

**Planning Rationale in Support of an Application  
for Site Plan Control**

**18 McArthur Avenue  
City of Ottawa**

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## **1.0 Introduction**

Holzman Consultants Inc. has been retained by 2407919 Ontario Limited (the “Owner”) to prepare a planning rationale in support of an application for Site Plan Control to permit a 3 storey residential building at 18 McArthur Avenue in the Rideau-Vanier Ward of the City of Ottawa (the “Subject Property”).

This report provides a description of the existing conditions, an explanation of the proposed development and a review of the applicable land use planning policies including the Provincial Policy Statement (PPS), the Official Plan (OP) policies and the Zoning By-law provisions. The summary and conclusions indicate that the proposed development is supported by the PPS, OP and Zoning By-law.

## **2.0 Site Overview**



**Exhibit ‘A’** - Aerial Photo (Subject Property outlined in red)

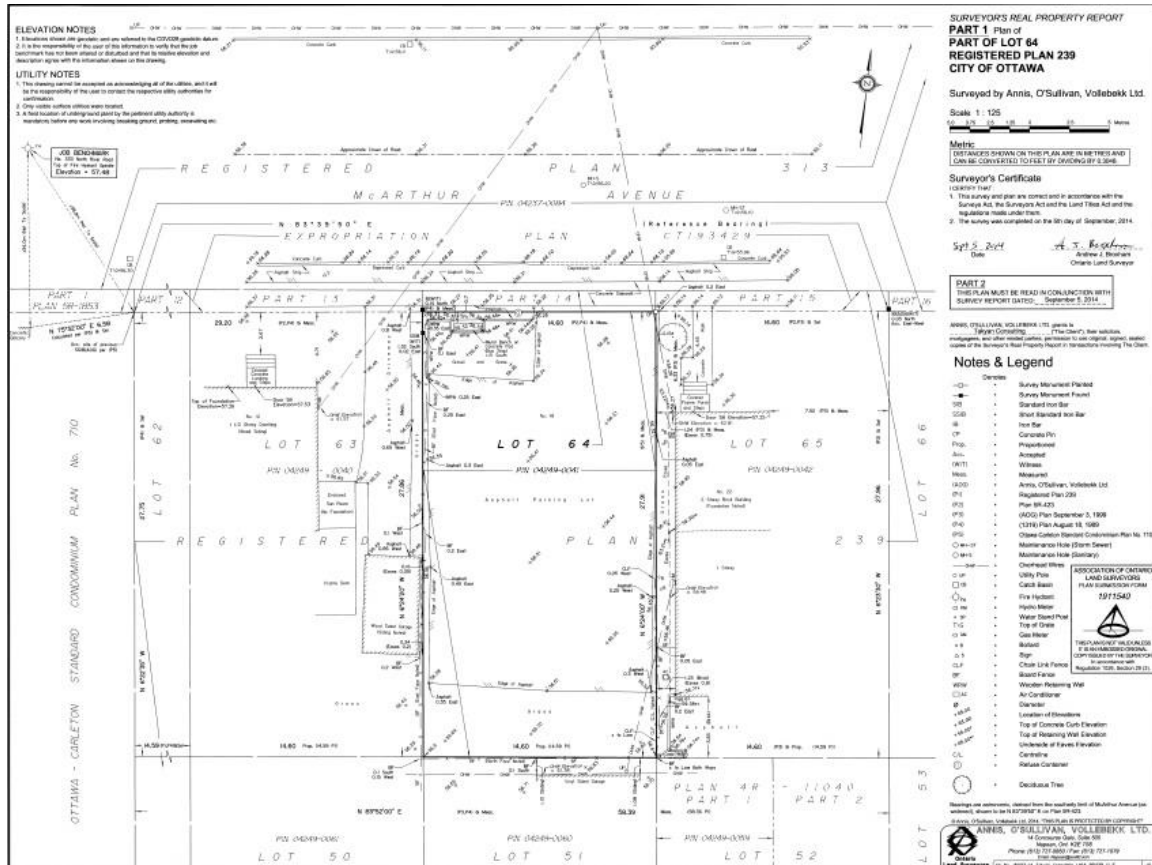
The Subject Property consists of a parcel of land located on the south side of McArthur Avenue, just east of North River Road. The Subject Property is the location of an existing surface parking area.



**Exhibit ‘B’** - Site Photo

The Subject Property is 407m<sup>2</sup> in size with approximately 14.6m of frontage on McArthur Avenue. It is surrounded by the following land uses:

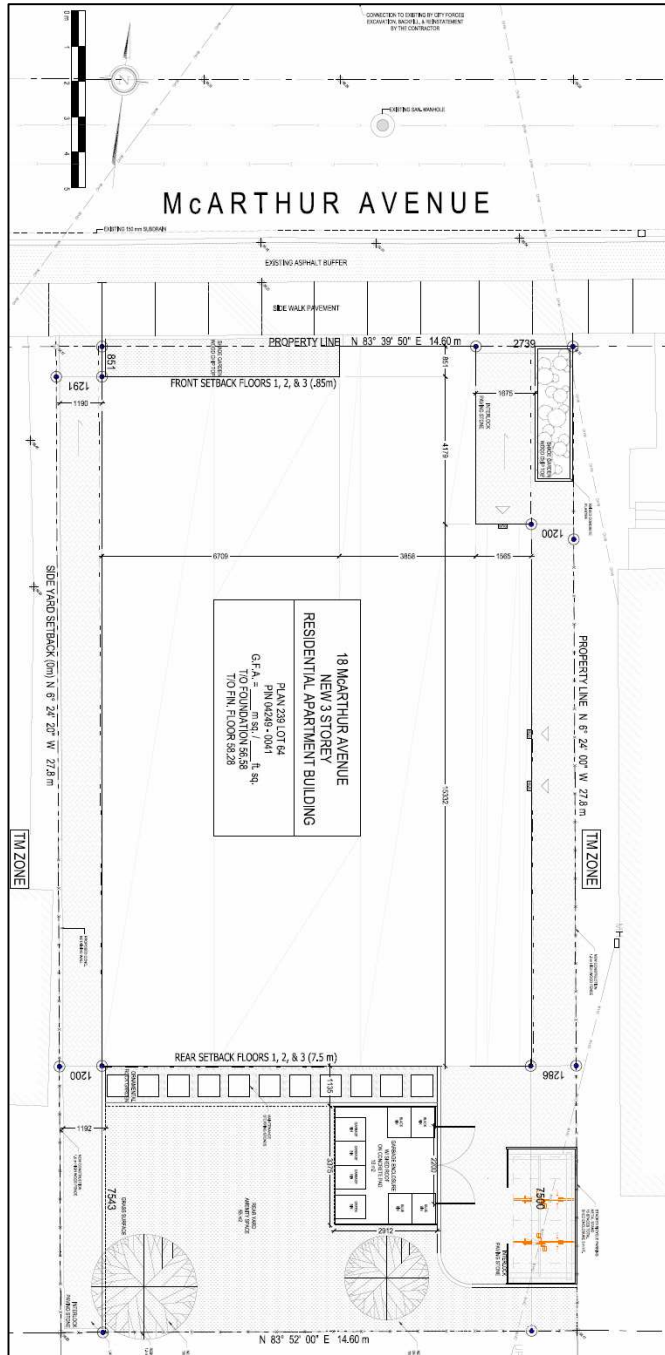
- North: Place Vanier Office Tower Complex (comprised of one 19 storey tower, one 16 storey tower and one 10 storey tower),
- East: Two storey Dental Office; One and two storey dwellings and businesses further east,
- South: Single and Semi-Detached Housing along Carlotta Avenue,
- West: A 2 storey dwelling, a 2 storey restaurant building and a 5 storey residential building (at the corner of McArthur and North River Road)



