

Petries Landing II
Building 8
8466 Jeanne D'arc Boulevard
Proposed Zoning By-Law Amendment and Site Plan
Planning Rationale Report and Design Brief - Revision
April 2021

Prepared for

Brigil

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1. Background

This report is intended to provide the necessary planning background and rationale in connection with a revised proposal to build a 4 storey 112-unit residential unit building at Brigil's Petries Landing II (PL2) project located at 8466 Jeanne D'arc Boulevard. (Please note that the previous submission was in connection with a 10 storey 214 residential unit proposal).

The site intended to accommodate this building (referred to as the Building 8 site) is currently subject to an approved site plan dated September, 2018 (Revision L) and a site plan agreement registered as instrument number OC1576807 for a 93 unit retirement home. The proposal calls for a revision to this approved site plan to recognize the plan referred to in this report. The proposal will also necessitate the approval of a zoning by-law amendment to permit an increase in density; both the zoning by-law amendment and site plan applications which have been submitted along with this report.

2. Site Context and Existing Land Use

The subject property is part of Block 2 of Plan 4M 1425 (see Figure 1). Plan 4M 1425 effectively created two development blocks (labeled 1 and 2) as well as a public street known as Prestige Circle for the project known as Petries Landing II. The subject property is also known as Part 3 of Plan 4R-31527 as shown on Figures 2 and 3.


