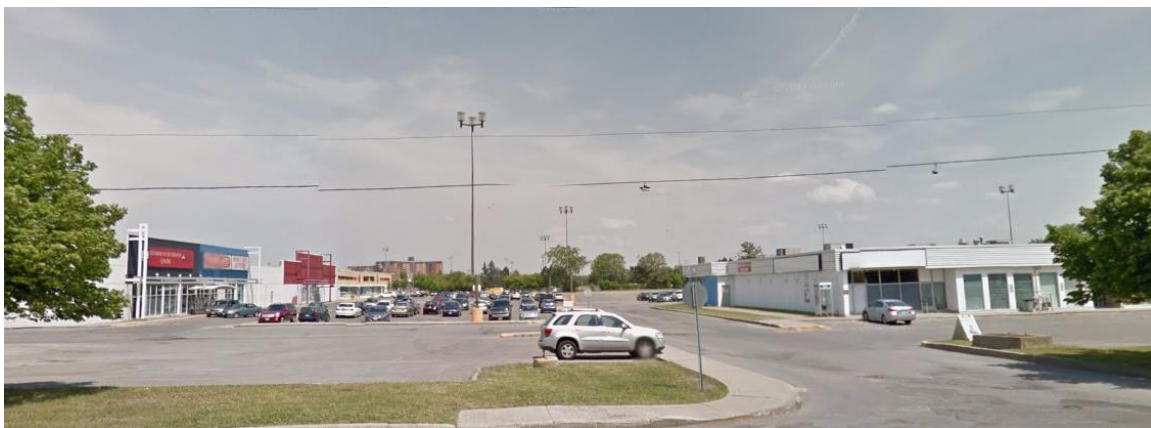


Exhibit 'B2' – Site Photo (Blair Place frontage)

The Subject Property is 78,751.83m² in size with frontage on both Ogilvie Road and Blair Place. The property is surrounded by the following land uses:

- North: Residential uses (across Ogilvie Road)
- East: Trillium Park and Earl Armstrong Arena
- South: Highway 174
- West: Business park and commercial uses

Blair station, currently part of the Transitway, is located over 800 m (as measured by available pedestrian routes) from the site.

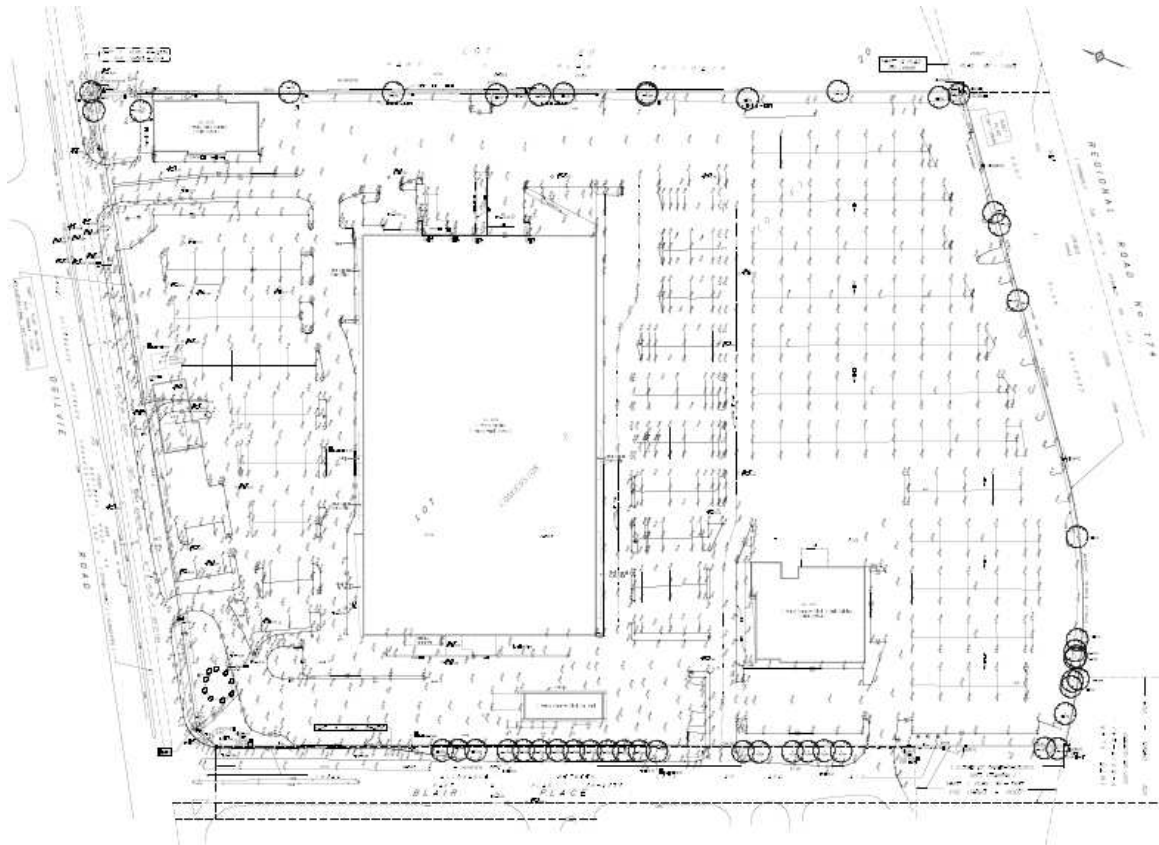


Exhibit 'C' – Survey Plan

The Subject Property is legally described as Part of Lot 20, Concession 2 and is denoted as PIN 043630103 (see **Exhibit 'C'** – Survey Plan).

3.0 Description of Proposed Development

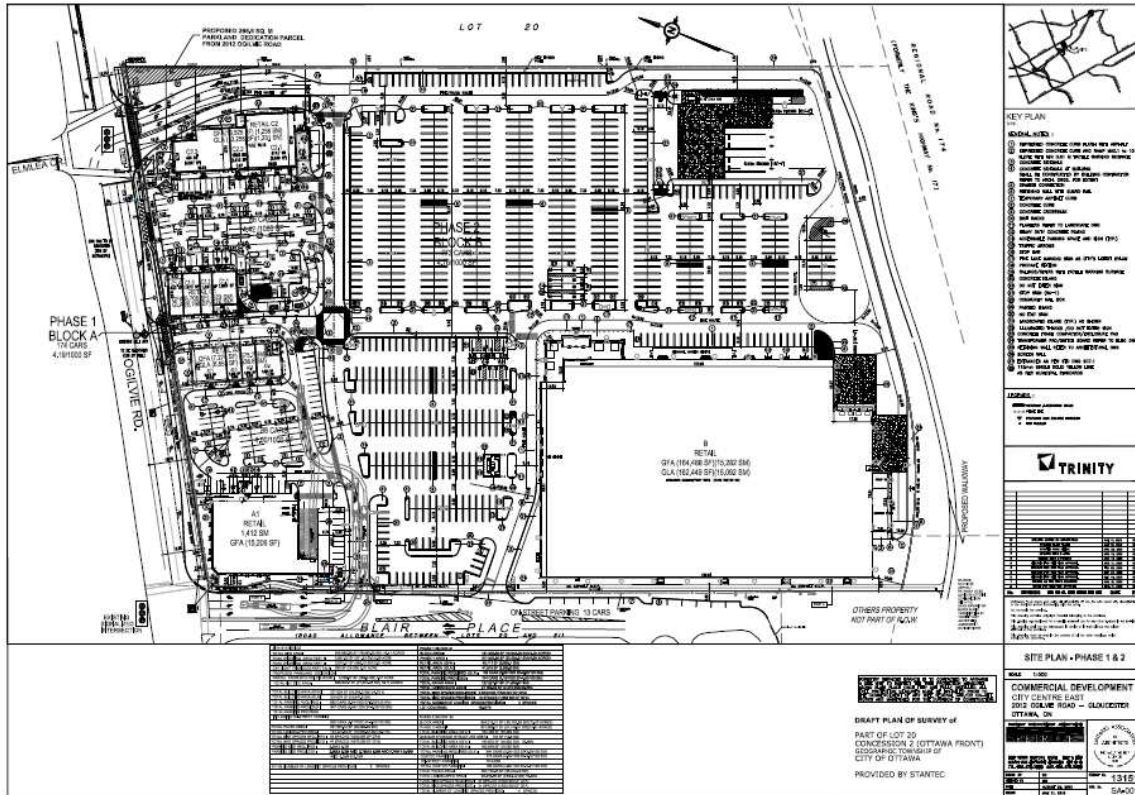


Exhibit 'D' – Proposed Revised Site Plan

A site plan application was approved in 2014 to redevelop the site (D07-12-13-0198). A subsequent site plan application was recently filed to amend the approved site plan for the site, pertaining to Phase 1 (D07-12-15-0122) (shaded in grey in **Exhibit 'D'** above). The amending application for Phase 1 has been approved. Phase 2 is the subject of this site plan application and seeks to revise the approved plan to permit the construction of a 15,294 m² retail warehouse store and associated gas bar. A 3m multi-use pathway will continue along Blair Place from Phase 1, along the western property line and will connect to the planned east-west pathway to the south of the Subject Property adjacent to the highway.

