

FOTENN

2190 HALIFAX DRIVE



July 30, 2019

Planning Rationale

Zoning By-law
Amendment and Site
Plan Control



Prepared for:



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July 30, 2019

Fotenn Planning + Design (“Fotenn”) has been retained by Urbandale Corporation (“Urbandale”) to assist in the preparation and submission of a Zoning By-law Amendment, and Site Plan Control application for the property municipally known as 2190 Halifax Drive in the City of Ottawa (“the subject properties”). Urbandale proposes to construct a 17-storey high-rise apartment building fronting onto Walkley Road while retaining two (2) existing 12-storey apartment buildings and two (2) clusters of townhouses presently existing on the subject property.

1.1 Purpose of the Amendments / Applications

It is Fotenn’s opinion that an Official Plan Amendment is not required to support the greater building height. Fotenn considered the current Official Plan (OPA no.76), Official Plan amendment no. 150 and the emerging policy direction from the appeals and settlement of OPA no.150 and consider the policies sufficient to support a 17-storey residential building.

The subject properties are currently zoned as Residential Fifth Density Subzone B (R5B H39) with a maximum height of 39 metres. A site-specific Zoning By-law Amendment is being requested to permit a greater building height, approximately 56.5 metres. The Zoning By-law Amendment application proposes to rezone the subject properties to the Residential Fifth Density Subzone B Zone, Special Exception Zone (R5B [XXXX]) to permit the greater height and amend other zone provisions including landscaped area and buffers.

A Site Plan Control application has been submitted concurrently to allow the development of the 17-storey rental apartment building.

1.2 Site Context

1.2.1 Subject Properties

The lands at 2190 Halifax Drive properties are located on the north side of Walkley Road, on the northwest corner of Halifax Drive and Walkley Road intersection, in the Elmvale Acres community. The property has approximately 226 metres of frontage on Walkley Road and 390 metres of frontage along Halifax Drive. The subject property has a total area of approximately 4.3 hectares (10.6 acres). The lands are currently developed with two (2) 12-storey high-rise building and two (2) clusters of townhouses. The site also contains existing above and below grade parking with existing driveway access onto Walkley Road and Halifax Drive.

The subject property is located along the Bus Rapid Transit route along Walkley Road and with an existing bus stop abutting the property near the southwest portion of the lands. The following bus routes service the property: #112 (Elmvale Billings Bridge), 644 (Canterbury H.S Greenboro) and 649 (Hilcrest H.S. Greenboro).

1.2.2 Surrounding Context

The area south of Walkley Road is designated as an urban employment area on Schedule B of the Official Plan and is currently developed as a business park. East of Halifax Road is a low-density and low-rise residential neighbourhood. Immediately west of the subject property is Canterbury High School and sports fields.

Figure 1 illustrates the subject properties in the local context.



Figure 1: Subject Properties and Surrounding Context

1.2.3 Road Network

The subject property is located on the northwest intersection of Walkley Road and Halifax Drive. Walkley Road is designated an Arterial Road and Halifax Road is designated a Collector Road on Official Plan Schedule E (Urban Road Network). Arterial Roads are designed to carry large volumes of traffic over the longest distances.

Walkley Road is identified as a Bus Rapid Transit (BRT) with grade separated crossings. Bus Transit Stations are shown at the intersection of Walkley Road and St. Laurent Boulevard and Conroy Road. The site is located approximately 647 metres and 510 metres from both stations. Halifax Road currently accommodates contains a local bus services for bus routes 48 (Hurdman Elmvale), 644 (Canterbury H.S Greenboro) and 649 (Hilcrest H.S. Greenboro). These bus routes provide service to the transit way and future Light Rail Transit (LRT) stations.

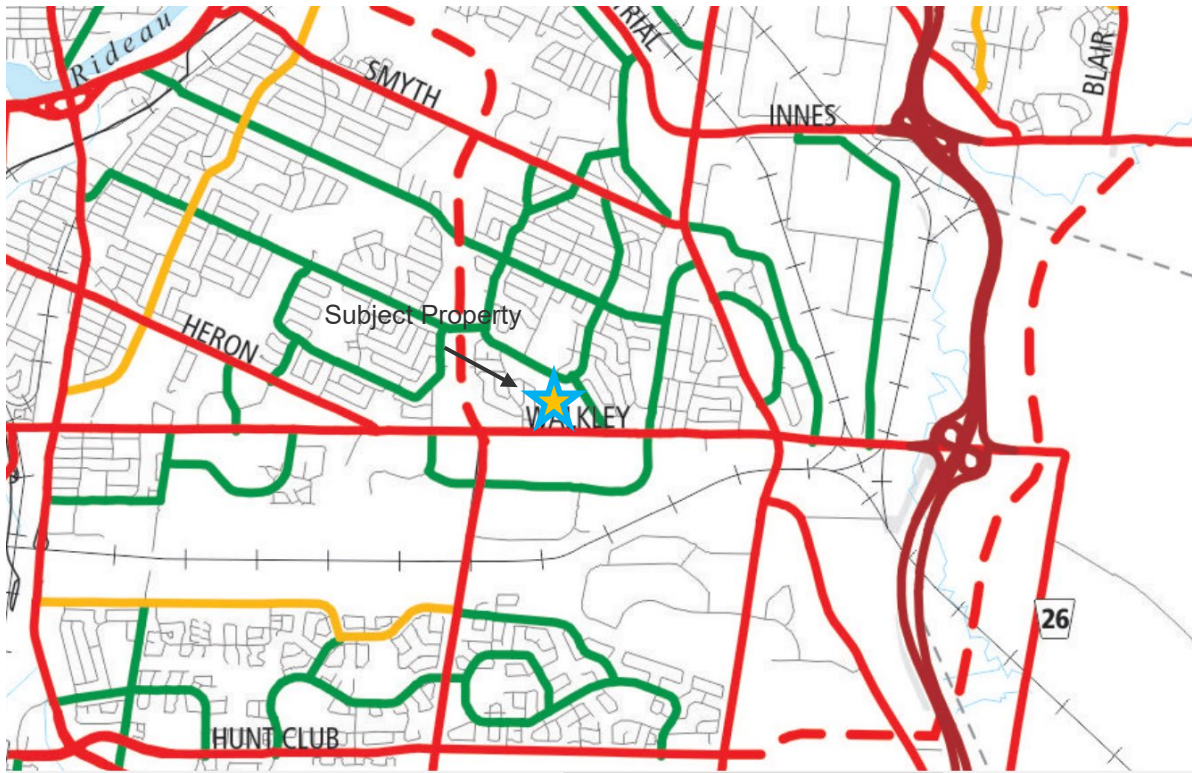


Figure 2: Extract from Official Plan Schedule E (Urban Road Network)

