

MARITIME WAY - FORMAL UDRP

June 4, 2021



SITE CONTEXT

North

To the north across Maritime Way is a five-storey hotel (Marriott Towne Place Suites), a 1.64 ha lot that is currently vacant and the 'William's Court' residential complex which comprises five buildings 5-12 storeys in height.

East

To the east is a 2.98 ha vacant parcel, from which the Subject Site was recently severed. Further east is a stormwater management pond.

West

To the west is 'Timberwalk', a recently completed seven storey retirement home. At 6301 Campeau Drive applications have been filed to develop 798 residential units in buildings of up to ten storeys. Across Kanata Avenue, the main commercial portion of the Kanata Town Centre, with a ten-storey hotel and a large area of retail further west including a Walmart.

South

To the south is provincial highway 417. Across the highway is the Holy Trinity Catholic High School, The Royal Canadian Legion Dominion Command and the Kanata Leisure Centre. Further south is predominantly residential.





Surrounding Uses

- | | | | |
|---|----------------------------------|---|------------------------------------|
| 1 | OC Transpo Terry Fox Park & Ride | 6 | Kanata Leisure Centre & Wave Pool |
| 2 | Loblaws Supermarket and pharmacy | 7 | Holy Trinity Catholic Highschool |
| 3 | Walmart Supercentre | 8 | Appletree Medical Centre |
| 4 | CIBC Bank and ATM |  | Existing OC Transpo (Within 250 m) |
| 5 | Morning Owl Coffee |  | Future Light Rail Transit Station |
| | |  | Pathways |

EXISTING CONDITIONS - SITE AND SURROUNDING CONTEXT



1. View of the Subject Site Looking West



2. Existing Conditions on the Subject Site

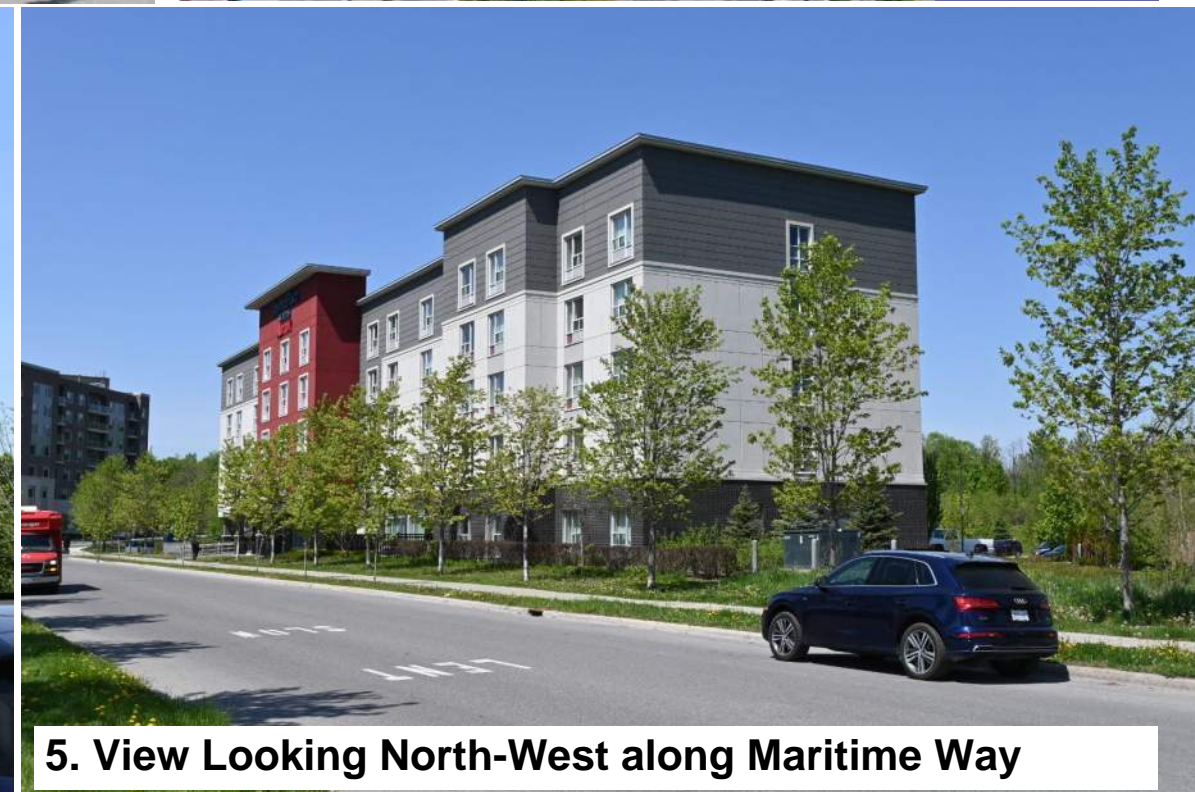
EXISTING CONDITIONS - SITE AND SURROUNDING CONTEXT