4.0 Design Brief



Exhibit 'E1' – Building Rendering, Eastern Elevation



Exhibit 'E2' – Building Rendering, Northern Elevation

The following points provide a high level design brief of the redevelopment as a whole. An analysis of the OP design and compatibility policies as well as the design guidelines will follow in the Policy Analysis section of this report (Sections 5.2 and 5.4).

- The building facades exposed to the parking area provide interest, through multiple entrances, glazing, wall articulation and architectural details.
- Building entrances are emphasized through awnings and colours.

- The volume and heights of variety of structures emphasizes the prominence of entrances throughout the complex.
- The mix of building materials (brick, blocks, EIFS, glass) has been chosen to reflect function and aesthetic qualities that exhibit quality of workmanship, sustainability and ease of maintenance.
- In general the appearance of building materials will be true to their nature and will not mimic other materials and are durable to the form.
- The project is developed with a consistent and coordinated colour of palette, with greater elaboration and detail at pedestrian levels where detail is best perceived.
- The proposed architectural treatment will use accent colours to stress the urban identity and location of each element to immediately provide wayfinding.
- Corporate colours of tenants are used to enhance the facades and the specific identity.
- The buildings make use of varying planes and varied parapet heights to provide relief and interest.
- Scale is controlled through the use of materials of greater textural interest and colour at low pedestrian related areas.
- Pedestrian comfort and scale is thereby enhanced through the use of modelled facades amid focal point strategies, particularly at the entry features.
- Glazing and wall articulation at pedestrian levels enhance the pedestrian enjoyment on the site.
- The multi-sided nature of the “blocks” on site ensure that all faces will be considered as “fronts”, some more significant, but all important.
- The prototype buildings are adjusted with architectural elements used on the rest of the buildings.
- Awnings and light fixtures are introduced on the elevations for pedestrian comfort and protection, and to provide visual diversity and interest to the entire complex.
- The palette of colours range from light to medium earth tones, with colour accents to create facade interest and detract from the monotony of single colour.
- Rooftop mechanical equipment will be centrally located on the rooftop where possible, to reduce site lines, or screened with materials complementary to the building.
- All of the receiving areas, where possible, are at the rear and side of the buildings so as not to detract from elevation facades.

- Signage in general will be consistent with some diversity to compliment the design character of the complex which will communicate and present effectively a coordinated image.
- All building signage will be designed as an integral, coordinated element of the building facade.
- Lighting of buildings will be of the same family to further promote the cohesive nature of the whole site.

5.0 Planning Context

The applicable policy framework includes an examination of the Province of Ontario's land use planning directives expressed in the Provincial Policy Statement and the City of Ottawa's policies expressed in the City of Ottawa Official Plan and the Blair Station Transit Oriented Development (TOD) Plan.

5.1 Conformity with the Provincial Policy Statement

The 2014 Provincial Policy Statement ("PPS") sets out a vision for land use planning in the Province of Ontario that encourages planning and development that is environmentally sound, economically strong, and that enhances quality of life. Land use planning policies are intended to promote efficient development patterns with an appropriate mix of housing, employment, open spaces and multi-modal transportation which are appropriate for and make efficient use of existing and planned infrastructure and public service facilities.

Section 1.1 provides policy guidance for efficient development and land use patterns. This section states: "*Healthy, liveable and safe communities are sustained by:*

- a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*
- b) accommodating an appropriate range and mix of residential (including second units, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
- c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
- d) avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;*
- e) promoting cost-effective development patterns and standards to minimize land consumption and servicing costs;*

- f) *improving accessibility for persons with disabilities and older persons by identifying, preventing and removing land use barriers which restrict their full participation in society;*
- g) *ensuring that necessary infrastructure, electricity generation facilities and transmission and distribution systems, and public service facilities are or will be available to meet current and projected needs; and*
- h) *promoting development and land use patterns that conserve biodiversity and consider the impacts of a changing climate.”*

The proposed development will provide an update to an aging retail space in an urban area, which is fully serviced, located on an arterial road and prime for redevelopment. The development is also in accordance with Section 1.1.3.1 which states that Settlement Areas “*shall be the focus of growth and development, and their vitality and regeneration shall be promoted*”.

Section 1.1.3.2 asserts that land use patterns within Settlement Areas will be based on:

- a) *“densities and a mix of land uses which:*
 - 1. *efficiently use land and resources;*
 - 2. *are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
 - 3. *minimize negative impacts to air quality and climate change, and promote energy efficiency;*
 - 4. *support active transportation;*
 - 5. *are transit-supportive, where transit is planned, exists or may be developed; and*
 - 6. *are freight-supportive; and*
- b) *a range of uses and opportunities for intensification and redevelopment in accordance with the criteria in policy 1.1.3.3, where this can be accommodated.”*

The site plan proposes development that is appropriate to the location and level of infrastructure and facilities of the area. Phase 2 builds on the pedestrian linkages of Phase 1 with a 3m multi-use pathway extending along the west side of the property for pedestrian and cycling access to the retail warehouse store and the proposed walkway at the rear of the property. This will facilitate active transportation and transit.

Section 1.3 covers policies aimed to “*promote economic development and competitiveness*”. In accordance with Section 1.3.1, planning authorities shall promote this by:

- *“providing for an appropriate mix and range of employment and institutional uses to meet long-term needs;*

- *providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses;*
- *encouraging compact, mixed-use development that incorporates compatible employment uses to support liveable and resilient communities; and*
- *ensuring the necessary infrastructure is provided to support current and projected needs.”*

The proposed Phase 2 development further enhances the commercial uses already approved for this site, enabling the surrounding community to become a more complete community. The retail uses diversify the economic base of the Beacon Hill – Cyrville ward and Ottawa as a whole, and will offer new employment opportunities. Phase 2 has been designed to optimize operational efficiency and therefore represents a compact form for this type of use. The necessary infrastructure to support the new retail warehouse is already in place as evidenced by the supporting engineering and traffic studies included in this application.

Section 1.5.1 promotes healthy and active communities through development of spaces that meet the needs of pedestrians and cyclists. The multi-use pathway on Blair Place connecting to the planned east-west multi-use pathway adjacent to Highway 174 will encourage active modes of transportation to and from the site, which will be linked to transit as the walkway connects westward to the Blair station.

Pursuant to Section 1.6.7 on Transportation Systems, the redevelopment will allow for enhanced connections to transit and improved pedestrian linkages which will create a greater modal split and encourage use of the rapid transit system.

Section 1.7.1 provides policy guidance for how long term economic prosperity should be supported, as follows:

- a) “promoting opportunities for economic development and community investment-readiness;*
- b) optimizing the long-term availability and use of land, resources, infrastructure, electricity generation facilities and transmission and distribution systems, and public service facilities;*
- c) maintaining and, where possible, enhancing the vitality and viability of downtowns and mainstreets;*
- d) encouraging a sense of place, by promoting well-designed built form and cultural planning, and by conserving features that help define character, including built heritage resources and cultural heritage landscapes;*

- e) *promoting the redevelopment of brownfield sites;*
- f) *providing for an efficient, cost-effective, reliable multimodal transportation system that is integrated with adjacent systems and those of other jurisdictions, and is appropriate to address projected needs to support the movement of goods and people;*
- g) *providing opportunities for sustainable tourism development;*
- h) *providing opportunities to support local food, and promoting the sustainability of agri-food and agri-product businesses by protecting agricultural resources, and minimizing land use conflicts;*
- i) *promoting energy conservation and providing opportunities for development of renewable energy systems and alternative energy systems, including district energy;*
- j) *minimizing negative impacts from a changing climate and considering the ecological benefits provided by nature; and*
- k) *encouraging efficient and coordinated communications and telecommunications infrastructure.”*

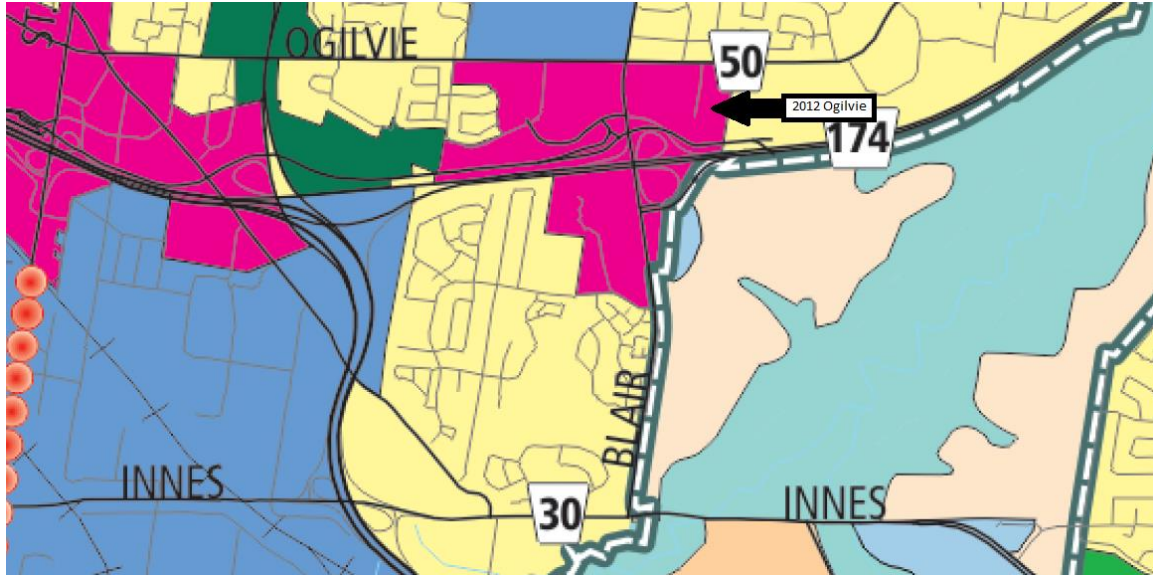
Further to Section 1.7.1, the proposed Phase 2 development supports long term economic prosperity through the strengthening of the existing commercial uses both on the property and in the surrounding area. Phase 2 optimizes the use of the land and infrastructure while providing an important commercial service for the renewal of the area, particularly as it intensifies with investment in rapid transit. The development provides for multi-modal transportation, which will enhance transit investments while also ensuring proper vehicular circulation to and from the site.