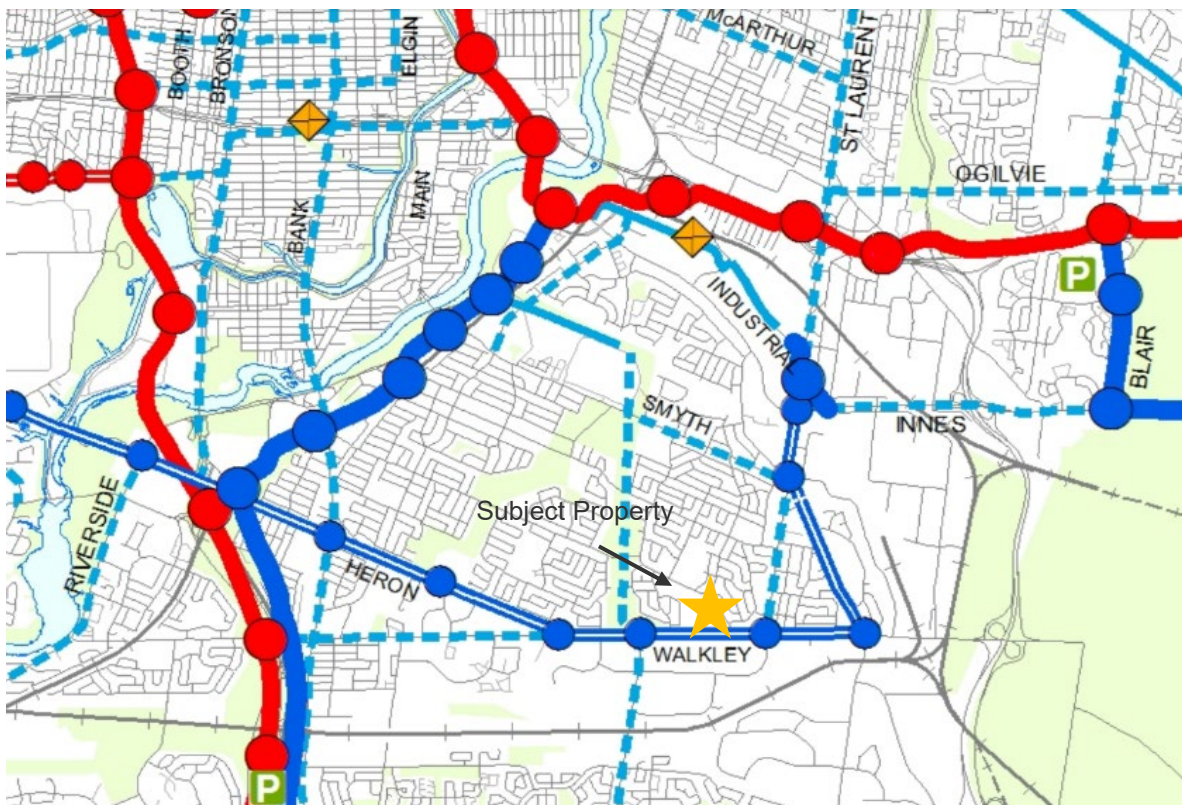


Figure 3: Extract from Official Plan Schedule D (Rapid Transit)

2.0 PROPOSED DEVELOPMENT

Urbandale proposes to construct a new rental apartment building containing 202 units on the south west corner of their property. The remainder of the property contains two (2) 13 storey rental buildings and 51 existing townhouse units in two separate clusters on the site.

The new building is proposed to be 17 storeys and 56.5m in height. The development will include the reconfiguration of the existing parking structure to accommodate the proposed building as well as additional underground parking. A total of 179 parking spaces are provided within P1 and P2 as well as 122 bicycle parking spaces. A total of 188 lockers are also located within the proposed underground parking garage. Surface parking is also provided along the western property limit and within the existing parking structure. Visitor bicycle parking spaces is also provided at grade.

There is an existing access onto Walkley Road which provides access to the existing 12-storey building and the parking structure. This access location is proposed to be maintained and driveway leading to the existing building is proposed to be slightly modified. Two (2) additional entrances are proposed onto Walkley Road, west of the existing access for enhanced site circulation and vehicular access.

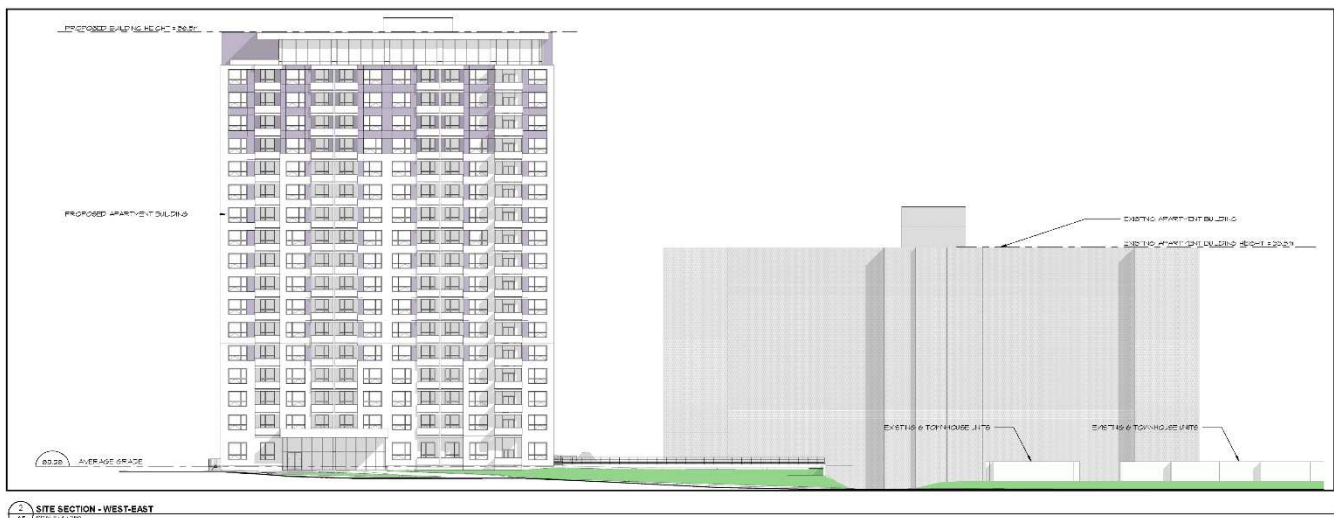


Figure 4: Site section (as seen from south)