Subject property shown as 

Figure 1: Plan 4M 1425

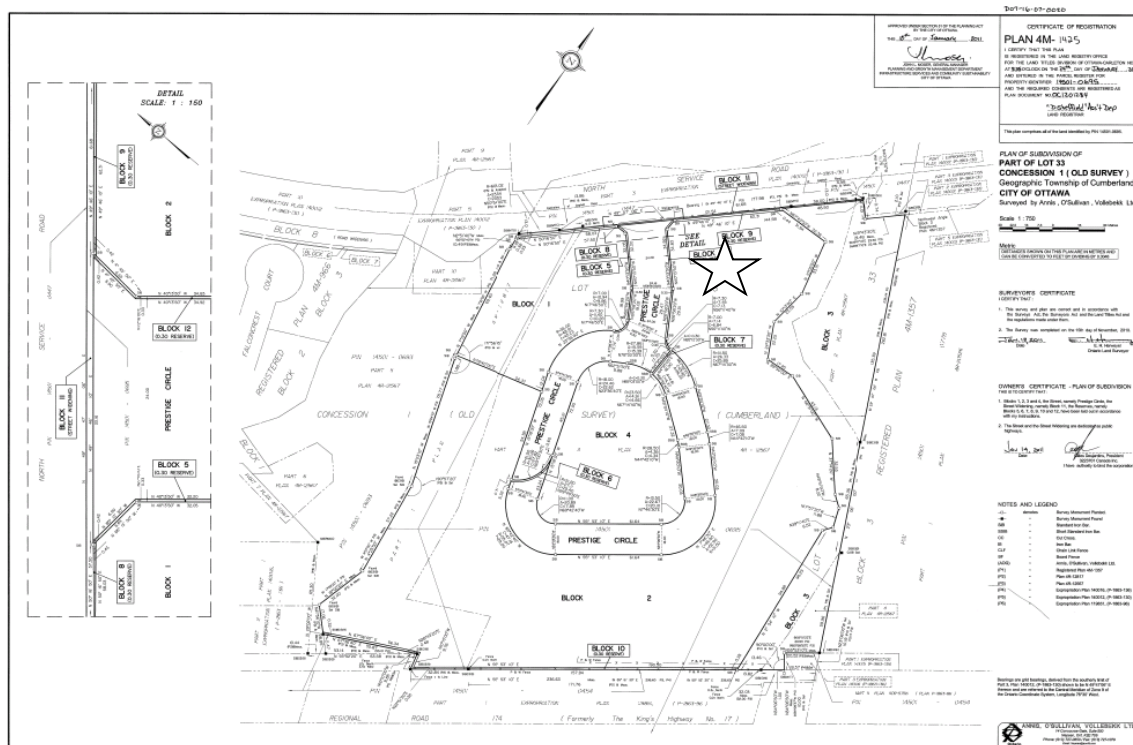


Figure 2: Plan 4R-31527

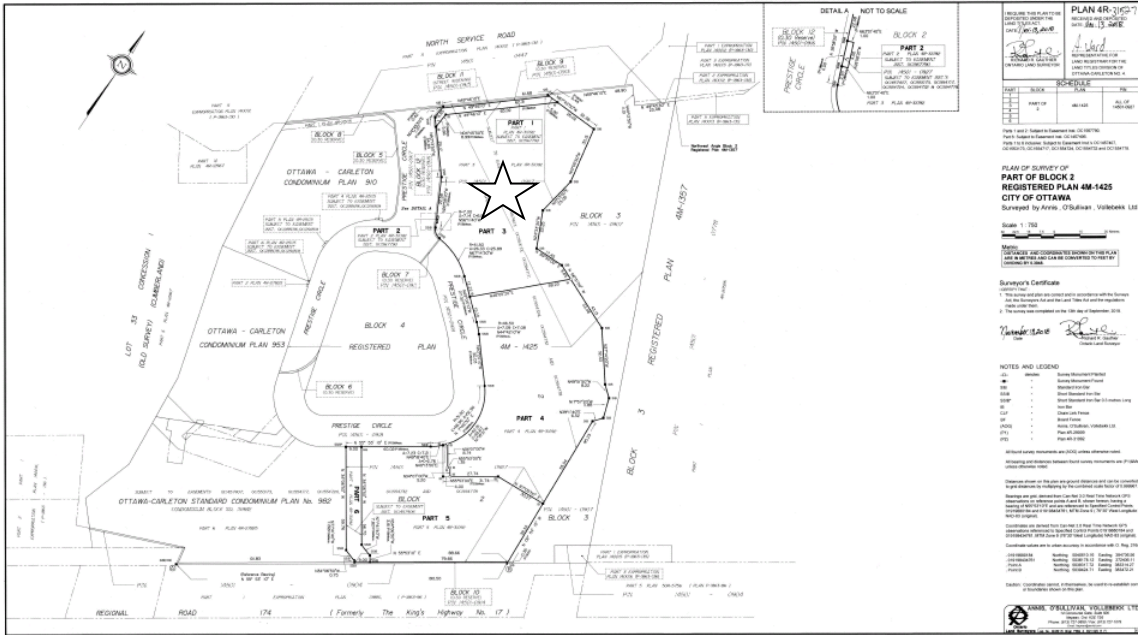
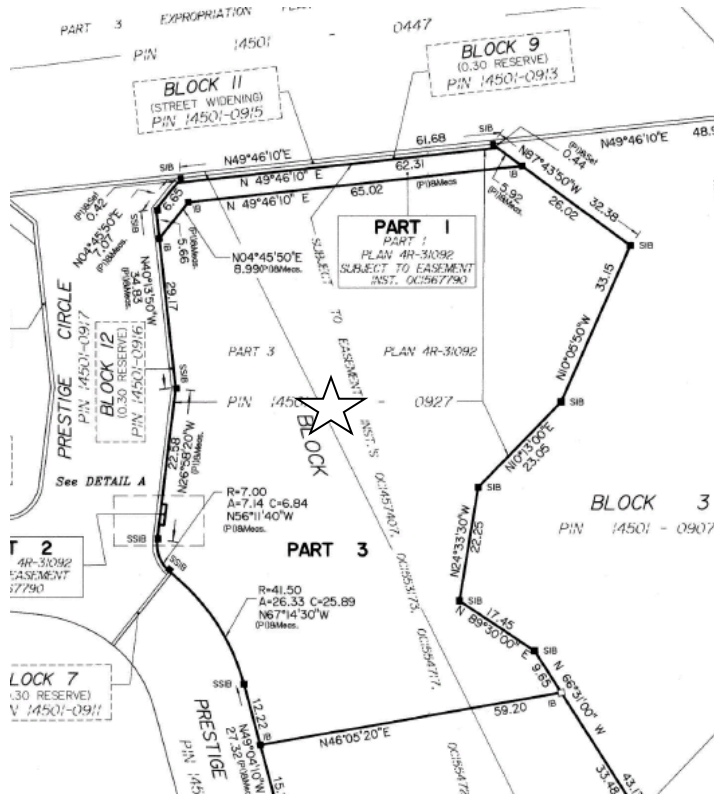


Figure 3: Plan 4R-31527 (Detail)



To date, Prestige Circle has been built as well as 2 residential buildings on Block 1 and 4 residential buildings on Block 2; construction is underway for one other buildings on Block 2 as contemplated in the approved site plan.

The property that is subject to this application is vacant.

The area around the subject property consists of the following uses:

- South: land under development to accommodate multi residential development as contemplated in the approved Petries Landing II site plan; further south is Highway 174;
- North: Jeanne D'arc Boulevard (formerly known as North Service Road) as well as open space connected to the nearby Ottawa River;
- West: Prestige Circle and existing multi residential development as contemplated in the approved Petries Landing II site plan;
- East: Bellevue Creek ravine and residential development further to the east.

3. Planning Context

City of Ottawa Official Plan (on line consolidation per December 2019)

The subject property is designated 'General Urban Area' in accordance with Schedule B of the City of Ottawa Official Plan. According to policy 3.6.1.1 of the Official Plan '*the General Urban Area designation permits all types and densities of housing, as well as employment, shopping, service, industrial, cultural, leisure, park and natural areas, entertainment and institutional uses*'.

Zoning Bylaw 2008-250

According to City of Ottawa Zoning By-law 2008-250, the subject property is zoned R5A [1409] which permits a variety of residential uses including apartment dwellings low rise and planned unit developments as proposed. See Figure 4. The R5A standards are presented in Table 1.

Figure 4: City of Ottawa Zoning By-law 2008-250



Table 1

I Sub-Zone	II Prohibited Uses	III Conditional Uses	IV Principal Dwelling Types	V Minimum Lot Width (m)	VI Minimum Lot Area (m ²)	VII Maximum Building Height (m)	VIII Minimum Front Yard Setback (m)	IX Minimum Corner Side Yard Setback (m)	X Minimum Rear Yard Setback (m)	XI Minimum Interior Side Yard Setback (m)	XII End-notes (see Table 164B)
A	Duplex, Detached, Linked-detached, Semi-Detached	Ancillary Uses ¹	Planned unit development	18	1,400	As per dwelling type	6	4.5	varies ²	varies ²	1,2
			Apartment dwelling, mid rise, Apartment Dwelling, high rise (By-law 2014-292)	25	1,000	varies ⁵¹	6	4.5	7.5	7.5	1, 51
			Apartment dwelling, low rise, Stacked	18	540	16	6	4.5	6	3	1
			Three Unit	18	540	11	6	4.5	6	1.5	1
			Townhouse	6	180	11	6	4.5	6	1.5	1

The [1409] exception, in turn, establishes site specific standards for the property including a maximum density and 100 units per hectare and a minimum density of 75 units per hectare. The details of exception 1409 are as follows:

- no building is permitted within 10.0 m of the lot line adjacent to North Service Road
- minimum required front yard for the end wall of a row dwelling is 4.0 m
- minimum required front yard where garages face a public street is 6.0 m
- minimum required front yard for all other buildings is 6.0 m
- minimum required rear yard is 7.5 m

- minimum required building spacing is 3.0 m
- maximum permitted building height is 10 storeys
- minimum required density is 75 units per hectare
- maximum permitted density is 100 units per hectare
- minimum distance between the rear wall of a row dwelling and a O1 zone is 7.5 m
- minimum distance between the end wall of a row dwelling and a O1 zone is 2.0 m
- minimum distance between apartment dwellings under 7 stories in height and a O1 zone is 7.5 m
- minimum distance between apartment dwellings 7 to 10 stories in height and a O1 zone is 10 m
- minimum required western side yard abutting retaining wall for all buildings is 6.0 m
- minimum required western side yard south of retaining wall for all buildings is 8.0 m;
- the dwelling units and land in a row dwelling development may be divided or severed into separate, legally conveyable parts without each part meeting all of the requirements of this by-law as long as the overall development complies with the requirements of this by-law
- subsection 109(3) does not apply

Subdivision Agreement:

The property is subject to a Subdivision Agreement dated August 30, 2010 which, in turn, includes standard development conditions which the owner must adhere to. In addition, a number of special clauses are provided including Schedule F Condition 11 in Schedule F which speaks to the issue of satisfying the minimum and maximum density requirements per the Zoning By-law. It states as follows:

Zoning By-Law Density – Block 2

“The Transferee of Block 2 or a portion thereof and or the remainder of the Subdivision for himself, his heirs, executors, administrators, successors and assigns acknowledges being advised that in order to meet the density requirements of Zoning By-law No. 2008-250 the construction of a minimum of 283 units to a maximum of 391 units on Block 2 is required. The Transferee further acknowledges being advised that Block 2 may be developed in phases and a concept plan will be required to be submitted with each site plan application. This concept plan must outline how the minimum density requirement will be achieved for Block 2 in its entirety.”