3. View Looking North-East along Maritime Way



4. Surrounding Context along Maritime Way



5. View Looking North-West along Maritime Way

EXISTING CONDITIONS - SITE AND SURROUNDING CONTEXT



6. Existing Building Heights along Maritime Way



7. View Looking North-East Across Kanata Ave towards the Subject Site

POLICY CONTEXT - OFFICIAL PLAN

Per **Section 3.6.2** of the Official Plan, the subject site is within a Mixed Use Centre and a Town Centre. The section speaks to the importance of these centres (Kanata is one of three in Ottawa):

Town Centres and Mixed Use Centres occupy strategic locations on the Rapid-Transit network and act as central nodes of activity within their surrounding communities and the city as a whole. These centres are a critical element in the City's growth management strategy, being areas with potential to achieve high densities and compact and mixed-use development oriented to rapid transit. More jobs and housing at these locations will increase transit ridership and draw more commuter travel to these locations. In the long term the centres will become complete, livable communities that attract people for the jobs, leisure, lifestyle, and business opportunities they provide.

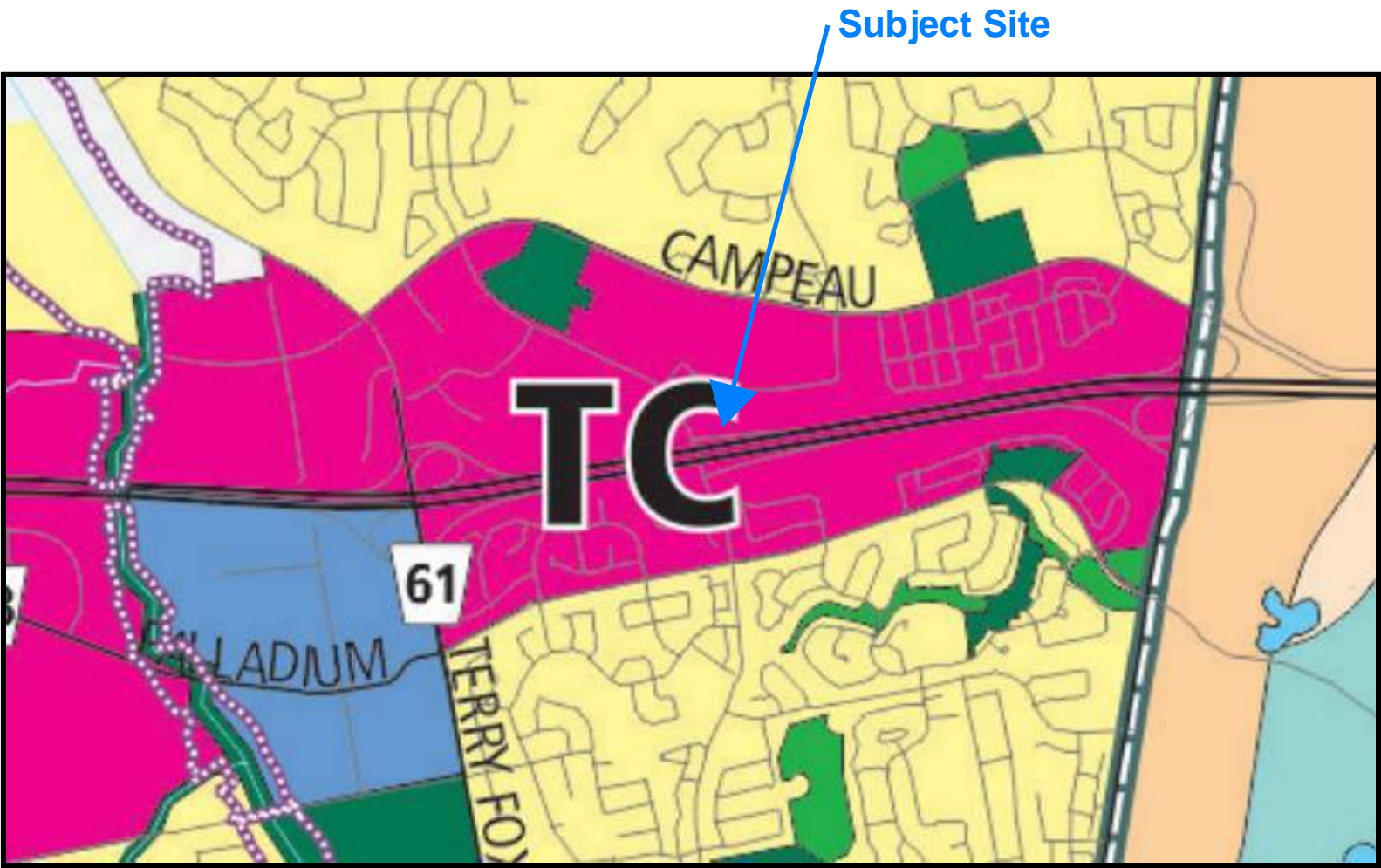
Policy 1 goes on to describe the criteria a Town Centre should meet. Criteria a), b) and c) are of particular relevance:

