A CULTURAL HERITAGE IMPACT STATEMENT

229-247 BEECHWOOD AVENUE, OTTAWA



Prepared For: Bintee Dev Inc.

Prepared By: Commonwealth Historic Resource Management

October 2020 Revised

Glossary of Terms Used

Adverse impact

A project has the potential to "adversely impact" the cultural heritage value of a project if it; requires the removal of heritage attributes, requires the destruction of a cultural heritage resource, obscures heritage attributes, is constructed in such a way that it does not respect the defined cultural heritage value of a resource.

Built Heritage

Includes buildings, structures and sites that contribute to an understanding of our heritage and are valued for their representation of that heritage. They may reveal architectural, cultural, or socio-political patterns of our history or may be associated with specific events or people who have shaped that history. Examples include buildings, groups of buildings, dams and bridges.

Cultural Heritage Resources

Includes four components: Built Heritage, Cultural Heritage Landscapes, Archaeological Resources, and documentary heritage left by people.

Cultural Heritage Landscape

Any geographic area that has been modified, influenced, or given special cultural meaning by people and that provides the contextual and spatial information necessary to preserve and interpret the understanding of important historical settings and changes to past patterns of land use. Examples include a burial ground, historical garden or a larger landscape reflecting human intervention.

Preservation

Preservation involves protecting, maintaining and stabilizing the existing form, material and integrity of an historic place or individual component, while protecting its heritage value.

Rehabilitation

Rehabilitation involves the sensitive adaptation of an historic place or individual component for a continuing or compatible contemporary use, while protecting its heritage value.

Restoration

Restoration involves accurately revealing, recovering or representing the state of an historic place or individual component as it appeared at a particular period in its history, while protecting its heritage value.

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1.0 INTRODUCTION

1.1 Introduction

This Cultural Heritage Impact Statement (CHIS) has been requested by the City of Ottawa. The purpose of the CHIS is to identify the cultural heritage resources and values that may be impacted by the construction of two low-rise apartment buildings at the municipal addresses extending from 229 through to 247 Beechwood Avenue. The proposed development is located in the Rockcliffe Park Heritage Conservation District (RPHCD), which was designated by the City of Ottawa in 1997 under Part V of the Ontario Heritage Act (OHA) (By-law 97-10).

This CHIS follows the content outline recommended by the City of Ottawa for Cultural Heritage Impact Statements. The following documents were used in the preparation of this report:

Parts IV and V of the Ontario Heritage Act;

Rockcliffe Park Heritage Conservation District Study, 1996-1997;

Rockcliffe Park Heritage Conservation District Plan, City of Ottawa, 2016. (By-law # 2016-89);

Beechwood Avenue Community Design Plan (BCDP), City of Ottawa. 2006;

Heritage Survey and Evaluation Forms 229, 231-235, 241, 245, and 247 Beechwood Avenue, City of Ottawa 2010;

Standards and Guidelines for the Conservation of Historic Places in Canada, Second Edition, 2010; and

Preliminary plans and elevations, Woodman Architect & Associates, June 2020.

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1.2 Site Location, Current Conditions and Introduction to Development Site The five properties 229 – 247 Beechwood Avenue are located in the Rockcliffe Park Heritage Conservation District in an area known as the 'Panhandle'. The properties front onto the west side of Beechwood Avenue bound by Corona Avenue to the north, Green Avenue to the south, and Acacia

Avenue to the west. Carsdale Avenue, which has been closed by the City, bisects the proposed development.

The five detached residential properties are set on discreet lots and contain two storey residences constructed between circa 1930 (229), circa 1905 (231- 235) and circa 1950 (241, 245, 247). The properties sit at the base of the Rockcliffe plateau and grades rise approximately 1m from the Beechwood Avenue property lines to the back of the properties that abuts a retaining wall that encloses lots fronting onto Black Maple Private. Grades rise 1m from south to north along the Beechwood Avenue frontage. The development sites have a frontage of approximately 81m and a depth of 29m to 32m.

Adjacent properties to the north include a one-storey wood frame cottage, and a two storey detached residence at the corner of Corona Avenue. The property adjacent to the south is a three storey contemporary detached residence fronting onto Green Avenue. To the west of the site are contemporary three storey detached residences fronting onto Black Maple Private. Across the street from the site is a four to eight storey condominium with commercial uses on the ground floor.



Figure 1: Aerial view illustrating the built context within the block and adjacent to the development site – 229, 231-235, 241, 245, and 247 Beechwood Avenue. Sites arrowed, north top of image. Source: Geoottawa



Figure 2: Block plan illustrating the built context surrounding the development sites 229, 231-235, 241, 245, and 247 Beechwood. (Arrowed). Note the new development on Black Maple Private that is accessed via Acacia and Carsdale Avenue. The eastern portion of Carsdale that bisects the two development sites has been closed by the City. Source: Geoottawa

1.3 Street Characteristics (Neighbourhood Character)

The west side of Beechwood Avenue extending between Green and Corona Avenues developed between circa 1905 through to the 1950s. Factors that influenced the slow uptake of lots in the area include the availability of lots elsewhere in the village, and the low desirability of the properties sitting at the base of the Rockcliffe plateau and backing onto the public work's yard. The initial development of the properties in the block (229 and 231-235) is more characteristic of the development that occurred to the west of Acacia Avenue in an area known as Clarkston. The two storey brick clad residences, one detached and the other a side-by-side duplex, are characteristic of the form and type of residences constructed in Vanier. The properties are set back uniformly from the street with small landscaped front yards, with a lawn, plantings, and a low stonewall at the sidewalk, the only street in Rockcliffe Park with a curb and sidewalk.

Beechwood Avenue, an arterial road will be widened sometime in the future and will reduce the front yards of all properties but most notably the two properties at 229 and 231-235 (June 2017 Topo Survey). Beechwood Avenue is a designated Mainstreet extending through to Corona Avenue to the north. When the arterial is widened, a concrete sidewalk with curb will be installed and street furnishings will be installed to match the furnishings in the St. Charles sector of Beechwood Avenue to the west (CDP 2006). This stretch of Beechwood Avenue has a modest curve, and the grade rises (1m) from the south to the north within the development site. The properties to the north of Carsdale Avenue (241, 245, and 247) have at or near grade entries, whereas the properties to the south of Carsdale (229 and 231-235) are accessed by a series of steps.