Site Plan:

The site intended to accommodate this building is currently subject to an approved site plan dated September 2018 (Revision L). See Figures 5 and 6.

Figure 5: Approved Site Plan Currently in Effect

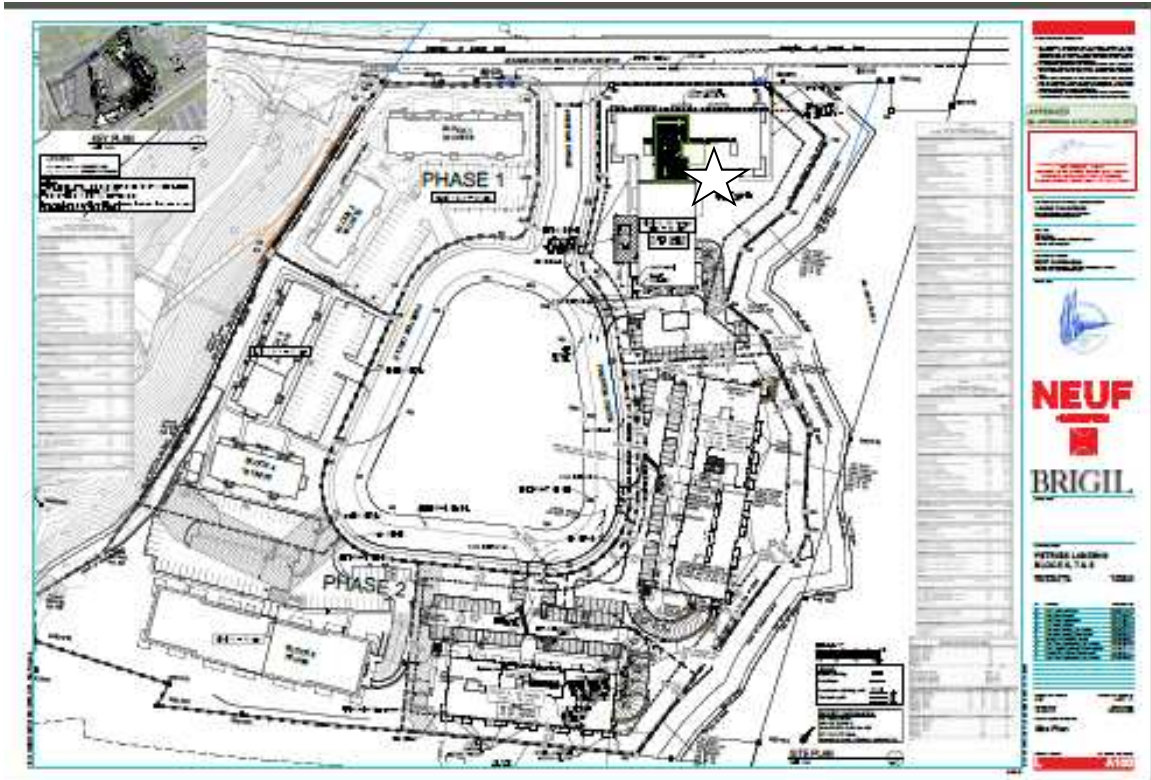
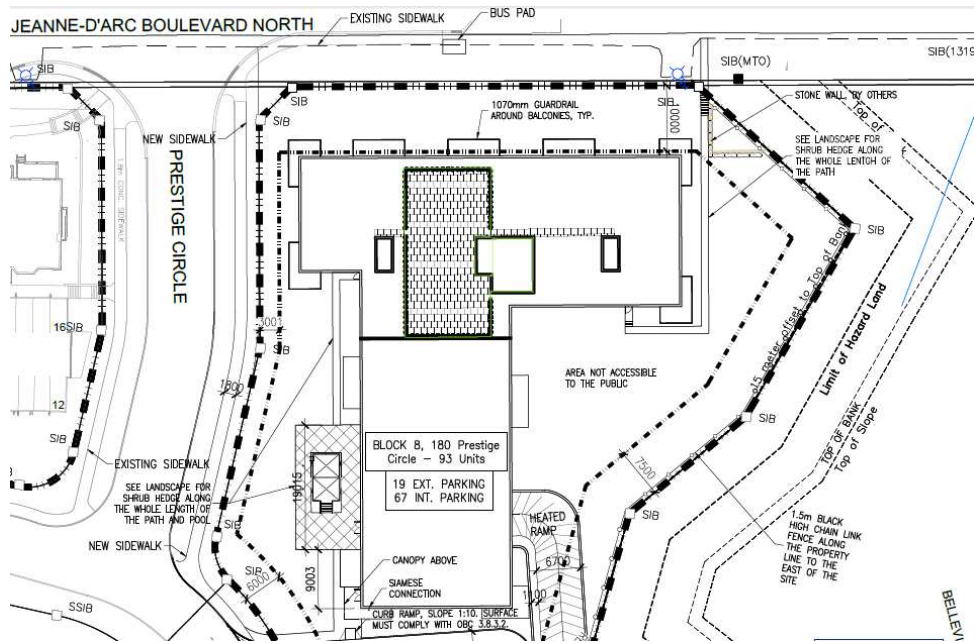


Figure 6: Approved Site Plan Currently in Effect (Detail)



4. Proposal and Required Approvals

The proposal calls for the construction of a 4-storey building intended to accommodate 112 units with corresponding amenity space and 1 floor of underground parking. With a total gross floor area (GFA) of around 9,346 square meters, the proposed building is intended to accommodate a variety of unit sizes as detailed in Table 2.