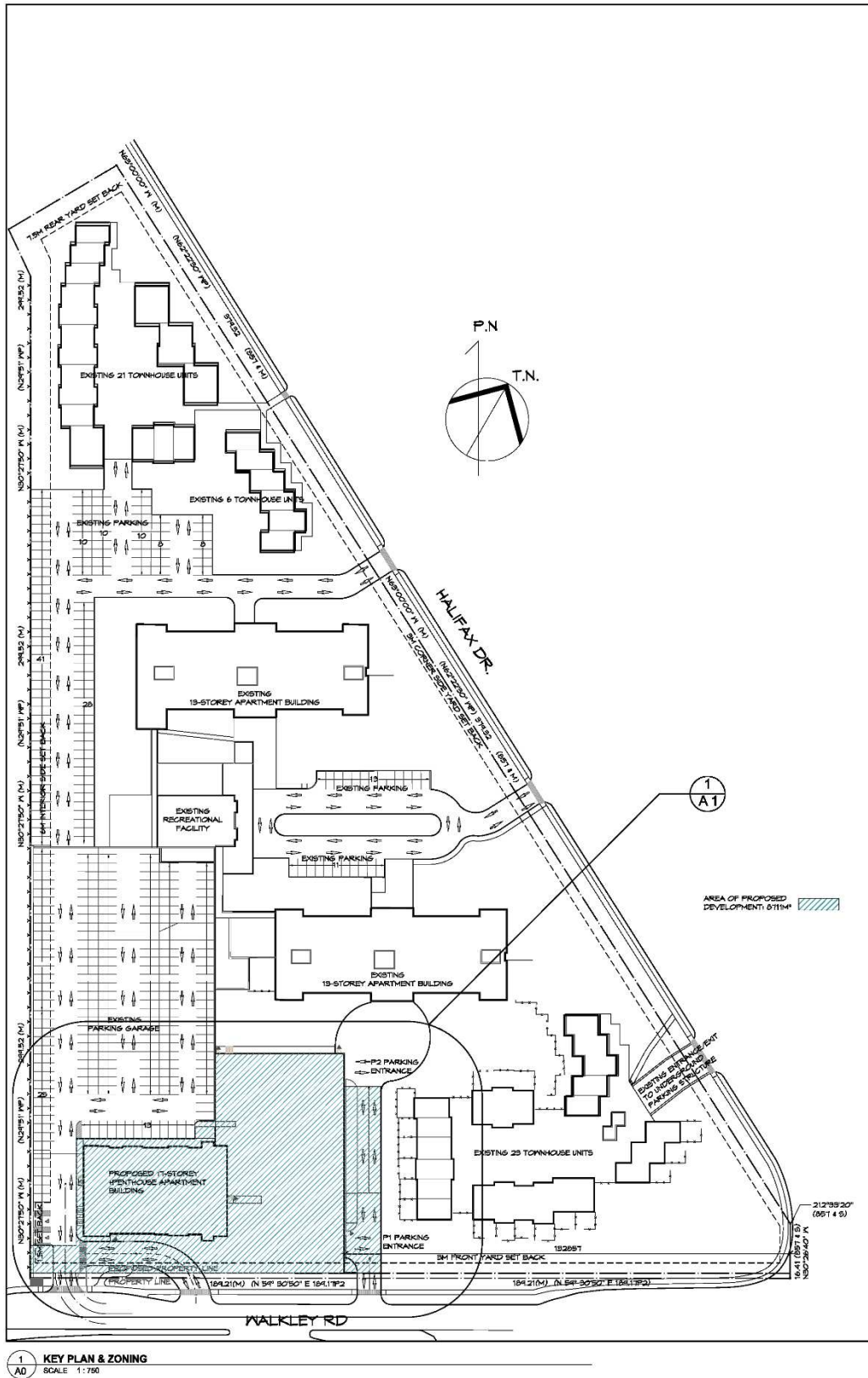


Figure 5: Key site plan

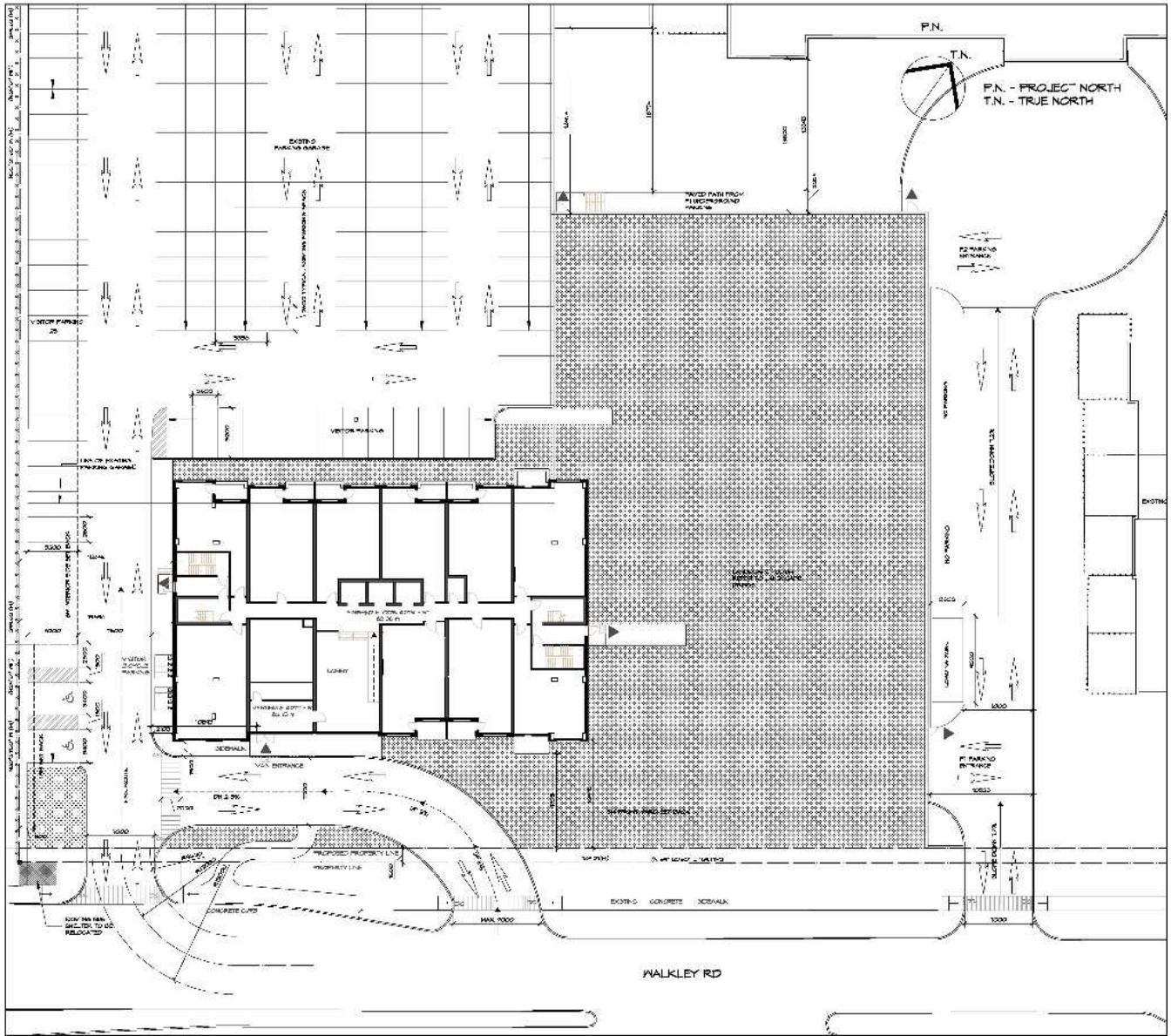


Figure 6: Site plan

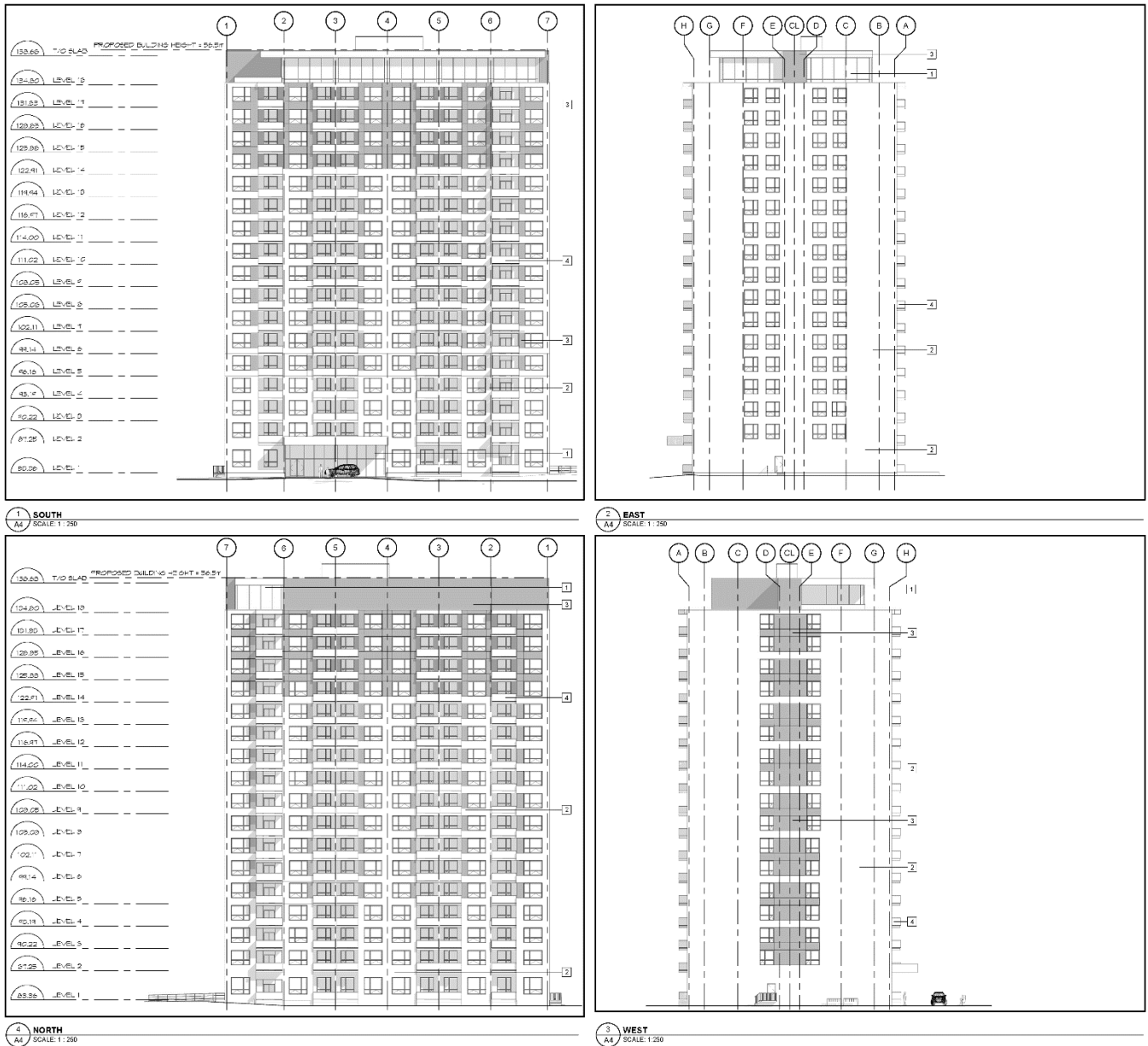


Figure 7: Elevations

3.1 Provincial Policy Statement

The Provincial Policy Statement (PPS), issued under the authority of Section 3 of the Planning Act and in effect since April 30, 2014, provides direction on matters of provincial interest related to land use planning and development. The Planning Act requires that decisions affecting planning matters “shall be consistent with” policy statements issued under the Act.

The PPS emphasizes intensification in built-up areas in order to promote the efficient use of land and existing infrastructure and public service facilities to avoid the need for unjustified and uneconomic expansion. To achieve this goal, planning authorities must identify and promote opportunities for intensification and redevelopment.

Section 1.1.1 (Managing and Directing Land Use to Achieve Efficient and Resilient Development and Land Use Patterns) of the PPS identifies the ways in which healthy, liveable and safe communities are sustained, including:

- a) Promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;
- b) Accommodating an appropriate range and mix of residential, employment, institutional (including places of worship, cemeteries, and long-term care homes), recreation, park and open space, and other uses to meet long-term needs; and
- c) Avoiding development and land use patterns which may cause environmental or public health and safety concerns;
- e) Promoting cost-effective development patterns and standards to minimize land consumption and servicing costs;
- f) Improving accessibility for persons with disabilities and older persons by identifying, preventing and removing land use barriers which restrict their full participation in society;
- g) Promoting development and land use patterns that conserve biodiversity and consider the impacts of a changing climate.