Figure 3: Street view looking south on Beechwood Avenue, 247 Beechwood is to the right and the roof of 231-235 Beechwood is visible above the trees to the left. Note the low stonewall at the sidewalk that will be removed when Beechwood Avenue is widened. Source: Google Earth



Figure 4: Street view looking north with 229 Beechwood Avenue to the left. Note the recent mid-rise development on the east side of Beechwood that extends the width of the development sites. Source: Google Earth



Figure 5: Street view looking south-west on Beechwood from Green Avenue. The brick clad building to the left is listed on the City's Heritage Register (BACDP 2006). Source: Google Earth



Figure 6: Street view looking north. The property at the corner 455 Green Avenue (left) is an example of recent development in the Panhandle of Rockcliffe Park. Source: Google Earth



Figure 7: Street view looking east on Black Maple Private/Carsdale to the west of the development site. Source: Google Earth



Figure 8: Street view of 259, 261, and 263 Beechwood Avenue to the north of Corona Avenue, a recent development within the HCD. Source: Google Earth

1.5 Heritage Context

The five buildings within the development site are categorized as Grade 2 heritage resources within the context of the RPHCD. The landscape setting, although atypical of properties in Rockcliffe Park is the primary heritage attribute. The future widening of Beechwood will have an adverse impact on the landscape character of the properties, most notably the two properties at 229 and 231-235. The front lawns will be reduced placing the buildings close to the street edge a feature more typical of properties in Vanier. There are no Grade 1 properties adjacent to the development site.

Address	Street	Date of Construction	Group	OHA Designation
229	Beechwood	c. 1930		Part V
231 - 235	Beechwood	c. 1905	Grade 2	Part V
241	Beechwood			Part V
245	Beechwood	c. 1950	Grade 2	Part V
247		c. 1950	Grade 2	Part V
	Beechwood			

Table 1: Development site properties heritage resource categorization or grouping. Source: Heritage Survey Forms 2010

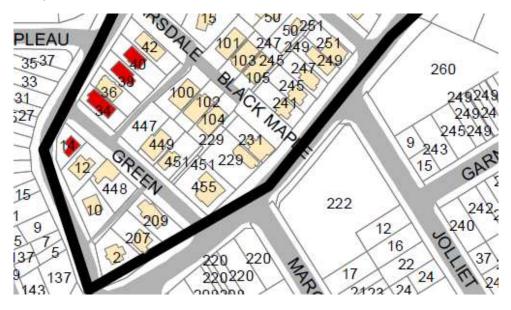


Figure 9: Detail plan of the Panhandle of the Rockcliffe Park HCD illustrating Grade 1 (red) properties. There are no Grade 1 properties adjacent to the development sites (Arrowed). Source: City of Ottawa 2010

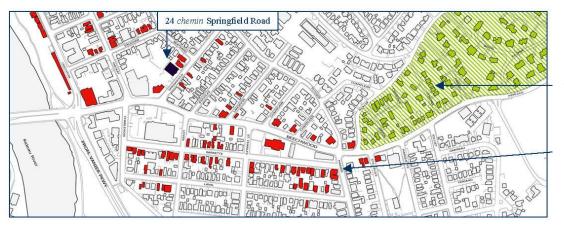


Figure 10:
Detail plan
of
Beechwood
Avenue
illustrating
properties
that are
listed on the
Heritage
Register
(red), and
the

panhandle of the Rockcliffe Park HCD (green). Source: Beechwood Avenue CDP City of Ottawa 2006.

1.4 Relevant Information from Council Approved Documents

Official Plan

The City of Ottawa includes provisions for Cultural Heritage Resources in Section 4.6 of the Official Plan. **Rockcliffe Park Secondary Plan (1993)**

The area is zoned multi-family residential in the Rockcliffe Park Secondary Plan.

Rockcliffe Park Heritage Conservation District Study (1997) and Plan (2016) (RPHCD)

The development sites are within the boundaries of the RPHCD, which was designated under Part V of the OHA (By-law 97-10). A District Plan was completed in 2016 (By-law # 2016-89).

Beechwood Avenue Community Design Plan (BACDP), 2006

The BACDP divided Beechwood Avenue into three distinct sectors. The development site is located within the Rockcliffe Park Sector extending from Beechwood Cemetery to Acacia Avenue. The development strategy for the Rockcliffe Sector noted that the area is not as well defined in character as the St. Charles Sector to the west. The north (west) side of Beechwood Avenue has few recognizable buildings and large trees that screen the buildings from the street.

In the future, two elements are needed to link the sector with the others:

- First, a comprehensive streetscape design should be implemented along the entire length of the street, and the Rockcliffe Sector streetscape should contain the same elements as the design of the St. Charles Sector; and,
- Secondly, a minor development node should be established at the Jolliet/Corona intersection.
 Infill to low-rise mixed-use buildings should be allowed to develop on the south (east) side of the avenue and infill residential buildings on the north (west) side.

Generally speaking, development intensity and building height should gradually rise from the east to the west. On the north side of the street, all developments should be residential in nature and all buildings should have a residential setback to match the existing patterns of development. Larger buildings should be designed as a collection of smaller buildings. The Mainstreet designation is deemed to end on the south side of Beechwood at the entrance to Beechwood Cemetery, and on the north side at the eastern property line of 255 Beechwood Avenue.

229-231 Beechwood Avenue (Report to Planning and Environment Committee, and Council. 14 Oct. 2008. Beechwood Cemetery View Official Plan Ref. No. ACS-2008-ICS-PLA-0216)

The maximum building height will be increased from approximately three and one-half storeys (11.0m) to four storeys (12.5m).). This is being done because it has been determined that the maximum building height, as described in the Beechwood Avenue Community Design Plan, can be achieved without encroaching into the view shed.



Figure 11: Schematic sections of the two buildings. The upper height limit for both buildings is 12.5m or four storeys. Source: Woodman Architects April 2020

2.0 HERITAGE RESOURCE DESCRIPTION AND HISTORY

2.1 Neighbourhood and Development Site History

The history of Rockcliffe Park is outlined in detail in the 1997 Heritage Conservation District Study. The development sites are located in an area known as the 'Panhandle' encompassing lots to the south of Corona Avenue bound by Beechwood Avenue and Acacia Street. The first development along Beechwood Avenue in Rockcliffe Park occurred prior to the establishment of the Rockcliffe Park Police Village in 1908. The brick clad two-storey side-by-side duplex at 231 – 235 Beechwood is an example. The Rockcliffe Park Police Village passed a number of by-laws relating to land use and required setbacks from the street that applied to development within the 'Panhandle' where the Public Works yard was located.