Table 2: Proposed Unit Breakdown

Unit type	Total unit	Average area/ unit (sq. m.)	Average area/ unit (sq. ft.)	Total area in the building (sq.m.)	Total area in the building (sq.ft.)
Studio	2	53,6	577,0	107,2	1153,9
1 bedroom	90	78,7	847,6	7078,2	76189,7
2 bedrooms	16	106,9	1150,9	1711	18417,2
3 bedrooms	4	112,5	1211,0	449,8	4841,6
Total	112			9346,2	100602,5

Interior communal amenity space is provided at ground level and roof top level which provides tenants with different and unique viewpoints of the surrounding natural environment.

A combined parking unit ratio of 1.4 based on 1.2 for residents and .2 for visitors will be provided in full compliance with Zoning Bylaw 2008-250. In total, the project includes 135 resident parking spaces and 23 visitor parking spaces for a total of 158 spaces of which the majority will be accommodated underground. The above ground parking is mostly located behind the building, hidden from the public street which allows for building to stand out further and ensures a safe and pleasant pedestrian realm. In addition, 56 bicycle parking spaces are also provided with the majority (41 spaces) located in the underground parking garage.

The building's orientation follows property lines facing Jeanne-D'Arc and Prestige Circle to help frame the street, emphasize the building's visibility at the intersection and reduce shadowing on adjacent buildings.

Site accesses from Prestige Circle as contemplated in the existing approved site plan. The entrance to the underground parking is accessed from the interior of the site from a bi-directional access.

Entrance to the building lobby is in proximity to the proposed exterior parking area for added functionality and to facilitate drop-offs, deliveries and pick-ups to residents. Two pedestrian connections are proposed along Prestige Circle and along Jeanne D'Arc to maximize site pedestrian circulation.

See Figure 7 proposed site plan.

Figure 7: Proposed Site Plan

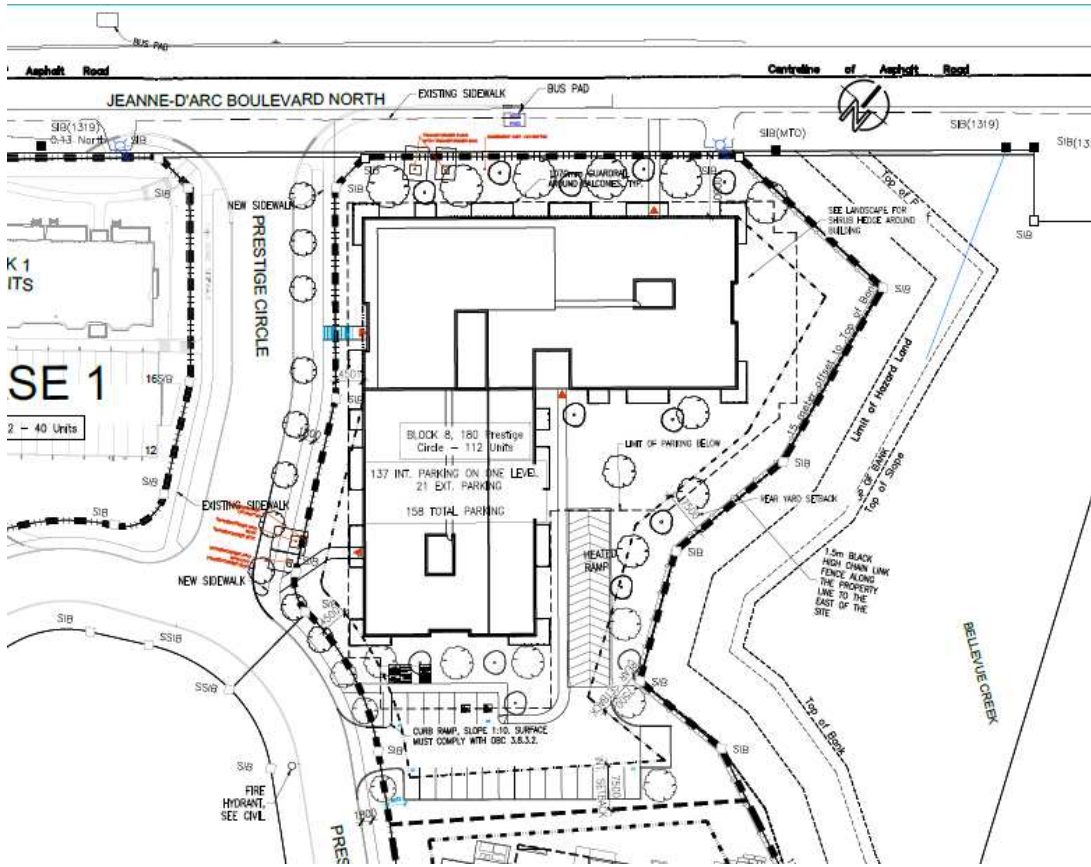


Figure 8: Proposed Elevations



Figure 9: Proposed Perspective South Side



Figure 10: Proposed Perspective West Side



Required Approvals

Zoning Bylaw Amendment

A zoning compliance chart (see Appendix A) has been prepared to demonstrate how each of the applicable standards of Zoning By-law 2008-250 comply with the proposed site plan. Based on a review of this chart, a zoning bylaw amendment will be required to permit an increase in density from a maximum of 100 units per hectare to 103 units per hectare.

Site Plan Approval

In addition to the required zoning change, a revision to the approved site plan will be required.

Subdivision Agreement Amendment

The property is subject to Schedule F Condition 11 of the subdivision agreement which requires that Block 2 of Plan 4M-1425 provide between 283 and 391 units. Please note that the total number of units for Block 2 per Plan 4M-1425 will be as follows: 116 existing units for buildings 3, 4 and 5, 79 units for building 6 and 92 units for building 7 as well as 112 proposed units for building 8 which provides for a total of 399 units. (see Appendix A). As this exceeds the upper range referred to in the subdivision agreement, an amendment to this agreement may be required.

Transportation Impact Assessment Report

According to the updated Transportation Impact Assessment Report prepared by Parsons and dated February 23, 2021 'the proposal at 8466 Jeanne D'Arc Boulevard Block 8 of Petrie's Landing II development is recommended from a transportation perspective' in keeping with the original analysis.

