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# Office Building 2707 Solandt Road

## Planning Rationale, Integrated Environmental Review Statement and Public Consultation Strategy

**OFFICE BUILDING  
2707 SOLANDT ROAD**

PLANNING RATIONALE, INTEGRATED ENVIRONMENTAL REVIEW STATEMENT AND  
PUBLIC CONSULTATION STRATEGY  
IN SUPPORT OF A SITE PLAN CONTROL AND ZONING BY-LAW AMENDMENT

Prepared For:  
**KRP Properties Inc.**

Prepared By:  
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October 4, 2019  
Novatech File: 119110  
Ref: R-2019-156

October 4, 2019

City of Ottawa  
Planning, Infrastructure and Economic Development Department  
110 Laurier Ave. West, 4<sup>th</sup> Floor  
Ottawa, Ontario  
K1P 1J1

Attention: Ms. Laurel McCreight, Planner II

**Reference: Solandt Office Building – 2707 Solandt Road  
Planning Rationale and Integrated Environmental Review Statement**

**Our File No.: 119110**

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Novatech has prepared this Planning Rationale, Integrated Environmental Review and Public Consultation Strategy on behalf of KRP Properties to support a Site Plan Control application for 2707 Solandt Road and a Zoning By-law Amendment application for both 2707 Solandt Road and the neighbouring property at 2505 Solandt Road. The properties are in the Kanata North Business Park.

KRP Properties intends to develop an eight storey office building at 2707 Solandt Road. A Zoning By-law amendment is required to permit the height of the proposed building. Longer term, they also intend to develop 2505 Solandt Road. This would be the subject of a future Site Plan Approval application, but it is considered appropriate to apply the same zoning in anticipation, as part of this application.

Should you have any questions or comments, please do not hesitate to contact me.

Sincerely,

**NOVATECH**



James Ireland, BUPD  
Project Planner

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## 1.0 | INTRODUCTION

Novatech was retained by KRP Properties to prepare a Planning Rationale in support of a Site Plan Control (SPC) application and Zoning By-law Amendment (ZBLA) application to allow for the development of lands municipally known as 2707 Solandt Road (hereafter the “subject site”) and legally described as PART BLOCK 29 PLAN 4M280 PART 2 ON PLAN 4R26736 CITY OF OTTAWA.

Application is also made to include the neighbouring property at 2505 Solandt Road in the ZBLA only. For clarity in this planning rationale, it is not considered part of the subject site and is referred to by its municipal address. Legally it is described as BLOCK 12 ON PLAN 4M-1096, SAVE AND EXCEPT PART 1 ON PLAN 4R-21488 OTTAWA.

An Integrated Environmental Review Statement (IERS), as required under Section 4.7 of the *Official Plan*, forms part of this Rationale. It demonstrates how the findings of the supporting studies influence the design of the proposed development with respect to effects on the environment and compliance with the appropriate policies contained in Section 4 of the *Official Plan*. A Public Consultation Strategy also forms part of this rationale.

## 2.0 | CONTEXTUAL ANALYSIS

### 2.1 SITE LOCATION AND LOCAL CONTEXT

The subject site, shown in Figure 1, is on the north west side of Solandt Road in the central part of the Kanata North Business Park. It is 2 hectares in area and roughly rectangular with approximately 120 metres of frontage along Solandt Road.



Figure 1: Subject site and 2505 Solandt Road (Image Source: GeoOttawa)

The subject site is undeveloped and forested – vegetation has re-generated on it since it was last cleared in the early 1990s.

To the north of the subject site is The Marshes Golf Course. To the west and south are three other commercial properties with at-grade parking. Across Solandt Road is 2500 Solandt Road, a three storey office building with a footprint of approximately 5000m<sup>2</sup>, currently occupied by Hewlett-Packard. Directly to the south is 425 Leggett Road, a two storey commercial building with a footprint of approximately 7900m<sup>2</sup>, occupied by multiple tenants. To the south west, 515 Leggett Drive is a 10 storey office building with a footprint of approximately 1400m<sup>2</sup>. Deloitte is the primary tenant and has naming rights.

To the east is 2505 Solandt Road, a previously approved parking lot that is currently unused. As it is under the same ownership as the subject site, some of the parking spaces here will be utilised by this proposal and a driveway and pedestrian connection between the two will be made. However, this site is not considered part of the subject site. **Application is made to apply the ZBLA to this site in addition to the subject site.**

Further afield, the Kanata North Business Park continues – as the subject site is roughly in the centre of the park, it is 540m to the nearest non-business park land use, the residential Marshes Village to the south east.