- a. Most of the centre is within an 800m walking distance of one or more rapid transit stations, and contains one or more arterial roads with all day, frequent transit service;*
 - b. There is opportunity to achieve high densities of jobs and housing through intensification and redevelopment of older sites and development of vacant land;*
 - c. High-Rise Buildings of 10 storeys and more can be accommodated in a manner that provides appropriate transition to the surrounding area;*
- The proposal is a good example of a site and a development that meets all these criteria.*

Policy 5 outlines the uses that Mixed Use Centres permit high density residential.

Policy 9 provides that: "All development applications and Community Design Plans for Mixed-Use Centres will be reviewed in the context of this Plan and in particular:

- 1. Will be evaluated in the context of the Design Objectives and Principles in Section 2.5.1 and the criteria set out in Section 4.11, particularly with regard to achieving a compact, mixed-use, transit-oriented, pedestrian-friendly environment and creating a place with visual interest;*
- 2. Will, where possible, contribute to a range of housing options in the area.*



POLICY CONTEXT - SECONDARY PLAN

Following is an assessment of the proposal against the relevant provisions of the Kanata Town Centre Secondary Plan. The plan's vision statement is that:

"The Kanata Town Centre will be an active, vibrant core area containing a diverse mix of urban uses arranged in a form and a scale, which is both human and appropriate to its function. It will be a uniquely identifiable place that caters to residents and attracts visitors and will be a constant source of pride for future generations."

At 5.7.4 Broad Issues, Residential is described as follows:

"As the City grows and matures, demographic indicators suggest the need for residential development, which caters to smaller household size, lower incomes, and households which are less dependent on the private automobile."

The proposal addresses this issue by providing 1 and 2 bedroom rental apartments which are inherently more affordable than larger ground based dwellings, within 400m of a BRT stop that will become an LRT station in the future.

Land Use designations are mapped in Schedule B1 and described at Section 5.7.5. The subject site is designated CBD (Central Business District). It is the intent of this designation that these lands become: "an urban place that provides for employment uses, complemented by higher density residential uses."

In relation to phasing, the plan states that: "It is expected that residential developments within the Central Business District may occur sooner than office and commercial developments and this is permitted provided the capacity to achieve the 10,000 job target within the Central Business District and Regional Shopping Centre designations is retained."

As a number of parcels in the CBD designation remain undeveloped, it is expected that the 10,000 jobs target can be achieved.