The proposed development is located within the Urban Area in the City’s Official Plan and represents an efficient use of land that has access to existing infrastructure and public service facilities. The proposed development provides for a cost-effective development pattern that makes use of existing services including access to the bus rapid transit routes that connects to transit priority corridors. The proposed development also adds to the existing rental housing supply surrounding the proposed development and contributes to a range of residential uses into the area. The residential building is abutting a high school (Canterbury), and in proximity to employment (St. Laurent Business Park) and commercial and service uses (food store and retail).

Settlement Areas

Policy 1.1.3.1 requires that settlement areas shall be the focus of growth and development, and their vitality and regeneration shall be promoted. Policy 1.1.3.2 states that land use patterns within settlement areas shall be based on:

- a) Densities and a mix of land uses which:
 - 1. Efficiently use land and resources;
 - 2. Are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and / or uneconomical expansion;
 - 3. Minimize negative impacts to air quality and climate change, and promote energy efficiency;
 - 4. Support active transportation;
 - 5. Are transit-supportive, where transit is planned, exists or may be developed.

The proposed development focuses growth within a settlement area and within a part of the community that already contain high rise apartment buildings. The proposed 17 storey building will make efficient use of existing infrastructure including water and wastewater as well as public transit services and is in close proximity to a number of public service facilities such as schools and parks, open space areas, and commercial uses/amenities. The proposal contributes to providing a transit-supportive neighbourhood with the development of an apartment building along a bus rapid transit corridor that can connect residents to priority transit stations and encourage transit ridership.