Site Servicing and Stormwater Management Brief

According to the revised Site Servicing and Stormwater Management Brief prepared by Stantec and dated March 26, 2021, the proposal can be serviced to City requirements as discussed below:

WATER SERVICING

The 200 mm diameter watermain on Prestige Circle provides adequate fire flow capacity as per the Fire Underwriters Survey. The service connections will also be capable of providing anticipated demand but exceeds the maximum objective pressure of 552 kPa (80 psi). Therefore, pressure reducing measures, such as a pressure reducing valve, will be required to service the proposed building per the Ontario Plumbing Code. The building will not require a booster pump to provide pressures greater than 40psi to the higher floors.

SANITARY SERVICING

The proposed sanitary sewer lateral is sufficiently sized to provide gravity drainage for the site. The proposed site will be serviced by a 200 mm diameter service lateral directing wastewater flows to the existing 300 mm dia. Prestige Circle sanitary sewer. A backflow preventer will be required for

the proposed building in accordance with the Ottawa sewer design guidelines and will be coordinated with building mechanical engineers. The proposed sanitary drainage pattern is in accordance with the City of Ottawa Sewer Design guidelines.

STORMWATER SERVICING

The proposed stormwater management plan is in compliance with the goals specified through the stormwater management section of IBI Group's Design Brief for Petrie's Landing and with the City of Ottawa Design guidelines. Rooftop, underground pipe, and surface storage in combination with ICDs are proposed to limit inflow from the site area into the minor system to the required target release rate.

The proposed building will have underground parking and as such, it is recommended that the proposed parking ramp be equipped with trench drains to capture the 100-year runoff. The proposed parking deck area F1002B will have a catchbasin connected to the internal plumbing of the building to capture the 100-year runoff. In addition, it is recommended that the proposed building be equipped with a sump pump and a backwater valve.

GRADING

Grading for the site has been designed to provide an emergency overland flow route as per City requirements and reflects the overall recommendations provided in the Geotechnical Investigation. Further geotechnical recommendations will be included in the next submission.

UTILITIES

All utilities (Hydro Ottawa, Bell Canada, Rogers Ottawa, and Enbridge Gas) have existing plants in the subject area. Exact size, location and routing of utilities will be finalized after design circulation.

5. Planning Rationale

The density increase is required as the current zoning limits the density to 100 upha and the proposal will generate a revised density of 103 upha. The following breakdown of yield and area explains the resulting revised density for this site:

Yield:

Buildings 1 and 2: 40 units

Buildings 3-5: 116 units

Building 6: 79 units

Building 7: 92 units

Building 8: 112 units (proposed)

Total : 439 units

Area:

Block 1 site area: .6101 ha

Block 2 site area: 3.6739 ha

Total site area: 4.284 ha

Density:

439 units / 4.284 ha which works out to 102.47units per ha. (rounded to 103 units per ha)

The proposed density increase is a function of the proposed unit size breakdown and the market demand for smaller units.

When the zoning for PL2 was established, a R5 zone was created to permit towns and apartments on the property up to 10 floors within a density range of 75 to 10 upha. At the time this zoning was established the market contemplated a mix of towns and apartments.

Since the R5 zoning was initially established for this property (ie in 2006), both market conditions and the planning policy context for the area have changed. The market for smaller affordable units has increased and thus only apartments/condos have been built to date; the market for such units is still active and thus the proposal before you. The planning policy context for the area has also changed since the original approval with the inclusion of several policy directives encouraging intensification on urban lands and the provision of more affordable units; this planning policy context change also included the City's decision to accommodate the LRT transit corridor along Hwy 174 with a planned station at Trim and Hwy 174.

In addition to the above the following section examines how the proposed zoning change to increase the site's density complies with the City of Ottawa Official Plan.

5.1 City of Ottawa Official Plan

The City of Ottawa Official Plan provides a complete set of policies and guidelines to evaluate development proposals such as the one that is subject to this report. The following review is based on the City of Ottawa's consolidated Official Plan available on line in December 2019 and includes amendments up to and including no. 234.

Section 1.3 – The Challenge Ahead

The City also needs to pursue a more affordable pattern of growth based on higher densities and increased use of transit. This pattern allows for more efficient use of municipal infrastructure and reduces the need to build and maintain roads over their life-cycle. This pattern is compact and allows for more efficient delivery of municipal services such as solid waste collection and emergency services that are costly to provide over large areas.

Comment: The proposed minor increase in density represents a *more affordable pattern of growth* in keeping with this goal. In addition, the proposal represents a slightly *more efficient use of municipal infrastructure*.

Section 2.5.1 re Urban Design and Compatibility

This section of the Official Plan contains design objectives intended to be applied to new developments.

To enhance the sense of community by creating and maintaining places with their own distinct identity.

Comment: The subject property is strategically located along the Ottawa River and near Petries Island thus affording an opportunity to accommodate individuals who want to enjoy living in a community near these distinctive natural amenities.

To define quality public and private spaces through development

Comment: The proposal will frame the municipal park across the street and will relate directly to the open space along the Ottawa River.

To create places that are safe, accessible and are easy to get to, and move through.

Comment: The proposal incorporates a walkway that will connect directly to the sidewalk located on the south side of Jeanne D'arc Boulevard as well as the sidewalk located on the east side of Prestige Circle, at two locations, thus allowing residents of this project with safe access to the abutting public space.

To ensure that new development respects the character of existing areas.

Comment: The proposal calls for the creation of 4 storey multi residential building similar in height to what already exists in this area.

To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.

Comment: The proposed building will accommodate an elevator which will allow a full range of age groups to adapt to this space over time if desired.

To understand and respect natural processes and features in development design

Comment: The proposal recognizes it's strategic importance next to the Ottawa River and orients the building to allow many of the units to have a view of the river and natural features in the area.

To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.

Comment: The proposed development represents a form of shared wall construction using state of the art energy efficient building practices. In addition, the subject property will soon benefit from the extension of the LRT with a station at Trim Road and Hwy. 174 which in turn affords the opportunity to reduce transportation energy consumption relative to car travel. This station will be accessible by OC Transpo bus route 38 available immediately adjacent to the site or by walking.