Figure 2: Surrounding Land Uses (Image Source: GeoOttawa, 2018)



## 2.2 PLANNING CONTEXT

### 2.2.1 CITY OF OTTAWA OFFICIAL PLAN

The subject site is designated as Urban Employment Area on 'Schedule B: Urban Policy Plan' in the *Official Plan*. Per Section 3.6.5 of the *Official Plan*, the Urban Employment Area addresses the objective of the Official Plan to “ensure that, over the long term, sufficient areas of land are reserved primarily for places of business and economic activity.” To qualify, an area must be capable of providing over 2000 jobs. Section 4.2 of this Rationale demonstrates how the proposed development complies with the applicable Employment Area policies.

Per 'Schedule D – Rapid Transit and Transit Priority Network' in the *Official Plan*, the subject site is also within 500-metres radius of the future Bus Rapid Transit Station stop at the corner of Solandt Road and March Road, as shown on Figure 8. This rapid transit corridor has not yet been built.



Figure 3: Excerpt of OP Schedule D indicating proximity of Subject site to the future BRT stop.

Official Plan 'Schedule E – Urban Road Network' identifies Solandt Road as an Existing Collector Road, along with Legget Drive. Herzberg Road / Terry Fox Drive is a major collector and March Road is an arterial. Although it is a collector road, Solandt Road in front of the subject site is a cul-de-sac only serving three existing commercial buildings and the subject site.

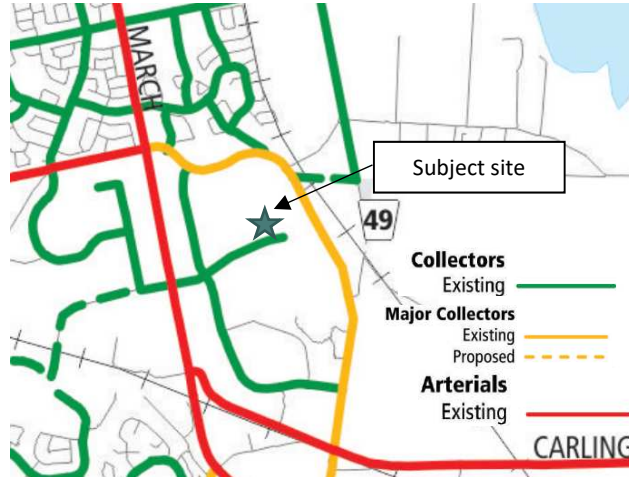


Figure 4: Excerpt of OP Schedule E indicating Subject site

Per Official Plan 'Schedule K – Environmental Constraints', the subject site is not affected by environmental constraints, as shown in Figure 5. It is close to a mapped area of organic soils but this may be outdated as the area is now The Marshes Golf Course. It is also close to mapped unstable slopes along Shirleys Brook. The proposed development respects the 30m setback set out in accordance with Section 4.7.3 of the Official Plan (Erosion Prevention and Protection of Surface Water).

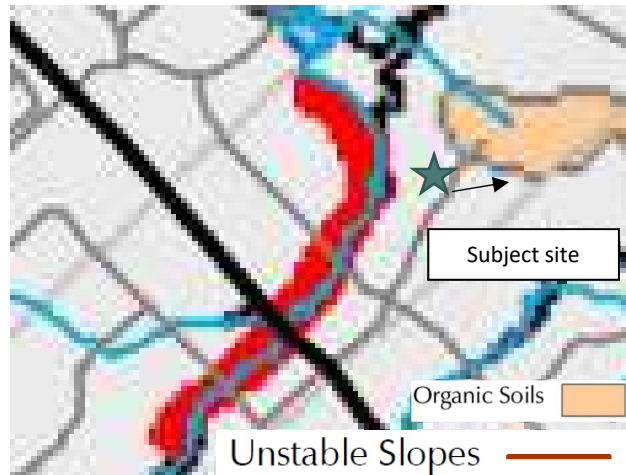


Figure 5: Excerpt of OP Schedule K indicating Subject site with a star



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### 2.2.3 CITY OF OTTAWA ZONING BY-LAW 2008-250

The current zoning of the site is Business Park Industrial subzone 6 (Kanata North Business Park) (IP6) under *Zoning By-law 2008-250*, as shown in Figure 6. An exception (1548) applies to the northern part of 2505 Solandt Road, but this is only to also permit golf course as a use and is not relevant to this proposal.