Policy 1.1.3.4 states that appropriate development standards should be promoted which facilitate intensification, redevelopment, and compact form, while avoiding or mitigating risks to public health and safety.

The proposed development is an appropriate form of intensification within an area where high rise apartments already existing and where there is easy access to the rapid transit route along Walkley Road. Further, the proposed development is located away from the low-rise uses with the exception of the rental townhouses which form part of Urbandale's land holdings. The placement of the proposed building mitigates any perceived impacts of the development on the surrounding low-rise neighbourhood.

Housing

Policy 1.4.3 requires that planning authorities provide for an appropriate range and mix of housing types and densities to meet projected requirements of current and future residents, in part by permitting and facilitating all forms of housing required to meet the social, health and well-being requirements of current and future residents, as well as all forms of residential intensification and redevelopment. Additionally, the policy requires directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs.

The proposed development provides additional rental housing to increase the range and mix of housing in the area and to assist in meeting housing projections for current and future residents. As previously discussed, the subject property is located in proximity to a number of public service facilities that will be able to service the proposed development as well as encourage the use of alternative modes of transportation due to its location next to a bus rapid transit corridor.

Infrastructure and Public Service Facilities

Section 1.6.6 of the PPS contains policies for sewage, water, and stormwater services. Policy 1.6.6.2 states that municipal sewage services and municipal water services are the preferred form of servicing for settlement areas. Policy 1.6.6.4 further specifies that in settlement areas, private services or private communal services may only be used for infilling and minor rounding out of existing development. The proposed development will be serviced by existing municipal water and wastewater. The Functional Servicing Report demonstrates that there is sufficient capacity in the existing services to accommodate the proposed development.

Policy 1.6.7.2 of the PPS requires that the efficient use of existing and planned transportation infrastructure shall be made. Policy 1.6.7.4 further specifies that a land use pattern, density and mix of uses should be promoted that minimize the length and number of vehicle trips and support current and future use of transit and active transportation. The proposed development makes efficient use of the existing transportation infrastructure including assists in minimize the number of vehicle trips due to its proximity to the bus rapid transit routes and sidewalks along the frontages of the property.

Policy 1.8.1 of the PPS requires that planning authorities shall support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and climate change adaptation through land use and development patterns which, among other things:

- a) Promote compact form and a structure of nodes and corridors;
- b) Promote the use of active transportation and transit in and between residential, employment, and institutional uses and other areas;

-
- e) Improve the mix of employment and housing uses to shorten commute journeys and decrease transportation congestion.

The proposed development represents a compact built form along a bus rapid transit corridor which encourages transit ridership and the use of alternative modes of transportation. Further, the proposed development has sidewalks along both the Walkley and Halifax street frontages to promote active transportation to adjacent institutional, commercial and employment uses.

Wise Use and Management of Resources

Section 2.1 of the PPS contains policies related to natural heritage. Policy 2.1.1 states that natural features and areas shall be protected for the long term. Policy 2.1.2 states that the diversity and connectivity of natural features in an area, and the long-term ecological function and biodiversity of natural heritage systems, should be maintained, restored or, where possible, improved recognizing linkages between and among natural heritage features and areas, surface water features, and ground water features. Further, policy 2.1.8 states that development and site alteration shall not be permitted adjacent to significant wetlands, unless it has been demonstrated that there will be no negative impacts on the natural features or their ecological functions. The proposed development does not contain or is not in proximity to an environmentally sensitive area or feature and therefore, has no negative impacts the natural heritage system.

The proposed development is consistent with the policies of the PPS. The proposal makes efficient use of existing public water, sanitary, and stormwater services to properties by providing intensification within a settlement area with an appropriate transition to abutting institutional and residential uses and buildings. The subject property is served by an existing road network and are in proximity to existing bus rapid transit services with connections to rapid transit services. The development will contribute to a broader range of dwelling types in an area already characterized by high-rise buildings.

3.2 City of Ottawa Official Plan (2003, as amended)

The subject properties are designated General Urban Area on the City of Ottawa Official Plan Schedule B – Urban Policy. The designation permits all types and densities of housing, as well as employment, retail uses, service, industrial, cultural, leisure, greenspace, entertainment, and institutional uses. The proposed residential use is a permitted use within the General Urban Area, the predominant and encouraged land use.

Managing Growth Within the Urban Area

Section 2.2.2 of the Official Plan outlines that there are opportunities for intensification within the General Urban Area, however, they will be at a smaller scale and will vary depending upon the existing built context and proximity to major roads and transit. Proposals for intensification are to have full regards for the existing built context and are to be in accordance with the urban design and compatibility provisions of Section 2.5.1 and 4.11 of the Official Plan.

Policy 1 of Section 2.2.2 defines intensification as the intensification of a property, building or area that results in a net increase in residential units or accommodation and includes the development of underutilized lots and infill development. Policy 3 states that all intensification will occur in accordance with the provisions of Sections 2.5.1 and 4.11. The proposed development represents intensification and an infill development on the underutilized portion of their property. Later in this report, a discussion on how the proposed development meets the provisions of Section 2.5.1 and 4.11 will be provided.

Policy 11 of Section 2.2.2 states that *“intensification may occur in a variety of built forms from low-rise to high-rise provide urban design and compatibility objective are met.”* Taller buildings should be located in areas that support Rapid Transit and Transit Priority networks. Policy 11 further states that low-rise intensification will be the predominant form of intensification in the General Urban Area. Although the policy states that the intensification within the General Urban Area will predominantly be in a low-rise form, the subject property has the ability to support taller buildings due to its unique context. The property is already characterized by existing high-rise buildings and

is located on a Bus Rapid Transit Corridor as identified on Schedule D – Rapid Transit and Transit Priority Network of the Official Plan. Additional residential densities and intensification can be supported in proximity to transit corridors, provided the urban design and compatibility policies of Section 2.5.1 and 4.11 are met.

Policy 12 of Section 2.2.2 states that the “*distribution of appropriate building heights will be determined by:*

- a. *The location of a proposal within a Target Area for Intensification identified in policy 4 above or by a Rapid Transit station or Transit Priority corridor, with the greatest density and tallest building heights being located closest to the station or corridor; and*
- b. *The Design and Compatibility of development within the surrounding existing context and planned function, as detailed in Section 4.11, with buildings clustered with other buildings of similar height.”*

The proposed development is located adjacent to the Bus Rapid Transit Corridor and has the ability to support a higher density and taller building given the existing surrounding context of the area and property. The subject property already accommodates existing tall buildings and the proposed development is located away from the low-rise neighbourhood on the north side of Halifax Drive.

The building height limits and classification identified in policy 13 and Figure 2.4, classify the subject property as High-rise (10-30 Storeys) based on the current zoning (R5B H(39)) for the property. The proposed development of the 17-storey apartment building is within the maximum building height range for high-rise classification and the Zoning By-law Amendment application will be required to increase the height in the zoning from 39.0 metres to 56.5 metres.

