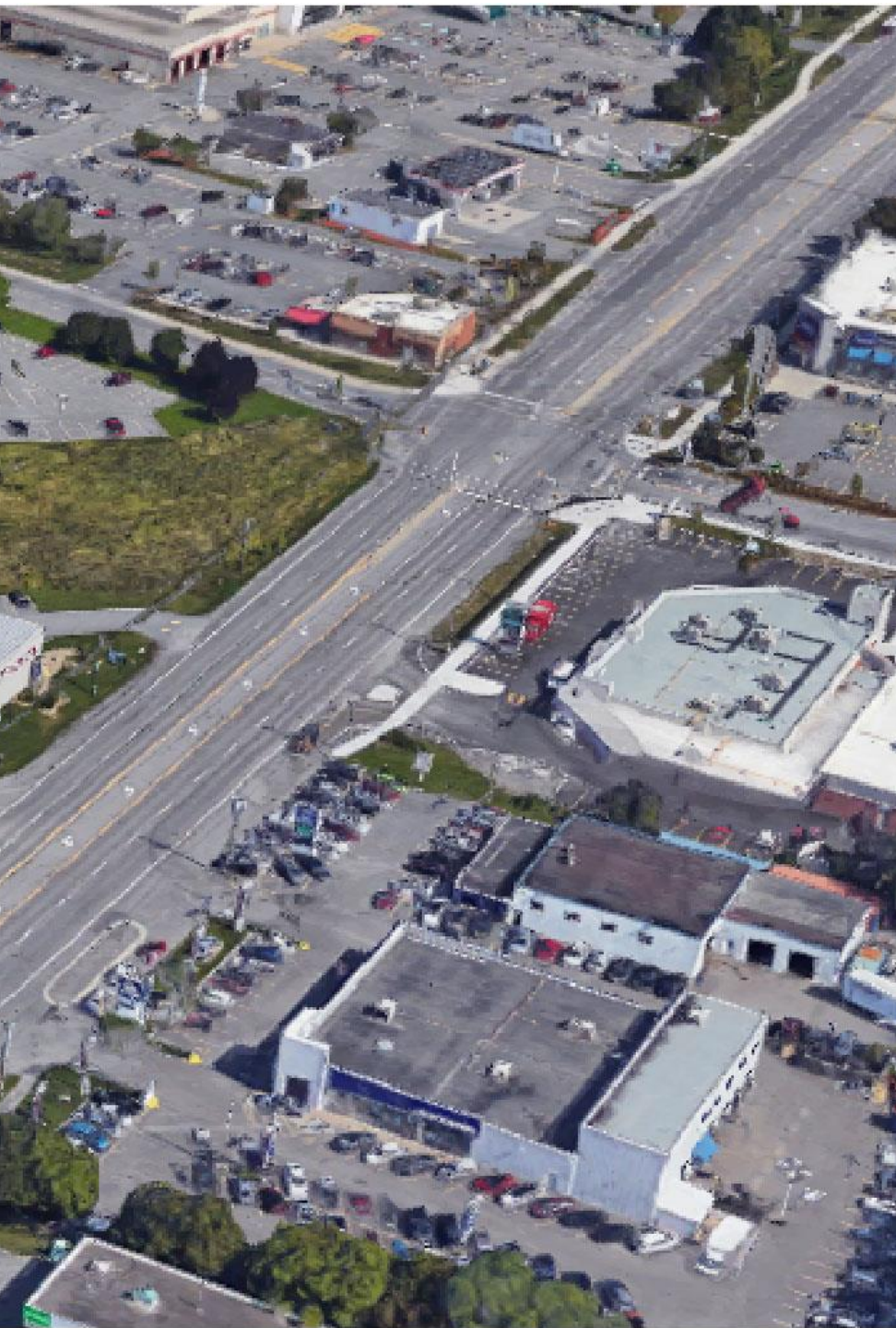


FOTENN

2165 ROBERTSON ROAD SITE PLAN CONTROL





Prepared for:



Huntington Properties.
1306 Wellington Street West, Suite
200
Ottawa, ON K1Y 3B2
huntingtonproperties.ca

Prepared by:



Fotenn Planning + Design
223 McLeod Street
Ottawa, ON K2P 0Z8
fotenn.com

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1.0 INTRODUCTION

Fotenn Consultants Inc., acting as agents for Robertson Road II Limited, on behalf of Huntington Properties, is pleased to submit the enclosed Site Plan Control application for the lands municipally known as 2165 Robertson Road in the City of Ottawa. The purpose of the applications is to permit a drive-through restaurant on the southern portion of the property and a retail facility with an accessory warehouse located in the same building on the northern, rear portion of the property. The property is currently occupied by surface parking and includes an outdoor storage area. The development represents commercial intensification on a site that is constrained by a 30-metre rail buffer to the rear (north) of the property and a hydro corridor buffer that bisects the parcel.

The subject lands are zoned Arterial Mainstreet [Exception 287]. Exception 287 requires approval through the Site Plan Control process for developments proposing more than one building in the zone, in order to ensure appropriate vehicular access and circulation. Additionally, as a non-residential building with a proposed Gross Floor Area of greater than 300 m², the Site Plan Control process is triggered per By-law 2014-256.