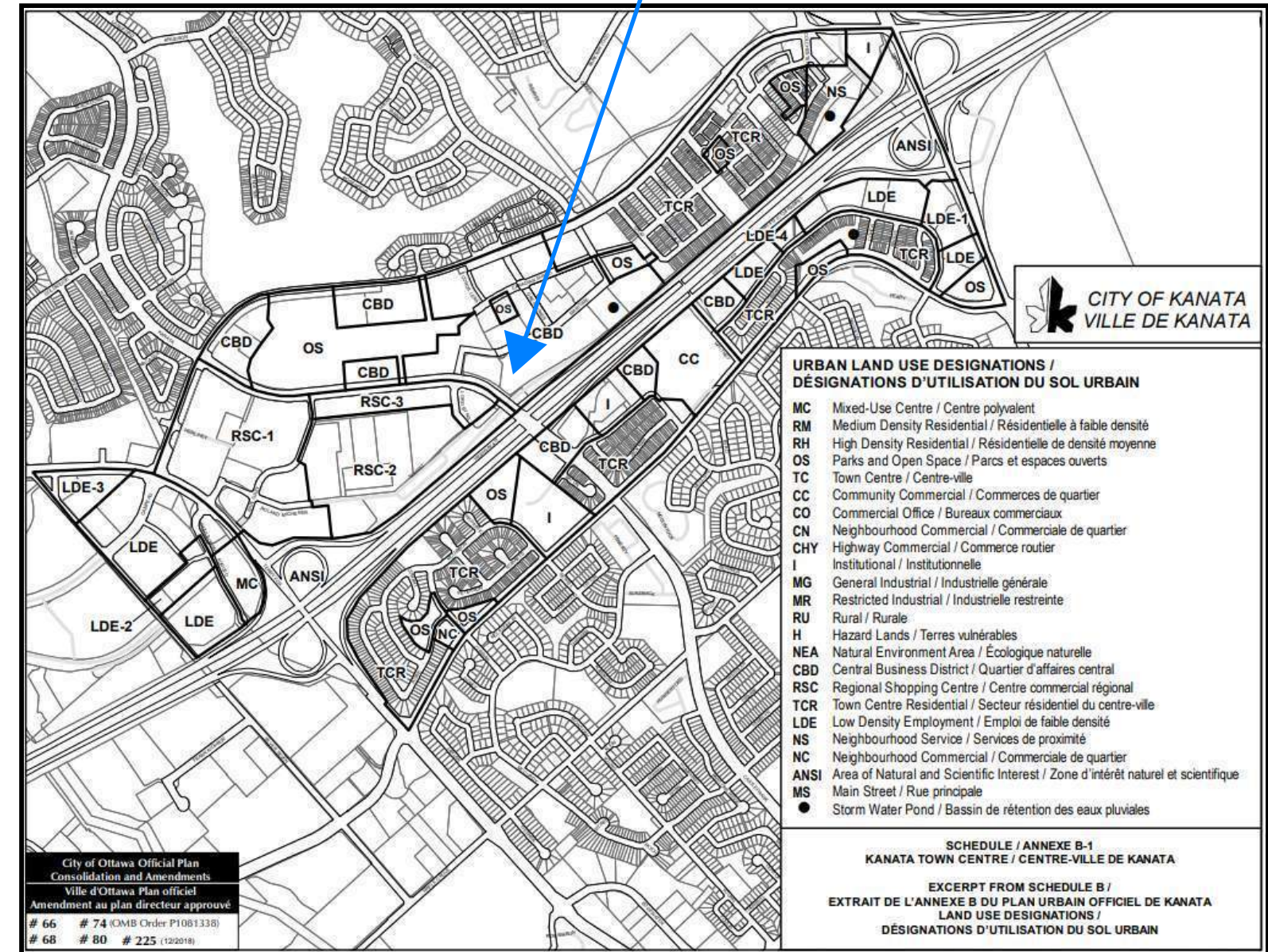
Schedule B2 and Section 5.7.5.8 set out building heights. The subject site is designated for 10-30 storey high-rise development. In combination with Schedule B2, the following applies to this designation:

1. Subject to a zoning amendment, a High-Rise 10 to 30 storey building may be considered where it is demonstrated that the development satisfies all of the following:

- A minimum of three storeys is required for residential and office development. [Amendment #136, LPAT # PL141243, September 18, 2020]*
- Meets the Urban Design and Compatibility policies in Section 4.11 of the Official Plan.*
- Meets the Designing Ottawa policies in Section 2.5.1 of the Official Plan.*
- The appropriate maximum building height will be determined on the basis of site conditions and constraints and the ultimate height must be supportable by the required submissions for a Zoning By-law Amendment, which will include but is not limited to a Transportation Impact Study, Geotechnical Study, Servicing Brief, Sun Shadow Study, and a Concept Plan demonstrating compliance with Section 4.11 of the Official Plan. [Amendment #136, October 16, 2014]*

In response to the above, the proposal is a minimum of seven storeys. It meets the policies at Sections 2.5.1 and 4.11 of the Official Plan. The required submissions form part of the Zoning By-law Amendment and Site Plan applications.