The primary purpose of the IP Zone is to: “*accommodate mixed office, office-type uses and low impact, light industrial uses in a business park setting, in accordance with the **Enterprise Area** designations of the Official Plan or, the **Employment Area or the General Urban Area** designation where applicable.*”

The zone has a height limit of 22m for the site, which is not enough for the proposed 8 storey building. Refer to Section 3.1.2 of this Rationale for a description of the proposed zoning by-law amendment.



Figure 6: Existing Zoning (Image Source: GeoOttawa, 2018)

## 3.0 | THE PROPOSAL

A pre-application consultation meeting was held with City Staff on July 11, 2019 to discuss the proposed development and application submission requirements. It was determined that an application for site plan control approval and an application for a zoning by-law amendment would be required. Following the meeting, the City provided comments on July 18, 2019 regarding environmental considerations, tree conservation, site servicing, traffic and roads, noise and site design. These comments have been considered and integrated into the proposed design, as described below. Further discussion with City staff



façade and yellow feature glazing on the front façade. The rooftop mechanical equipment is well screened; the screen also provides a background for future anchor tenant signage.

Stairs are increasingly becoming a desirable means to access the lower floors and to move between floors in a tenancy, rather than just an emergency exit requirement. Accordingly, the design highlights the stairs at the front of the building with a generous glazed stairwell. The ground floor combines an attractive corporate entry foyer with convenient bicycle facilities.

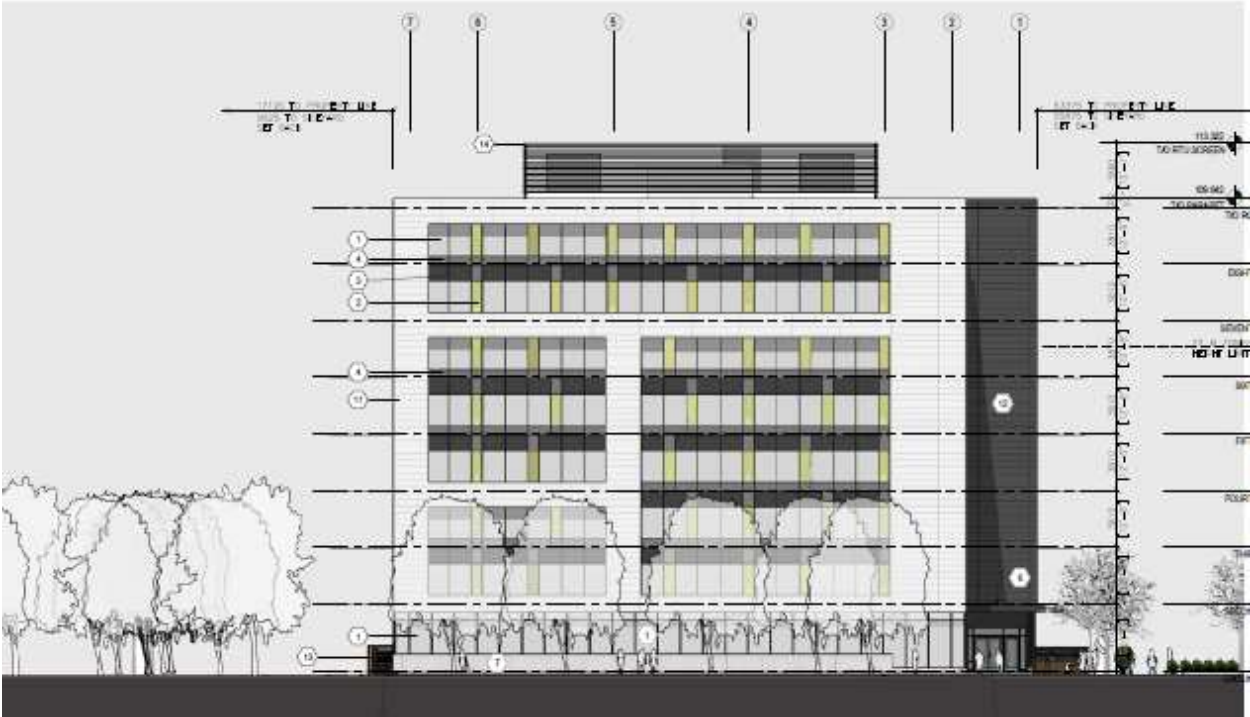


Figure 8: Solandt Road elevation of the proposed building

### SITE ACCESS AND PARKING

Access will comprise two double width vehicle accesses from Solandt Road and a pedestrian access directly from the sidewalk. A driveway and sidewalk link directly between subject site and the existing parking lot at 2505 Solandt Road will be introduced to provide vehicular and direct pedestrian access to the building.

