AVAILABLE) OR OPSD 1109.030. 4. IF THE WATERMAIN MUST BE DEFLECTED TO MEET ALIGNMENT, ENSURE THAT THE AMOUNT OF DEFLECTION USED IS EQUAL TO OR LESS THAN THAT WHICH IS RECOMMENDED BY THE MANUFACTURER. 5. USE APPROVED SADDLE CONNECTION WITH MAIN (CORPORATION) STOP AS PER CITY OF OTTAWA STANDARD DRAWING 'W26'.

6. CONNECTION TO EXISTING BY CITY FORCES. EXCAVATION, BACKFILLING AND

SWABING, CHLORINATION AND CONTINUITY TESTING FOR PROPOSED WATER

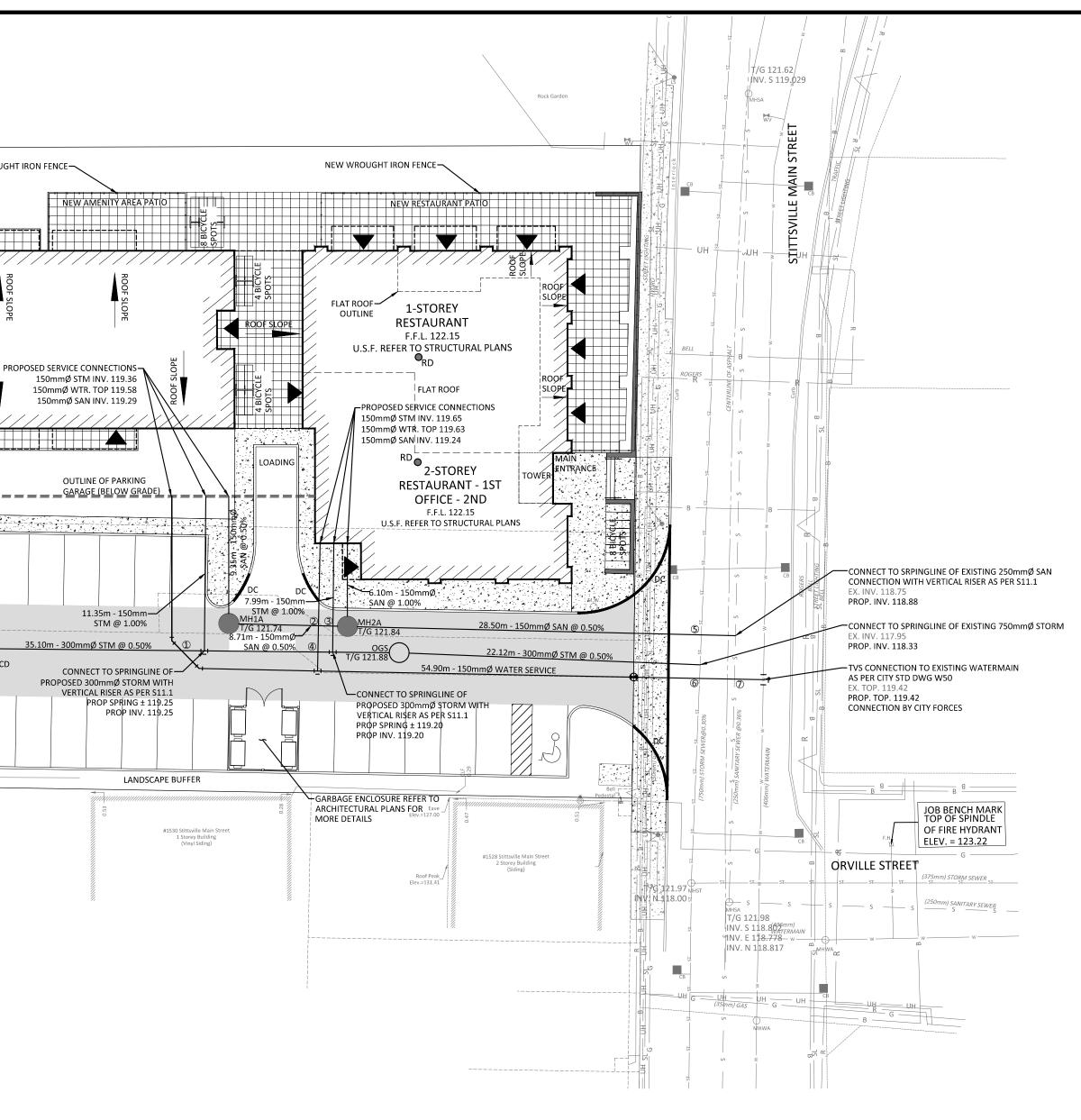
SERVICES IS TO FOLLOW CITY OF OTTAWA SPECIAL PROVISIONS #SP-4491 &

REINSTATEMENT IS TO BE COMPLETED BY THE CONTRACTOR.

SP-4494.