Overall, the redevelopment of the Shoppers City East Centre constitutes an investment in the community which will revitalize an outdated retail centre. The centre has been designed to facilitate pedestrian circulation and provide quality building designs.

5.2 Conformity with the City of Ottawa Official Plan

The City of Ottawa Official Plan (“OP”) was adopted in 2003 and provides a vision for future growth of the city and a policy framework to guide future development. The OP recently underwent a 5-year review resulting in Official Plan Amendment (OPA) No. 150, which was adopted by Council in December 2013 and subsequently appealed to the Ontario Municipal Board (the appeal is still ongoing). Pursuant to Council direction regarding new development applications, the policies of OPA 150 will be analyzed with respect to the Subject Property.

Section 3.6.2 – Mixed Use Centre



Mixed Use Centre 
Exhibit 'F': Schedule 'B' of the Official Plan, Urban Policy Plan

The Subject Property is designated Mixed Use Centre on Schedule 'B' of the Official Plan, and is located at the north-eastern edge of the Mixed Use Centre's boundary. Mixed Use Centres "*occupy strategic locations on the rapid transit network and act as central nodes of activity within their surrounding communities and the city as a whole*". These areas are projected to achieve higher densities and a greater mix of uses to increase transit ridership. Although the redevelopment proposes all commercial uses, the redevelopment as a whole makes more efficient use of the land and infrastructure available than what currently occupies the site, and the commercial uses will support the higher density employment and residential uses that either already exist or are projected to develop over time within the Mixed Use Centre. This in turn encourages a revitalized, complete and liveable community and reduces the need to travel outside of the centre.

In the preamble to Section 3.6.2, the OP recognizes that Mixed Use Centres are to become more transit supportive through intensification and mixed use development over time. The OP states that "*the challenge is to preserve this potential as the centres develop to meet today's markets. The retail success of some centres and large office developments in others have required large parking areas to serve surrounding communities where an automobile is needed for many types of trips. In many centres, the mix of uses is incomplete and there is a need for housing or jobs or other uses to achieve the area's potential to become a complete, liveable community. The key to preserving this potential is to maintain a grid pattern of roads and rights-of-way that define walkable blocks and*

sites for future intensification.” Both phases of the Shoppers City East redevelopment propose significant parking areas in order to support the retail success of the tenants, given that many of the current residents of the area use cars to access shopping and services. However, the redevelopment includes significant pedestrian linkages which will connect to current and future multi-use pathways and transit, creating a more pedestrian and transit friendly environment that can evolve over time, preserving the potential vision for the Mixed Use Centre as a whole. A grid-based internal road network is proposed which could support future intensification, particularly along the street frontages of the site. Furthermore, as the Subject Property is located at the edge of the Mixed Use Centre’s north-eastern boundary, furthest away from the transit station where the highest densities would be proposed, it provides a good transition to the lower density General Urban Area designation to the north and east.

In accordance with Policy 5, the Mixed Use Centre designation permits a broad range of uses including retail uses and service uses. Phase 2 is consistent with these permissions. Policy 9 requires that development proposals enhance opportunities for walking, cycling and transit. As noted, a multi-use pathway continuing from Phase 1 into Phase 2 and connecting to adjacent properties as well as the Blair transit station is proposed.

Section 2.5.1 – Designing Ottawa

Section 2.5.1 seeks to ensure that new development incorporates a high quality of community and urban design and is compatible with surrounding land uses. This section outlines a number of design objectives to be considered during the development review process. These objectives are as follows:

1. *To enhance the sense of community by creating and maintaining places with their own distinct identity.*
The warehouse building has been located on Phase 2 of the Subject Property to preserve the smaller scale retail and service uses along Ogilvie Road in Phase 1. It has been located along the street edge of Blair Place, with the gas bar located at the far south-east corner, away from pedestrians and cyclists. This preserves the higher priority streetscape and identity of Ogilvie Road, while providing an important community use set back from the streetscape.
2. *To define quality public and private spaces through development.*
The warehouse building elevations are typical of the proposed tenant and are similar to other locations in Ottawa.
3. *To create places that are safe, accessible and are easy to get to, and move through.*
Phase 2 is well connected to Phase 1 both from a vehicular and pedestrian standpoint. There are a number of pathways and connections

for pedestrians to minimize conflict with vehicles. There are also a number of access points from Ogilvie and Blair Place that will facilitate easy movement to and throughout the site.

4. *To ensure that new development respects the character of existing areas.*
The redevelopment will enhance the character of existing areas by providing a transition from the Mixed Use Centre to the outlying General Urban Area.
5. *To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.*
The centre in its entirety is characterized by a variety and choice in terms of retail and services and has been designed to facilitate active transportation and increase transit ridership as the Mixed Use Centre evolves over time.
6. *To understand and respect natural processes and features in development design.*
The site plan application supports the continued redevelopment of the site, which will be an improvement over the current condition.
7. *To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.*
Opportunities for energy efficiency will be explored through building construction. By virtue of new construction in accordance with today's OBC, the new buildings will be more energy efficient than those that exist today.

As per Policy 2 and based on the Mixed Use Centre designation, the Subject Property is considered a Design Priority Area; however, further to discussion with City Staff, review by the Urban Design Review Panel for the warehouse building and gas bar is not being required.

Section 4.11 – Urban Design and Compatible Development

In addition to the design objectives of Section 2.5.1, a development proponent must also implement the policies of Section 4.11 into their proposal which make reference to objective criteria that can be used to evaluate compatibility of development applications including height, bulk or mass, scale relationship, building/lot relationships, and the distance between buildings. The OP aims to achieve compatibility through appropriate land use designations and the corresponding Zoning By-law which “*establishes more specific permitted use lists and development regulations within areas and on individual sites in a manner that achieves compatibility among proximate uses and built forms.*”

The OP states the following: *“At the scale of neighbourhoods or individual properties, issues such as noise, spillover of light, accommodation of parking and access, shadowing, and micro-climatic conditions are prominent considerations when assessing the relationships between new and existing development. Often, to arrive at compatibility of scale and use will demand a careful design response, one that appropriately addresses the impact generated by infill or intensification. Consequently, the issue of ‘context’ is a dominant theme of this Plan where it speaks to compatibility and design.”*

The following policies from Section 4.11 are relevant to the Phase 2 redevelopment:

Policy 5: *“Compatibility of new buildings with their surroundings will be achieved in part through the design of the portions of the structure adjacent to existing buildings and/or facing the public realm. Proponents of new development will demonstrate, at the time of application, how the design of their development fits with the existing desirable character and planned function of the surrounding area in the context of:*

- a. Setbacks, heights and transition;*
- b. Façade and roofline articulation;*
- c. Colours and materials;*
- d. Architectural elements, including windows, doors and projections;*
- e. Pre- and post-construction grades on site; and*
- f. Incorporating elements and details of common characteristics of the area.”*

The warehouse building consists of a typical design for this type of retailer. The warehouse has been located to the rear of the property in Phase 2, with smaller retail and service uses adjacent to the Ogilvie Road frontage in Phase 1. The scale and design of the building is consistent with the design of other large format retailers in the area, including the Canadian Tire to the west.

Policy 6: *“The City will require that all applications for new development:*

- a. Orient the principal façade and entrance(s) of main building(s) to the street. Where a building abuts more than one street, the building façade and entrances will be oriented in order of priority to the arterial, collector and local street;*
- b. Include windows on the building elevations that are visible from public spaces;*
- c. Use architectural elements, massing, and landscaping to accentuate main building entrances.”*

The western façade of the warehouse building has been located along Blair Place. The multi-use pathway along Blair Place provides a direct connection to the entrance of the building at the northern façade. The gas bar has been tucked away at the south-east corner of the Subject Property.

Policy 8: *“Developments that include loading facilities, service areas, mechanical equipment (including roof-top), vents and metering devices should incorporate these requirements into the building design in such a way that they cannot be seen from, and their operation does not impact, the public street, pedestrian and cycle pathways or adjacent ground-oriented residences. This may be achieved through:*

- a. Containment, with a preference to using the same architectural detail, style and materials as the proposed development; and/or*
- b. Screening (e.g. trees, landscaped berms, decorative walls and fences).”*

The elements referred to in Policy 8 have been located to minimize exposure to the public realm. Loading and garbage areas have been provided at the rear.