As illustrated on Figure 1, the subject property is located in Bells Corners, a community located inside Ottawa's Greenbelt, within the former City of Nepean. The subject property fronts onto Robertson Road, approximately 450 metres west of the intersection of Moodie Drive and Robertson Road. As is visible in Figure 1, the surrounding area is generally developed with low to medium-density commercial and office uses.



Figure 1 Subject Property in Regional Context

Robertson Road is an existing four-lane arterial road with a median turning lane and bike lanes in each direction. It is designated as an Arterial Mainstreet and functions as one of the two major transportation corridors in the community, connecting east to Ottawa's centre and west to Kanata. The context of the subject site is auto-oriented, with relatively long walking distances from the site to nearby amenities; the nearest commercial amenity space other than a Tim Horton's drive-through is approximately 300 metres walking distance.

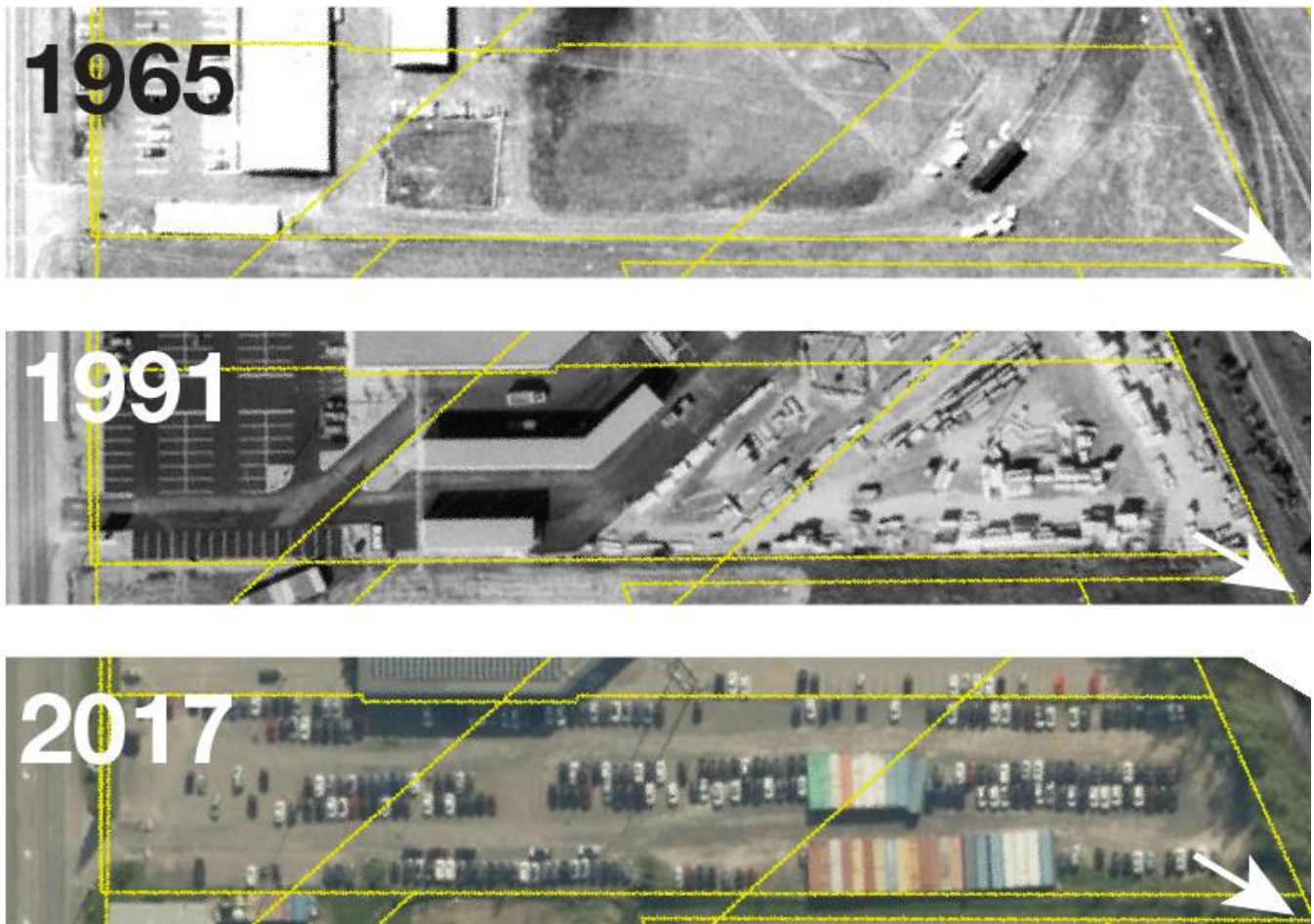


Figure 2 Buildings Shown in Historical Photos.

The property is currently occupied by surface parking. The site is diagonally bisected by a hydro corridor. To the north, the subject site abuts the non-active Carleton Place rail corridor, owned by the City of Ottawa. Overall, the subject property has approximately 44.24 metres of frontage along Robertson Road, and is approximately 1.1 hectares in area (11,391m²). The site is generally rectangular in shape with an angled rear lot line.