- AND SHALL CONFORM TO ASTM B88 TYPE 'K' SOFT. WATERMAINS AND/OR WATER SERVICES ARE TO HAVE A MINIMUM COVER OF 2.4m. OTHERWISE THERMAL INSULATION IS REQUIRED AS PER CITY STANDARDS (IF
- STANDARDS (IF AVAILABLE) OR AS PER OPSD STANDARDS. INDUSTRIAL/COMMERCIAL SERVICE CONNECTIONS TO BE 50mm COPPER PIPING
- WATERMAIN NOTES 1. CONSTRUCT ALL WATERMAINS AND APPURTENANCES IN ACCORDANCE WITH CITY
- 11. DYE TESTING IS TO BE COMPLETED ON SANITARY SERVICE TO CONFIRM PROPER CONNECTION TO THE SANITARY SEWER MAIN.
- 10. CONTRACTOR TO TELEVISE (CCTV) ALL PROPOSED SEWERS ONSITE, OUTLET CONNECTION TO THE MAIN AND PIPES 150mmØ OR GREATER PRIOR TO BASE COURSE ASPHALT. UPON COMPLETION OF CONTRACT, THE CONTRACTOR IS RESPONSIBLE TO FLUSH AND CLEAN ALL SEWERS & APPURTENANCES.
- WITHIN 1.0m OF BUILDING. ALL ENDS OF SERVICES TO BE PROPERLY CAPPED AND LOCATED WITH 2"x4"x8' LONG MARKER.
- INSULATE ALL SEWERS AND/OR SERVICES THAT HAVE LESS THAN 1.5m OF COVER WITH THERMAL INSULATION AS PER CITY OF OTTAWA STANDARD W22. 9. SUPPLY AND INSTALL ALL PIPING AND APPURTENANCES AS SHOWN AND DETAILED TO
- SEWERS AND CONNECTIONS 150mm DIAMETER AND SMALLER TO BE PVC SDR 28 OR APPROVED EQUIVALENT. SEWERS AND CONNECTIONS 200mm DIAMETER AND LARGER TO BE PVC SDR 35 OR APPROVED EQUIVALENT.
- 6. TO MINIMIZE DIFFERENTIAL FROST HEAVING, TRENCH BACKFILL (FROM PAVEMENT SUBGRADE TO 2.0m BELOW FINISHED GRADE) SHALL MATCH EXISTING SOIL CONDITIONS.
- 4. SUB-BEDDING, IF REQUIRED SHALL BE AS PER THE DIRECTION OF A GEOTECHNICAL ENGINEER. 5. BACKFILL TO AT LEAST 300mm ABOVE TOP OF PIPE WITH GRANULAR "A" OR SAND.
- MINIMUM 95% STANDARD PROCTOR DRY DENSITY. CLEAR STONE BEDDING SHALL NOT BE PERMITTED.
- UNLESS NOTED OTHERWISE. 3. BEDDING SHALL BE A MINIMUM 150mm OF GRANULAR "A", COMPACTED TO
- OR AS PER OPSD STANDARDS. 2. SEWER TRENCHING AND BEDDING SHALL CONFORM TO OPSD 802.010 AND 802.013
- SEWER NOTES . CONSTRUCT ALL SEWERS AND APPURTENANCES TO CITY STANDARDS (IF AVAILABLE)