There are 196 at-grade parking spaces on the subject site, broken into smaller rooms by landscaping. The existing parking lot at 2505 Solandt Road will be utilised in its current form for an additional 247 spaces, with a direct vehicle access between the two properties added. The total of 443 spaces meets the rates in the Zoning By-law.

### 3.2 ZONING BY-LAW AMENDMENT

The current Business Park Industrial, Subzone 6 (IP6) permits the office use, but an exception is required to allow the proposed development. The exception will increase the permitted height on the site from the current 22m to 44m. The height of 44m is used in the exception for the existing 10 storey Deloitte building to the south of the subject site (urban exception 301). It is intended that the ZBLA be applied to both the site of the proposed office building (2707 Solandt Road) and the neighbouring property at 2505 Solandt Road, as it is likely that it too will be developed in a similar manner longer term. It would be subject to a future SPC application.

## 4.0 | POLICY JUSTIFICATION

### 4.1 PROVINCIAL POLICY STATEMENT

Section 3 of the *Planning Act* requires that decisions affecting planning matters ‘*shall be consistent with*’ the policies of the *Provincial Policy Statement* (PPS). The PPS is organized into three main policy sections: (1) Building Strong Healthy Communities, (2) Wise Use and Management of Resources, and (3) Protecting Public Health and Safety. The following sub-sections explain how the proposed development is consistent with the applicable policies of the respective PPS sections.

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#### 4.1.1 BUILDING STRONG HEALTHY COMMUNITIES

**Section 1.1 of the PPS** is focused on managing and directing land use to achieve efficient and resilient development and land use patterns.

**Policy 1.1.1** *Healthy, liveable and safe communities are sustained by:*

- (a) *Promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long-term*
- (b) *accommodating an appropriate range and mix of residential (including second units, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
- (c) *avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
- (d) *avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;*
- (e) *promoting cost-effective development patterns and standards to minimize land consumption and servicing costs;*
- (f) *improving accessibility for persons with disabilities and older persons by identifying, preventing and removing land use barriers which restrict their full participation in society;*
- (g) *ensuring that necessary infrastructure, electricity generation facilities and transmission and distribution systems, and public service facilities are or will be available to meet current and projected needs; and*

*(h) promoting development and land use patterns that conserve biodiversity and consider the impacts of a changing climate.*

The proposed development contributes to a healthy, liveable and safe community as described above because:

- it facilitates a large number of jobs on a relatively small vacant lot in an existing designated employment area that is already serviced;
- the jobs introduced by the proposal contribute to the financial viability of the province and the City of Ottawa. Kanata North Business Park is Canada's largest agglomeration of tech businesses;
- it does not create environmental or public health and safety concerns, and:
- it will conserve biodiversity.

**Policy 1.1.3.1** *Settlement areas shall be the focus of growth and development, and their vitality and regeneration shall be promoted.*

The Subject site is in the Settlement Area.

**Policy 1.1.3.2** *Land use patterns within settlement areas shall be based on:*

*(a) densities and a mix of land uses which:*

- 1. efficiently use land and resources;*
- 2. are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
- 3. minimize negative impacts to air quality and climate change, and promote energy efficiency;*
- 4. support active transportation;*
- 5. transit-supportive, where transit is planned, exists or may be developed; and*
- 6. are freight-supportive; and*

The proposed development efficiently uses land and resources by:

- concentrating a large number of jobs on a relatively small vacant lot in an existing designated employment area;
- being in a serviced area surrounded by developed lands;
- being an energy efficient building that minimizes impact on air quality, and:
- supporting transit use by increasing employment density in proximity to a bus rapid transit route.