**Exhibit 'C' - Survey Plan**

The Subject Property is legally described as PLAN 239 LOT 64 and is denoted as (see **Exhibit 'C'** – Survey Plan).

### 3.0 Description of Proposed Development



**Exhibit 'D'** - Proposed Site Plan

The Subject Application proposes the construction of a 3 storey residential building with 8 dwelling units (two on each floor, including the basement). Each unit includes four bedrooms, accommodating approximately 32 residents; which the owner intends to lease to on a rental basis. The total gross floor area for the entire proposed building is 706.3m<sup>2</sup> (7,602ft<sup>2</sup>).



**Exhibit 'E' - Front Elevation**

The proposed building will be set close to the street with access to a detached bike parking unit and garbage enclosure, both located in the rear yard behind the building. On the ground floor, at-grade access will be oriented toward the McArthur Street frontage. Landscaping and a 1.5m high wood fence enclose the rear yard uses from the southerly and side-abutting properties.

The building's façade will be comprised of a combination of stone/brick veneer siding and metal siding with a brushed steel/matte gray finish. An 80m<sup>2</sup> amenity space will be located on the rooftop in addition to a 55m<sup>2</sup> rear yard amenity area. A minor variance will be filed for a relief from parking requirements on the basis of the site's excellent connectivity through transit, walking, and cycling. Consequently, the provisions for bicycle parking will be quadruple the minimum requirements.

## **4.0 Planning Context**

The applicable policy framework includes an examination of the Province of Ontario's land use planning directives expressed in the Provincial Policy Statement and the City of Ottawa's policies expressed in the City of Ottawa Official Plan.

### **4.1 Conformity with the Provincial Policy Statement**

The Provincial Policy Statement ("PPS") sets out a vision for land use planning in the Province of Ontario that encourages planning and development that is environmentally-sound, economically-strong and that enhances quality of life. Land planning policies are intended to promote efficient development patterns with an appropriate mix of housing, employment, open spaces and multi-modal transportation which are appropriate for and make efficient use of existing and planned infrastructure and public service facilities. The relevant policies to the subject application are as follows.

Section 1.1 provides policy guidance for efficient development and land use patterns. This section states: "*Healthy, liveable and safe communities are sustained by:*

- a) *promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*
- b) *accommodating an appropriate range and mix of residential (including second units, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
- c) *avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
- d) *avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;*
- e) *promoting cost-effective development patterns and standards to minimize land consumption and servicing costs;*
- f) *improving accessibility for persons with disabilities and older persons by identifying, preventing and removing land use barriers which restrict their full participation in society;*
- g) *ensuring that necessary infrastructure, electricity generation facilities and transmission and distribution systems, and public service facilities are or will be available to meet current and projected needs; and*
- h) *promoting development and land use patterns that conserve biodiversity and consider the impacts of a changing climate."*

The proposed development will accommodate new rental residential occupancy on a site that is located in an area prime for renewal; in the process increasing the

range and mix of residential housing. The property is located on a Traditional Mainstreet, with direct access to an arterial road in an urban area well served by existing infrastructure, including bus service and community amenities. Redevelopment of the site therefore constitutes efficient and cost-effective development.

Section 1.1.3 directs growth and regeneration to settlement areas, like the urban area of Ottawa. As per Section 1.1.3.2, *“Land use patterns within settlement areas shall be based on:*

- a) densities and a mix of land uses which:*
  - 1. efficiently use land and resources;*
  - 2. are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
  - 3. minimize negative impacts to air quality and climate change, and promote energy efficiency;*
  - 4. support active transportation;*
  - 5. are transit-supportive, where transit is planned, exists or may be developed; and*
  - 6. are freight-supportive; and*
- b) a range of uses and opportunities for intensification and redevelopment in accordance with the criteria in policy 1.1.3.3, where this can be accommodated.”*

The density proposed is appropriate for the neighbourhood and makes efficient use of the land and infrastructure available. As the Subject Property is located on a mainstreet already served well by bus transit and projected as a transit priority corridor, the development will support existing transit ridership and future transit expansions. The proposed development, being oriented to the street, will enhance the neighbourhood’s walkability and support active transportation. The existing surface parking lot is ripe for redevelopment and the site presents an excellent opportunity for reasonable intensification and redevelopment in accordance with the settlement area policies.

Section 1.4 outlines the provincial policies for housing and requires the provision of *“an appropriate range and mix of housing types and densities”*. In order to do so, planning authorities shall permit and facilitate residential intensification, particularly in areas where *“appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs.”* Furthermore, planning authorities shall promote *“densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed”*



The proposed development meets all of the provincial policies regarding housing and residential intensification. The development proposes an appropriate residential density in an area with existing infrastructure, community services and transit. Active transportation will be promoted substantially within this development given that it will not have vehicle parking, but will have twice the minimum required bicycle parking. Further, the dwelling units proposed offer multiple bedrooms which is often lacking in similar types of developments, thereby adding to a wider range and mix of housing types to lower income larger families or student accommodation.

In addition to Section 1.3 regarding economic development and competitiveness, Section 1.7.1 provides policy guidance for how long term economic prosperity should be supported, as follows:

- a) *“promoting opportunities for economic development and community investment-readiness;*
- b) *optimizing the long-term availability and use of land, resources, infrastructure, electricity generation facilities and transmission and distribution systems, and public service facilities;*
- c) *maintaining and, where possible, enhancing the vitality and viability of downtowns and mainstreets;*
- d) *encouraging a sense of place, by promoting well-designed built form and cultural planning, and by conserving features that help define character, including built heritage resources and cultural heritage landscapes;*
- e) *promoting the redevelopment of brownfield sites;*
- f) *providing for an efficient, cost-effective, reliable multimodal transportation system that is integrated with adjacent systems and those of other jurisdictions, and is appropriate to address projected needs to support the movement of goods and people;*
- g) *providing opportunities for sustainable tourism development;*
- h) *providing opportunities to support local food, and promoting the sustainability of agri-food and agri-product businesses by protecting agricultural resources, and minimizing land use conflicts;*
- i) *promoting energy conservation and providing opportunities for development of renewable energy systems and alternative energy systems, including district energy;*
- j) *minimizing negative impacts from a changing climate and considering the ecological benefits provided by nature; and*
- k) *encouraging efficient and coordinated communications and telecommunications infrastructure.”*