- 14. ALL DIMENSIONS AND INVERTS MUST BE VERIFIED PRIOR TO CONSTRUCTION, IF THERE IS ANY DISCREPANCY THE CONTRACTOR IS TO NOTIFY THE ENGINEER PROMPTLY. • ELECTRICAL SERVICE - HYDRO OTTAWA, • GAS SERVICE - ENBRIDGE, • TELEPHONE SERVICE - BELL CANADA, • TELEVISION SERVICE - ROGERS. 17. INSTALLATION TO BE IN ACCORDANCE WITH CURRENT CODES AND STANDARDS OF APPROVAL AGENCIES HYDRO OTTAWA, BELL AND THE CITY. 18. ALL PROPOSED CURB SHALL BE CONCRETE BARRIER CURB AS PER CITY OF OTTAWA STANDARD DRAWING SC1.1 UNLESS SPECIFIED. 19. ALL EXISTING REDUNDANT PRIVATE APPROACHES FRONTING THIS DEVELOPMENT MUST BE REMOVED TO THE SATISFACTION OF THE CITY. 20. NO EXCESS DRAINAGE, EITHER DURING OR AFTER CONSTRUCTION, IS TO BE DIRECTED TOWARDS NEIGHBORING PROPERTIES. 21. NO ALTERATION OF EXISTING GRADES AND DRAINAGE PATTERNS ON PROPERTY BOUNDARIES.
- 15. ELECTRICAL, GAS, TELEPHONE AND TELEVISION SERVICE LOCATIONS ARE SUBJECT TO THE INDIVIDUAL AGENCY:
- 13. CONTACT THE CITY FOR INSPECTION OF ROUGH GRADING OF PARKING LOTS, ROADWAYS AND LANDSCAPED AREAS PRIOR TO PLACEMENT OF ASPHALT AND TOPSOIL. ALL DEFICIENCIES NOTED SHALL BE RECTIFIED TO THE CITY SATISFACTION PRIOR TO PLACEMENT OF ANY ASPHALT, TOPSOIL, SEED & MULCH AND/OR SOD.
- 11. DO NOT ALTER GRADING OF THE SITE WITHOUT PRIOR APPROVAL OF THE CITY. 12. ALL ROADWAY, PARKING LOT, AND GRADING WORKS TO BE UNDERTAKEN IN ACCORDANCE WITH CITY STANDARDS AND SPECIFICATIONS. THE CONTRACTOR IS TO PROVIDE POSITIVE DRAINAGE AWAY FROM THE BUILDING.
- DELINEATORS, MARKERS AND BARRIERS.
- 9. ALL DISTURBED AREAS TO BE RESTORED TO ORIGINAL CONDITION OR BETTER UNLESS OTHERWISE SPECIFIED. 10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL AND SAFETY MEASURES DURING THE CONSTRUCTION PERIOD, INCLUDING THE SUPPLY, INSTALLATION, AND REMOVAL OF ALL NECESSARY SIGNAGE,
- 8. TOPSOIL TO BE STRIPPED AND STOCKPILED FOR REHABILITATION. CLEAN FILL TO BE PLACED IN FILL AREAS AND COMPACTED TO 95% STANDARD PROCTOR DENSITY.
- 7. EXCAVATE AND DISPOSE OF ALL EXCESS EXCAVATED MATERIAL, SUCH AS ASPHALT, CURBING AND DEBRIS, OFF SITE AS DIRECTED BY THE ENGINEER AND THE CITY.
- UTILITIES WHETHER OR NOT SHOWN ON THESE DRAWINGS. IF THERE IS ANY DISCREPANCY THE CONTRACTOR IS TO NOTIFY THE ENGINEER PROMPTLY. 6. RESTORE ALL TRENCHES AND SURFACES OF PUBLIC ROAD ALLOWANCES TO CONDITION EQUAL OR BETTER THAN ORIGINAL CONDITION AND TO THE SATISFACTION OF THE CITY AUTHORITIES.
- 4. THE CONTRACTOR IS RESPONSIBLE FOR ALL LAYOUT. 5. THE CONTRACTOR IS TO DETERMINE THE EXACT LOCATION, SIZE, MATERIAL AND ELEVATION OF ALL EXISTING UTILITIES PRIOR TO COMMENCING CONSTRUCTION. PROTECT AND ASSUME ALL RESPONSIBILITY FOR EXISTING
- 3. THE CONTRACTOR IS TO OBTAIN AND PAY FOR ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY BEFORE COMMENCING CONSTRUCTION.
- FAIRHALL, MOFFATT, WOODLAND LTD. SURVEY PLAN #AA15600 DATED APRIL 16, 2020 AND CANNOT BE RELIED UPON TO BE ACCURATE OR COMPLETE. THE PRECISE LOCATION OF THE CURRENT PROPERTY BOUNDARIES AND EASEMENTS CAN ONLY BE DETERMINED BY AN UP-TO-DATE LAND TITLES SEARCH AND A SUBSEQUENT CADASTRAL SURVEY PERFORMED AND CERTIFIED BY AN ONTARIO LAND SURVEYOR.
- **GENERAL NOTES** 1. THE ORIGINAL TOPOGRAPHY, GROUND ELEVATION AND SURVEY DATA SHOWN ARE SUPPLIED FOR INFORMATION PURPOSES ONLY, AND IMPLY NO GUARANTEE OF ACCURACY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY ALL INFORMATION SHOWN. THIS PLAN IS NOT A CADASTRAL SURVEY SHOWING LEGAL PROPERTY BOUNDARIES AND EASEMENTS. THE PROPERTY BOUNDARIES SHOWN HEREON HAVE BEEN DERIVED INFORMATION SUPPLIED BY (OR SHOWN ON)
- NEW WROUGHT IRON FENCE / / / / / / / / / / / / / / / / / / / / \//////X/ OUTLINE OF PARKING 4-STOREY GARAGE (BELOW GRADE) RESIDENTIAL ROOF SLOPE FLAT ROOF ROOF SLOPE F.F.L. 122.15 ROOF SLOPE ●<sub>RD</sub> U.S.F. REFER TO STRUCTURAL PLANS OUTLINE OF PARKING GARAGE (BELOW GRADE) 6% APPROACH LSCB 1 -----GARBAGE ENCLOSURE REFER TO-ARCHITECTURAL PLANS FOR MORE DETAILS C/W IPEX 66mm ICD FLOW = 6.00L/sHEAD = 2.49m· Y \_\_\_ – X — T111111111111 Vinyl Shed 01.0



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6 7		750mmØ ST 150mmØ WT	R SERVICE INV 1 M SEWER TOP 1 R SERVICE INV 1 N SEWER OBV 1	. <u>18.71</u> 119.31		0.58	_			SANITAR		ILE	(	⊠ C	•	OPSD 219.180) NT CONTROL DEVICE	:
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