**Section 1.3 of the PPS** provides policies on employment and employment areas.

**Policy 1.3.1** *Planning authorities shall promote economic development and competitiveness by:*

- a. providing for an appropriate mix and range of employment and institutional uses to meet long-term needs;*



- b. *providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses;*
- c. *encouraging compact, mixed-use development that incorporates compatible employment uses to support liveable and resilient communities; and*
- d. *ensuring the necessary infrastructure is provided to support current and projected needs.*

The proposed development promotes economic development and competitiveness by:

- facilitating new quaternary sector jobs (research, development, and information) in the Kanata North Business Park. These jobs are the *raison d'être* for the business park and are the most resilient long term;
- diversifying Ottawa's economic base with private sector employment – Kanata North Business Park is Canada's largest agglomeration of tech businesses, and:
- being in a high density, compact building.

**Policy 1.3.2** Employment areas

This policy provides guidance to planning authorities in setting up employment areas. The proposal is within the established Kanata North Business Park employment area.

**Section 1.6 of the PPS** provides policies on infrastructure and public service facilities.

**Policy 1.6.3** *Before consideration is given to developing new infrastructure and public service facilities:*

- (a) *the use of existing infrastructure and public service facilities should be optimized*

The site is in a serviced area. A Servicing Brief was prepared for the proposed development and details how the site will be serviced by connections to existing services. Please see Section 6.3 for a summary of the report.

**Section 1.8 of the PPS** provides policies on energy conservation, air quality and climate change.

**Policy 1.8.1** *Planning authorities shall support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and climate change adaptation through land use and development patterns which:*

- a. *promote compact form and a structure of nodes and corridors;*
- b. *promote the use of active transportation and transit in and between residential, employment (including commercial and industrial) and institutional uses and other areas;*
- c. *focus major employment, commercial and other travel-intensive land uses on sites which are well served by transit where this exists or is to be developed, or designing these to facilitate the establishment of transit in the future;*
- d. *focus freight-intensive land uses to areas well served by major highways, airports, rail facilities and marine facilities;*



- e. *improve the mix of employment and housing uses to shorten commute journeys and decrease transportation congestion;*
- f. *promote design and orientation which:*
  - 1. *maximizes energy efficiency and conservation, and considers the mitigating effects of vegetation; and*
  - 2. *maximizes opportunities for the use of renewable energy systems and alternative energy systems; and*
- g. *maximize vegetation within settlement areas, where feasible.*

In relation to a) and e), the proposed eight storey building is compact in form and adds to the concentration of employment uses in the Kanata North Business Park which contributes to the mix of residential and commercial land uses in the Kanata area. In relation to b) and c), the site is within convenient walking distance of a future bus rapid transit corridor. The site plan incorporates sheltered and secure bicycle parking consistent with active transportation policies. The further addition of jobs to a significant employment cluster adds to the case for improved transit service to the area long-term. In relation to g), the site plan incorporates significant areas of planting, in keeping with the existing character of the business park.

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#### 4.1.2 WISE USE AND MANAGEMENT OF RESOURCES

**Section 2.1 of the PPS** provides policies on Natural Heritage. Section 2.1.1 sets out the goal of this section of the PPS: *'Natural features and areas shall be protected for the long term'*

Sections 2.1.2 and 2.1.3 outline the importance of natural heritage systems.

Sections 2.1.4 and 2.1.5 set out areas that development and site alteration shall not be permitted in unless it has been demonstrated that there will be no negative impacts on the natural features or their ecological functions. These areas include significant wetlands, woodlands, valley lands, areas of natural and scientific interest and areas of wildlife habitat.

The Species at Risk Environmental Impact Statement (EIS) prepared by Bernie Muncaster and submitted with the application confirms that the subject site is not in one of these areas.

**Section 2.2 of the PPS** provides policies on Water.

**Policy 2.2.2** *Development and site alteration shall be restricted in or near sensitive surface water features and sensitive ground water features such that these features and their related hydrologic functions will be protected, improved or restored.*

The subject site is not in or near sensitive surface water features and sensitive ground water features.

**Section 2.3 of the PPS** provides policies on Agriculture.

**Policy 2.3.1** *Prime agricultural areas shall be protected for long-term use for agriculture.*

The Subject site is not located within or adjacent to prime agricultural land.

**Section 2.4 of the PPS** provides policies on Minerals and Petroleum.

**Policy 2.4.1** *Minerals and petroleum resources shall be protected for long-term use.*

No mineral or petroleum resources exist on or adjacent to the Subject site.

**Section 2.5 of the PPS** provides policies on Mineral Aggregate Resources.

**Policy 2.5.1** *Mineral aggregate resources shall be protected for long-term use and, where provincial information is available, deposits of mineral aggregate resources shall be identified.*

No mineral aggregate resources exist on or adjacent to the Subject site.