Historically, industrial uses occupied the site, as shown in Figure 2. As described in the Phase One Environmental Site Assessment prepared by Pinchin Ltd. and submitted with this application, two buildings associated with the Beaver Lumber yard were erected on the southern portion of the site sometime before 1965, and demolished post-1976. Prior to 1991 lumber storage buildings were erected within the Hydro Corridor, and these were demolished between 2007 and 2008. Given the presence of historical buildings on the site, the proposed project is eligible for Tax Increment Equivalent Grants (TIEG) under the Bell's Corners Community Improvement Plan (CIP).

3.0 PROPOSED DEVELOPMENT

The applicant proposes an infill development for the subject property that adds a retail building, and a drive-through use on the currently underutilised site. The proposed uses and Site Plan respect the Hydro Corridor and Rail Corridor easements while maximizing the utility of the site. The drive-through establishment is proposed to be at the front of the site, while the rear of the site will be occupied by a single building (Building B) containing an HVAC retail store and distribution use with accessory warehouse and offices.

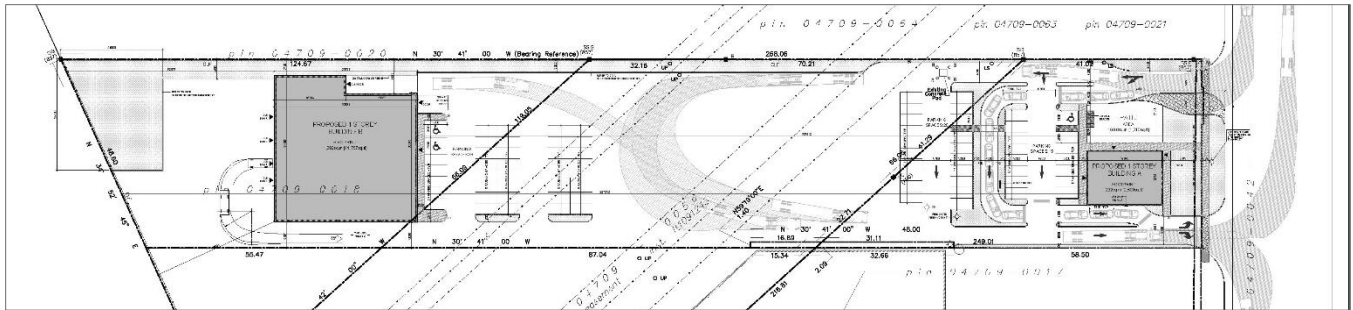


Figure 3 Snapshot of Site Plan

Vehicle circulation requirements have strongly shaped the site design. Vehicle entrance and egress from the site will be accommodated by one-way entrance and exit lanes at the east and west of the lot frontage. Access to the drive-through queuing lane and parking, located to the rear of the drive-through restaurant, is directly off these lanes. One screened loading space for semitrailer trucks and a loading space for smaller trucks is provided on the front façade of the proposed HVAC retail/distribution use, while four (4) spaces for smaller trucks are located to the rear of Building B on the site.