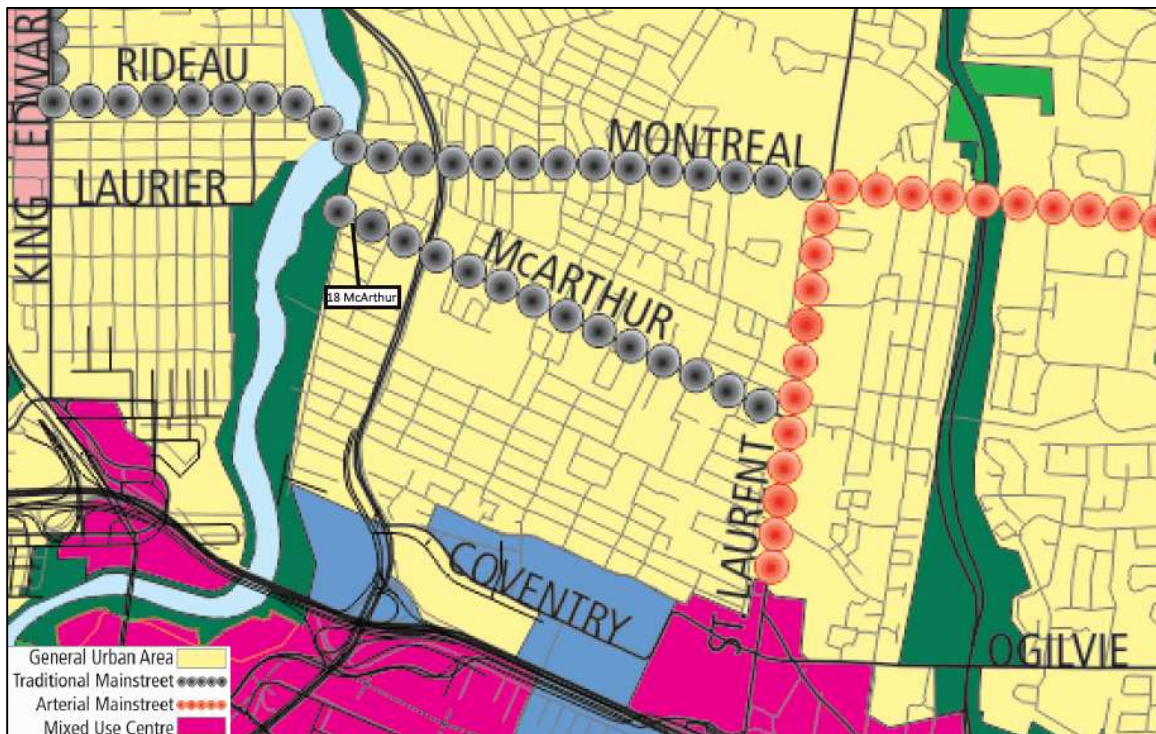
The proposed development will replace a surface parking area on a Traditional Mainstreet with a vibrant residential building that will provide opportunities for further contribution to the local economy by offering convenient residential population and future patrons for local commercial offerings, will enhance a renewing mainstreet and will further the transition of McArthur Avenue into an

appropriate Traditional Mainstreet character. In addition, small infill projects as is proposed in this case provide an opportunity for smaller development companies to bring unique projects to the marketplace.

#### 4.2 Conformity with the City of Ottawa Official Plan

The City of Ottawa Official Plan (“OP”) was adopted in 2003 and provides a vision for future growth of the city and a policy framework to guide future development. The OP recently underwent a 5-year review and Official Plan Amendment (OPA) No. 150 was approved by Council in December 2013. It is the policies of OPA 150 that will be analyzed with respect to the Subject Application.

##### Section 3.6.3 – Mainstreets



**Exhibit 'F'** - Schedule 'B' of the Official Plan, Urban Policy Plan

As noted on Schedule 'B' above (**Exhibit 'F'**), the Subject Property is designated as 'Traditional Mainstreet'. It is therefore subject to the policies of Section 3.6.3.

According to Section 3.6.3, mainstreets “offer significant opportunities for intensification through medium-density and mixed-use development along streets that are Transit Priority Corridors or are well-served by transit”. The policies of the designation aim to “encourage more dense and mixed-use development that supports, and is supported by, increased walking, cycling and transit use” while taking “into account the character of the street and adjacent areas.”

The OP distinguishes between two types of mainstreets – Arterial Mainstreets and Traditional Mainstreets. Traditional mainstreets are characterized by “a tightly-knit

*urban fabric, with buildings that are small-scale, with narrow frontages set close to the street.*” Traditional Mainstreets typically have a pleasing pedestrian environment and are supportive of transit. Ground floor uses are generally commercial with residential uses on upper floors. Parking is usually located on the street with minimal on-site parking located in the rear yard or underground. Intensification is expected to happen on uses such as aging parking lots.

The McArthur Avenue Traditional Mainstreet is unusual in that it is less pedestrian friendly than other Traditional Mainstreets. There is no parking permitted on the street so the flow of traffic is generally faster and many of the buildings have large surface parking areas (sometimes in the front yard). There is a wide variety of built form from high-rise office towers, to mid-rise residential buildings, to single detached homes, to large format retail with surface parking. There are 3 high-rise towers directly north of the property and a mid-rise residential building located on the north-east and south-east corners of the North River Road – McArthur Avenue intersection. Further east are mainly 2 storey dwellings and dwellings that have been converted to commercial buildings as well as low-rise apartment buildings and a proposed 8 storey mixed use building to be located further east on McArthur Avenue. As a result, the existing context is beginning to intensify and move towards the planned context for Traditional Mainstreets, particularly at the corners of significant roads, which will create a more pedestrian-friendly, mixed-use environment.

In terms of policies for new development, Policy 1 encourages development that is compact, human-scale, and pedestrian-oriented. The proposed development meets this policy by orienting the front façade of the building close to the street, and removing a surface parking lot in favour of a compact, 3-storey development to make efficient use of the land available.

As per Policy 5, a broad range of uses are permitted in the Traditional Mainstreet designation including *“retail and service commercial uses, offices, residential and institutional uses.”* Furthermore, *“uses may be mixed in individual buildings or occur side by side in separate buildings.”* The development proposes residential dwelling units across from office uses and among a nearby array of residential and commercial uses. As such, the development is in keeping with the uses permitted by the designation and offers residential occupancy for those wishing to live within a walkable distance from their place of work.

Policy 9 states that surface parking is not permitted between the building and the main street in order to *“avoid interruption of building continuity along the Traditional Mainstreet street frontage”* and *“minimize impacts on pedestrians”*. The proposed development has no automobile parking, and the bicycle parking unit is located to the rear of the building in order to avoid any interruption or negative impacts to the pedestrian environment.