Policy 18: *“The portion of the building(s) which are adjacent to the public realm will be held to the highest building design standards by incorporating specific building design features:*

- a. Design the building(s) first storey to be taller in height to retain flexibility or opportunity for ground floor uses in the future;*
- b. Locate front building façades parallel to the street; however consideration may be given to allow for interruptions of continuous building facades at strategic locations to provide pocket parks, plazas or other open spaces that provide a supportive function to the street activity or enable views and vistas;*
- c. Transparent windows at grade to give views into the building to observe the function of the building and out of the building to enhance natural surveillance;*
- d. Using architectural treatments (e.g. projections from continuous building lines, awnings, canopies, alcoves and bays) to soften the interface between buildings and the public realm;*
- e. Sufficient lighting sources for public uses after dark and to accentuate and animate buildings, natural features, public monuments and public spaces;*
- f. Utilize façade treatments to accentuate the transition between floors and interior spaces to provide visual interest and relief; and*
- g. Signage that contributes to the character of the surrounding area and architectural design of the building through appropriate architectural design elements, materials, and colour.”*

Policy 19: *“The portion of the development which impacts the public realm will be held to the highest site design standards and should incorporate enhanced public realm improvements, such as:*

- a. weather protection elements, (eg. colonnades, and awnings);*
- b. shade trees, median planting and treatments and other landscaping;*
- c. wider sidewalks and enhanced pedestrian surfaces;*
- d. coordinated furnishings and utilities, transit stops, and decorative lighting;*
and

e. memorials and public art commissioned for the location.”

Further to Policies 18 and 19 and as previously mentioned, the western façade of the building has been located along the Blair Place street edge with a connection from the multi-use pathway to the northern building entrance. Along the Blair Place frontage, a strip of landscaping will be provided between the multi-use pathway and the building façade. Further landscaping has been located around the gas bar to minimize exposure to the public realm.

Section 4.3 – Walking, Cycling, Transit, Roads and Parking Lots

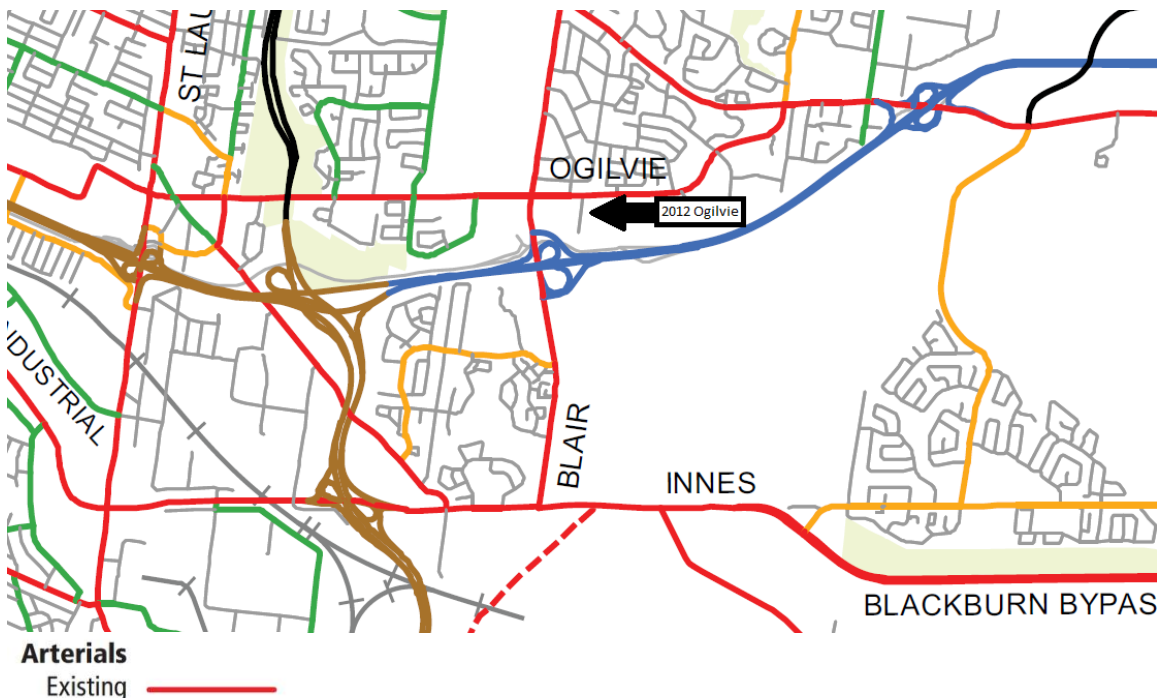


Exhibit 'G': Schedule 'E' of the Official Plan, Urban Road Network

As per Schedule 'E' of the Official Plan, Ogilvie Road is a designated existing arterial road which can accommodate the type of traffic volumes anticipated for Phase 2 of the redevelopment. Furthermore, OPA 150 designates this portion of Ogilvie Road as a bicycle spine route. This has been acknowledged in the location of the multi-use pathway along the Ogilvie Road frontage in Phase 1.

According to Section 4.3 of the OP, *“the City, when reviewing development applications, will assess the adequacy of the transportation network to meet the needs of the proposed development.”* In this case, the shopping centre as a whole is well served by roads, transit and bicycle linkages. The transportation network, as demonstrated in the Transportation Study, can support the redevelopment of Phase 2.

5.3 Blair Transit Oriented Development (TOD) Plan

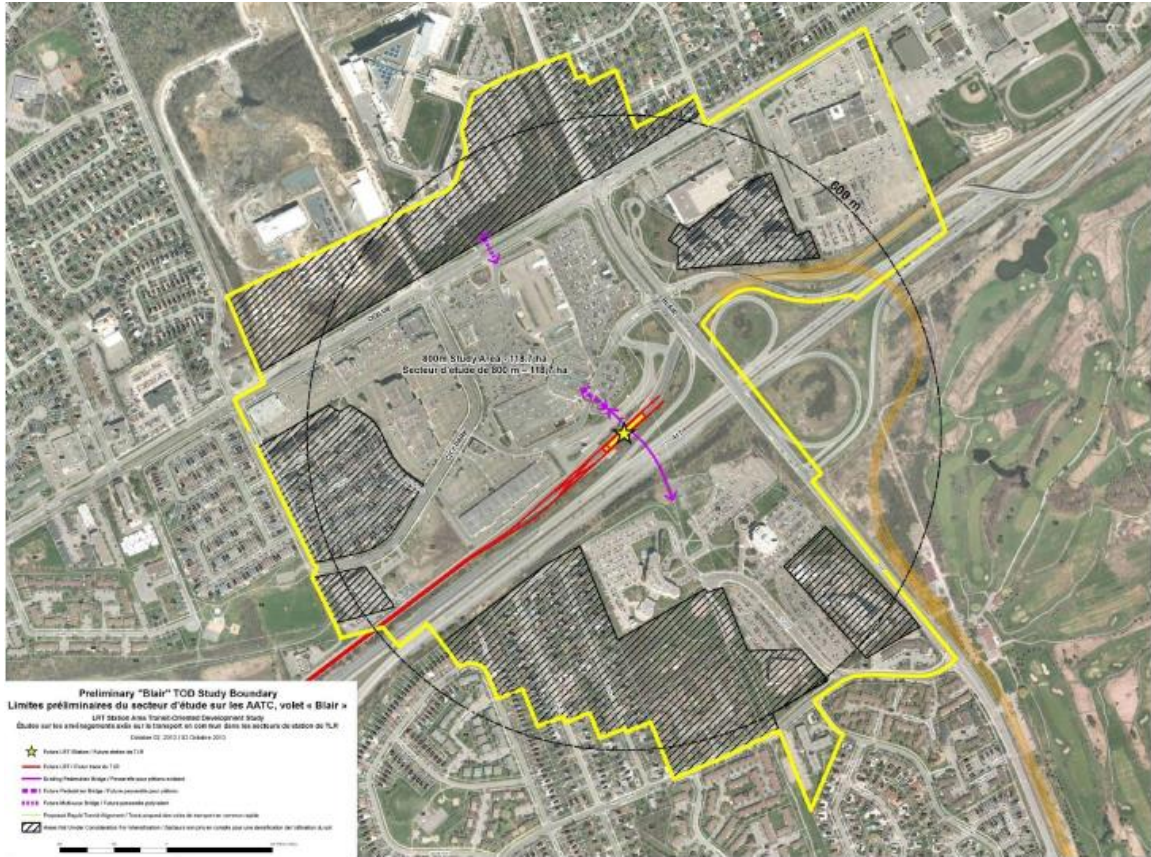


Exhibit 'H': Blair TOD Plan Area

As shown above, the Blair TOD Area is anchored by the Blair Rapid Transit Station, which is currently a bus rapid transit station that will be replaced by a light rail station in the near future. The majority of the Shoppers City East site lies outside of the 600m radius from the transit station, with the exception of the south-western corner of the property. Using available pedestrian routes, the property is over 800m from the transit station.