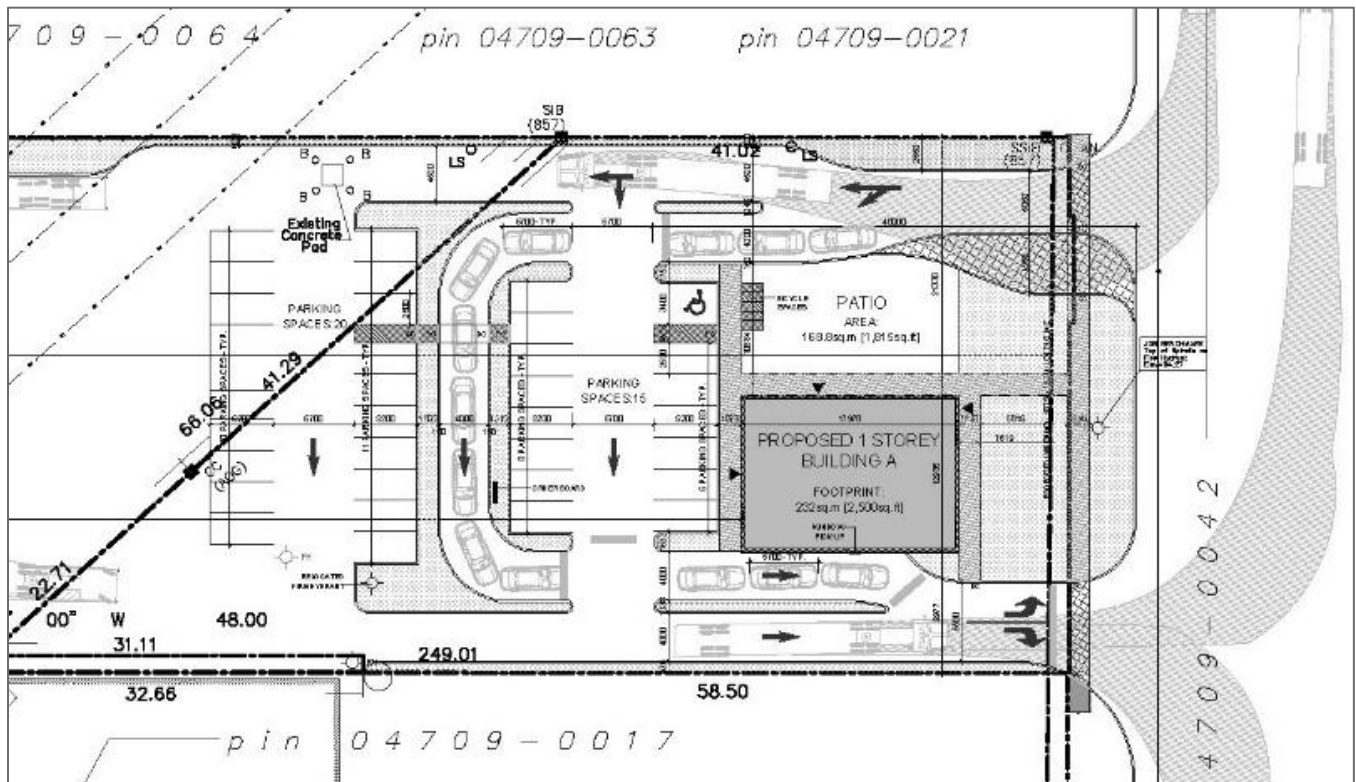


Figure 4 Snapshot of the Site Plan Showing the Drive-Through Restaurant

The drive-through facility (“Building A”) has a footprint of 232 square metres, two rear parking lots containing 15 and 21 associated parking spaces, a patio to the northeast of the building, and a 12-space queuing line. A 1.8 metre sidewalk and 5.8-metre landscaped area are located in front of the building, and direct sidewalk access is proposed to the restaurant’s front door. The 37.5-metre of Right of Way protection for Robertson Road required by Annex 1 of the Official Plan has been incorporated into the site plan through a 2.35-metre buffer.

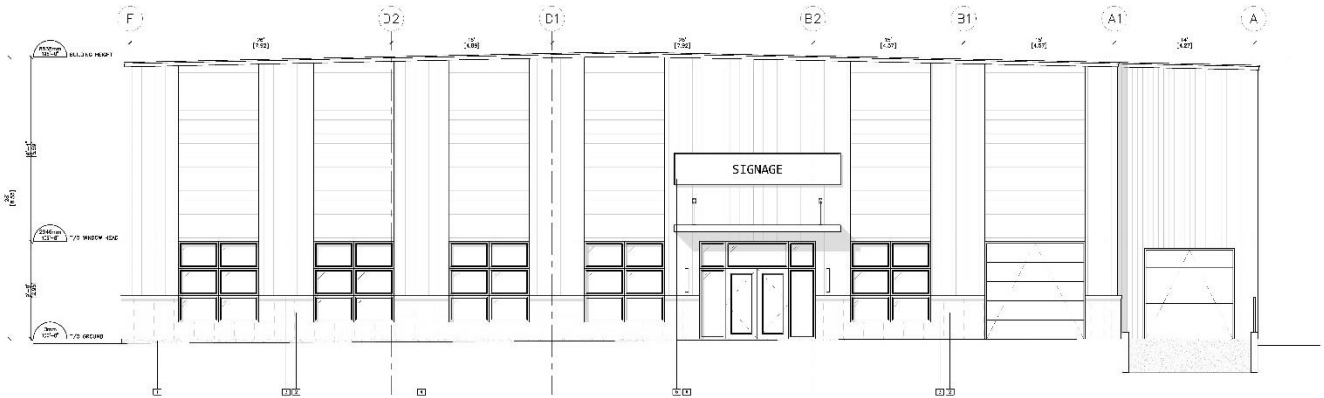


Figure 5 Elevation Drawing of the Front (East) Façade of the HVAC Retail/Distribution Use

A 30-metre wide Hydro Corridor bisects the site at an angle, as shown in Figure 3. Parking and manoeuvring room for semi-trailers is proposed for this space, maximizing the utility of a significant section of the site which cannot be built upon. The proposed HVAC retail facility extends from the edge of the Hydro Corridor to the 30-metre rail corridor buffer, presenting the largest building possible with a rectangular footprint. The HVAC distribution facility itself is to consist of a showroom with accessory salesperson offices and a storage area.

Development plans for the proposed site were initiated based on the existence of the Bell’s Corners CIP, approved by Ottawa City Council, which offers Tax Incentive Equivalent Grants (TIEG) for redevelopments that encourage the creation or expansion of industrial, commercial and office uses within the community. The TIEG is a critical component of the proposed development’s financing, and CIP application will be submitted concurrently with the Site Plan Control process. The subject site’s location relative to the CIP area is shown in Figure 6.