General Urban Area

As noted above, the General Urban Area permits the development of a full range and choice of housing types to meet the needs of the community. Section 3.6.1 outlines while a broad mix of uses is supported in the designation, it does not imply that all uses will be permitted everywhere. The Zoning By-law will regulate the location, scale and type of lands use in accordance with the provisions of this Plan. Further, this section states that the “*City supports infill development and other intensification within the General Urban Area in a manner that enhances and complements the desirable characteristics and ensures the long-term vitality of the many existing communities that make up the city.*”

The below is the most recent Official Plan direction related to building heights in the General Urban Area, in large part as per previous settlements and upcoming direction.

Policy 3 of Section 3.6.1 states the building heights in the General Urban Area will continue to be predominantly Low-rise. However, where Secondary Plans or zoning that currently permits building heights greater than four Storeys will remain in effect. The proposed development has a current zoning that permits a greater height and the proposed Zoning By-law Amendment intends to increase the permitted height from 39m to 56.5m.

Policy 4 outlines that notwithstanding Policy 3, new taller buildings may be considered for sites that:

- a. Front an Arterial Road on Schedule E or F of this Plan and which are:
 - i. Within 800 metres walking distance of a Rapid Transit Station on Schedule D of this Plan, or
 - ii. On a Transit Priority Corridor on Schedule D of this Plan. For the purposes of this policy only, the “Transit Street” defined in the Riverside South Community Design Plan is considered an Arterial Road;
- b. are in an area characterised by taller buildings or sites zoned for taller buildings.

The proposed building, with a greater height of up to 17-storeys meets all of the below criteria and is reasonable and appropriate for the lands. The proposed development of a new taller building front onto an Arterial Road (Walkley Road) on Schedule E and is located on a Bus Rapid Transit route along Walkley Road with a bus stop adjacent to the property. Walkley Road, over time, will have enhanced bus rapid service, with grade separated BRT stations. The site is within 800 metre walking distance to two (2) future BRT stations (647m to St. Laurent Boulevard /Walkley and 511m to Conroy Road/Walkley Road Station). Further, the subject property is located in an area characterised by taller building since the property contains existing high-rise buildings with 13 Storeys. The

proposed 17-storey building represents a modest increase to the existing buildings on the property and fits within the existing building context on the property. The location of the proposed building is located away of the low-rise neighbourhood north of Halifax Drive and will not impact the industrial properties on the east side of Walkley Road. The additional building is in closest proximity to a high school and outdoor sport field and running track. There are no issues of shadowing, loss of privacy, or other inconveniences.

Policy 5 of Section 3.6.1 states that when considering a proposal for residential intensification through infill or redevelopment in the General Urban Area, the City will, among others:

- a) Recognize the importance of new development relating to existing community character so that it enhances and builds upon desirable established patterns and built form;
- b) Apply the policies of Section 2.5.1 and Section 4.11;
- c) Consider its contribution to the maintenance and achievement of a balance of housing types and tenures to provide a full range of housing for a variety of demographic profiles throughout the General Urban Area; and
- d) Assess ground-oriented multiple housing forms, such as duplex, triplex and fourplex, as one means of intensifying within established low-rise residential communities.

The proposed development builds upon and enhances the established pattern and built form of the area and the subject property. The subject property contains existing high-rise residential apartment buildings as well as two (2) clusters of townhouses. The development of an additional high-rise apartment building is appropriately located away from the low-rise residential uses along Halifax Drive and further east along Bingham Street and Carregie Street. The existing 12-storey building already characterizes that section of Elmvale Acres. The proposed building is also appropriately setback from the adjacent townhouse development. There are no adverse impacts to the rental Urbandale townhouses, given the location of the townhouses, south of the existing high-rise building, and east of the new development. The townhouses contribute to the projects overall transition to two (2) storey townhouses and surface parking in the Regency Court complex along Halifax Drive to minimize shadowing or any loss of privacy.

The proposed new apartment building provides for additional rental units to the community increasing the range and mix of housing supply to the area. Further, due to the property's location along the Bus Rapid Transit Route, the proposed development assists in promoting transit uses and has the ability to contribute to a greater supply of transit users.

As discussed below, the proposed development meets in intent of the urban design and compatibility policies of Section 2.5.1 and 4.11.

Urban Design and Compatibility

Section 2.5.1 establishes a set of Design Objectives and Principles to ensure compatibility in urban design. The urban design principles are concerned with the details relating to how buildings, landscapes and adjacent public spaces look and function together.

This section contains policies to ensure new development is compatible with the existing community character. The Official Plan recognizes that *“compatible development means development that, although it is not necessarily the same as or similar to existing buildings in the vicinity, nonetheless enhances an established community and coexists with existing development without causing undue adverse impact on surrounding properties.”* The proposed development contains a similar building typology to the existing 12-storey bar-typology buildings on the property. The development, however, proposes additional height than the existing buildings, a change from 39m to 56.5m in height. The separation between the proposed and existing buildings as well as the location of the proposed building assists to mitigate impacts to the surrounding properties. The following outlines how the proposed development meets the objectives and principles outlined in Section 2.5.1 to ensure the development proceeds in accordance with the urban design and compatibility policies.

The proposed development meets the objectives and principles as follows:

- / Enhances the sense of community by proposing a quality building with a mix of building materials in proximity to a bus rapid transit route, with orientation towards the public street and away from the low-rise residential neighbourhood north of Halifax Drive.
- / Defines quality public and private spaces through development by orienting the proposed apartment building to the public street and providing convenient pedestrian access to the public sidewalk along Walkley Road. The proposed development also provides for private open spaces that will include landscaping to enhance the streetscape along Walkley Road and the relationship to the existing buildings on the property.
- / Creates places that are safe, accessible and are easy to get to, and move through by proposing two vehicular accesses from the adjacent Arterial Road and providing a pick-up / drop-off area at the main entrance. There is an existing public transit stop along Walkley Road will be accessible via the sidewalk which contains a connection to the proposed development.
- / Respects the character of existing areas by proposing an apartment building on a property that contains existing apartment buildings. The location of the proposed new 17-storey building is located close to the street and away from low-rise residential uses. The setbacks and separation of the proposed building respects the surrounding building and adds to the pattern, rhythm, character and context of the area and property.
- / Considers adaptability and diversity by intensifying development on lands within proximity to a bus rapid transit route as well as in proximity to employment uses to the south of Walkley Road and commercial uses to the east of Halifax Drive.
- / Understands and respects natural processes and features in development design by utilizing practical means of retaining stormwater.
- / Maximizes opportunities for sustainable transportation modes due to the proximity of the development to the bus rapid transit route along Walkley Drive. Green spaces are also proposed on the top of the underground parking surfaces promote a more sustainable design.