Policy 10 encourages redevelopment and infill on Traditional Mainstreets in order to *“optimize the use of land through intensification, in a building format that encloses and defines the street edge with active frontages that provide direct pedestrian access to the sidewalk.”* The Subject Property is a prime example of a site well overdue for redevelopment and the proposal is an exemplary form of redevelopment that will make efficient use of the land in a manner that defines the street edge. The 1.7m access path is well-proportioned for comfortable pedestrian access to the building. Furthermore, the redevelopment of the site presents a unique opportunity for a small scale project that will add diversity to the neighbourhood and Traditional Mainstreet while setting a precedent for future infill projects of this scale to greater develop the neighbourhood in the direction of the Traditional Mainstreet context.

In accordance with Policy 12, mid-rise building heights between two and six storeys are permitted within the Traditional Mainstreet designation. The development proposes a total of 3 storeys and complies with this policy.

The proposed development is in keeping with the ultimate build-out scenario for this block of McArthur Avenue and the Traditional Mainstreet policies of the OP. The development will add a much wider opportunity for tenancy in the area, thereby allowing for more investment into the community.

### ***Section 2.5.1 – Designing Ottawa***

Section 2.5.1 seeks to ensure that new development incorporates a high quality of community and urban design and is compatible with surrounding land uses. This section outlines a number of design objectives to be considered during the development review process. These objectives are as follows:

1. *To enhance the sense of community by creating and maintaining places with their own distinct identity.*

The proposed development takes a parking lot which separates the built form on either side of it and promotes a development that adds continuity and definition to the streetscape. It emphasizes and reflects the scale and character of the neighbourhood. Further, it sets precedent for invigoration of this mainstreet in developing a more prominent identity.

2. *To define quality public and private spaces through development.*

As shown on the elevations, the proposed development consists of quality architecture with contemporary, high quality materials that will add visual interest to the pedestrian environment with high quality finishes that will enhance the streetscape.

- 3. To create places that are safe, accessible and are easy to get to, and move through.*

The design of the building incorporates prominent windows and doors to help create “eyes on the street”, thereby resulting in a safer environment along McArthur Avenue.

- 4. To ensure that new development respects the character of existing areas.*

The McArthur Avenue mainstreet is in the process of revitalizing with mixed use buildings beginning to replace older, less dense and less pedestrian friendly forms of development. The proposed development is in keeping with the character of the renewal and is respectful of the proposed mid-rise nature of the area as well as the existing low density residential development to the south.

- 5. To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.*

The proposed development incorporates residential units that, while intended for rental, can feasibly become condominiums based on changes in market interest. Further, if desired, the main floor units can become office space with minor conversion, in the process creating a mixed-use building; something promoted in Traditional Mainstreet areas.

- 6. To understand and respect natural processes and features in development design.*

Although the Subject Property is located in an urban area with a relatively flat topography, natural processes and features have been respected in the development design.

- 7. To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.*

Opportunities for energy efficiency will be explored through building construction.

Policy 2 considers Traditional Mainstreets as Design Priority Areas. Review by the Urban Design Review Panel is required for new development on Arterial Mainstreets unless that development is below 9 units for residential development and below 1,858m<sup>2</sup> total gross floor area for commercial development. Review by the Urban Design Review Panel is therefore not required for the proposed development.

### **Section 4.11 – Urban Design and Compatible Development (OPA 150)**

In addition to the design objectives of Section 2.5.1, a development proponent must also implement the policies of Section 4.11 into their proposal which make reference to objective criteria that can be used to evaluate compatibility of development applications including height, bulk or mass, scale relationship, building/lot relationships, and the distance between buildings. The OP aims to achieve compatibility through appropriate land use designations and the corresponding Zoning By-law which *“establishes more specific permitted use lists and development regulations within areas and on individual sites in a manner that achieves compatibility among proximate uses and built forms.”*

The OP states the following: *“At the scale of neighbourhoods or individual properties, issues such as noise, spillover of light, accommodation of parking and access, shadowing, and micro-climatic conditions are prominent considerations when assessing the relationships between new and existing development. Often, to arrive at compatibility of scale and use will demand a careful design response, one that appropriately addresses the impact generated by infill or intensification. Consequently, the issue of ‘context’ is a dominant theme of this Plan where it speaks to compatibility and design.”* In order to achieve these considerations, the following policies from Section 4.11 must be examined in the context of the subject application:

Policy 5 states that *“compatibility of new buildings with their surroundings will be achieved in part through the design of the portions of the structure adjacent to existing buildings and/or facing the public realm. Proponents of new development will demonstrate, at the time of application, how the design of their development fits with the existing desirable character and planned function of the surrounding area in the context of:*

- a. Setbacks, heights and transition;*
- b. Façade and roofline articulation;*
- c. Colours and materials;*
- d. Architectural elements, including windows, doors and projections;*
- e. Pre- and post-construction grades on site; and*
- f. Incorporating elements and details of common characteristics of the area.”*

The Subject Property is located in a revitalizing area that is characterized by low density uses being replaced by modestly higher density uses. This type of renewal requires an analysis of compatibility with the acknowledgement that a renewing neighbourhood will include intensification.

A number of exterior materials and colours are used to create visual interest. The large windows are located on the front façade with the smaller windows located in the rear so that the privacy of the residences to the south is maintained. The front façade features landscaping and quality architecture to draw the eye of pedestrians walking by. The flat roofline coheres with other buildings of similar

scale on the street. The massing of building, at 3 storeys, does not infringe on the single detached residential, but nonetheless sets a precedent for future intensification in the area to be of an adequate height for increasing density without overwhelming the surrounding area.

Policy 6 states that *“the City will require that all applications for new development:*

- a. Orient the principal façade and entrance(s) of main building(s) to the street. Where a building abuts more than one street, the building façade and entrances will be oriented in order of priority to the arterial, collector and local street;*
- b. Include windows on the building elevations that are visible from public spaces;*
- c. Use architectural elements, massing, and landscaping to accentuate main building entrances.”*

The principal façade and entrance to the building have been oriented to McArthur Avenue. There are numerous generously scaled windows on every storey of the front façade that are visible from public spaces.

Policy 8 states that *“developments that include loading facilities, service areas, mechanical equipment (including roof-top), vents and metering devices should incorporate these requirements into the building design in such a way that they cannot be seen from, and their operation does not impact, the public street, pedestrian and cycle pathways or adjacent ground-oriented residences. This may be achieved through:*

- a. Containment, with a preference to using the same architectural detail, style and materials as the proposed development; and/or*
- b. Screening (e.g. trees, landscaped berms, decorative walls and fences).”*

Mechanical equipment has been enclosed or located so that it is not seen from the public realm.