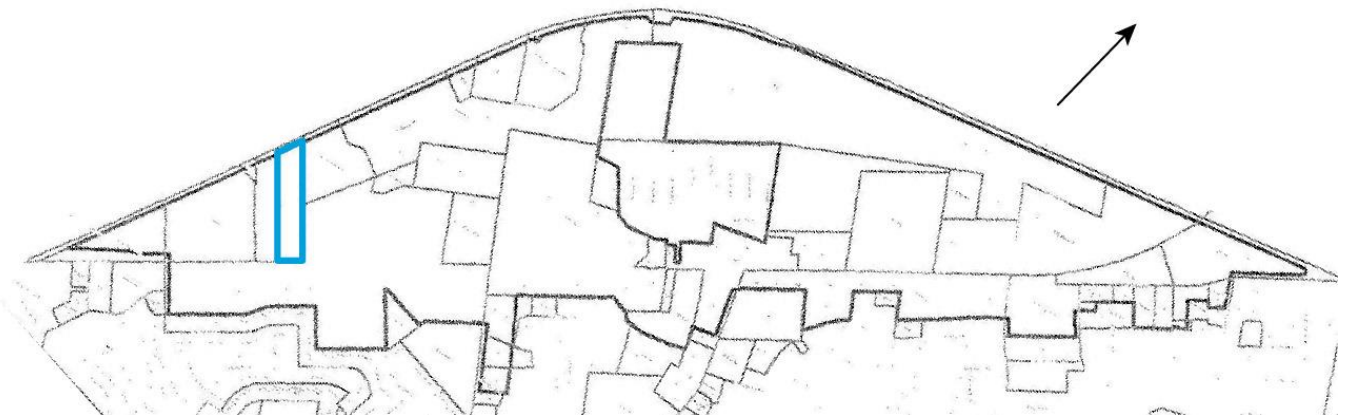


Figure 6 Subject Site (Blue) in Relation to Bells Corners CIP Boundaries

4.0 POLICY + REGULATORY FRAMEWORK

4.1 Provincial Policy Statement (PPS)

The Provincial Policy Statement (PPS) was issued under Section 3 of the Planning Act in April 2014. It provides direction on matters of provincial interest related to land use planning and development. The Act requires that decisions affecting planning matters “shall be consistent with” policy statements issued under the Act.

The proposal meets the applicable policies of the PPS:

- / Promotes an efficient development and land use pattern which sustains the financial well-being of the Province and municipalities over the long term;
- / Accommodates an appropriate range and mix of uses;
- / Promotes cost-effective development patterns and standards to minimize land consumption and servicing costs;
- / Contributes to a mix of land uses which efficiently use land and resources;
- / Proposes a land use that is appropriate for, and efficiently uses, the infrastructure and public service facilities which are planned or available, and avoids the need for their unjustified expansion;
- / Part of a land use pattern within a settlement area that supports a range of uses and capitalizes on an opportunity for intensification and redevelopment in an area with suitable existing infrastructure and public service facilities;
- / Provides opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and takes into account the needs of existing and future businesses;
- / Intensifies employment uses within a designated employment area in proximity to a major goods movement corridor through the redevelopment of underutilized land; and,
- / Supports long-term economic prosperity through the redevelopment of a brownfield site.

The Site Plan application is consistent with the policies of the PPS.

4.2 City of Ottawa Official Plan (2003, as amended)

In 2013, the City of Ottawa reviewed its Official Plan which resulted in numerous changes to policy references and to land use designations. Ottawa Council adopted Official Plan Amendment (OPA) 150 to implement the changes in December 2013, which was subsequently approved by the Ministry of Municipal Affairs and Housing (MMAH) and then appealed by several parties. Settlement of several appeals in 2018 has brought into force most of the policies which apply to the proposed development. For the purposes of this Planning Rationale, the policies of the City of Ottawa Official Plan have been reviewed and analyzed for the proposed development.

Table 1 of Annex 1 of the Official Plan protects a 37.5 metre right-of-way along Robertson Road.

4.2.1 Section 2 – Managing Growth

Section 2 of the Official Plan sets out the City’s strategic directions for managing growth. Section 2.1, “Patterns of Growth”, projects a growth of 49,000 jobs inside the Greenbelt between 2011 and 2031, to a total of 547,000 jobs. Because many of these jobs require strategic locations and larger lot sizes, the Official Plan protects an adequate supply of suitable employment land to help diversify the local economy and ensure the future economic prosperity of Ottawa. These protected areas are shown in Figure 8.



Figure 7 Subject Property in Relation to 600 m Radius of Future Bell's Corners Rapid Transit Station

Section 2.2.2, “Managing Growth,” directs that development should be compact and efficiently located to support investment in infrastructure and alternative transportation modes and to protect agricultural land and natural environment areas. Section 2.2.3 encourages intensification of Urban Employment Areas, defined as the development of a property that results in a net increase in jobs or gross floor area, including by infilling of underutilised land (Policy 2, Section 2.2.2).

One of the underlying objectives articulated in Section 2 is to encourage a shift from automobile-oriented uses to pedestrian, cyclist and transit-oriented communities. This goal is balanced against the significant economic value of employment uses which may be dependent on automobiles/trucks. As per Section 2.2.2 Policy 4b, the intent of the Official Plan is to guide the development of Arterial Mainstreets towards denser and more urban forms, recognizing that the transformation of these areas will be achieved in phases. The development is within 600 metres of a future rapid transit station, as shown in Figure 7.