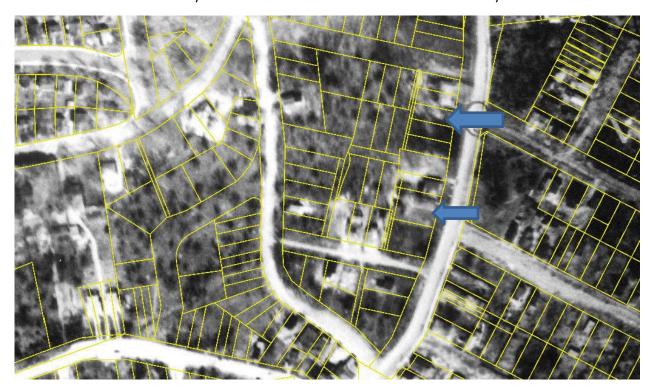
In 1922, the Police village council declared a restricted area prohibiting the use of land for any purpose other than detached private residences establishing a twenty-five-foot setback for buildings excluding the 'Panhandle' south of Corona Avenue. A fifteen-foot setback was required for new houses built elsewhere in the Police Village, excepting Beechwood Avenue. ¹ The property at 229 Beechwood was

¹ Rockcliffe Park Heritage Conservation District Study 1997 pg. 25

constructed circa 1930 and set back from the street matching the setback of the adjacent property at 231–235 Beechwood.

In 1936, the restricted area was extended into the 'Panhandle' to include the streets bounded by Corona Avenue, lower Acacia, and Beechwood, as well as land east of McKay Lake. A fifteen-foot setback was required on lots within these boundaries. In 1946, further land-use restrictions were introduced including a minimum square footage and a minimum value for all new buildings. In the area south of Corona, the figures were reduced to 1000 sq. ft. A setback of fifteen feet was required for accessory or out buildings. ² Following the Second World War, the housing shortage became increasingly apparent. To address this, the National Housing Act was passed, and standard plans were created in order to encourage quick construction. The properties at 241, 245, and 247 were built to these standardized plans.³

Following municipal amalgamation in 2003, the public work's yard was sub-divided into residential lots and the area redeveloped with single detached residences fronting onto Black Maple Private that is accessed off Acacia via Carsdale Avenue. The lot at 455 Green Avenue adjacent to 229 Beechwood was redeveloped, and a new single detached residence constructed similar in the design, form and materiality to the development fronting onto Black Maple Private. In 2006, the Beechwood Community Design Plan was approved, and density provisions were removed from the two sites to align with the 1993 Rockcliffe Park Secondary Plan in which the two sites are zoned multi-family residential.



² Ibid. pgs. 25-26.

³ Heritage Survey Forms 241, 245, and 247 Beechwood Avenue. City of Ottawa 2010.

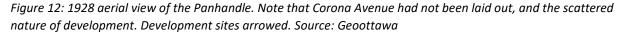




Figure 13: 1965 aerial view of the Panhandle. Note the Public Works yard in the centre of the block. Development sites arrowed. Source: Geoottawa

2.2 Built Heritage

The five existing residences within the development site can be grouped into two general categories. The three residences at 229 and 231-235 Beechwood are representative examples of two storey brick clad buildings constructed during the 1900 – 1930 period in Vanier. The two-storey brick clad building at 449 Green Avenue is similar in design and style constructed during the same period and would be the only building within the 'Panhandle' comparable with Rockcliffe Park.

The second group of residences 241, 245, and 247 Beechwood Avenue are representative examples of post war housing constructed to the standardized designs developed by CMHC.



Figure 14: Street view of the circa 1930 two storey single detached property at 229 Beechwood (left), and the circa 1905 two-storey side-by-side duplex at 231 - 235 Beechwood. Both buildings are clad in brick with entrances and porches fronting onto the street. Note the stone retaining wall at the street, and the shrub planting in the front yard. Source: Google Earth

2.3 Environment Streetscape/Landscape

The 'Panhandle' has a distinct character and is the only area in Rockcliffe Park with smaller lots that are more characteristic of properties developed in the surrounding neighbourhoods of Vanier and Lindenlea. A factor for the atypical development pattern was that the Villages public work's yard was located centrally within the block bound by Green, Corona, and Beechwood Avenues and properties backed onto the yard. Beechwood Avenue developed gradually to become a commercial thoroughfare in the area with mixed commercial and residential on the south side, and residential properties on the north side. The buildings are characterized by limited setbacks from the street, higher density, modest front and side yards, sidewalks and curbs at the street, small stone retaining walls at the sidewalk, and random street trees. The combination of property features relate the properties to one another. Although atypical for Rockcliffe Park, create a continuous streetscape. This section of Rockcliffe Park has and continues to serve as a transitional zone to the surrounding neighbourhoods of Vanier and Lindenlea.



Figure 15: Street view of the circa 1950 two-storey single detached properties at 245 (left), and 247 (centre)
Beechwood Avenue built to standard plans developed by the CMHC. The small cottage-style residence at 249 (right)
Beechwood is not part of the proposed development. Note the stone retaining wall at the street, and the simple landscape treatment. Source: Google Earth



Figure 16: Street view looking south on Beechwood. 247 Beechwood is to the right, and the roof of 231-235 Beechwood is visible to left. Note the stone retaining wall at the street, and the simple landscape treatment. Source: Google Earth

3.0 STATEMENT OF CULTURAL HERITAGE VALUE

3.1 Statement of Cultural Heritage Value

A "Statement of Cultural Heritage Value" is the foundation of all heritage conservation district plans. The statement below is based on the original statement in the 1997 Rockcliffe HCD Study but has been shortened and adapted in consultation with the author of the original Rockcliffe Park study to reflect the current requirements of the Ontario Heritage Act.

Statement of Cultural Heritage Value and Heritage Attributes

Rockcliffe Park is a rare and significant approach to estate layout and landscape design adapted to Canada's natural landscape from 18th-century English precedents. Originally purchased from the Crown by Thomas McKay, it was laid out according to the principles of the Picturesque Tradition in a series of "Park and Villa" lots by his son-in-law Thomas Keefer in 1864. The historical associations of the village with the McKay/Keefer family, who were influential in the economic, social, cultural and political development of Ottawa, continue and the heritage conservation district is a testament to the ideas and initiatives of various key members of this extended family, and their influence in shaping this area.