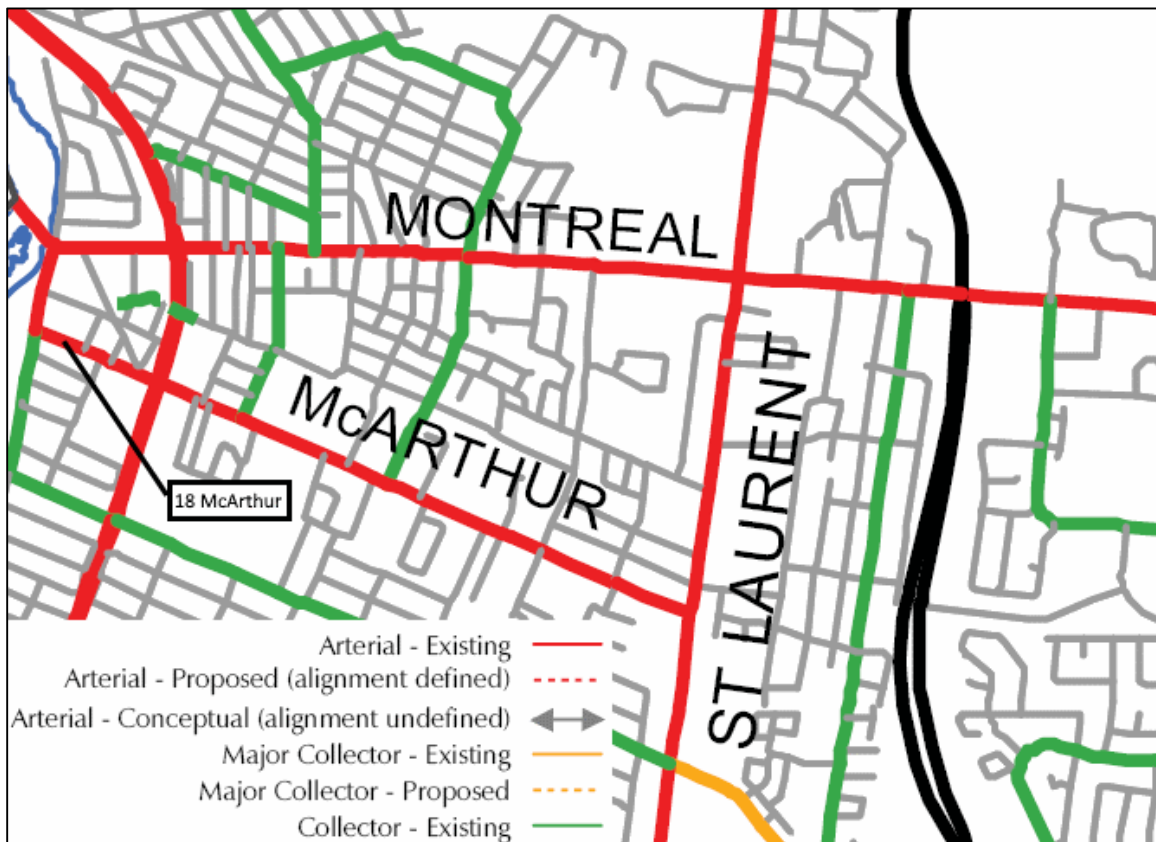
Policy 9 states that where no approved secondary plan exists with established massing criteria, *“the City will assess the appropriateness of the proposal relying upon its approved Design Guidelines, as applicable, and the following criteria:*

- a. Building height, massing and scale permitted by the planned function of adjacent properties as well as the character established by the prevailing pattern of abutting development and development that is across the street;*
- b. Prevailing patterns of rear and side yard setbacks, building separation and landscaped open spaces and outdoor amenity areas as established by existing zoning where that pattern is different from the existing pattern of development;*
- c. The need to provide a transition between areas of different development intensity and scale as set out in policy 12 of this section.”*

As the existing character and planned function of the area differ because the mainstreet is in a state of evolution, the building height, massing and scale proposed is exactly in line with what is permitted by the zoning by-law and is respectful of existing development that could have privacy or shadowing impacts. The development is further buffered by a 1.5m high fence and a minimum 1.2m wide landscape buffer on three sides.

Policy 11 states that *“transition refers to the integration of buildings that have greater height or massing than their surroundings. Transition is an important building design element to minimize conflicts when development that is higher or has greater massing is proposed abutting established or planned areas of low rise development. Proponents for developments that are taller in height than the existing or planned context or are adjacent to a public open space or street shall demonstrate that an effective transition in height and massing, that respects the surrounding planned context, such as a stepping down or variation in building form has been incorporated into the design”*. Policy 12 outlines the tools for achieving height and massing transitions. The neighbourhood is characterized by a wide variety of different height from high rise to single storey dwellings. The proposed mid-rise development incorporates transitioning tools to minimize conflicts.

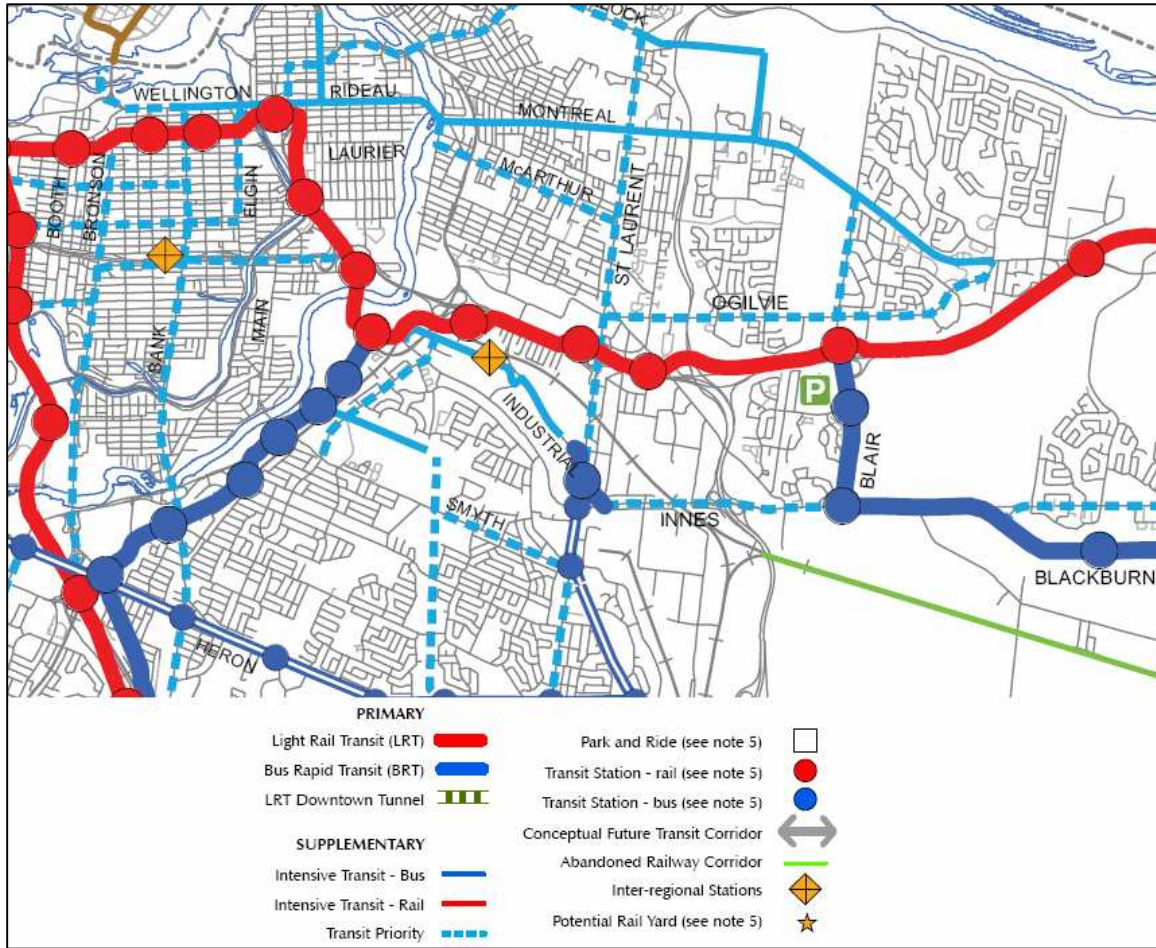
**Section 4.3 – Walking, Cycling, Transit, Roads and Parking Lots**