The proposed development meets the objectives of Section 2 of the Official Plan by creating jobs through employment intensification.

4.2.2 Section 3 – Land Use Designations

Schedule ‘B’ of the Official Plan designates the subject site as Arterial Mainstreet. The site is adjacent to a significant Urban Employment Land area, as shown in dark blue on Figure 8. Both Arterial Mainstreets and Urban Employment lands serve important structural functions in the Official Plan. Arterial Mainstreets, a stand-alone designation, were generally developed after 1945 and typically present an urban fabric of larger lots, larger buildings, varied setbacks, lower densities and a more automobile-oriented environment, often within a divided cross section of four or more lanes. On Arterial Mainstreets, development is meant to occur in a way that facilitates the gradual transition to a more urban pattern of land use. This means that, in general over time, higher density employment and residential uses should be introduced, where appropriate.

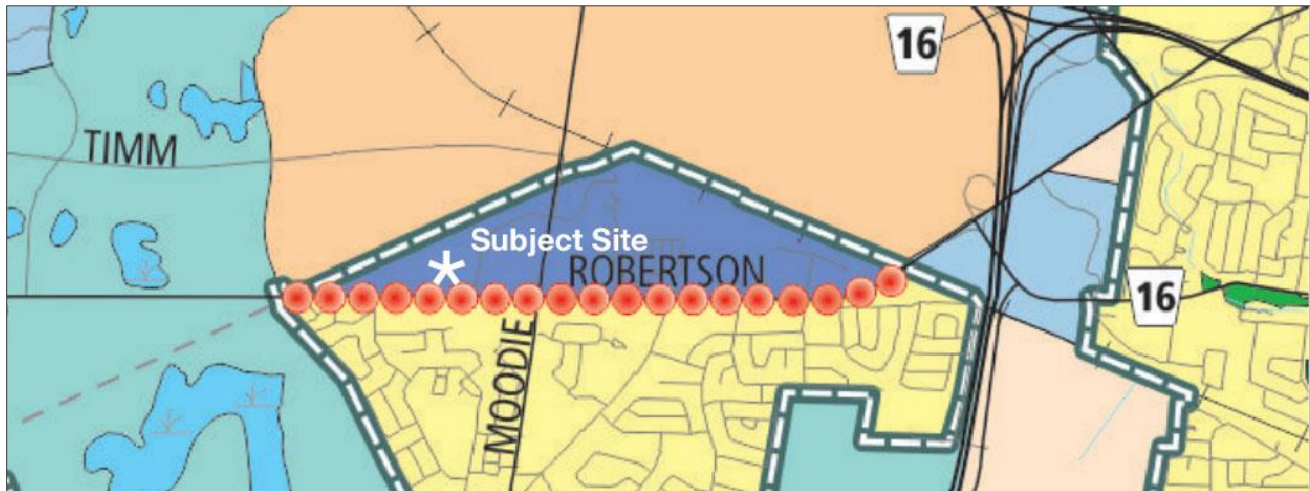


Figure 8 Extract of City of Ottawa's Official Plan, Schedule B (Urban Policy)

Urban Employment Areas are larger areas of land, located in proximity to major transportation facilities including arterial roads, that are reserved primarily for places of business and economic activity. Urban Employment Areas not located in proximity to highway interchanges are expected to provide employment uses that serve the local population; complementary uses to meet the day-to-day needs of employees, such as service commercial, are also appropriate in the designation.

The proposed development meets the intent of the “Arterial Mainstreet” designation by infilling and creating more compact development along the Robertson Road mainstreet. The proposed development also responds to and complements the policies of the surrounding “Urban Employment Area” designation.

4.2.3 Urban Design and Compatibility Policies

Section 2.5, “Creating Liveable Communities” defines compatibility as development that, while it may not be the same as existing buildings in the vicinity, enhances and coexists with the existing built form and landscape without causing undue adverse impact to neighbouring properties. New development is to be evaluated on the basis of Design objectives set out in Sections 2.5.1 and 4.11, both titled “Urban Design and Compatibility”.

The subject site intersects with the Robertson Arterial Mainstreet Design Priority Area. In these areas, all private developments adjacent to the public realm will be reviewed for their contribution to an enhanced pedestrian environment and their response to the distinct character and unique opportunity of the area. However, given the size of the proposed development, it is exempt from review by the Urban Design Review Panel. The proposed development meets the applicable Objectives set out in Section 2.5.1 of the Official Plan:

- / Enhances the sense of community by proposing a use that is compatible with surrounding existing and proposed land uses;
- / Defines quality public and private spaces through development by establishing a retail/service use near the street edge and delineating the surrounding site with landscaping;
- / Creates places that are safe, accessible and are easy to get to and move through by proposing curbing and drive aisles to encourage rational circulation patterns;

-
- / Ensures that new development respects the character of existing areas by designing the two proposed buildings to integrate into the existing and planned built environment;
 - / Considers adaptability and diversity by infilling on currently underutilized land to support an increased number of jobs and an expanded range of employment options; and
 - / Understands and respects natural processes and features by incorporating a drainage swale and stormwater management facility into the site.