Section 4.11 of the Official Plan outlines policies that set the stage for high quality urban design as well as criteria to evaluate the compatibility of new developments within the City. Policy 2 of Section 4.11 contains criteria for evaluating the compatibility of proposed developments. The proposal meets the applicable criteria:

- / **Traffic:** As outlined in the Transportation Report prepared by Dillon Consulting, there is available capacity along Walkley Road to accommodate the vehicular traffic anticipated to be generated by the proposed development. Additionally, the proximity of the subject property to the bus rapid transit stop will encourage many users of the proposed development to use public transportation and to discourage vehicular trips.
- / **Vehicular Access:** Two new points of ingress and egress to the proposed development are proposed at the western edge of the subject property along Walkley Road. These two (2) access points will be right-in/right-out only due to the existing median along Walkley Road. The existing full moves access along Walkley Road is proposed to be maintained and will service the proposed new building as well as the existing apartment building on the property.
- / **Parking Requirements:** The proposed development contains both surface and underground parking and proposes to exceed the minimum parking requirement.
- / **Outdoor Amenity Areas:** The proposed development is located away from the low-rise residential uses on the east side of Halifax drive and will not impact their outdoor amenity areas. Further, given that there are existing apartment buildings on the subject site in proximity to the townhouses, the impact from the proposed development on their amenity spaces will be negligible.

- / **Loading Areas, Service Areas, and Outdoor Storage:** One (1) delivery space is proposed along the full moves access onto Walkley Drive on the west side of the driveway to avoid any off site impacts to abutting streets.
- / **Lighting:** Lighting impacts from the proposed development on adjacent uses are not anticipated.
- / **Noise and Air Quality:** Noise or air quality impacts from the proposed development are not anticipated.
- / **Sunlight:** The majority of shadowing falls north and east of the proposed development. Some shadowing is proposed to impact the townhouse residential units in the evening particularly around the winter solstice, when use of amenity areas is uncommon. The shadows from the proposed development continue to a minor increase in the shadows that already exist as a result of the existing high-rise buildings.
- / **Microclimate:** The proposed development is not anticipated to result in microclimate impacts on adjacent properties.
- / **Supporting Neighbourhood Services:** The subject property is located in an established urban area with a variety of commercial and recreational amenities. The subject property is located adjacent to Canterbury High School as well as in close proximity to Hawthorne Public School and Brian Kilrea Arena. There are commercial uses east of the subject property along Walkley. The property is immediately adjacent to a bus rapid transit route and stop which provides convenient access to other rapid transit routes that service downtown Ottawa and other destinations in the city.

Policy 9 of Section 4.11 states that in addition to locations (Central Area, Mixed use Centres and Town Centre, Employment Areas and Traditional and Arterial Mainstreets) outlined in Policy 8, *“high-rise buildings may be considered in the following locations, provided all other policies of this Plan have been met:*

- a. *Within areas characterized by high-rise buildings that have direct access to an arterial road, or;*
- b. *Within 600 meters of a rapid transit station as identified on Schedule D, or;*
- c. *Where a community design plan, secondary plan or other similar Council-approved planning document identifies locations....., or;*
- d. *Within areas identified for high-rise buildings where these building profiles are already permitted in the Zoning By-law approved by Council, or;*
- e. *Within areas where a built form transition as described in policy 12 below is appropriate.”*

While the subject property is not identified as a location for high-rise buildings as per policy 8, the subject property meets the intent of other locations that can support high-rise buildings as per policy 9. The subject property has direct access to an arterial road and the proposed development fronts onto the arterial road, Walkley Road. The proposed development is also located along a bus rapid transit corridor as identified on Schedule D of the Official Plan. In addition, the property contains building profiles characteristic of high-rise buildings and the Zoning By-law permits heights of 39m.

The proposed Zoning By-law Amendment seeks to increase the maximum permitted height to permit 17 storeys with a height of 56.5m. This is a marginal increase to the heights permitted by the current zoning by-law and due to the siting, separation and setbacks of the proposed building there are no anticipated negative impacts on the surrounding area.

Policy 14 of Section 4.11 contains policy direction for infill and redevelopment within the interior portions of stable, low-rise neighbourhoods. The subject property is located within the General Urban Area where the predominant form of development is low-rise. The location of the subject property is unique in that it is located on the edge of the General Urban Area and has frontage onto an arterial road with a Bus Rapid Transit route. The site is on the perimeter of the Elmvale Acres neighbourhood, with a more obvious relationship with Walkley Road and the Canterbury high school and outdoor facilities. The new building will have limited synergy with Halifax Drive, already defined by the two (2) high-rise buildings, indoor pool, internal courtyard and surface parking area. The new building

is oriented towards Walkley Road. Further, the subject property already contains two high-rise apartment buildings and is currently zoned to permit a maximum height of 39 m.

The proposed Zoning By-law Amendment to increase the maximum permitted height from 39m to 56.5m is appropriate for the property given the siting, setbacks and separation of the proposed buildings. The proposed development meets the majority of the R5B zoning requirements and setbacks providing for adequate setbacks and design of the development. Further, the proposed additional height is not anticipated to have negative impacts on the amenity areas of adjacent low-rise residential uses given the presence of high-rise buildings already on site.

It is our opinion that the proposed high-rise building is an appropriate form of intensification given the context of the property and the location on an arterial road identified as a with a bus rapid transit corridor with future grade-separated transit stations. As such, it is our opinion that the proposed development is in conformity with the City of Ottawa's Official Plan and the proposed development represents an appropriate and desirable form of development.