The City of Ottawa Official Plan provides a complete set of policies and guidelines to evaluate development proposals such as the one that is subject to this report. This section of the report assesses how the proposed development conforms to section 4.11 of the City of Ottawa Official Plan policies and guidelines.

- *Policy 4.11 (Urban Design and Compatibility)*

1. *A Design Brief will be required as part of a complete application, except where identified in the Design Brief Terms of Reference. The focus of this Brief will vary depending on the nature of the development. The Brief shall evaluate consistency and demonstrate that the following content is considered and/or incorporated into the development proposal with:*

1. *The provisions of this Plan that affect the design of a site or building;*

2. *Design Guideline(s) approved by Council that apply to the area or type of development; and*
3. *The design provisions of a community design plan or secondary plan.*

[Amendment #150, LPAT July 19, 2019]

Comment: This report in conjunction with the design submission by Neuf Architects is intended to satisfy the Design Brief requirements of the Official Plan. With respect to item 3), there is no community design plan or secondary plan in place for this area.

Views

Depending on its location, the mass or height of new development may enhance or impact the views visible from public view points, such as public monuments, bridges, civic spaces, landforms, and other valued spaces. View corridors and view planes can be established to guide and regulate the height and mass of development within a defined area, so as to protect the public view.

2. *Development applications for all High-Rise 31+ buildings will demonstrate how the proposed building will contribute to and enhance the skyline of the city and existing prominent views or vistas or create new vistas. Community design plans or other plans approved by Council may identify prominent important views. Skyline is defined in Section 2.5.6, policy 14.*

Comment: Not applicable as the proposed building is only 4 floors in height.

3. *The City will protect the views of the Parliament Buildings from two locations in Beechwood Cemetery. The view area, or viewshed, and the two locations, the Tommy Douglas Memorial and Poet's Hill, are identified on Annex 12. New buildings or structures should be located to compliment or enhance the view of these important landmark buildings. A building or structure is deemed to obstruct the view if it visually blocks the foreground view or visually changes the background silhouette of the Parliament Buildings when viewed from the identified locations. For each property in the viewshed, no Zoning By-law amendment or minor variance shall be permitted that would permit a proposed building to obstruct the view unless it is demonstrated that the view is*

- already impacted and would not be further impacted by the proposal. Site plan control approval, other regulations and City maintenance practices may also be adjusted to ensure that fences, signs, trees and other elements do not obstruct the view.*
4. *Policies to protect views of the Parliament Buildings and other national symbols that apply to development applications in the Central Area are contained in Section 3.6.6 Central Area. [Amendment #150, LPAT July 19, 2019]*

Comment: Not applicable as the proposed building is not within sight of the Parliament Buildings.

Building Design

Good building design contributes to successful neighbourhood integration and the compatibility of new development with the existing or planned character of its surroundings. The façades of buildings influence the feel and function of public spaces and define the edges of the pedestrian environment. Good building design is required throughout the city. In the City's design priority areas and areas subject to the design priority policies, building design is intended to support the image of Ottawa as a Capital city and contribute to a positive experience for residents and visitors.

5. *Compatibility of new buildings with their surroundings will be achieved in part through the design of the portions of the structure adjacent to existing buildings and/or facing the public realm. Proponents of new development will demonstrate, at the time of application, how the design of their development fits with the existing desirable character and planned function of the surrounding area in the context of:*
 1. *Setbacks, heights and transition;*
 2. *Façade and roofline articulation;*
 3. *Colours and materials;*
 4. *Architectural elements, including windows, doors and projections;*
 5. *Pre- and post-construction grades on site; and*
 6. *Incorporating elements and details of common characteristics of the area.*

Comment: Architectural treatments such as materiality, colours and projections have been carefully chosen to be compatible with the surrounding while contributing to high-quality architecture.

The proposed building has been re-designed to match the height of existing buildings in PL2 and within the permitted height zoning standards. Setbacks are maximized while maintaining an adequate proximity to the street.

Balconies are positioned and grouped to articulate the façade along its length. These articulations will also allow for the building to be visually interesting. Rooftop amenities have been provided to mitigate overlook issues.

Given the site's context of primarily earth-tone low-rise buildings, a careful arrangement of materials have been chosen. Main materials include white and light gray panels and masonry which help to lighten up the structure. Wood panels are also proposed in areas to contrast the monotone colour pallet of the main cladding materials.

Doors and windows are positioned in a way to create inviting entrances to the building. The doors and windows are also similar in size to existing surrounding constructions.

6. The City will require that all applications for new development:

- 1. Orient the principal façade and entrance(s) of main building(s) to the street.*
- 2. Include windows on the building elevations that are adjacent to public spaces;*
- 3. Use architectural elements, massing, and landscaping to accentuate main building entrances.*

Comment: The proposed building is oriented both north-south along Prestige Circle, and east-west along Jeanne D'arc Boulevard. As a result, both street facing facades have been designed and treated as principal facades. The main entrance is connected by a concrete pedestrian walkway to Prestige Circle, with easy access to Jeanne D'Arc Boulevard and the existing transit stop. In addition, a main secondary entrance is provided in the rear of the building to accommodate occupants and visitors arriving by automobile. All building entrances are linked with a seamless connection to existing public sidewalks and are highlighted using canopies as

well as landscaping. Significant landscaping is provided along the existing public sidewalk at pedestrian level to maximize the relation with the natural surroundings.

7. *The intersections of arterial and collector roads can serve as gateways into communities and can support high levels of pedestrian and vehicular traffic, the greatest density of housing, and other land uses and services, and commercial services and other land uses that are focal points for a community. The City will encourage development proposals at such locations to include the following:*

1. *Strong architectural design elements that feature the corner or street axis by: locating buildings close to the street edge, and/or orienting the highest and most interesting portion of a building (e.g. the main entrance) to the corner or axis which has a view of the terminus.*
2. *Capitalizing on design possibilities for both street façades (by wrapping the materials used on the front façade around the building where any façades are exposed to the public realm); and*
3. *Soft landscaping features, special paving materials, and/or curb extensions to shorten the distance across the street and larger sidewalk area to accommodate sidewalk activity.*

Comment: While the proposal is not located at the intersection of a collector and arterial road, it is located at the intersection of Prestige Circle being a local road and Jeanne D'arc Boulevard which is a collector. As such the property represents a local gateway location. To this end, the building's design proposes facades exposed the public streets are wrapped in the same materials to provide continuity and coherence. The landscaping along both building frontages will enhance the area by providing buffering and future tree canopy to compliment the architecturally enhanced facades that augment the pedestrian realm. Articulating projections (balconies) along the existing sidewalk on Prestige Circle creates architectural variety which provide visual interest to pedestrians.