Rockcliffe Park today is a remarkably consistent reflection of Keefer's original design intentions. Although development of the residential lots has taken place very gradually, the ideas of estate management, of individual lots as part of a larger whole, of Picturesque design, of residential focus, have survived. This continuity of vision is very rare in a community where development has occurred on a relatively large scale over such a long time period.

The preservation of the natural landscape, the deliberately curved roads, lined with mature trees, and without curbs or sidewalks, the careful landscaping of the public spaces and corridors, together with the strong landscaping of the individual properties, create the apparently casual and informal style so integral to the Picturesque tradition. The preservation and enhancement of topographical features, including the lake and pond, the internal ridges and slopes, and the various rock outcroppings, has reinforced the original design intentions. The views to and from the Ottawa River, the Beechwood escarpment, and the other park areas are integral to the Picturesque quality of Rockcliffe Park. Beechwood Cemetery and the Rockeries serve as a compatible landscaped boundary from the earliest period of settlement through to the present. The various border lands create important gateways to the area, and help establish its particular character.

The architectural design of the buildings and associated institutional facilities is similarly deliberate and careful, and reflects the casual elegance and asymmetry of the English country revival styles, such as the Georgian Revival, Tudor Revival and Arts and Crafts. Many of the houses were designed by architects in these styles. The generosity of space around the houses, and the flow of this space from one property to the next by continuous planting rather than hard fence lines, has maintained the estate qualities and park setting envisioned by Keefer.

Statement of Heritage Attributes

The Rockcliffe Park Heritage Conservation District comprises the entire former village of Rockcliffe Park, an independent municipality until amalgamation with the City of Ottawa in 2001. Section 41.1 (5) c of the Ontario Heritage Act requires the Heritage District Plan to include a "description of the heritage attributes of the heritage conservation district and of properties in the district." A "Heritage Survey Form" outlining the heritage attributes for every property in the HCD has been compiled and evaluated. The forms are held on file with the City of Ottawa.

Description of Heritage Attributes

The attributes of the Rockcliffe Park HCD are outlined here. Several of the attributes (highlighted) related specifically to the Panhandle:

- •
- •

- The atural features that distinguish the HCD, including McKay lake and its shoreline, the varied terrain, and topography;
- The unobtrusive siting of the houses on streets and the generous spacing relative to the neighbouring buildings;
- The variety of mature street trees and the dense forested character that they create;
- The profusion of trees, hedges, and shrubs on private property;
- Varied lot sizes and irregularly shaped lots;
- Generous spacing and setbacks of the buildings;
- Cedar hedges planted to demarcate property lines and to create privacy;
- The dominance of soft landscaping over hard landscaping;
- Wide publicly-owned verges;
- The remaining Villa lots laid out in McKay's original plan;
- The high concentration of buildings by architect Allan Keefer, including 725 and 741 Acacia, 11
 Crescent Road;
- The rich mix of buildings types and styles from all eras, with the Tudor Revival and Georgian Revival styles forming a large proportion of the total building stock;
- The predominance of stucco and stone houses over and the relative rarity of brick buildings;
- The narrow width of many streets, such as McKinnon and Kinzua Roads;
- The historic road pattern that still reflects the original design established by Thomas Keefer;
- The low, dry stone walls in certain areas of the Village, including around Ashbury College;
- The existing garden features that enhance the public realm and distinguish certain private properties, including the garden gate at 585 Manor Ave, and the white picket fence at 190 Coltrin Road;
- Informal landscape character with simple walkways, driveways, stone retaining walls and flowerbeds;
- The "dog walk," a public footpath that extends from Old Prospect Road to corner of Lansdowne Road and Mariposa Avenue;
- The public open spaces including the Village Green and its associated Jubilee Garden;
- Institutional and recreational buildings including the three schools, Rockcliffe Park Public School, Ashbury College and Elmwood School for Girls and the Rockcliffe Park Tennis Club;
- The significant amenities of the Caldwell-Carver Conservation Area, McKay Lake and the Pond,
- The multi-unit buildings, small lots, and more modest houses in the area bounded by Oakhill to the east, Beechwood to the south, and Acacia to the west and north, referred to as the "Panhandle," that characterize the south and west boundaries of the District.
- The regular front yard setbacks on some streets such as Sir Guy Carleton Street, Blenheim Drive and Birch Avenue
- The irregular front yard setbacks on some streets, such as Mariposa Avenue between Springfield and Lisgar Roads, Crescent Road, Acacia Avenue and Buena Vista between Springfield and Cloverdale Roads

4.1 Description of the Proposed Development

The proposal is to construct two apartment blocks, one three storey block to the south and the second a four storey block to the north of Carsdale Avenue fronting onto Beechwood Avenue. The apartment block at 229 Beechwood will contain thirty-one studio, eleven one-bedroom, and six two-bedroom units. The apartment block at 241 Beechwood will contain forty-five studio, seven one-bedroom units, and three two-bedroom units. Amenity areas for both buildings are provided in the rear yard and on the roof. Ten surface parking spaces are proposed between the two buildings, and fifty bicycle parking spaces will be provided split evenly between the two buildings. The closed section of Carsdale Avenue will provide access to the proposed waste enclosures at the rear of the two buildings.

The elevations below illustrate the finishes and materials' palette. The traditional rhythm of buildings separated by openings is maintained although not as fine grained it is in keeping with this section of Beechwood. The building heights mirror the podium height across the street and successfully integrate the streetscape.





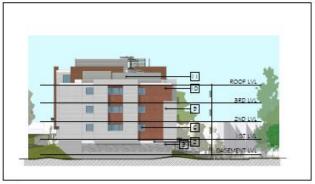
1	STONE CLADDING EQUIVELANT TO ARRISCRAFT WHITE RENAISSANCE MASONRY	8	METAL CLADDING EQUIVELANT TO LONGBOARD DARK CHERRY
2	STONE SILL EQUIVELANT TO ARRISCRAFT WHITE ROCKED CAMBRIDGE SILLS	9	METAL CLADDING EQUIVELANT TO LONGBOARD WHITE OAK
3	STONE CLADDING EQUIVELANT TO ARRISCRAFT MIDNIGHT GREY WITH ONLY - LAURIER	10	BRICK, CLADDING EQUIVELANT TO RED SMOOTH BRAMPTON BRICK.
4	DRY STONE RETAINING WALL TO MATCH EXISTING ON SITE	1.1	CORRUGATED METAL CLADDING (COLOR LIGHT GREY)
5	CHARCOAL ALUMINUM DOOR SYSTEM		
G	CHARCOAL ALUMINUM WINDOW SYSTEM		
7	CHARCOAL ALUMINUM GLASS FAILING		

2 NORTH ELEVATION A4 1:150

Figure 17: Rendered east and north elevations of the proposed development looking along Beechwood Avenue. The buildings' massing as expressed in the arrangement of materials is successful in mitigating the scale of the building and suggesting a collection of smaller buildings. The balconies enhance the residential expression. The material legend provides an indication of the finishes and material palette. Source: Woodman Architect August 2020.