Policy 2 of Section 4.11 establishes criteria for evaluating the urban design and compatibility of new developments. The proposed development meets the requirements as follows:

- / **Traffic:** As confirmed by the Transportation Impact Assessment Strategy Report prepared by Parsons, the site-generated demand will not require a change of concept in the adjacent road and transit network concepts, and the Site Plan is recommended from a transportation perspective.

Vehicular Access: The proposal includes curbing and drive aisles to ensure rational vehicular circulation and access. Ingress and egress to and from the drive-through is provided via a 'U' shaped driveway with one-way access on east and west sides of the site; the retail facility on the northern portion of the site will share these drive aisles for loading and parking access.

- / **Parking Requirements:** Parking has been provided in accordance with the provisions of the Zoning By-law.

- / **Outdoor Amenity Areas:** No impacts on outdoor amenity areas are anticipated as a result of the proposed development.

- / **Loading Areas, Service Areas, and Outdoor Storage:** All outdoor storage and loading areas both within and external to the parking lots will be enclosed or located in side/rear-yards and screened to a height of 2.0 metres, per the requirements of the Zoning By-law.

Lighting: Lighting for the site has been designed in accordance with City of Ottawa guidelines, using features that meet the criteria for Full Cut-Off Classification, and with a maximum spillage onto adjacent properties of approximately 0.5 fc.

- / **Noise and Air Quality:** Noise and air-quality impacts on adjacent uses, which are primarily commercial and industrial, are not anticipated from the development.

- / **Sunlight:** No shadowing impacts are anticipated from the development

- / **Microclimate:** No microclimate impacts are anticipated from the development.

- / **Supporting Neighbourhood Services:** The proposed drive-through retail establishment contributes to neighbourhood services at an appropriate scale, while the proposed office/warehouse use will expand the range of employment and economic activities in the community.

The proposed development is compatible with existing development and achieves the urban design goals of the Official Plan as set out in Sections 2.5.1 and 4.11.

4.3 Urban Design Guidelines

4.3.1 Urban Design Guidelines for Drive-Through Facilities

The Urban Design Guidelines for Drive-Through Facilities were approved by City of Ottawa Council on May 24, 2006. The purpose of the guidelines is to provide urban design guidance at the planning application stage in order to assess, promote and achieve appropriate development of drive-through facilities. The proposed development meets the following applicable guidelines:

- / Provides ample landscaping to enhance the streetscape and define the street edge;
- / Locates public amenities close to the building entrances through a patio to building's north;
- / Provides the main customer entrance to the drive-through facility through a front-facing door with direct access to the sidewalk;
- / Locates the stacking lane and surface parking at the side and to the rear of the drive-through building, with stacking lanes delineated using decorative pavement and pervious islands;
- / Buildings include defined spaces to accommodate signs that respect building scale, architectural features, signage uniformity and established streetscape design objectives; and
- / All garbage storage areas, outdoor storage and utility equipment are enclosed and/or screened from view.

4.3.2 Urban Design Guidelines for Arterial Mainstreets

Approved by Council in May 2006, the Urban Design Guidelines for Development along Arterial Mainstreets provides urban design guidance at the planning application stage in order to assess, promote and achieve appropriate development along Arterial Mainstreets. The proposed development meets the following applicable guidelines:

- / Three hardy trees are to be planted in the landscape area at the front of the lot to ensure healthy tree growth;
- / The new development and built form is generally compatible with the existing physical character while gradually introducing a more active and defined street frontage;
- / The front façade of the drive-through facility faces the public street;
- / The lighting has been designed to minimize glare and light spilling onto surrounding uses.

4.4 City of Ottawa Comprehensive Zoning By-law

The subject site is zoned Arterial Mainstreet, Exception 287 – AM[287]. The intent of the AM zone is to accommodate a broad range of uses including retail, service commercial, offices, residential and institutional uses in mixed-use buildings or side by side in separate buildings and to provide development standards that will promote intensification while ensuring compatibility with the surrounding uses. The proposed uses of drive-through facility, office, restaurant and retail store are permitted in the AM Zone as per Section 185 (1)(b).

A site-specific exception, Exception 287, applies to the site, the specific text of which indicates the zoning is IP[287] – Business Park Industrial Zone Exception 287. This is in error and City staff have confirmed that the correct zoning is Arterial Mainstreet Exception 287. Therefore, only the following provisions apply to the site:

- / To be considered as one lot for zoning and parking purposes
- / Only one building permitted in the ~~IP[287]~~ AM[287] zone unless site plan control approval shows additional building will not detrimentally affect vehicular access to the lot or vehicular circulation on the lot will be properly integrated with other buildings on the lot. [strikethrough added, inserted text in grey];
- / “Light industrial,” “printing plant” and “warehouse” are listed as additional permitted uses.

Section 55 of the Zoning By-law governs accessory uses. Accessory uses are permitted in any zone where they exist to aid and contribute to the principal use. In the case of the proposed development, an office use is permitted in the parent Zone, and a “warehouse” use is permitted by Exception 287. Both the warehouse and office uses are accessory to the retail use and will be located in the same building.