Subject Site



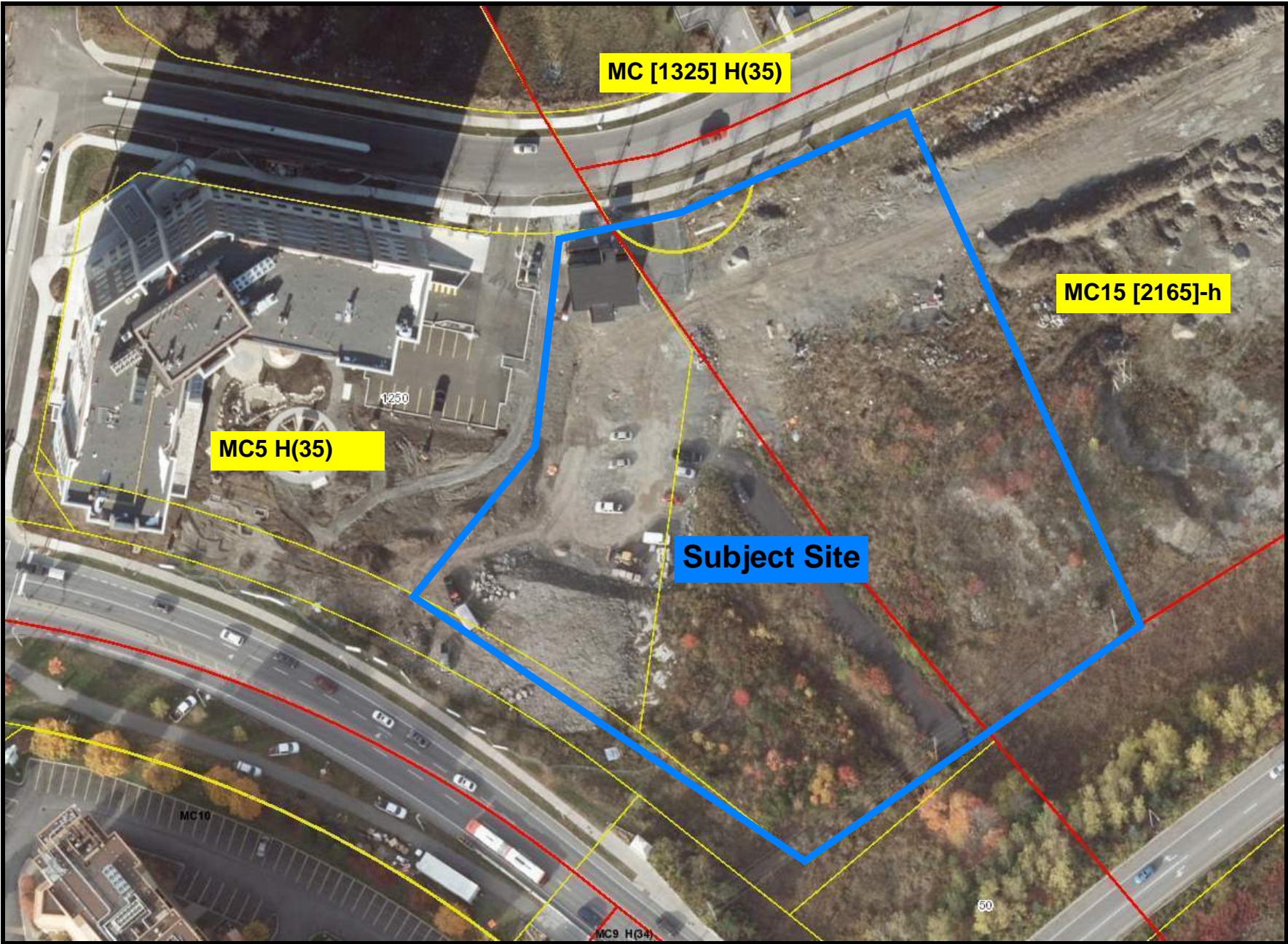
REGULATORY CONTEXT - ZONING BY-LAW

The site is currently in two zones – the western part is Mixed Use Centre sub-zone 5 with a height limit (MC5 H[35]). The eastern part is Mixed Use Centre sub-zone 15 with an exception and a hold (MC15[2165]-h).

The purpose of the MC zone is to:

- 1) ensure that the areas designated Mixed-Use Centres in the Official Plan, or a similar designation in a Secondary Plan, accommodate a combination of transit-supportive uses such as offices, secondary and post-secondary schools, hotels, hospitals, large institutional buildings, community recreation and leisure centres, day care centres, retail uses, entertainment uses, service uses such as restaurants and personal service businesses, and high- and medium-density residential uses; (By-law 2015-293)
- 2) allow the permitted uses in a compact and pedestrian-oriented built form in mixed-use buildings or side by side in separate buildings; and
- 3) impose development standards that ensure medium to high profile development while minimizing its impact on surrounding residential areas.

Apartment dwelling high-rise is a permitted use in both the MC5 and MC15 sub-zones. They also list zoning provisions additional to the MC zone. Of note, the MC5 zone has a height limit of 35m and the exception that applies to the eastern part of the site (Exception 2165) introduces a height limit of 67m.



URBAN DESIGN GUIDELINES FOR HIGH-RISE BUILDINGS

Approved by City Council in 2018, the City of Ottawa's Urban Design Guidelines for High-Rise Buildings are to be used during the review of development proposals to promote and achieve appropriate high-rise development. The design guidelines will be applied wherever high-rise residential buildings are proposed. Following is an assessment of the proposal against the applicable sections of the guidelines.