**Exhibit 'G'** - Schedule 'E' of the Official Plan, Urban Road Network

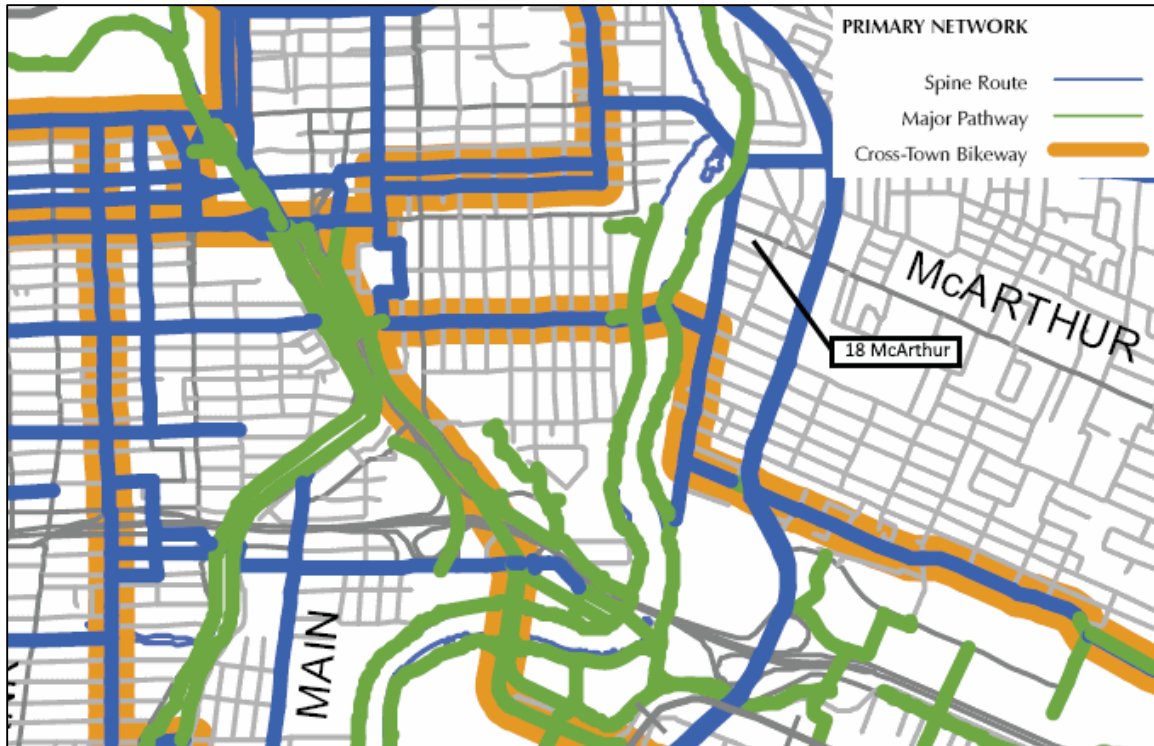


As per Schedule 'E' of the Official Plan, McArthur Avenue is designated as an Existing Arterial Road.



**Exhibit 'H'** - Schedule 'D' of the Official Plan, Rapid Transit and Transit Priority Network

As per Schedule 'D', McArthur Avenue is a designated transit priority corridor (see **Exhibit 'H'** above). As per Schedule 'C', The subject property is in close proximity to all major facets of the primary urban cycling network (see **Exhibit 'I'** below).



**Exhibit 'I'** - Schedule 'C' of the Official Plan, Urban Cycling Network

According to Section 4.3 of OPA 150, “the City, when reviewing development applications, will assess the adequacy of the transportation network to meet the needs of the proposed development.” In this case, the development is well served by roads, cycling routes, and transit. In particular, the site is in a position where residents can enjoy some of the most thoroughly developed parts of Ottawa’s public transit and active transportation networks. As a result, despite the lack of on-site parking, the transportation network is more than adequate to support the proposed development.

### 4.3 Conformity with the Urban Design Guidelines for Development along Traditional Mainstreets

The objectives of the Urban Design Guidelines for Development along Traditional Mainstreets is:

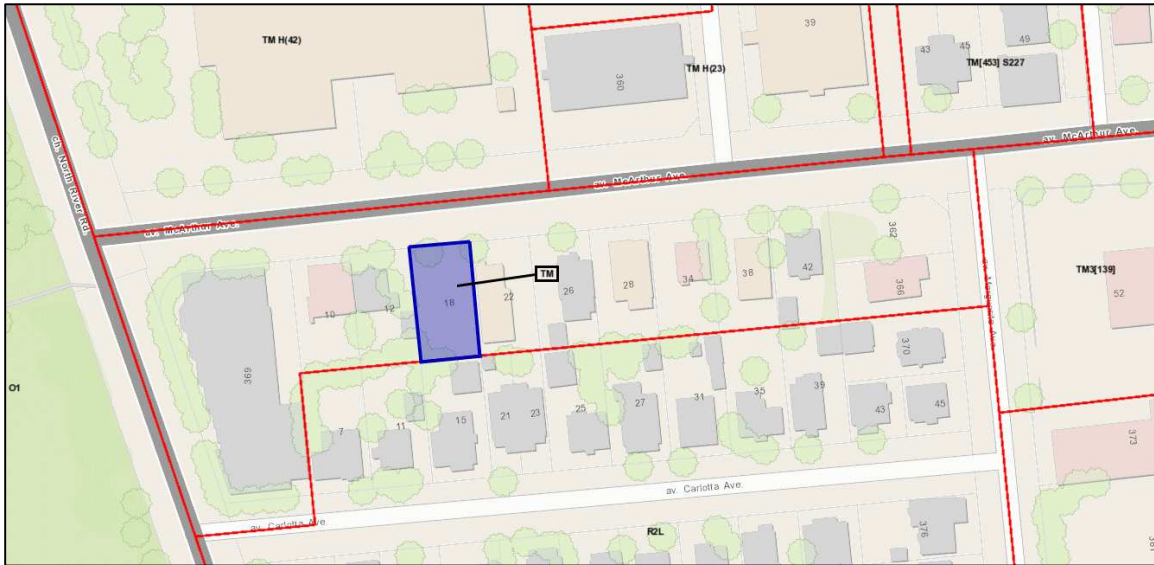
- *To promote development that will enhance and reinforce the recognized or planned scale and character of the streets*
- *To promote development that is compatible with, and complements its surroundings*
- *To achieve high-quality built form and strengthen building continuity along Traditional Mainstreets*
- *To foster compact, pedestrian-oriented development linked to street level amenities*