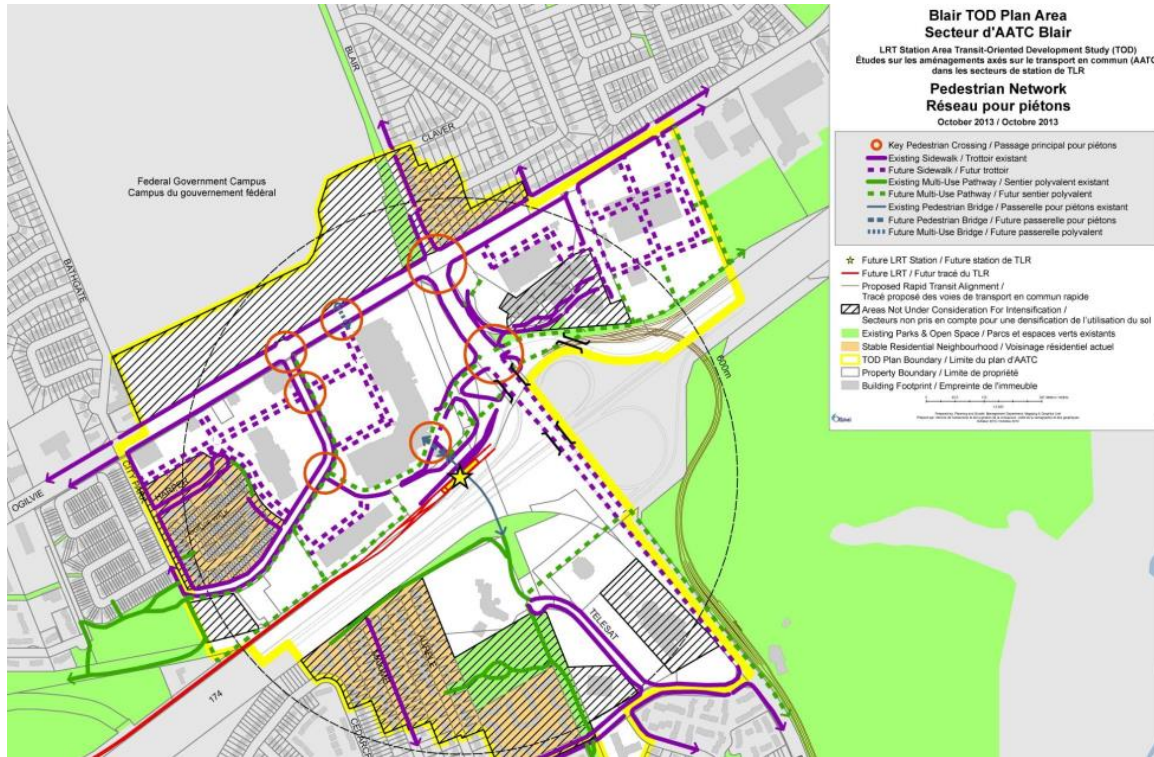


Exhibit '1': Blair TOD Plan – Pedestrian Network

As shown above, the Blair TOD Plan aims to shorten pedestrian and cycling routes to the transit station and break up some of the larger blocks with additional connections. As already mentioned, Phase 2 of the redevelopment will build on the connections proposed in Phase 1, will break up the block and will provide a connection to the pathway at the rear of the property which will link to the transit station as well as Trillium Park.

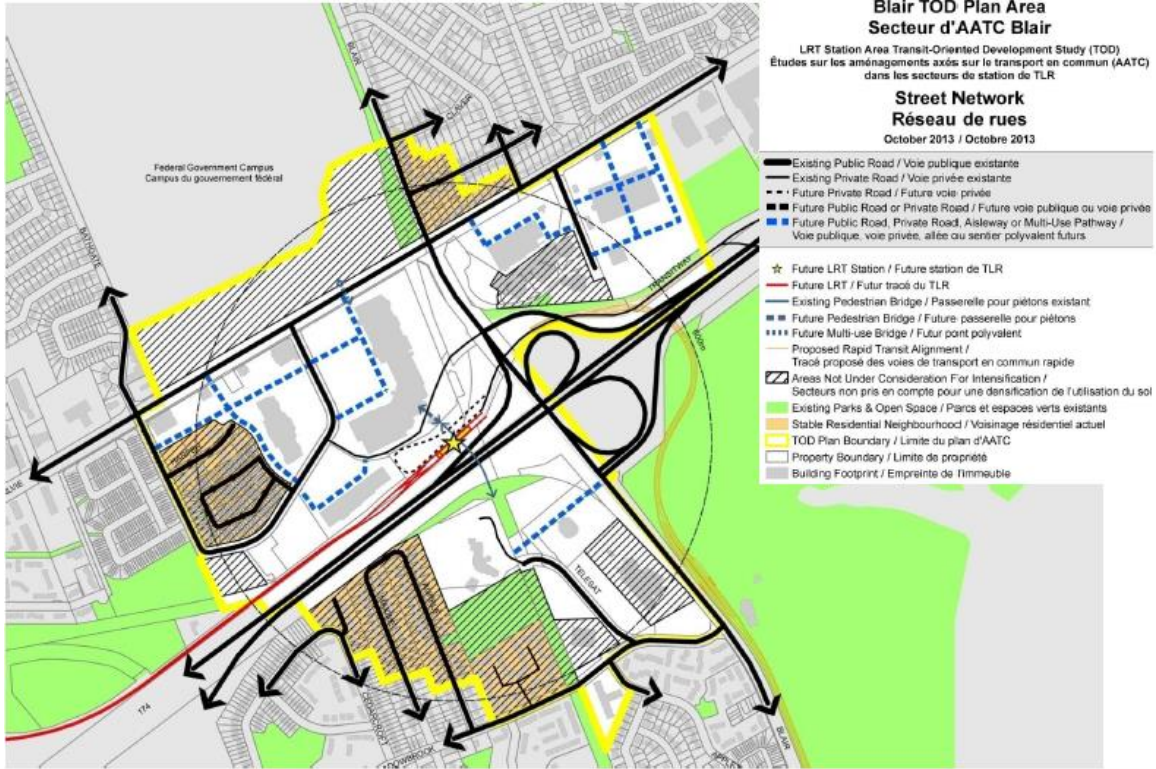


Exhibit 'J': Blair TOD Plan – Street Network

The redevelopment of both Phase 1 and 2 proposes a block pattern similar to what is proposed by the Blair TOD Plan Street Network above. A landscaped central entrance will break up the parcel in the middle. An east-west route is provided at the edge of Phase 1, leading into Phase 2, with accesses from Blair Place and the most easterly access from Ogilvie.

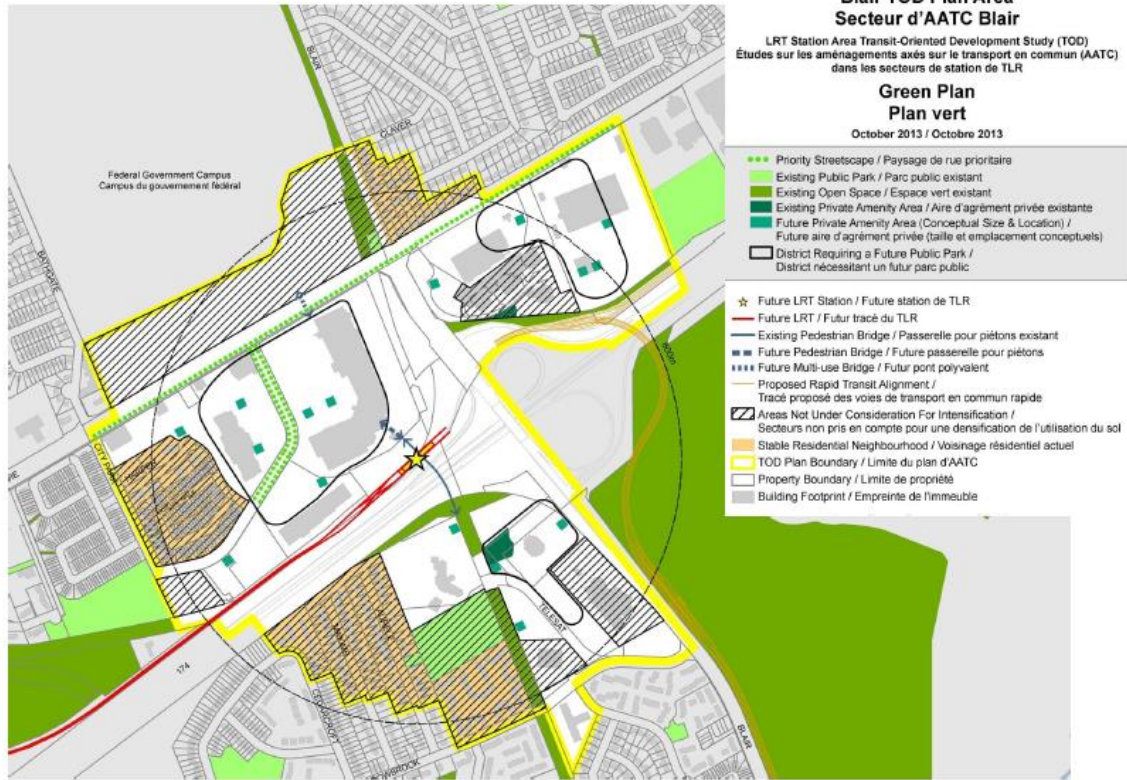


Exhibit 'K': Blair TOD Plan – Green Plan

The redevelopment will respect the existing open space to the rear of the property and proposes landscaping throughout the site to further green the space.