1. Context

At the site level:

1.12 Include base buildings that relate directly to the height and typology of the existing or planned streetwall context.

The existing streetwall context of this section of Kanata Avenue comprises the nine storey Holiday Inn on the west side and the seven storey Timberwalk retirement home on the east, with landscaped setbacks. The proposal is consistent with this as it has a seven storey base building facing Kanata Avenue and completing this section of Kanata Avenue.

1.13 An angular plane, typically 45°, measured from the relevant property lines, should be used to provide a frame of reference for transition in scale from proposed high-rise buildings down to lower scale areas.

There is an angular plan transition from the towers to the podium. They are located at the edge of the Kanata Town Centre, separated from Kanata South by the wide right of way required by Highway 417. The transition to the surrounding area to the north, east and west is via the seven-storey podium. Combined with the separation of the buildings from neighbouring mid-rise buildings, the transition is suitable.

2. Built Form

Base-middle-top:

2.3 Depending on the function and context, high-rise buildings can take many different forms to serve both the experience and expression functions:

a. high-rise building that includes three distinctive and integrated parts base, middle, and top is generally accepted as a good approach to built form design in order to effectively achieve many urban design objectives.

The proposal is consistent with this policy, having a clear base, middle and top.

Placement:

2.13 Place the base of a high-rise building to form continuous building edges along streets, parks, and public spaces or Privately Owned Public Space (POPS):

a. where there is an existing context of streetwall buildings, align the facades of the base with adjacent building facades;

b. in the absence of an existing context of streetwall buildings, create a new streetwall condition to allow for phased development and evolution.

The facades of the buildings address Maritime Way and Kanata Avenue, enclosing and animating the streets. The existing streetwall context of this section of Kanata Avenue comprises the nine storey Holiday Inn on the west side and the seven storey Timberwalk retirement home on the east. The proposal is consistent with this as it has a seven storey base building facing Kanata Avenue and completing this section of Kanata Avenue.



URBAN DESIGN GUIDELINES FOR HIGH-RISE BUILDINGS

Height and Transition:

2.15 The maximum height of the base of a proposed high-rise building should be equal to the width of the ROW (Diagram 2-6) to provide sufficient enclosure for the street without overwhelming the street.

The rights of way that the site has frontage to vary in width 26m for Maritime Way and 50m for Kanata Avenue (and 150m for Highway 417 but this less of a public realm). The proposed podium is approximately 22.5m high, consistent with the narrowest right of way. A lower podium would not provide adequate enclosure, particularly to Kanata Avenue as it enters the town centre.

2.19 For sites where the adjacent context is lower-scale and not anticipated to change:

- the height of the base or the portion of the base immediately adjacent to the neighbouring lower-scale buildings should match the height of the neighbouring buildings (Diagram 2-6 and 2-7); and
- provide a transition in height on the base through setbacks and architectural articulation (Diagram 2-6 and 2-7).

The buildings closest to the subject site (the retirement home and the Marriott hotel on Maritime Way and the Holiday Inn Hotel on Kanata Avenue) are mid-rise buildings of 5 to 9 storeys without distinct podiums. The podium of this taller proposal reflects this, being 7 storeys. The distinct outline of the buildings, with the podium extending well beyond the towers in one direction clearly differentiates the podium.

Articulation and materials:

2.20 Respect the character and vertical rhythm of the adjacent properties and create a comfortable pedestrian scale by:

- breaking up a long façade vertically through massing and architectural articulation to fit into the existing finer grain built form context (Figure 2-13);
- determining appropriateness of larger-scale façades in certain areas, such as along the ceremonial routes (Figure 2-14); and
- introducing multiple entrances, where possible, through creative store layout and organization where a large format retail use is located on the ground floor.

The ground level of the proposal is extensively glazed and activated by amenity uses (e.g. gym, lounge) and ground level apartments. The buildings have multiple entrances fronting the area in between the two buildings and connected by curbed sidewalks. The proposed cladding emphasizes these entrances and animates the street level, podium and tower portions of the development while breaking up the longest portions of the façade.

2.24 Encourage small tower floor plates to minimize shadow and wind impacts, loss of skyviews, and allow for the passage of natural light into interior spaces (Figure 2-15):

- the maximum tower floor plate for a high-rise residential building should be 750m² (Diagram 2-8);

The floor plates of approximately 720m² and 880m² are generally consistent with this policy, and allow for natural light to enter between the buildings and into interior spaces at all levels.

2.25 Provide proper separation distances between towers to minimize shadow and wind impacts, and loss of skyviews, and allow for natural light into interior spaces:

- the minimum separation between towers should be 23m (Diagram 2-9);

The two towers are separated by 23m at the narrowest point, as the vee shape between the towers results in a wider separation closer to Maritime Way.