- *To accommodate a broad range of uses including retail, services, commercial uses, offices, residential and institutional uses where one can live, shop and access amenities*

In accordance with the guidelines, the proposed development:

- Incorporates 0m front yard setback to create a strong street edge.
- The existing concrete sidewalk will be continued in front of the building with entrances to the building from the sidewalk.
- The building is rich in architectural detail and will create a benchmark for the rhythm and pattern of McArthur as it redevelops. The 0m front yard and 3 storey height will match the low to mid-rise scale of development along this side of McArthur and provide a transition and buffer from the high-rise developments across the street to the low density residential to the south. The setback and height transitioning is compatible with the setbacks beginning at the corner of North River Road and McArthur Avenue and will blend nicely with existing and future forms of development.
- Clear windows and doors have been incorporated at the pedestrian level façade.
- The 3 storey height of the building helps it achieve a more human scale of development and ensures the building does not prevent sunlight from reaching the street and sidewalks.
- The development will promote pedestrian-oriented development in that the building occupies nearly the entire frontage, with bicycle parking being contained behind the building and accessed by a pathway.
- All utility equipment has been enclosed within the building and the garbage enclosure has been located to the rear of the building and screened from other uses.

#### 4.4 Conformity with the City of Ottawa Comprehensive Zoning By-law No. 2008-250



**Exhibit 'J'** - Zoning Map (GeoOttawa, Subject Property Shaded in blue)

As per **Exhibit 'J'** above, the subject site is zoned TM which is the Traditional Mainstreet Zone.

The purpose of the TM – Traditional Mainstreet Zone is to:

- 1) *“accommodate a broad range of uses including retail, service commercial, office, residential and institutional uses, including mixed-use buildings but excluding auto-related uses, in areas designated Traditional Mainstreet in the Official Plan;*
- 2) *foster and promote compact, mixed-use, pedestrian-oriented development that provide for access by foot, cycle, transit and automobile;*
- 3) *recognize the function of Business Improvement Areas as primary business or shopping areas; and*
- 4) *impose development standards that will ensure that street continuity, scale and character is maintained, and that the uses are compatible and complement surrounding land uses.”*

The proposed development is a sterling example of the proposed vision for the TM zone as it pertains to residential development. The proposed development is pedestrian oriented, prioritizes public and active transportation, ensures street continuity, and possesses a compatible scale and character with what is existing alongside an adaptability to future surrounding development.

As per Section 197, the following residential uses are permitted in this zone:

- **apartment dwelling, low rise**
- apartment dwelling, mid-high rise
- bed and breakfast
- converted dwelling
- dwelling units
- group home
- retirement home
- retirement home, converted
- rooming house
- rooming house
- rooming units

Apartment dwelling, low rise use as defined in the By-law are permitted by the TM zone.

The above uses are permitted subject to the following provisions:

<b>TABLE 197 – TM ZONE PROVISIONS</b>		
<b>I ZONING MECHANISMS</b>	<b>II PROVISIONS</b>	<b>III PROPOSED DEVELOPMENT</b>
<b>(a) Minimum lot area</b>	No minimum	407m <sup>2</sup>
<b>(b) Minimum lot width</b>	No minimum	14.6m
<b>(c) Maximum front yard setback</b>	2 m, subject to the provisions of subsection 197(4) below.	0m
<b>(d) Interior side yard setbacks</b>	(i) Maximum	3 metres between a non-residential use building or a mixed-use building and another non-residential use building or mixed-use building, except where a driveway is provided, in which case the setback must be a maximum of 6 metres where the driveway leads to a parking area of 20 or more spaces
	(ii) Minimum	The maximum setback provisions of row (d)(i) above do not apply to the following cases
		The interior side yard setback for the proposed residential

		<p>and the following minimum setbacks apply:</p> <p>(1) 3 metres for a non-residential use building or a mixed-use building abutting a residential zone, and</p> <p>(2) 1.2 metres for a residential use building</p> <p>All other cases – no minimum (maximum setback provisions of row (d)(i) apply in these cases)</p>	<p>use is above the minimum of 1.2 metres for a residential building. In particular, it has a setback of 1.3m on the west side and 2.7m (for pathway leading to bicycle parking) on the east side; no driveway is provided</p>
<b>(e) Minimum corner side yard setback</b>		3 m, except for any part of a building above 15 metres for which an additional 2 metre setback must be provided	N/A
<b>(f) Minimum rear yard setback</b>	(i) rear lot line abutting a residential zone	7.5 m	7.5 m
	(ii) rear lot line abutting a public laneway	4.5 m	N/A
	(iii) for residential use building	7.5 m	7.5 m
	(iv) other cases	No minimum	N/A
<b>(g) Building height</b>	(i) minimum	6.7 metres for a distance of 20 metres from the front lot line as set out under subsection 197(5) below	The building will exceed the minimum building height of 6.7m
		(1) 20 metres but not more than 6 storeys, except where	The proposed building height consists of 3

	(ii) maximum	otherwise shown on the zoning maps	storeys and is under 20m in height.
		(2) where the building height is greater than four storeys or 15 metres, at and above the fourth storey or 15 metres whichever is the lesser a building must be setback a minimum of 2 metres more than the provided setback from the front lot line as set out under subsection 197(5) below, and from the a corner side lot line	N/A
		(3) no part of a building on a lot with a rear lot line abutting an R1, R2, R3 or R4 Zone may project above a 45 degree angular plane measured at a height of 15 metres from a point 7.5 metres from the rear lot line, projecting upwards towards the front lot line (see illustration below). (By-law 2012-349)	N/A
<b>(h) Maximum floor space index</b>		No maximum	N/A
<b>(i) Minimum width of landscaped area</b>	(i) abutting a residential zone	3 m; may be reduced to one metre where a minimum 1.4 metre high opaque fence is provided	A minimum one metre width of landscaped area has been provided in the rear yard. A 1.5m high opaque fence has been provided along the rear lot line.
	(ii) in all other cases	No minimum, except that where a yard is provided and not used for required driveways, aisles, parking or loading spaces, the whole yard must be landscaped	Where a yard is not used for pathways and bicycle parking, the whole yard has been landscaped.
<b>(j) minimum width of landscaped area around a parking lot</b>		<i>see Section 110 – Landscaping Provisions for Parking Lots</i>	

The uses are further subject to the following applicable provisions:

Section 197(5): The front yard setback is from McArthur Avenue.