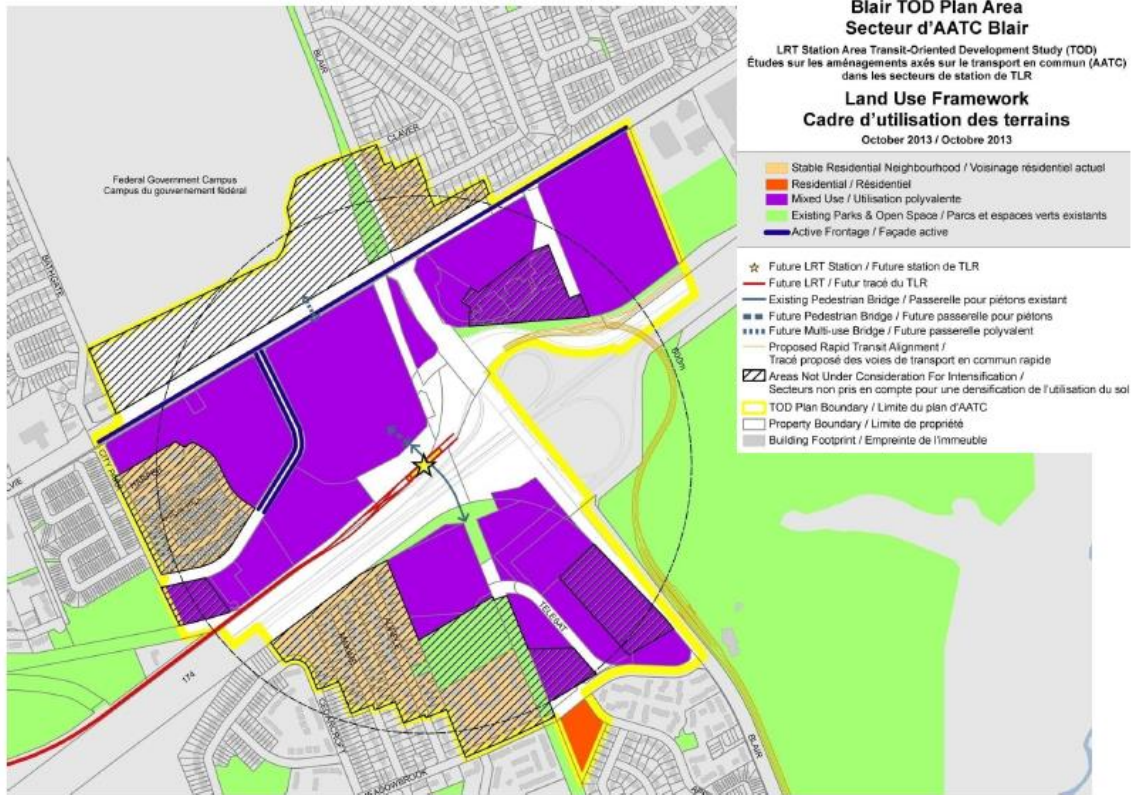


Exhibit 'L': Blair TOD Plan – Land Use Framework

The land use framework permits a mix of uses on the Subject Property, but does not specify the mix as this is specified in the zoning by-law (to be discussed in Section 5.5 of the Planning Rationale). The use proposed is consistent with the commercial uses permitted by the plan.

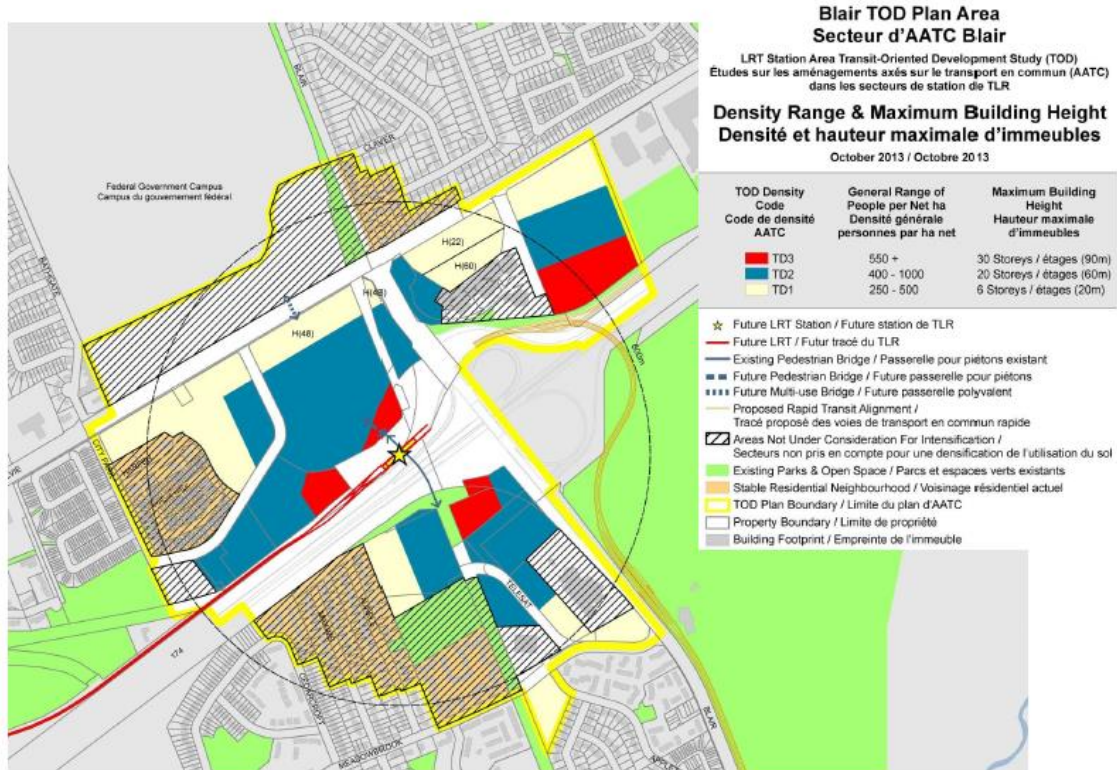


Exhibit 'M': Blair TOD Plan – Density Range and Maximum Building Height

The Plan proposes different density ranges for the Subject Property, with the highest density proposed closest to the highway. The TOD Plan seeks to achieve transit supportive densities over time. Further to Section 11.1 of the Plan, *“landowners are not required to develop their lands precisely as shown on the TOD illustrations, land use plan and tables since they represent only one option for TOD supportable design and are not prescriptive. The implementing TOD zoning requires minimum densities and certain specific design requirements. The intent is to permit flexibility in land use mix and built form architectural expression to respond to the market requirements of the day.”* The plan therefore proposes an option for development but relies on the zoning in place to guide development on a site by site basis. An analysis of the zoning will follow in Section 5.5.

5.4 Conformity with the Urban Design Guidelines

The Urban Design Guidelines for Transit Oriented Development and for Large Format Retail are relevant to the proposed development.

The proposal incorporates the Urban Design Guidelines for Transit Oriented Development in the following ways:

- Attracting and generating pedestrian traffic through ameliorated connections;

- Breaking up the of the parcel with a new internal street network resembling a grid pattern;
- Creating a pedestrian short cut to transit and recreation with a linkage to the planned east-west walkway to the rear of the property;
- Using different materials and painting patterns to identify pedestrian crosswalks;
- Providing bicycle parking in a visible and convenient location that avoids conflicts with shopping carts;
- Locating the large supply of parking needed to support the warehouse behind the first phase of development to reduce the visual impact of parking on Ogilvie Road;
- Creating a parking lot that has direct and safe pedestrian access from vehicles;
- Minimizing conflicts between vehicles and pedestrians with a well-integrated pedestrian circulation network;
- Locating loading and refuse areas at the rear of the building.

The proposed development incorporates the Urban Design Guidelines for Large Format Retail in the following ways:

- Providing a 6m setback along Blair Place in order to accommodate landscaping and the multi-use pathway along the street edge;
- Orienting the long side of the building (western façade) parallel to the street;
- Creating an internal circulation pattern which allows for logical movement throughout the site and does not preclude future intensification over time;
- Providing safe and clearly marked pedestrian walkways;
- Orienting parking spaces to minimize the number of traffic aisles that need to be crossed by pedestrians to reach the building's entrance.

5.5 Conformity with the City of Ottawa Comprehensive Zoning By-law No. 2008-250



Exhibit 'N' – Zoning Map (GeoOttawa)

As per **Exhibit 'N'** above, the property is subject to three different Transit Oriented Development Subzones (TD1, TD2 and TD3) and the same zoning exception no. 2086. The different zones permit additional density from north to south so that the most southerly portion of the property is permitted the most density as it is located closest to the Blair LRT station. Exception no. 2086 preserves some of the permitted uses and performance standards of the zoning in place previous to the adoption of the TOD Plan and corresponding TD zoning.