8. *To maintain a high quality, obstacle free pedestrian environment, all servicing, loading areas, and other required mechanical equipment and utilities should be internalized and integrated into the design of the base of the building where possible. If they cannot be internalized these services are to be screened from public view (i.e. trees, landscaping, decorative walls and fences etc.) and are to be acoustically dampened where possible. The location and operation these areas and equipment should be designed to maintain a pedestrian friendly environment and not impede public use of the sidewalk.*

Comment: The residential use will not require a loading or service area. Outdoor storage is not proposed as part of this application. The buildings garbage/recycling area is located at basement level and will be accessible by the residents. A temporary loading area is however proposed at the east corner of the property which will be used to store garbage and recycling containers on pickup days only. Both building facades that interact with public sidewalks are free of obstacles that might create a visual nuisance or safety issue. Furthermore, the existing sidewalk on Prestige circle and Jeanne D'Arc Boulevard are located a generous distance away from the building. If mechanical components are required for the basement, landscaping or decorative screens will be utilized to hide them from public view.

9. *Roof-top mechanical or telecommunications equipment, signage, and amenity spaces should be incorporated into the design and massing of the upper floors of the building.*
[Amendment #150, LPAT July 19, 2019]

Comment: The proposed design calls for a rooftop amenity space that will deliver impressive panoramic views of the river. All rooftop interior space as well as mechanical equipment are incorporated in the building's massing which is located towards the corner intersection to help feature that section of the building as well as the street axis.

Massing and Scale

Complementary to building design, the massing and scale of new development also contributes to successful neighbourhood integration and the compatibility of new development with the character of the surrounding community. Massing and scale describe the form of the building,

how tall it is, how much of the lot it occupies and how it is positioned in relation to the street and surrounding buildings.

10. Where a secondary planning process establishes criteria for compatibility of new development or redevelopment in terms of the character of the surrounding area, the City will assess the appropriateness of the development using the criteria for massing and scale established in that Plan. Where there are no established criteria provided in an approved Plan, the City will assess the appropriateness of the proposal relying upon its approved Design Guidelines, as applicable, and the following criteria:

- 1. Building height, massing and scale permitted by the planned function of adjacent properties as well as the character established by the prevailing pattern of abutting development and development that is across the street;*
- 2. Prevailing patterns of rear and side yard setbacks, building separation and landscaped open spaces and outdoor amenity areas as established by existing zoning where that pattern is different from the existing pattern of development;*
- 3. The need to provide a transition between areas of different development intensity and scale as set out in policy 12 of this section.*

Comment: There is no community design plan or secondary plan in place for this area.

11. The City may require a Shadow Analysis and/or Wind Analysis as part of a complete application, except where identified in the Wind/Shadow Terms of Reference. The study(s) will evaluate the potential impacts of the development on the adjacent properties and pedestrian amenity areas. The intent of each Analysis is to demonstrate how these impacts have been minimized or avoided.

Comment: Shadow analysis has been performed internally to evaluate the impacts of the building's mass and proximity to existing structures. The building's orientation and placement on the property helps to avoid any impacts related to shadow projection. In addition, given that there is no construction located in the North-West and North-East quadrants, impacts to existing or future structures are practically non-existent.

12. Transition refers to the integration of buildings that have greater height or massing than their surroundings. Transition is an important building design element to minimize conflicts when development that is higher or has greater massing is proposed abutting established or planned areas of Low-Rise development. Proponents for developments that are taller in height than the existing or planned context or are adjacent to a public open space or street shall demonstrate that an effective transition in height and massing, that respects the surrounding planned context, such as a stepping down or variation in building form has been incorporated into the design.

Comment: The proposed building's massing has been shaped to respect the surrounding context. The proposed building height matches the surrounding constructions. The building design placement on the site distances itself from the surrounding existing constructions and helps to minimize integration conflicts. A distance of approximately 36 meters separates the proposed building with existing constructions located to the South-East (Block 7) and South-West (Block 1).

13. Building height and massing transitions will be accomplished through a variety of means, including:

- 1. Incremental changes in building height (e.g. angular planes or stepping building profile up or down);*
- 2. Massing (e.g. inserting ground-oriented housing adjacent to the street as part of a high-profile development or incorporating podiums along a Mainstreet);*
- 3. Building setbacks and step-backs. [Amendment #150, LPAT July 19 ,2019]*

Comment: The building massing of the proposed development pays careful attention to the existing character of the area. To respect massing between the proposed building and the public realm, there has been a significant reduction in the proposed building height from earlier submissions. In addition, the use of more sizable and monumental components as well as coloured material on the façade help to accentuate the building.

14. Generally, High-Rise buildings, which consist of three integrated parts, a base, a middle and a top, can achieve many of the urban design objectives and address the impacts described above in the following ways;

1. *The base of a high-rise building should respect the scale, proportion, and character of the surrounding buildings, adjacent streets, parks, and public or private open spaces and animate such spaces.*
2. *The tower, which typically includes a middle and a top, should step back from the base where possible. The tower design can reduce the building impacts identified above by incorporating an appropriate separation from existing or future adjacent towers located on the same lot or on an adjacent lot. The responsibility for providing an appropriate tower separation shall generally be shared between owners of abutting properties where high-rise buildings are permitted. A separation distance of 23m has been the City's general guidance but actual separation requirements may vary in different parts of the City depending on the context.*
3. *Floor plates may also vary depending on the uses and the context. Generally, towers with a larger floor plates may require a greater separation from adjacent towers.*

Comment: Not applicable as the proposed building is only 4 storeys.

15. *Secondary Plans may provide area-specific directions for the design of high-rise buildings.*

Comment: There is no community design plan or secondary plan in place for this area.