2.29 Step back the tower, including the balconies, from the base to allow the base to be the primary defining element for the site and the adjacent public realm, reducing the wind impacts, and opening skyviews:

- a step back of 3m or greater is encouraged.
- the minimum step back, including the balconies, should be 1.5m; and

The podium of the buildings extends well beyond the towers, closest to the street. This design, and distinct building outline clearly differentiates podium from tower. The base of the buildings addresses the public realm, and the vee shape creates an inviting area with visitor parking, pedestrian connections and landscaping between the two podiums.

2.35 The top should be integral to the overall architecture of a high-rise building, either as a distinct or lighter feature of the building or a termination of the continuous middle portion of the tower.

2.36 Integrate roof-top mechanical or telecommunications equipment, signage, and amenity spaces into the design and massing of the upper floors.

Mechanical elements have been designed to line up with elements of the building cladding, and as such are visually minimized on top of the tower, and the podium.



URBAN DESIGN GUIDELINES FOR HIGH-RISE BUILDINGS

3. Pedestrian Realm

3.1 Provide a minimum 6m space between the curb and the building face along the primary frontages of a high-rise building, including the City-owned portion within the right-of-way (ROW) and the building setback area:

- a. the pedestrian clearway must be within the ROW;
- b. on a street with commercial character, introduce hard surfaces between the curb and the building face to maximize the walkable area and provide flexible spaces to accommodate seasonal uses such as outdoor patios, where appropriate; and
- c. on a street with residential character, introduce landscaping and/or residential patios between the sidewalk and the building face to allow for public-private transition.

The proposal has a landscape setback to the curb on Maritime Way and Kanata Avenue of more than 6m, and additional landscaped area is proposed in the vee shaped area between the two buildings. The emerging character of the area is of mid-rise buildings that front the street, combined amenity or access areas mostly enclosed by buildings but also visible from the street. Good examples of this is are the Williams Court development further east on Maritime Way and the Timberwalk retirement home next to the Subject Site. The proposal is consistent with this character.

3.8 Where appropriate, break up larger street blocks or larger development parcels by introducing mid-block pedestrian or multi-use connections, public or private, outdoor or indoor to increase and enhance the overall pedestrian accessibility and walkability of the area.

The proposal makes use of portion of a large vacant parcel along Maritime way, and as the rest of the site is currently undeveloped, there are few opportunities to introduce pedestrian connections, but the site design allows for future connections to be made on, or around the site.

3.9 When a mid-block connection is on private lands, it should be properly signed and designed to welcome pedestrians and may be integrated into the lobby or atrium of a high-rise building.

The proposal connects directly to Maritime Way and Kanata Avenue and provides a connection between the two. Maritime Way, Kanata Avenue and Lord Byng Way provide a convenient connection to the BRT and future LRT station which is 400m-500m. The activated facades facing the public realm animate it.



Red Oak Retirement Residence - Maritime Way frontage

SUSTAINABILITY STATEMENT

Create intensified, mixed-use development, incorporating amenities and encourage public realm at nodes and gateways by concentrating height and mass and articulating facade features to create scale and encourage pedestrian flow.

Transition in the scale and density of the built form on the site next to lower density neighbourhoods to create accessibility to rapid transit hub.

The ground floor of the base to be animated and transparent encouraging safety and properly illuminated transition areas. Articulate with quality materials, rhythm, and design features as active and animated surfaces.

Landscape the area in front of a buildings and in between them to create a welcoming pedestrian approach. Foundation planting, trees, street furniture, permeable paved areas and walkways to the public sidewalk to encourage pedestrian use and activity as well as creating shade for the users.

Well insulated building envelope and increased energy efficiency informed with recommendations from a building energy report including energy efficient HVAC and electrical installations.

Use of clear glass low-e argon sealed unit glazing and doors to achieve good thermal efficiency.

Use of green vegetative roofs for water retention.

Use of white membrane roofs and light colored pavers to minimize solar reflection.