The purpose of the TD – Transit Oriented Development Zone is to:

- (1) *“Establish minimum density targets needed to support Light Rail Transit (LRT) use for lands within Council approved Transit Oriented Development Plan areas;*
- (2) *Accommodate a wide range of transit-supportive land uses such as residential, office, commercial, retail, arts and culture, entertainment, service and institutional uses in a compact pedestrian-oriented built form at medium to high densities;*
- (3) *Locate higher densities in proximity to LRT stations to create focal points of activity and promote the use of multiple modes of transportation; and,*
- (4) *Impose development standards that ensure the development of attractive urban environments that exhibit high-quality urban design and that establish priority streets for active use frontages and streetscaping investment.”*

In accordance with Section 195, the following non-residential land uses are permitted. The relevant uses have been bolded.

- amusement centre
- animal care establishment
- animal hospital
- artist studio
- bank
- bank machine
- bar
- broadcasting studio
- cinema
- community centre
- community garden
- community health and resource centre
- convenience store
- court house
- day care
- diplomatic mission
- drive-through facility
- emergency service
- group home
- home-based business
- home-based day care
- hospital
- hotel
- instructional facility
- library
- medical facility
- municipal service centre
- museum
- nightclub
- office
- personal service business
- place of assembly
- place of worship
- post office
- post-secondary educational institution
- production studio
- recreational or athletic facility
- research and development centre
- residential care facility
- restaurant

- retail food store
- **retail store**
- school
- service and repair shop
- shelter
- small batch brewery
- sports arena
- technology industry
- theatre
- training centre

The uses proposed for Phase 2 of the site plan application consist of a retail store and a gas bar. The store will also have a seasonal garden centre and a tire centre. Although an automobile service station and gas bar are not generally permitted in the TD zone, exception no. 2086 permits the following additional uses:

- any use that legally existed on January 22, 2014
- **automobile service station**
- car wash
- **gas bar**

Transit Oriented Development Zone Provisions

The above land uses are permitted subject to the following provisions in Table 195. The table has been modified to reflect the overriding provisions of exception number 2086 (non-applicable provisions struck out as the development does not exceed a 22m maximum building height and a maximum floor space index of 1.1 as per urban exception 2086). An additional column has been added to the table to demonstrate zoning compliance.

TABLE 195 – TD ZONE PROVISIONS

I ZONING MECHANISMS	II PROVISIONS	III ZONING COMPLIANCE
(a) Minimum lot area	No minimum	N/A
(b) Minimum lot width	No minimum	N/A

I ZONING MECHANISMS		II PROVISIONS	III ZONING COMPLIANCE
(c) Minimum front yard and corner side yard setback	(i) abutting a lot in a residential zone	3 metres	
	(ii) abutting the rapid transit corridor	2 metres	
	(iii) parking garage that is not incorporated into another building	10 metres	
	(iv) residential use building	3 metres	
	(v) all other cases	0.5 metres	6.0m (Corner Side Yard – Blair Place)
(d) Minimum interior side yard setback	(i) abutting a lot in a residential zone	3 metres	
	(ii) abutting the rapid transit corridor	2 metres	
	(iii) all other cases	No minimum	9.14m
	(iv) despite (i), (ii) and (iii) above that part of a building more than 6 storeys in height	12 metres	

I ZONING MECHANISMS		II PROVISIONS	III ZONING COMPLIANCE
(e) Minimum rear yard setback	(i) rear lot line abutting a lot in a residential zone	6 metres	
	(ii) rear lot line abutting the rapid transit corridor	2 metres	9.14m
	(iii) all other cases	No minimum	
	(iv) despite (i), (ii) and (iii) above that part of a building more than 6 storeys in height	12 metres	
(f) Minimum building height	-	6.7 metres and 2 storeys	10m
(g) Maximum building height	(i) in any area up to and including 15 metres from a property line abutting a R1, R2 or R3 zone	14.5 metres	
	(ii) in all other cases	as shown by the suffix "H" on a zoning map, or specified in a subzone or exception where applicable	10m
(h) Minimum width of landscaped area		no minimum, except that where a yard is provided and not used for required driveways, aisles, parking, loading spaces or outdoor commercial patio, the	All yards not provided for driveways, aisles, parking and loading will be landscaped.

I ZONING MECHANISMS	II PROVISIONS	III ZONING COMPLIANCE
	whole yard must be landscaped	

The following zoning provisions are also required by the TD zone. In accordance with exception 2086, the non-applicable provisions have been struck out given that the proposed development is under 22m in height and has a floor space index of less than 1.1 as per Exception 2086.

Sec. 195(5): *“Outdoor storage is prohibited.”*

No outdoor storage is proposed.

~~Sec. 195(6): “where the wall of the 1st storey of a building is within 10 metres of a lot line abutting a public street and where the building is more than 6 storeys in height the wall facing the street must be stepped back at either the 2nd, 3rd, 4th, 5th, 6th or 7th storey at least a further 2.5 metres from the wall of the storey below.”~~

~~Sec. 195(7): “where two buildings on the same lot are both more than 6 storeys in height that part of the buildings greater than 6 storeys tall must be a minimum of 24 metres away from each other.”~~

~~Sec. 195(8): “for lots greater in area than 1250 m², 2% of the total lot area must be provided as outdoor communal space located at grade anywhere on the lot and such area can also be used towards complying with any amenity area requirements.”~~

~~Sec. 195(9): “Column II of Table 101 – Minimum Parking Space Rates in Section 101 applies to all land zoned TD despite the location of the land on Schedule 1.”~~

~~Sec. 195(10): “Column III of Table 103 – Maximum Number of Parking Spaces Permitted in Section 103 applies to all land zoned TD despite the location of the land on Schedules 1, 2A and 2B.”~~

Sec. 195(11): *“Despite clauses 100(1)(a) and (c), in the TD Zone parking spaces required or provided under this by-law may be available for use by any other land use located either on or off site, but these spaces are not intended to serve as the required parking for these other land uses.”*

The parking spaces are not intended to be used for other land uses located off site.

Sec. 195(12): *“For other applicable provisions, see Part 2 – General Provisions, Part 3 – Specific Use Provisions, and Part 4 – Parking, Queuing and Loading Provisions.”*

An analysis of these parts of the by-law will be provided further in this section.

~~Sec. 195(13) – Active Frontage Street Areas Provisions~~

~~Sec. 196 – TD Subzones~~

Part 2 – General Provisions

Sec. 66(1): *“Facilities relating to the handling and transfer of propane and natural gas, including tanks and associated compressors, pumps and other similar facilities must not be located in any required front, side, corner side or rear yard, nor closer than 30 metres to any lot line abutting a residential zone.”*

The propane station and gas station are not located in any required yards. The Subject Property does not have a lot line abutting a residential zone.

The site plan is not impacted by any of the other sections in Part 2.

Part 3 – Special Use Provisions

The site plan is not impacted by any of the sections in Part 3.

Part 4 – Parking and Loading Provisions

Section 101 - Minimum Parking Space Rates:

Sec. 101(1): *“Off-street motor vehicle parking must be provided for any land use at the rate set out in Table 101 below.”*

Sec. 101(4): *“Where a lot is located within 600 metres of a rapid transit station identified on Schedule 2A or Schedule 2B of this by-law, the minimum required motor vehicle parking rate is reduced where specified in Table 101. The 600 metre distance is measured as the shortest perpendicular distance between the lot lines of the lot containing the use and the centre of the rapid transit station platform.”*

Only the most south-westerly corner of the Subject Property is located within the 600m radius of the Blair rapid transit station.

Sec. 101(5): *“Despite subsection (4), where the lot is separated from the rapid transit station by a highway, grade-separated arterial roadway, railway yard, watercourse, private lands or any other major obstacle such that the actual*

walking distance to the rapid transit station is increased to beyond 800 metres, the reduced minimum parking rate specified in Table 101 does not apply and the standard parking rate for that use applies instead.”

The property is separated from the rapid transit station such that the walking distance is more than 800m to the rapid transit station. As such, the full parking rate applies.

Table 101- MINIMUM PARKING SPACE RATES

I Land Use		Minimum Number of Parking Spaces Required			
		II Area A on Schedule 1 and MC Zone at Tunney's Pasture (Central Area)	III Area B on Schedule 1 other than MC Zone at Tunney's Pasture (Inner City Area)	IV Area C on Schedule 1 (Suburban Area)	V Area D on Schedule 1 (Rural Area)
(cs) Shopping Centre	(i) Within 600 metres of a rapid transit station (see Sections 101(4) and 101(5))	0.75 per 100m ² of gross leasable floor area	3 per 100m ² of gross leasable floor area	3.4 per 100m ² of gross leasable floor area	
	(ii) Other cases (also see Section 101(9))		3.4 per 100m ² of gross leasable floor area	3.6 per 100m ² of gross leasable floor area	

The Subject Property is located within Area C on Schedule 1 and is not subject to the parking reduction in proximity to a rapid transit station. It is therefore subject to a minimum parking space rate of 3.6 per 100m² of gross leasable floor area. No parking is required for a gas bar. Phase 2 is the location of the 15,092m² retail store which therefore requires a total of 543 parking spaces. 786 parking spaces have been provided.