WEST ELEVATION



10	STING CLASSING CONTRACT TO ARREST ARTT REQUESTION SUPPORT	8	METAL CLUBBING SOUNTLANT TO LONGROUND BARK ONDRY
2	STORE SILL SOUNDLEST TO BICKLISHT VINTE ROCKED CAMBRIDGE SILLS	9	HITTEL CLASSING SCHARLEST TO LONGROUND WATE CAR.
3	STORE CLASSICS COUNTRIEST TO ARRECTAFT MODIFIEST CREEK CHAPT CREEK LAURER	10	RECEIVED NO SOUND AND TO RECEIVE SHOULD SHOU
4	DRY STONE RETURNING WALL TO NATION EXSTRANCES HERE	TE	CONSUMERO SETAL CLASSING. COLOR MONT ORDIN
5	CHARCOSE ADMINISTRAÇÃO (MATEM		£
6	CHROOM BURNING SHECK SHEETS		
7	CHARCOSE ASSISTANCE CLARGE VALUES		5

2 SOUTH ELEVATION

Figure 18: West and South Elevations are less detailed but provide well-finished elevations towards neighbouring properties.



Figure 19: Rendered perspective view of the proposed development. The four-storey building's massing successfully mitigates the scale of the building. Using a similar palette of materials with reversed colours helps to provide diversity along the street. Woodman. Architect, 2020.



Figure 20 and 21: Rendered perspective views of the proposed development from the south and north. The four storey buildings massing as expressed in the arrangement of materials successfully mitigates the scale of the building. The residential expression is enhanced with projecting and recessed balconies to give depth to the facades. Source: Woodman Architect, 2020.





Figure 22: Aerial view looking north.



Figure 23: View looking south. The height of the new development and its relationship with the podium of the apartment block across the street establishes enclosure and comfortable scale to this section of the street.

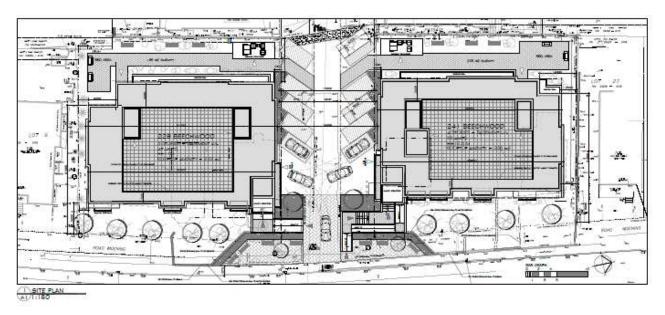


Figure 24: Site plan illustrating the proposed treatment of the grounds, including a small street park between the two buildings takes the opportunity to replace two street trees. and offering a place to sit while waiting for buses.



Figure 25: rendered landscape plan. Source James Lennox & Associates Landscape Architect. October 2020.

5.0 IMPACT OF DEVELOPMENT

5.1 Rockcliffe Park Heritage Conservation District Plan Guidelines

This section specifically addresses the impacts of the development proposal on the cultural heritage values of the Rockcliffe Park Heritage Conservation District (RPHCD). The guidelines contained in the 2016 District Plan are used to assess the impacts. The heritage attributes and character-defining features of the RPHCD are itemized in Section 3.0.

In 2016, Council approved a heritage conservation district Plan as defined in the Ontario Heritage Act (By-law # 2016-89). The management guidelines in the Plan are applicable in the assessment of the current development proposal. As part of the Plan, the city in association with the Rockcliffe Park Heritage Committee evaluated each building using Ontario Regulation 9/06: Criteria for Determining Cultural Heritage Value or Interest under the Ontario Heritage Act (O.Reg. 9/06)". The process included background research, and evaluation of the cultural heritage values – design, history, and environment and context - of each property. Properties were categorized as Grade 1, or Grade 2 heritage resources.

The five properties at 229, 231-235, 241, 245 and 247 Beechwood are categorized as Grade 2 heritage resources. As noted in the comments in the HSF forms for the properties under street characteristics, Beechwood Avenue is an anomaly in that it is the only street in Rockcliffe Park with a curb and sidewalk, and buildings along the street are characterized by shallow setbacks, high density, and modest yard spaces. Small yards, modest landscape plantings and shallow lawns characterize the landscape and open space and a clear separation between street, sidewalk and property is characteristic of this stretch of Beechwood. The narrative also notes that the Panhandle has a distinct character from the rest of Rockcliffe Park, and the area serves as a transitional zone to the adjacent neighbourhoods of Vanier and Lindenlea.

Guidelines from the District Plan are in italic followed by a Discussion.

7.3 Guidelines for Existing Buildings and Landscapes

7.3.1 Demolition and Relocation (Applicable Guidelines)

5. While acknowledging that the retention of both Grade 1 and Grade 2 buildings in the HCD is an objective of this Plan, the demolition of Grade 2 buildings in the HCD may be considered. Any application to demolish a Grade 2 building in the HCD shall be accompanied by plans for the proposed replacement building.

Discussion: The development proposal entails the demolition of the Grade 2 buildings on the five properties.

6. Any application to demolish an existing Grade 2 building will be reviewed with consideration of its historical and architectural significance, its contribution to the historic character of the streetscape, and the appropriateness of the proposed redevelopment. Demolition will be permitted only where the existing building is of little significance, and the proposed building is sympathetic to the traditional surrounding natural and cultural environment. All new construction will comply with the relevant guidelines contained within this plan.

Discussion: The Heritage Survey Form assigns a score to each property for environment, history, and architecture with a total score of 100. The scores for the five properties range from a low of 9 (247 Beechwood) to a high of 40 (229 Beechwood) and the balance of the properties averaging in the range of 30. All of the properties scored low in architecture or design; the landscape setting was identified as the primary heritage attributes of the properties. Based on the low scores, the integrity of the buildings and the appropriateness of the new development a request for demolition is reasonable.