Section 197(6): “*Storage must be completely enclosed in a building*” – There is no storage located outside of an enclosed area.

Section 197(7): “*A maximum of one private approach may be provided for each property having a lot frontage of 45 metres or less*” – The Subject Property’s frontage is less than 45m and no private approach is being provided.

Section 197(8): “*Despite Section 107 – Aisle and Driveway Provisions for Parking Lots and Parking Garages, the minimum driveway width is 3 metres for parking lots with less than 20 parking spaces*” – No automobile parking area has been proposed.

Section 197(11): “*For other applicable provisions, see Part 2 – General Provisions, Part 3 – Special Use provisions, and Part 4 – Parking and Loading Provisions.*”

### **Part 2 – General Provisions**

N/A

### **Part 3 – Specific Use Provisions**

N/A

### **Part 4 – Parking, Queuing and Loading Provisions**

#### **Section 101 (Minimum Parking Space Rates)**

Section 101(1): “*Off-street motor vehicle parking must be provided for any land use at the rate set out in Table 101 below. In addition, visitor parking must be provided in accordance with Section 102.*” The Subject Property is located in Area B. Minimum parking rates are described below for reference, however as discussed previously, a minor variance application will be filed to request a relief from parking provisions.



**Table 101- MINIMUM PARKING SPACE RATES**

USES RELATED – RESIDENTIAL		
Area B on Schedule 1 other than MC Zone at Tunney’s Pasture (Inner City Area)		
<i>(b) Apartment Building, Mid – High Rise and Low Rise (Subject to By-law 2014-292)</i>	(i) within 600 metres of a rapid transit station(see Sections 101(4) and 101(5))	0.5 per dwelling unit
	(ii) other cases	0.5 per dwelling unit

The required parking for the proposed development, being 8 dwelling units, is 4 parking spaces. With this being considered, a minor variance is will be filed to remove the need for parking from this site. This is on the basis of the site’s aforementioned excellent accessibility to transit and active transportation opportunities. Ongoing studies are suggesting reduced parking standards for certain areas of the City.

**Section 102 (Minimum Visitor Parking)**

As per *Section 102*, this development does not require any visitor parking given that it has less than twelve dwelling units and is within Area B (see table below).

I Land Use		Minimum Number of Visitor Parking Spaces Required			
		II Area A on Schedule 1 (Central Area)	III Area B on Schedule 1 (Inner City)	IV Area C on Schedule 1 (Suburbs)	V Area D on Schedule 1 (Rural)
<b><i>(a) Apartment Building, Mid - High Rise; Low Rise (Subject to By-law 2014-292)</i></b>	(i) For first 12 dwelling units	None	None	0.2 per dwelling unit	
	(ii) For the next 300 dwelling units (By-law 2009-302)	1. West of the Rideau Canal- none  2. East of the Rideau Canal- 0.1 per dwelling unit	0.2 per dwelling unit		

**Section 109 (Location of Parking)**

Section 109(1): In accordance with this section, no parking has been provided in the front yard.

**Section 110 (Landscaping Provisions for Parking Lots)**

Section 110(2): As no parking space is proposed, a landscape buffer is not required.

Section 110(3): The garbage and recycling enclosure is located in the rear yard parking area, over 9m away from the lot line abutting McArthur Avenue. The garbage room is screened from view with an opaque screen of 2m and a canopy. The garbage area is located in the rear yard and is situated a distance of 3m or greater from any lot line.

**Section 111 (Bicycle Parking Space Rates and Provisions)**

Section 111(1): “Bicycle parking must be provided for the land uses and at the rate set out in Table 111A.”

TABLE 111A - BICYCLE PARKING SPACE RATES	
I LAND USE	II MINIMUM NUMBER OF SPACES REQUIRED
(b) apartment building, low rise; apartment building, mid-high rise; dwelling unit in the same building as a non-residential use; stacked dwelling without a garage or carport for each dwelling unit (Subject to By-law 2014-292)	0.50 per dwelling unit

The residential portion (8 units) requires 4 bicycle spaces. The bicycle parking area contains room for 16 bicycles – the increased capacity assists in accommodating for the lack of parking on-site.

**Section 137 (Amenity Area)**

Section 137(1): Amenity area must be provided for a residential use that is a permitted use in the zone in which it is located, in accordance with Table 137. As the proposed development consists of 8 units, an amenity area of at least 48m<sup>2</sup> is required. An amenity space of 80m<sup>2</sup> has been provided on the roof of the building along with an at grade amenity space area of 55m<sup>2</sup> in the rear yard. This therefore accommodates for both the total requirement for amenity space and the requirement for communal amenity space as per Table 137(3)

## **5.0 Plans and Technical Studies**

The background studies and plans that have been prepared in support of the completion of the development application for the subject property are as follows:

1. Site Servicing Plan
2. Grade Control and Drainage Plan
3. Stormwater Management Report
4. Site Servicing Study
5. Geotechnical Investigation
6. Erosion and Sediment Control Plan
7. Noise/Vibration Study
8. Site Plan
9. Landscape Plan/Tree Conservation Report
10. Survey Plan
11. Architectural Building Elevation Drawings
12. Design Brief
13. Phase I Environmental Site Assessment
14. Phase II Environmental Site Assessment

## **6.0 Summary and Conclusions**

1. The application for site plan control is consistent with the Provincial Policy Statement as it makes efficient use of the land and infrastructure in addition to offering a range of residential configurations at an appropriate density.
2. The application is consistent with the uses permitted on Traditional Mainstreets and is of a height and density that is envisioned by the OP and supportive of a transit priority corridor. The development supports the aim of the OP to encourage active modes of transportation.
3. The development constitutes good design in keeping with the urban design guidelines for Traditional Mainstreets.
4. The design, density and height are in keeping with the planned context of the McArthur Traditional Mainstreet while being respectful of and compatible with existing development. The development will provide a transition in density from north to south and will contribute to the revitalization of McArthur Avenue.
5. The proposed development is of a size that does not require review by the Urban Design Review Panel.
6. The development complies with all zoning provisions with the exception of the relief of parking requirements. Minor variances from this section of the by-law will be applied for. The required zoning compliance issues have been examined and meet the four tests under the Planning Act.

Based on the above noted rationale, the application for Site Plan Control is appropriate and represents sound land use planning.

Prepared by;

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