Sec. 103 – Maximum Limit on Number of Parking Spaces Near Rapid Transit Stations:

Sec. 103(1): *“Where a lot is located within 600 metres of a rapid transit station shown on Schedule 2A or Schedule 2B of this by-law, the number of motor vehicle parking spaces provided for a use on that lot must not exceed the maximum limits specified in Table 103. The 600 metre distance is measured as the shortest perpendicular distance between the lot lines of the lot containing the use and the centre of the rapid transit station platform.”*

Only the most south-westerly corner of the Subject Property is located within the 600m radius of the Blair rapid transit station.

Sec. 103(2): *“Despite subsection (1), where the lot is separated from the rapid transit station by a highway, grade-separated arterial roadway, railway yard, watercourse, private lands or any other major obstacle such that the actual walking distance to the rapid transit station is increased to beyond 800 metres, the maximum limit on the number of parking spaces specified in Table 103 does not apply.”*

The property is separated from the rapid transit station such that the walking distance is more than 800m to the rapid transit station. As such, the proposed development is not subject to the maximum limit on number of parking spaces near rapid transit stations.

Section 106 - Parking Space Provisions:

Sec. 106(1): *“A motor vehicle parking space must have:*

- a) a minimum width of 2.6 metres and a maximum width of 2.75 metres; and*
- b) a minimum length of 5.2 metres, except for parallel parking where a minimum length of 6.7 metres is required.”*

The proposed parking space width for Phase 2 of the site plan is 3.05m, which is above the maximum of 2.75m. There is no maximum parking space length required by the by-law. The additional width is required as customers of the retail store will be purchasing large items in bulk and will require additional width in order to safely load their vehicles with items purchased. A minor variance application will be required for relief from this provision.

Section 107 - Aisle and Driveway Provisions:

Sec. 107(1): *“The following regulations apply to parking lots and parking garages, whether as principal or accessory uses:*

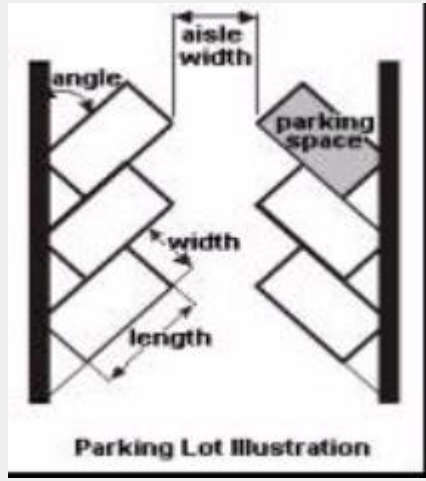
Sec. 101(1)(a): *A driveway providing access to a parking lot or parking garage must have a minimum width of:*

- i. three metres for a single traffic lane, and*
- ii. 6.7 metres for a double traffic lane.*

All driveways are over 6.7m wide.

Sec. 101(1)(c): “An aisle providing access to parking spaces in a parking lot or parking garage must comply with the minimum required width specified in Table 107.”

Table 107- Minimum Required Aisle Width

I Angle of Parking (degrees)	II Minimum Required Aisle Width (metres)	
(a) 0-40	3.5	
(b) 41-55	4.3	
(c) 56-70	6.5	
(d) 71-90	6.7	

The parking spaces are oriented at a 90 degree angle; therefore, the aisle widths must be a minimum of 6.7m. All aisle widths comply with the minimum required.

Section 110 - Landscaping Provisions for Parking Lots:

Sec. 110(1): “Except in the case of an industrial zone, a minimum of 15% of the area of any parking lot, whether a principal or an accessory use, must be provided as perimeter or interior landscaped area comprised of the following:

- a) a landscaped buffer must be provided between the perimeter of the parking lot and a lot line in accordance with Table 110. A driveway may cross the landscaped buffer; and
- b) in addition to the landscaped buffer, interior landscaping may be provided including various landscaped islands, landscaped medians, pedestrian pathways or public plazas to meet the minimum 15% requirement.”

Table 110- Minimum Required Width of a Landscaped Buffer of a Parking Lot (OMB Order, File #PL080959 issued September 18, 2009)

I Location of Landscaped Buffer	Minimum Required Width of Landscaped Buffer		
	II For a parking lot containing 10 or fewer spaces	III For a parking lot containing more than 10 but fewer than 100 spaces	IV For a parking lot containing 100 or more spaces
(a) Abutting a street	3 metres		
(b) Not abutting a street	None	1.5 metres	3 metres

The total landscaped area is 16.36% whereas 15% is required by the by-law.

The minimum width of the landscaped buffer abutting Blair Place is 6m (which includes the multi-use pathway), which exceeds the 3m required by Table 110.

The minimum width of the landscaped buffer on the eastern side of the property, not abutting the street, is 1.35m at the narrowest section, which is below the required 3m for a landscaped buffer not abutting a street for a parking lot containing more than 100 parking spaces. If necessary, this provision will be dealt with through a minor variance application.

Sec. 110(3): *“All outdoor loading and refuse collection areas contained within a parking lot must be:*

- a) located at least nine metres from a lot line abutting a public street;*
- b) located at least three metres from any other lot line; and*
- c) screened from view by an opaque screen with a minimum height of two metres.”*

All outdoor loading and refuse collection areas are located over 9m from a lot line abutting a public street, over 3m from any other lot line and are screened by an opaque screen with a minimum height of 2m.

Section 111 - Bicycle Parking Space Rates and Provisions:

Sec. 111(1): *“Bicycle parking must be provided for the land uses and at the rate set out in Table 111A for lands located in Areas A (Central Area), B (Inner City Area) and C (Suburban Area) on Schedule 1...”*

The bicycle parking rate for a shopping centre is 1 per 500m² of gross floor area. The gross floor area for the Phase 2 retail building is 15,294m²; therefore, a total of 31 bicycle spaces are required, which have been provided in a well-lit location with convenient access to entrances near the Seasonal Garden Centre. The spaces have also been located to minimize conflicts with shopping carts.

Section 113 - Loading Space Rates and Provisions:

Sec. 113(1): *“Except in the case of uses in an LC Zone and on lots abutting Rideau Street, Sparks Street and Bank Street in Area A (Central Area) of Schedule 1, off-street motor vehicle loading spaces must be provided for the specified land uses at the rate set out in Table 113A.”*

Sec. 113(2): *“Where more than one use is located in a building or on a lot, vehicle loading spaces must be provided for each use in accordance with the rate set out in Table 113A.”*

Table 113A:

I Land Use	Minimum Number of Vehicle Loading Spaces Required per Square Metres of Gross Floor Area							
	II Less than 350 m ²	III 350-999 m ²	IV 1000-1999 m ²	V 2000-4999 m ²	VI 5000-9999 m ²	VII 10000-14999 m ²	VIII 15000-24999 m ²	IX 25000 m ² and over
(c) Retail food store, retail store, shopping centre, except in the TM Zone (see 113(4) above)	0		1	2				

Table 113A requires 2 off-street loading spaces for a shopping centre/retail store with a GFA of 15,294m².

Table 113C requires that half of the loading spaces provided be oversized spaces for a retail store with over 2,000m² in GFA.

4 oversized loading spaces have been provided, which exceeds the minimum required by the by-law.

Table 113B specifies the provisions for loading spaces as follows:

I Zoning Mechanism		Regulations		Zoning Compliance
		II Standard Size Space	III Oversized Space (see Table 113C for Number of Oversized Spaces Required)	
(a) Minimum Width in metres of Driveway Accessing Loading Space		(i) Single traffic lane - 3.5 (ii) Double traffic lane - 6		Driveway access is greater than 6m.
(b) Minimum Width in metres of Aisle Accessing Loading Space, by Angle of Loading Space	(i) 45° or less	5	11	
	(ii) Between 45° and 60°	6.3	14	
	(iii) 60° to 90°	9	17	The aisle width is greater than 17m.
(c) Minimum Width in metres of Loading Space		3.5	4.3	The width of each space is approximately 4.42m
(d) Minimum Length in metres of Loading Space	(i) Parallel	9	13	
	(ii) Other cases	7	13	23.27m

(e) Minimum Vertical Clearance in metres for Loading Space	4.2	The loading spaces have a vertical clearance of over 4.2m.
(f) Permitted Location of Loading Space	Permitted in all locations other than in a required front yard or required corner side yard, or in a required yard abutting a residential zone	The loading spaces are not located in a required yard and do not abut a residential zone.

6.0 Technical Studies

The background studies that have been prepared in support of the development application for the subject property are as follows:

- Functional Servicing and Stormwater Management Report, May 2016,
- Transportation Impact Study, May 17, 2016,
- Geotechnical Investigation, October 18, 2013,
- Phase 1 and 2 Environmental Site Assessment, October 2013.

7.0 Summary and Conclusions

1. The application for site plan revision is consistent with the Provincial Policy Statement as the proposed development contributes to a strong and diversified economic base, improves access to transit, provides stronger pedestrian connections and makes efficient use of available land and infrastructure.
2. The development provides an appropriate transition from proposed higher density uses around the Blair transit station within the Mixed Use Centre.
3. The uses proposed will provide retail and services to residents as the TOD Area intensifies, reducing the need to travel elsewhere in the city to access similar services.
4. The site plan complies with the majority of the zoning provisions. A minor variance application will be filed for relief from the maximum parking space size provision and the landscape buffer provision.

Based on the above noted rationale the application for Site Plan Revision is appropriate and represents sound land use planning.

Prepared by;

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