7. When a building is proposed for demolition and replacement, the Environment Section of the heritage survey form and existing conditions shall be reviewed to identify significant landscape features to be retained.

Discussion: Significant landscape features for all the properties is limited to low stone retaining walls at the sidewalk that mitigates grades as they rise to the back of the properties. A tree survey has been completed as part of the redevelopment. Trees that are being removed will be replaced.

7.3.3 Landscape Guidelines

The conservation and enhancement of the HCD's distinctive landscape is pivotal to all projects within it. The Guidelines below provide direction to individual property owners with regard to the conservation and enhancement of the landscape.

Front Yards, Plant Material, Trees and Walkways

1. The dominance of soft landscape over hard landscape is an essential heritage attribute of the HCD and shall be retained in order to maintain a green setting for each property.

Discussion: The site plan illustrates a number of trees being planted in the lawn within the 5m setback from the street. A low stone retaining wall will be installed at the sidewalk when the road widening has been completed.

The retaining walls at the sidewalk are the primary heritage attributes of the five properties and are being used effectively to define the original lot patterns and enhance the perception of the two buildings as a collection of smaller buildings.

The site plan illustrates a shared access to the two buildings, which will be landscaped with both hard and soft landscape components.

Landscape projects shall respect the attributes and established character of the associated streetscape and the HCD.

Discussion: The proposed landscaping generally respects the attributes and established character of this stretch of Beechwood Avenue. (see the landscape plan figure 23)

2. Front yards shall have a generous area of soft landscaping which may include lawns, shrubs and flowerbeds, specimen or groupings of trees. The tradition of using native plant material is encouraged. Existing elements such as lawns, flower beds, glades of trees, shrubs, rocks and low stone walls shall be maintained and hard surfacing shall be kept to a minimum.

Discussion: The proposed landscaping generally respects the attributes and established character of this stretch of Beechwood Avenue. Depth to bedrock, which is assumed to be very close to the finished grade at the sidewalk, may limit the viability of the soft landscape components, including the proposed

trees. It may be necessary to increase available soil volumes by increasing the height of the retaining wall at the street.

Fences

1. The use of fences to delineate lots was not typical for much of the history of the HCD. The continuation of soft borders between lots is encouraged. When fences are required for safety, they shall not be located in the front yard, and shall comply with the City's Fence by-law.

Discussion: A wood fence is located along the lot line between the 229 Beechwood and 455 Green Avenue. The proposed low stone retaining wall at the sidewalk could be returned into the lot and terminate at the wood fence. A metal fence runs along the rear of the property.

Lot sizes

1. The retention of existing lots, large and small, is important to the preservation of the character of the HCD. New lots created through severance or by joining smaller lots together shall be consistent with the general lot sizes within the associated streetscape and the zoning bylaw in force at the time in order to respect the character of the associated streetscape.

Discussion: The proposal entails the consolidation of two lots to the south and three lots to the north of the closed section of Carsdale Avenue to create two large lots. The scale of the proposed development is not in character with the typical lot pattern in the 'Panhandle'. As noted in the BACDP, larger buildings should be designed as a collection of smaller buildings, which is the approach that is illustrated in the perspective views dated June 23, 2020. The views also illustrate the existing retaining walls on the properties, which would be effective in delineating the vestiges of the original lot pattern.

The site is located along a 'Traditional Mainstreet' and zoned to accommodate the proposed low-rise apartment buildings. The entrances to the two units is at the front corner next to Carsdale. The preconsultation minutes suggested that grade entry units should be considered to mitigate or break up the scale of the development in relation to the street, and questioned the shared entry to each building with a large lawn extending the length of each building that is more typical of detached residential buildings. The residential expression is enhanced with projecting and recessed balconies along with material and changes in colour give some depth to the facades and break up the scale. The front lawn is broken up with plantings and trees. The drystone wall is retained where possible and rebuilt to preserve the street to lot relationship.

4. All lots will be large enough to provide generous open space around buildings, thus protecting the continuity and dominance of the soft landscape.

Discussion: The proposed side yard setbacks are 1.5m to the three storey building to the south (455 Green) and 2.5m to the one storey property to the north (249 Beechwood). The proposed scale of the two developments at three and four storeys each with a 30m footprint extending along the street needs some mitigation measures to increase soft landscape planting. The proposed shared lane leading to the back of the property will be landscaped with soft and hard surface features. Consider increasing the south side yard setback to 2.5m to match the proposed north side yard setback.



Figure 24: view from the north showing the north elevation which is well articulated. The relationship of the four-storey apartment with the small cottage next door is mitigated by the setback and the materials that help to scale the apartment.



Figure 25: View of the rear treatment and the relationship to the residential units to the west. A combination of the grade changes as the land drops down to Beechwood and the scale of the adjacent units provides a comfortable fit.

7.4 Additions and New Construction

7.4.2 Guidelines for New Buildings

2. New buildings shall contribute to and not detract from the heritage character of the HCD and its attributes.

Discussion: The heritage character of the Panhandle, particularly the interior lots fronting onto Beechwood Avenue is atypical of the HCD. The proposed development is enclosed by recently developed infill housing to the south and west that has a distinct character of its own. Heritage attributes of the Panhandle as stated in the SOS are 'The multi-unit buildings, small lots, and more modest houses in the area'. The heritage survey forms note that the area provides a transition to the Lindenlea and Vanier neighbourhoods.

3. Construction of new buildings will only be permitted when the new building does not detract from the historic landscape characteristics of the associated streetscape, the height and mass of the new building are consistent with the Grade I buildings in the associated streetscape, and the siting and materials of the new building are compatible with the Grade I buildings in the associated streetscape. Where there are no Grade I buildings in the associated streetscape, the height and mass of the new building shall respect the character of the existing buildings and shall not have a negative impact on the associated streetscape or the cultural heritage value of the HCD. These situations will be reviewed on a case-by-case basis in consultation with the community in accordance with Section 4.1 of this Plan.

Discussion: The development sites are surrounded by recent infill development to the south (455 Green) and west (Black Maple Private), and two Grade 2 properties to the north 249 and 251 Beechwood. The proposed three storey block to the south generally respects the height and to a lesser extent, the mass of the adjacent infill development at 455 Green where a 1.5m side yard is proposed. *Figure 24* illustrates the proposed four storey block to the north is set adjacent to a one storey wood clad cottage where a 2.5m side yard setback is proposed to mitigate the height. The two sites to the

north (249 and 251) are zoned for multi-family residences and the Beechwood Avenue CDP envisioned a three storey multi-unit residential building on the site.