16. *The Zoning By-law will establish performance measures such as minimum tower separation distances and yard setbacks and may require minimum lot sizes for High-Rise buildings. Proposals for a high-rise building that include performance measures that deviate from the Zoning By-law shall demonstrate that the impacts identified in policy 14 can be satisfactorily avoided or reduced.*

Comment: The proposed density increase is minor in nature representing an increase of 3 %.

Outdoor Amenity Areas

Outdoor amenity areas are the private and communal areas of a property that are designed to accommodate a variety of leisure activities.

- 19. Applicants will demonstrate that the development minimizes undesirable impacts on the existing private amenity spaces of adjacent residential units through the siting and design of the new building(s). Design measures include the use of transitions or terracing and the use of screening, lighting, landscaping, or other design measures that achieve the same objective.*

Comment: Given the nature of surrounding properties, there is no adverse impact on adjacent outdoor amenity areas surrounding the subject property. Only 8 units of the Building 7 property have balconies or terraces that partially give towards the proposed development since they wrap around the corners. In response to this context, the building is positioned more than 28 meters away from the South-East property line and 34 meters from the nearest balcony of the Block 7 property. Amenity space for the Block 1 property do not face the proposed development. Furthermore, shadows are minimal and projected away from all existing and future surrounding construction. While respecting City requirements, site lighting will not only provide a safe and secure environment but also ensure no adverse impacts on adjacent properties.

- 20. Applications to develop residential or mixed-use buildings incorporating residences will include well-designed, usable amenity areas for the residents that meet the requirements of the Zoning By-law, and are appropriate to the size, location and type of development. These areas may include private amenity areas and communal amenity spaces such as: balconies or terraces, rooftop patios, and communal outdoor at-grade spaces (e.g. plazas, courtyards, squares, yards). The specific requirements for the private amenity areas and the communal amenity spaces shall be determined by the City and implemented through the Zoning By-law and site plan agreement. [Amendment #150, LPAT July 19, 2019]*

Comment: Not applicable as proposal is not a mixed use development.

6. Conclusion

The proposed site plan conforms to the Official Plan as discussed in this report.

Save and except for the required density increase, the proposed site plan can be accommodated in full compliance with all other applicable zoning standards of the R5A [1409] zone as discussed in this report.

Appendix A: Zoning Compliance Chart

180 Prestige Circle (based on site plan dated April 14, 2021)

Applicable sections from 2008-250

Property is zoned RSA[1409]

Assuming the following yard locations per May 10 2019 email from City staff

- ... Jeanne d'Arc as front yard
- Prestige Circle as corner yard
- Ravine to the east as interior yard
- Property line between subject property and building to the south as rear yard

Exception 1409

no building permitted within 10 m of lot line adjacent to North Service Road
 minimum required front yard for the end wall of a row dwelling is 4.0 m
 minimum required front yard where garages face a public street is 6.0 m
 minimum required front yard for all other buildings is 6.0 m
 minimum required rear yard is 7.5 m
 minimum required building spacing is 3.0 m
 maximum permitted building height is 10 storeys
 minimum required density is 75 units per hectare
 maximum permitted density is 100 units per hectare
 minimum distance between the rear wall of a row dwelling and a O1 zone is 7.5 m
 minimum distance between the end wall of a row dwelling and a O1 zone is 2.0 m
 minimum distance between apartment dwellings under 7 stories in height and a O1 zone is 7.5 m
 minimum distance between apartment dwellings 7 to 10 stories in height and a O1 zone is 10 m
 minimum required western side yard abutting retaining wall for all buildings is 6.0 m
 minimum required western side yard south of retaining wall for all buildings is 8.0 m
 the dwelling units and land in a row dwelling development may be divided or severed...
 subsection 109(3) does not apply

Zoning Requirement	Proposal	Amendment Required	Comment
	10	10	
	N/A	-	
	N/A	-	
	6	10	
	7.5	7.5 +	
	N/A	-	
	10	4	
	75		
	100	103	yes rounded from 102.47 ; see Note 1
	N/A	-	
	N/A	-	
	N/A	-	
	N/A	-	
	N/A	-	
	N/A	-	
	N/A	-	
	N/A	-	
	N/A	-	
	N/A	-	
	parking permitted	0	

Table 164A (Apartment Dwelling, Mid-High Rise)

Minimum Lot Width (m)	25	28.5	
Minimum Lot Area (sq. m)	1000	0.7476	
Maximum Building Height (m)	10	4	
Minimum Front Yard Setback (m)	6	10	balconies project 2.3 m into required yard
Minimum Corner Yard Setback (m)	4.5	4.5	balconies project 2.3 m into required yard
Minimum Rear Yard Setback (m) ; see endnote 4 from Table 162E	7.5	28.309	
Minimum Int. Side Yard Setback (m);	7.5	15.423	

Section 163(9)

Minimum landscape area (% of site)	30	43.2	
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Sections 101 and 102

Minimum No. of parking spaces per Apartment Dwelling, Mid-High Rise	1.2	1.205	135 spaces/112; 2 are accessible
Minimum No. of visitor parking spaces per Apartment Dwelling, Mid-High Rise	0.2	0.205	23 spaces/112

Section 106

Parking space width (m)	2.6 min 2.75 max	2.6	
Minimum parking space depth (m)	5.2	5.2	

Section 107

Driveway access : min width (m) two way	6.2	6.7	
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Section 109

Parking in required front and corner yard	not permitted	0	
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Section 110

Minimum landscape area of parking lot (%)	15	35.4	
Minimum landscape buffer width of parking lot not abutting street (m)	1.5	3	
Minimum landscape buffer width of parking lot abutting street (m)	3	3	
Minimum distance between refuse collection area within a parking lot and: ...street (m)	9	47	
...any other lot line	3	3	

Section 111

Bicycle Parking Space Provisions (per unit)	0.5	0.5	56/112
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Section 137

Total amenity area (6 sq. m per unit)	672	3029	
Communal amenity area: 50% of total required 'total amenity area' sq. m.	336	2072	

Note 1

	Area	Units	
Block 1		0.6101	
Bldgs 1 and 2			40
...subtotal			40
Block 2	3.6739		
Bldgs 3-5			116
Bldg 6			79
Bldg 7			92
Bldg 8			112
...subtotal			399
Total	4.284		439
Density u/ha	102.4743231		