The table below evaluates the proposed development against the AM zoning provisions as they apply to the site:

Performance Standard	Requirement	Proposed
Lot Area	no minimum	11,392 m ²
Lot Width	no minimum	44.24 m
Front Yard Setback	no minimum	7.62 m
Interior Side Yard Setback	no minimum	varies/min 3.73 m
Rear Yard Setback	no minimum	varies/min 30 m
Building Height (maximum)	30 metres or 9 storeys	max 8.4 m
Width of Landscaped Area (minimum)	15% of any parking lot in addition to 3 m buffer abutting public street and 1.5 m buffer around other sides of parking lots between 10 and 100 spaces	Total landscaped area = 85% of parking area 1.5 m (min) buffers
Parking Space (minimum) (Area C, Table 101)	Restaurant (10/100m² GFA)	80% of 23 spaces = 18 spaces
	Retail (3.4/100m² GFA)	37 spaces
Disabled Parking Spaces (By-law 2017-301, Part C)	4%*36 spaces = 1.4 (restaurant); 4%*40 = 1.6 (store) Equal amount type A (5.2x3.4 m + 1.5 m access aisle) + B (5.2x2.4 m + 1.5 m access aisle)	Restaurant: 1 type A Store: 2 type B
Bicycle Parking (Table 111A)	Restaurant: 232 m ² @ 1/250 m ² = 0.92 = 1 space Retail store: 1092 m ² @ 1/250 m ² = 4.36 = 4 spaces Minimum 0.6 x 1.8 m; must be conveniently located	5 spaces on restaurant patio
Drive-through queuing (Table 112)	For restaurant: 7 queuing spaces before/at order board and a minimum total of 11 spaces	12 total
Queuing spaces (dimensions)	3 m wide x 5.7 m long	3 x 5.7 m (min)
Loading Space Rates	None for a retail use of less than 2,000 m ² GFA	6
Outdoor storage	Permitted, must be interior side or rear yard completely enclosed from street	
Outdoor loading and refuse collection areas within a parking lot	min 9 m setback from lot line abutting street min 3 m setback from any other lot line screened by min 2 m tall opaque screen	2 m opaque screen (front loading space)

5.0 SUPPORTING STUDIES

5.1 Site Servicing Study and Stormwater Management Report

A Site Servicing Study and Stormwater Management Report has been prepared for the subject site by DSEL Ltd., dated December 2018. This study found that the existing municipal water and sanitary networks have

sufficient capacity to service the proposed development. A treatment train including landscaped areas, enhanced grass swales and a modified constructed wetland are proposed to manage quality and flow rates of stormwater runoff to City of Ottawa and RVCA standards.

5.2 Transportation Impact Assessment

A Transportation Impact Assessment was undertaken by Parsons. The Transportation Strategy Report for 2165 Robertson Road, dated December 17, 2018, confirms that site-generated demand will not require a change of concept in adjacent road and transit network concepts. Based on the findings of the study, the Site Plan is recommended from a transportation perspective.

5.3 Landscape Plan and Tree Conservation Report

A coordinated landscape plan and tree conservation report has been prepared by James B. Lennox and Associates Landscape Architects, dated December 21, 2018. All trees on adjacent properties are proposed to be retained, while existing trees on the site itself are proposed to be removed. New soft and hard landscaping is proposed across the site's frontage, and three new trees are proposed. Landscaped buffers and islands increase the landscaped parking lot area, while proposed stormwater management facilities will also be landscaped.

5.4 Phase I ESA

A Phase I Environmental Site Assessment (Phase I ESA) was conducted for this Site by Pinchin. The ESA identified no ongoing or historical potentially contaminating activities (PCAs) within the study site. Six off-site PCAs were identified but none were considered to result in areas of potential environmental concern. Accordingly, it is Pinchin's opinion that the subject site is suitable for the filing of a Site Plan Approval application based on the findings of the Phase I ESA only.

6.0 CONCLUSION

It is our professional opinion that the proposed Site Plan Control application to permit a drive-through restaurant and a retail store on the subject site is appropriate and represents good planning:

- / The application meets the policies of the Provincial Policy Statement;
- / The application meets the policies of the Arterial Mainstreet designation in the City of Ottawa Official Plan;
- / The primary proposed uses - restaurant and retail - are permitted in the parent AM Zone of the City of Ottawa Zoning By-law (2008-250), and are appropriate in similar contexts across the city;
- / Additionally, drive-through facilities are permitted on Arterial Mainstreets and in the AM Zone, indicating that the vehicular traffic and circulation functioning for the drive-through use is appropriate for the site;
- / The proposed accessory office use is permitted in the parent AM zone, while the proposed accessory storage/warehouse use is permitted by site-specific exception 287;
- / The proposed site plan meets the performance standards contained in the City of Ottawa Zoning By-law (2008-250);
- / The proposed Site Plan responds to applicable guidelines from the City of Ottawa Urban Design Guidelines for Arterial Mainstreets and the Urban Design Guidelines for Drive-Through Facilities; and
- / The associated studies and plans, including site servicing, geotechnical, grading and drainage, and Environmental Site Assessment, indicate that the proposed development is feasible and desirable from a technical perspective.

Sincerely,



Matthew McElligott, MCIP RPP
Senior Planner



Bria Aird, M.Pl.
Planner