4. New buildings shall be of their own time but sympathetic to the character of their historic neighbours in terms of massing, height and materials. New buildings are not required to replicate historical styles.

Discussion: The design approach for the two buildings is to use materials to define the massing as a collection of smaller buildings. The proposed materials – brick, limestone units, long board simulated wood finish, – are sympathetic to the new infill developments to the south and west.

6. Existing grades shall be maintained.

Discussion: The proposed development responds to the existing 1.0m increase in grades from south to north within the development site that is reflected in the three and four storey form of the two buildings. The two buildings also respond to the cross slope across the depth of the lots, with window wells proposed for the rear of the buildings. The footprints of the two buildings (20m x 30m) are more or less the same.

There is no indication of the extent of bedrock removal from the two sites to accommodate the two buildings.

- 7. In order to protect the expansive front lawns, and the generous spacing and setbacks of the buildings, identified as heritage attributes of the HCD, the following Guidelines shall be used when determining the location of new houses on their lots:
- a. New buildings on interior lots shall be sensitively sited in relation to adjacent buildings. Unless a new building maintains the front yard setback of a building it is replacing, the front yard setback of the new building shall be consistent with that of the adjacent building that is set closest to the street. A new building may be set back further from the street than adjacent buildings.

Discussion: The proposed buildings are set back 5.0m in line with the recent development to the south (455 Green) and the existing properties to the north.

8. Windows may be wood, metal clad wood, steel or other materials as appropriate. Multi-paned windows should have appropriate muntin bars.

Discussion: The fenestration pattern and the proposed balconies on the renderings is typical of a residential expression and will be painted metal sash.

9. The use of natural materials, such as stone, real stucco, brick and wood is an important attribute of the HCD, and the use of materials such as vinyl siding, aluminium soffits, synthetic stucco, and manufactured stone will not be supported.

Discussion: The materials used on the more recent infill housing to the south, and west would be appropriate – stone, composite board, wood and prefinished metal trim. A red brick pays tribute to the

early brick clad buildings in the Panhandle of the HCD. The longboard siding is reminiscent of the horizontal siding seen throughout the district.

10. Terraces on the top storey of buildings do not form part of the heritage character of the HCD; however, a terrace on the top storey may be permitted if it is set back from the roof edge, it and its fixtures are not visible from the surrounding public realm, and the terrace does not have a negative effect on the character of the surrounding cultural heritage landscape.

Discussion: There is a rooftop terraces on each of the apartments have been enclosed to provide a comfortable space and to discourage overlooking of neighbouring properties.

11. Terraces and balconies below the top storey (for example, on a garage roof, or one storey addition) may be recommended for approval if they do not have a negative effect on the character of the surrounding cultural heritage landscape.

Discussion: The front entrances for both buildings are at the corner. They set the buildings off and are reminiscent of the verandah at 234 Beechwood Avenue that wrapped around the corner.

12. If brick and stone cladding is proposed, it will extend to all façades and not be used solely on the front façade. Other cladding materials may be appropriate.

Discussion: See the plans and elevations which respect this guideline and offer material consistency on all elevations.

13. The use of modern materials such as plastic or fiberglass to replicate architectural details such as columns, balusters or bargeboard is not acceptable and will not be permitted.

Discussion: This is not applicable to new development; however, the material palette does not include plastics or fiberglass replication.

5.7 Development Impacts

Positive impacts of the proposed development on the cultural heritage values of the Rockcliffe Park HCD include:

- The proposed development of multi-unit residential buildings on the interior lots fronting onto Beechwood Avenue is a positive impact in that it will support through density the traditional Mainstreet;
- The proposed infill development adds to the rich mix of building types and styles from all eras and is in keeping with recent architectural expressions.
- The design approach employs materials to define the buildings as a collection of smaller buildings, and the retention of low stone retaining walls that define the original lots are positive impacts on the streetscape rhythm.
- The parkette at the street with widened sidewalk and plantings is a nice addition to the narrow sidewalk and will be appreciated by pedestrians waiting for buses. The use of a similar paver to what is used in the courtyard across the street helps to unify this section of Beechwood.
- The heights are in keeping with the height of the podium across the street.

Adverse impacts of the proposed development include:

- The contrast in scale between the four storey building and the adjacent one storey building to the north;
- The proposed roof top amenity area may have the potential to interfere with the privacy of the units to the west in Black Maple Private. The concept drawings do not define the relationship of the two buildings to the properties in Black Maple Private;

6.0 ALTERNATIVES AND MITIGATION STRATEGIES

6.1 Alternatives

Throughout the design process, alternatives were explored.

Landscape:

Low stone retaining walls were retained and rebuilt where necessary to define the original lot lines between properties that will add to the visual rhythm of the streetscape and mitigate the 30m frontage of the two buildings.

The same pavers as the ones across the street are being used to emphasis the public realm and sense of community.

Trees have been planted inside the property to prevent the need to remove them when the street is widened.

The depth to bedrock, which is assumed to be close to the existing finish grade at the street property line, will necessitate an increased soil volume to support soft landscape features – trees, shrubs, etc. An increased soil volume could be achieved by raising the height of the proposed retaining wall at the sidewalk.

Architecture:



Materiality – A red brick and the red wood siding used on 455 Green are an appropriate tone that is compatible with the two remaining early brick clad building at 449 and 451 Green Avenue.

The distinct materiality and colour of exterior finishes between the two buildings helps differentiate and buildings.

Beechwood Avenue has a modest curve in the area that may provide some opportunity to stagger the building setbacks or allowed balcony projections.

6.2 Mitigation measures

The following are mitigation measures that could be explored, include:

The balconies illustrated on the June 23, 2020 perspectives are more expressive of a residential use. The balconies address the street and help to mitigate the scale of the building within the streetscape. False balconies where a railing is applied to the exterior wall face should be avoided; and,

The four-storey building would be more appropriately set against the three storey building at 455 Green Avenue, and the three storey building would be more appropriate set against the one storey building to the north. The CDP suggests that the building height



should increase from east (north) to west (south) following the gradual descent of Beechwood Avenue. It is recognized that with the existing zoning this is not possible.

The two sites to the north (249 and 251) are zoned for multi-family residences and the Beechwood Avenue CDP envisioned a three storey multi-unit residential building on the site.