**Section 2.6 of the PPS** provides policies on Cultural Heritage and Archaeology.

**Policy 2.6.1** *Significant built heritage resources and significant cultural heritage landscapes shall be conserved.*

No significant built heritage resources or cultural heritage landscapes exist on or adjacent to the subject site.

**Policy 2.6.2** *Development and site alteration shall not be permitted on lands containing archaeological resources or areas of archaeological potential unless significant archaeological resources have been conserved.*

The subject site does not contain archaeological resources or areas of archaeological potential.

**Policy 2.6.3** *Planning authorities shall not permit development and site alteration on adjacent lands to protected heritage property except where the proposed development and site alteration has been evaluated and it has been demonstrated that the heritage attributes of the protected heritage property will be conserved.*

The Subject site is not adjacent to protected heritage property.

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#### 4.1.3 PROTECTING PUBLIC HEALTH AND SAFETY

**Section 3.1 of the PPS** provides policies on Natural Hazards.

**Policy 3.1.1** *Development shall generally be directed to areas outside of:*

- a. *hazardous lands adjacent to the shorelines of the Great Lakes - St. Lawrence River System and large inland lakes which are impacted by flooding hazards, erosion hazards and/or dynamic beach hazards;*
- b. *hazardous lands adjacent to river, stream and small inland lake systems which are impacted by flooding hazards and/or erosion hazards; and*

c. *hazardous sites.*

The proposed development is not within natural hazard lands or sites. Shirleys Brook runs past the site, so the meander belt and 1 in 100 year flood line as mapped by the Mississippi Valley Conservation Authority affect the western-most part of the site. However, no development is proposed in these areas.

**Section 3.2 of the PPS** provides policies on Human-Made Hazards.

**Policy 3.2.1** *Development on, abutting or adjacent to lands affected by mine hazards; oil, gas and salt hazards; or former mineral mining operations, mineral aggregate operations or petroleum resource operations may be permitted only if rehabilitation or other measures to address and mitigate known or suspected hazards are under way or have been completed.*

**Policy 3.2.2** *Sites with contaminants in land or water shall be assessed and remediated as necessary prior to any activity on the site associated with the proposed use such that there will be no adverse effects.*

The Subject site is not affected by human-made hazards or contaminants, as confirmed in the Phase 1 Environmental Site Assessment prepared by Golder dated August 2019, which is summarized in Section 6.1 of this rationale.

## 4.2 OFFICIAL PLAN

### 4.2.1 LAND USE DESIGNATION AND COMPATIBILITY

As stated in Section 2.2.1 of this Rationale, the subject site is designated Urban Employment Area per Schedule B of the *Official Plan*. Section 3.6.5 of the *Official Plan* contains policies for this land use designation:

**Policy 1** outlines the nature of Urban Employment Areas. The subject site is within an established and significant Urban Employment Area, the Kanata North Business Park.

**Policy 2** directs that office uses be permitted in the zoning by-law that applies to Urban Employment Areas. Accordingly, the proposed office use is permitted within the Industrial Business Park, sub-zone 6 (Kanata North Business Park).

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### OFFICIAL PLAN SECTION 2.5.1

Section 2.5.1 of the *Official Plan* presents Design Objectives in the form of statements which express how the City wants to influence the built environment as the city evolves. The following is a list of the Design Objectives with a description below each regarding the proposed development:

***To enhance the sense of community by creating and maintaining places with their own distinct identity.***

Kanata North Business Park has an identity typical of technology or business parks developed in the 1990s and 2000s, with a mix of low and mid-rise modern office buildings set on large lots with at grade parking and extensive landscaped areas. Distinctive developments in the business park that set it apart include

the Brookstreet Hotel and The Marshes Golf Club. The proposal maintains the identity of the business park.

***To define quality public and private spaces through development.***

The proposed development is an example of high-quality architecture design that will be visible from public spaces, adding to the experience of the public. As a private space for the tenants, it is an efficient and flexible contemporary office building that incorporates various features that enhance the building's urban design contribution and make it a desirable place to work.

***To create places that are safe, accessible and are easy to get to, and move through.***

The proposed development improves passive surveillance because it presents an active, glazed façade to the street. In addition to the public sidewalk in front of the development, there is an internal network of walkways for employees and visitors.