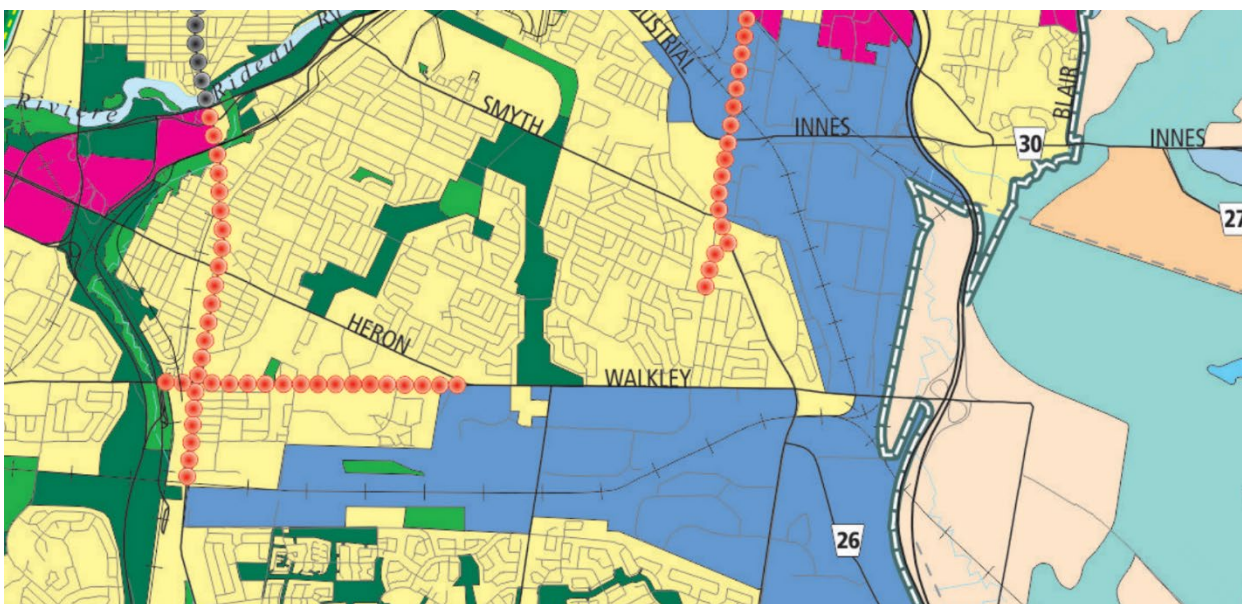


Figure 8: Extract from Schedule B Urban Policy Plan

3.3 Official Plan Amendment (OPA) 150

In 2013, the City of Ottawa updated its Official Plan, resulting in numerous changes to its land use policies. The Ministry of Municipal Affairs and Housing issued approval of OPA 150 in April 2014, but the Amendment is currently under appeal before the Ontario Municipal Board (OMB). The Local Planning Appeal Tribunal (LPAT), formerly the OMB, rendered decisions some of the outstanding matters with regards to OPA 150 on October 22, 2018. A number of the policies relating to Section 3.6.1 and 4.11 have now been incorporated into the City's Official Plan. The discussion above reflects the most recently approved policies as per the LPAT decision.

3.4 Transit-Oriented Development Guidelines

The Transit-Oriented Development Guidelines were approved by City Council on September 26, 2007, and provide guidance for development design in areas around rapid transit stations. The proposed development meets several of these guidelines, including:

- / Proposes transit-supportive land uses immediately adjacent to a bus rapid transit stop.
- / Designs pedestrian connections that are convenient, comfortable, safe, easily navigable, continuous, and barrier-free that leads directly to transit.
- / Provides convenient and attractive bicycle parking that is close to building entrances.
- / Locates the surface parking lot at the rear of the building and not between the public right-of-way and the functional front of the building.
- / Designs access driveways to be shared between facilities.
- / Proposes underground parking so as to reduce impediments to pedestrian flows.

3.5 City of Ottawa Zoning By-law 2008-250

The subject property is currently zoned Residential Fifth Density Subzone B (R5B) H 39. The Residential Fifth Density Subzone B permits a range of uses including an apartment building, high-rise with a maximum height of 39 m. A Zoning By-law Amendment is required to permit an increase in the maximum permitted height from 39m to 56.5m and to provide site specific zone standards to facilitate the proposed development. The

The Zoning By-law Amendment application proposes to rezone the subject property to a site specific Residential Fifth Density Subzone B (R5B H56.5 [XXXX]). The purpose of the R5B H56.5 [XXXX] Zone is to:

1. Allow an increase in height from 39m to 56.5m
2. Reduce the amount of landscaping required; and
3. Reduce the width of a landscape buffer.



Figure 9: Extract from Zoning Map of Comprehensive Zoning By-law 2008-250

Table 1 below evaluates the proposed development against the applicable performance standards in the R5B H39 Zone:

Table 1: Zoning Evaluation

Mechanism		Provision	Proposal	Compliance
Minimum lot area		675m ²	43,150m ²	✓
Minimum lot width		22.5 m	227 m	✓
Minimum front yard and corner side yard setbacks		3 m	11 m	✓
Minimum interior side yard setback		1.5m	15 m	✓
Minimum rear yard setback		7.5m	245 m	✓
Maximum building height		39 m	56.5 m	✗
Maximum floor space index		N/A	1.2	✓
Landscape Area		30% of the lot area	<30%	✗
Minimum width of landscaped area	Abutting a street	3 m	>3 m	✓
	Abutting an institutional zone	3 m	0 m	✓
	Other cases	No minimum	N/A	✓
Minimum required parking spaces	Dwelling, Mid-high Rise Apartment	0.5 per dwelling unit 202 x 0.5 = 101 spaces	141 spaces	✓
	Minimum visitor parking spaces	0.2 per dwelling unit (no spaces required for the first 12 dwelling units) 190 x 0.2 = 38 spaces	38 spaces	✓
Minimum parking space dimensions		2.6 m x 5.2 m	2.6 m x 5.2 m	✓
Minimum driveway width	Double traffic lane (parking lot)	6.7 m	7m	✓
	Double traffic lane (parking garage)	6.0 m	6.7 m	✓
Minimum width of landscape buffer		Minimum 15% of the area of any parking lot must be provided as perimeter or interior landscaped area	<15%	✗

Mechanism		Provision	Proposal	Compliance
	Abutting a street (>100 spaces)	3 m	>3 m	✓
	Not abutting a street (>100 spaces)	3 m	0 m	✗
Minimum required bicycle parking spaces	High-rise	0.5 per dwelling unit 202 x 0.5 = 101 spaces	122	✓
Minimum number of vehicle loading spaces required	None required for residential uses		1	✓
Amenity Area	6m ² per dwelling unit. 202 x 6 = 1,212m ²		1,428m ²	✓
	Communal: 50% of the required total amenity area		1,038m ² communal	

4.0 REQUESTED AMENDMENTS

Amendments to the Zoning By-law are required to permit the proposed development. Details of the proposed amendments are summarized below:

4.1 Zoning By-Law Amendment

A site-specific Zoning By-law Amendment is being requested to permit the proposed development. As summarized in Table 1 above, an Exception Zone is requested to provide relief from the following zoning provisions:

- / Permit a maximum building height of 56.5 metres, whereas the maximum building height in the R5B H39 zone is 39 metres.
- / To reduce the amount of landscaping to less than the minimum required 30%.
- / To reduce the amount of perimeter or interior landscaped area to less than the minimum required 15%.
- / To reduce the width of the landscaped buffer not abutting a street to 0m.

The Zoning By-law Amendment application proposes to rezone the subject properties to Residential Fifth Density Zone H56.5, Special Exception Zone (R5B H56.5 [XXXX]).

5.0 CONCLUSION

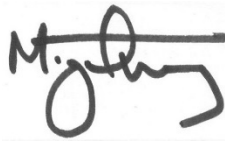
It is our professional planning opinion that the Zoning By-law Amendment, and Site Plan Control applications constitute good planning and are in the public interest. As outlined in the preceding sections:

- / The proposed development meets the policies established in the 2014 Provincial Policy Statement (PPS).
- / The proposed development is consistent with the policies of the General Urban Area designation in the Official Plan, as well as the urban design and compatibility objectives and criteria established in Sections 2.5.1 and 4.11.
- / A range of studies confirm that the proposed development is functional appropriate.

Sincerely,



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