Introducing ground floor street access at the south portion of 229 Beechwood Avenue where the grade drops off could offer a bit of variety and help retain the relationship of individual homes along the street.

6.3 Conclusions

The redevelopment of this section of Beechwood Avenue is respectful of the traditional character of Rockcliffe Park's Panhandle and is an appropriate approach to Beechwood Avenue Community Design Plan.

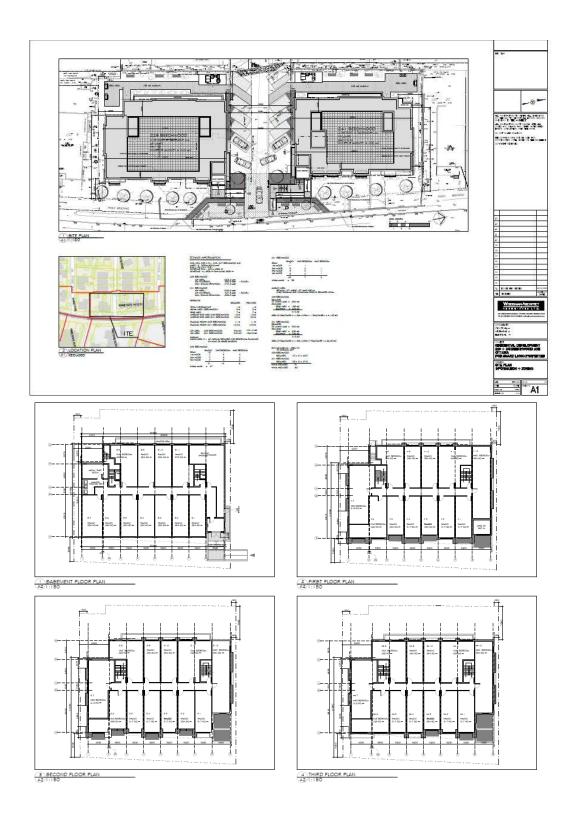
The proposal entails the consolidation of two lots to the south and three lots to the north of the closed section of Carsdale Avenue to create two large lots. The scale of the proposed development is not as fine grained as the traditional lot pattern in the 'Panhandle'. As noted in the BACDP, larger buildings should be designed as a collection of smaller buildings, which is the approach taken. The separation between buildings also helps to convey the street rhythm and supports the pastoral qualities and neighbourly traditions of the Village.

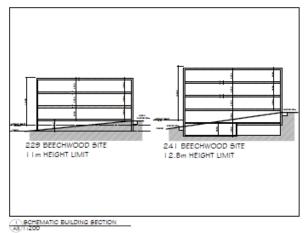
Beechwood Avenue extending through to Corona Avenue has been designated a 'Traditional Main Street', and a comprehensive streetscape design will be implemented along the entire length of the street through to Corona Avenue on the west side of Beechwood Avenue. Given that the proposed development is located along the primarily commercial Main Street that is rapidly developing, this development will serve as a successfully example. The building heights relate to the podium across the street and a similar palette of finishes and colours is used. The scale is respectful of the residential neighbourhood that lies adjacent to the site. The landscape treatment of the small parkette uses the

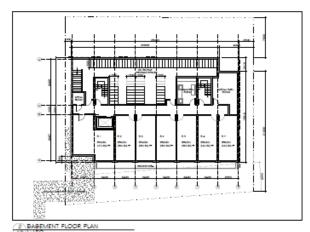
same pavers as the courtyard across the street acknowledging the public realm and capitalizing on a shared mid-block open space.

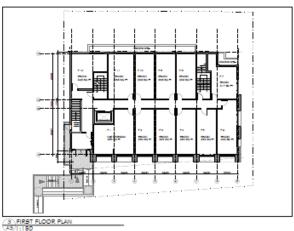
The future widening of Beechwood Avenue will have an adverse impact on the five mid-block properties when completed. The only distinguishing heritage attributes of the properties are the low stone retaining walls at the sidewalk, which will be removed when the street is widened. Trees have been planted inside the property to prevent the need to remove them when the street is widened. The renderings also illustrate the existing retaining walls on the properties, which will be effective in delineating the vestiges of the original lot patterns. Portions of the stonewalls are also being replaced on the property and will interpret these defining features.

7.0 229 -241 Plans and Elevations

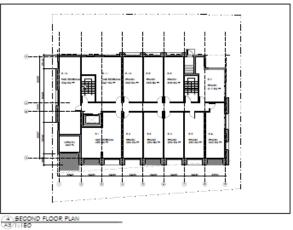


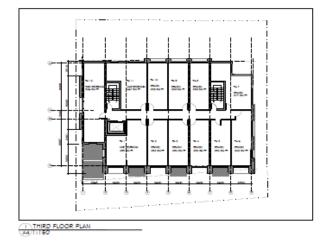


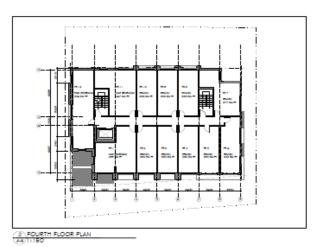












8.0 AUTHORS QUALIFICATIONS

Commonwealth Resource Management is an integrated consulting and management firm that offers a range of professional services related to conservation, planning, research, design, and interpretation for historical and cultural resources. A key focus of the practice is planning and assessment of heritage resources as part of the development process.

John J. Stewart, B.L.A., O.A.L.A., C.S.L.A., CAHP, a principal of Commonwealth is a specialist in the planning and design of cultural resources, building conservation, and commercial area revitalization. A graduate of the University of Guelph, he received additional training at Cornell University (USA) and Oxford University (UK) and holds a diploma in the Conservation of Monuments from Parks Canada, where he worked as Head, Restoration Services Landscape Section. Before Commonwealth's formation, Stewart served for four years as the first director of Heritage Canada's Main Street Program.

Stewart is a founding member of the Canadian Association of Heritage Professionals. He has served as the Canadian representative of the Historic Landscapes and Gardens Committee of ICOMOS and the International Federation of Landscape Architects. Stewart is a panel member with the Ottawa Urban design Review Panel and a board member of Algonquin College Heritage Carpentry Program.

lan Hunter, is a specialist in the research and assessment of cultural heritage resources and building conservation. Experience in the heritage conservation field extends over 30 years primarily working for Commonwealth Historic Resource Management.

The research and project file were undertaken by Ian Hunter, John Stewart managed the project and prepared the report.