***To ensure that new development respects the character of existing areas.***

Kanata North Business Park has a character typical of technology or business parks with a mix of low and mid-rise modern office buildings set on large lots with at grade parking and extensive landscaped areas. The site layout and design of the proposal respects the character of the existing business park area.

***To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice. [OMB decision #2649, September 21, 2006]***

The proposed building is a flexible design and can be occupied by a single tenant or numerous tenants. By including 2505 Solandt Road in the ZBLA, future development in the area can be more flexible and adaptable.

***To understand and respect natural processes and features in development design.***

The proposed development has been designed with understanding of the existing natural conditions as discussed in the Integrated Environmental Review in Section 6.0 of this rationale. The EIS guided the introduction of a 30m buffer to Shirleys Brook and confirmed that the ditch along the eastern side of the subject site is a maintained part of the City's stormwater infrastructure and as such does not provide habitat.

***To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.***

The proposed building is a compact form of commercial development which reduces land consumption. The building's location encourages the use of transit. The building will meet applicable energy efficiency requirements.

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OFFICIAL PLAN SECTION 4.11

In accordance with Section 4.11 of the *Official Plan*, development applications are evaluated against compatibility criteria to measure their appropriateness within the surrounding local context. The following is a compatibility analysis of the proposed development based on the relevant criteria:

**Building Design**

- 5. Compatibility of new buildings with their surroundings will be achieved in part through the design of the portions of the structure adjacent to existing buildings and/or facing the public realm. Proponents of new development will demonstrate, at the time of application, how the design of their development fits with the existing desirable character and planned function of the surrounding area in the context of:**
- a. Setbacks, heights and transition;**
  - b. Façade and roofline articulation;**
  - c. Colours and materials;**
  - d. Architectural elements, including windows, doors and projections;**
  - e. Pre- and post-construction grades on site; and**
  - f. Incorporating elements and details of common characteristics of the area.**

The proposals immediate neighbours are low-rise office buildings set far apart on large landscaped lots. The proposal maintains this separation and large landscaped areas but is more comparable in height to the towers at 515, 535 and 555 Leggett Drive which are 10 storeys. The contemporary glazed design of the proposal is consistent with these buildings.

- 6. The City will require that all applications for new development:**
- a. Orient the principal façade and entrance(s) of main building(s) to the street.**
  - b. Include windows on the building elevations that are adjacent to public spaces;**
  - c. Use architectural elements, massing, and landscaping to accentuate main building entrances.**

The proposal meets all these requirements. The main façade is glazed and faces the street and the building entry is clearly telegraphed in the design.

- 8. To maintain a high quality, obstacle free pedestrian environment, all servicing, loading areas, and other required mechanical equipment and utilities should be internalized and integrated into the design of the base of the building where possible. If they cannot be internalized these services are to be screened from public view (i.e. trees, landscaping, decorative walls and**

*fences etc.) and are to be acoustically dampened where possible. The location and operation these areas and equipment should be designed to maintain a pedestrian friendly environment and not impede public use of the sidewalk.*

All servicing, loading areas, and other required mechanical equipment and utilities are integrated into the design of the building.

- 9. Roof-top mechanical or telecommunications equipment, signage, and amenity spaces should be incorporated into the design and massing of the upper floors of the building. [Amendment #150, LPAT July 19, 2019]**

Roof-top mechanical is located behind a screening enclosure that fits with the massing of the building. This screening enclosure will be used for any future signage.

### **Outdoor Amenity Areas**

***Outdoor amenity areas are the private and communal areas of a property that are designed to accommodate a variety of leisure activities.***

- 19. Applicants will demonstrate that the development minimizes undesirable impacts on the existing private amenity spaces of adjacent residential units through the siting and design of the new building(s). Design measures include the use of transitions or terracing and the use of screening, lighting, landscaping, or other design measures that achieve the same objective.**

The closest residential area is 540m from the subject site. This separation distance ensures that the proposed development will have no impact the outdoor amenity areas of the closest residential area.

## **5.0 | PUBLIC CONSULTATION STRATEGY**

The developer met with the Ward Councillor Jenna Sudds on July 30, 2019 to brief her on the project. Since then they have been in regular contact to provide progress updates.

It is proposed to consult with the public through the legislated public consultation requirements, as outlined under 'Community and Neighbourhood Notification' at: <https://ottawa.ca/en/city-hall/planning-development-and-construction/development-information-residents/development-application-process-overview> This is considered sufficient on the basis that the proposed building is centrally located in the existing Kanata North Business Park and is 160m from the closest building and 540m from the closest residential area. Neighbours will have the opportunity to comment on the proposal, including the proposed increase to the height limit via both the SPC and ZBLA notification requirements.

## **6.0 | INTEGRATED ENVIRONMENTAL REVIEW STATEMENT**

Section 4.7 of the *Official Plan* states that *design components will be considered as inputs into the development process and must be assessed and considered prior to establishing an initial design or lot pattern*. Section 4.7.1 states that *the integrated environmental review considers as a whole the significant*



*findings from individual support studies ... [and] ensures that development proceeds in keeping with the analysis and recommendations of any watershed and subwatershed studies and federal or provincial environmental assessments documents, where applicable.*

The following technical studies have been prepared in accordance with Section 4.7 of the *Official Plan* and as per the City's recommendations at the pre-consultation meeting.

## 6.1 PHASE 1 ENVIRONMENTAL SITE ASSESSMENT

A Phase 1 Environmental Site Assessment (ESA) was prepared by Golder (Report No. 19125909, dated August 2019). The purpose of the ESA was to assess the past and present uses of the Subject site and adjacent properties to identify any environmental concerns that may impact the Subject site. The ESA concludes that no potentially contaminating activities (PCAs) were identified and that a Phase II ESA is not required for the Subject site.

## 6.2 ENVIRONMENTAL IMPACT STATEMENT & TREE CONSERVATION REPORT

A combined Environmental Impact Statement and Tree Conservation Report was prepared by Muncaster Environmental Planning Inc. dated August 28, 2019 to assess the potential for any Species at Risk on the Subject Site. The main findings from the EIS/TCR report are:

- Most of the site is currently forested, following agricultural use until the 1980s. The young forest is small and is disturbed by non-native species and cutting. No specimen trees or potential wildlife cavity trees were observed in the forest. There are no off-site forests contiguous with the on-site forest;
- The fish habitat in Shirley's Brook and potential Blanding's turtle habitat are the significant natural heritage features, as identified in the City of Ottawa Official Plan and the Provincial Policy Statement, associated with the site. The proposed development is not anticipated to impact these features with a 30m natural setback retained from the normal highwater mark of the channel;
- Other than barn swallows flying over the golf course to the north, no Species at Risk were observed for the site, including no butternut observations on or adjacent to the site. No potential structures for chimney swift or barn swallow are present. No butternut trees were noted on or within 50 m of the Subject Site; and
- Due to the footprint of the proposed office building, associated surface parking, and required urban servicing and associated grading, no tree retention is anticipated for the site outside of the Shirley's Brook corridor. A permit will be required under the City of Ottawa's Tree By-law No. 2009-200 prior to removal.

### 6.3 SITE SERVICING

A servicing brief was prepared by Novatech to detail the existing and proposed servicing conditions for the subject site. The report concludes that water, wastewater and stormwater systems required to develop the subject site are designed in accordance with the MOE and City of Ottawa's current level of service requirements. Stormwater management is via surface storage in the proposed parking lot before it flows to Shirleys Brook.

### 6.4 GEOTECHNICAL STUDY

A Geotechnical Study was prepared by Golder Associates Ltd. (Report No. 18111016, dated September 2019) to determine the general soil and groundwater conditions across the site. The report concludes that the subsurface conditions on the subject site generally consist of topsoil and silty sand overlying a deposit of silty clay to clay over a discontinuous deposit of glacial till over sandstone bedrock. Based on these findings, the report concludes that there are no issues that would preclude development from occurring on the subject site and provides recommendations in support of the proposed development. Please see the report for details.

## 7.0 | CONCLUSION

It is our assessment that the proposed development is consistent with the *Provincial Policy Statement 2014* and conforms to the *City of Ottawa Official Plan* and other relevant guidelines. This planning rationale, along with the associated technical studies, supports the proposed development. Office is a permitted use and the proposed building is appropriate in the Kanata North Business Park.

The proposed exception to accommodate the height of the proposed eight storey building can be approved as it allows for more efficient land use and concentration of employment. It is consistent with the four adjacent 10 storey office buildings at 515, 535 and 555 Legget Drive and the 18 storey Brookstreet Hotel at 525 Legget Drive.

The proposed development is an appropriate and desirable addition to the community and represents good planning.

Sincerely,

**Novatech**



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