PLANS OF PROPOSED DEVELOPMENT

Site Plan

Floor Plans

Elevations

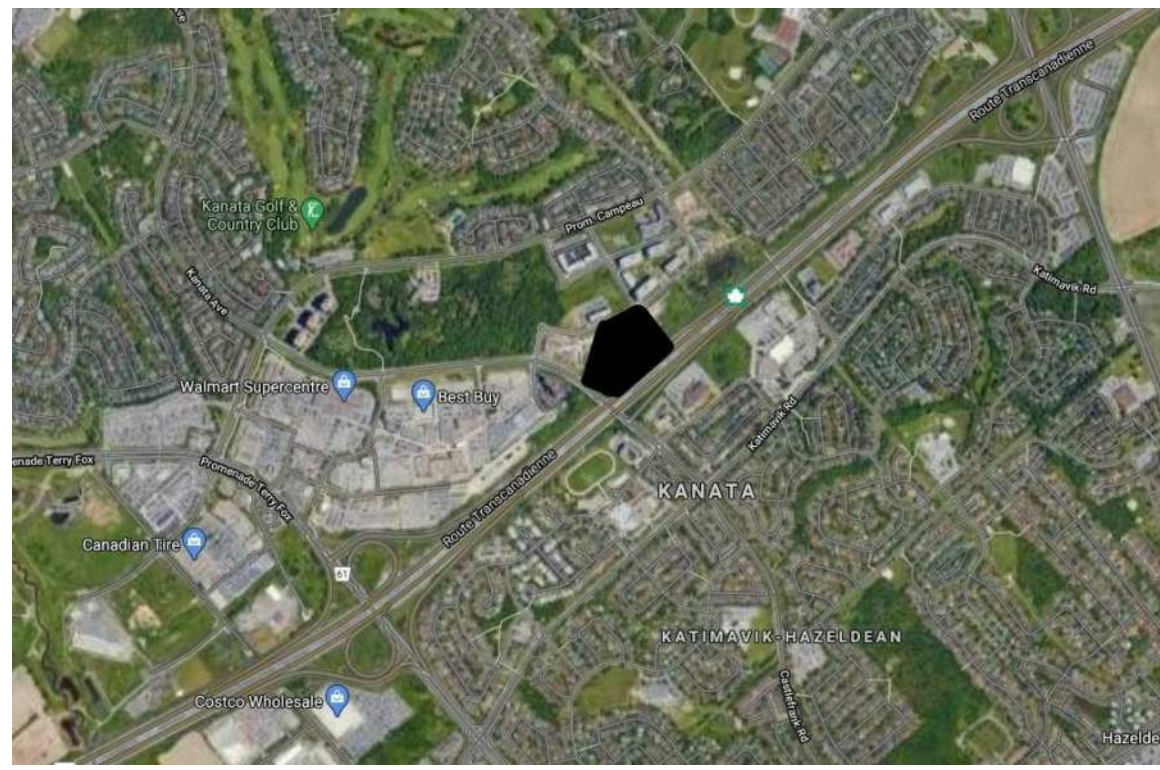
Section

Sun Study

3D Renders

Landscape Plan

General Plan of Services



KEY PLAN

ZONE AM10		
PROVISION	REQUIRED	PROVIDED
Min Lot Width	no minimum	+/- 69.65 m
Min Lot Area	no minimum	+/- 12 808 m ²
Max Building Height	67m	+/- 93.5 m
Min Front Yard Setback	no minimum	3.05 m / 3.09 m
Min Corner Side Yard Setback	no minimum	16.74 m
Min FSI	2	+/- 4.88
Min Interior Side Yard Setback	no minimum	14.70 m / 15.13 m

SITE AREA :	+/- 12 808 sq.m. (To be confirmed by surveyor)
SITE COVERAGE :	+/- 2 207 m ² (East Tower) +/- 1 968 m ² (West Tower) Total = +/- 4 175 m ² = 32.6 %
GROUND PARKING AREA :	+/- 2 298 m ² = 17.9 %
LANDSCAPED AREA (EXCLUDING PARKING) :	+/- 6 335 m ² = 49.5 %

RENTAL - EAST TOWER

PROPOSED GROSS FLOOR AREA :	+/- 21 964 m ²
BASEMENT G.F.A. :	+/- 0m ²
GROUND FLOOR G.F.A. :	+/- 635 m ²
RENTAL FLOORS G.F.A. (2nd to 30th floor) :	+/- 21 329 m ²
PRIVATE AMENITY AREA (G.F.A.) :	+/- 1 953 m ²
COMMUNAL AMENITY AREA :	+/- 1 925 m ²
NUMBER OF FLOORS AND BUILDING HEIGHT :	28 FLOORS + MECH. / +/- 87.50m
DWELLING UNITS :	300
PARKING STALLS :	315 (300 INSIDE / 15 VISITORS OUTSIDE)
PROVIDED BICYCLE STALLS :	150 (142 INSIDE / 8 OUTSIDE)

NUMBER OF SUITES REQUIRED TO BE BARRIER-FREE :
300 UNITS = 45 UNITS HAVE TO BE BARRIER-FREE
THEY WILL BE DISTRIBUTED BETWEEN THE 28 FLOORS

RENTAL - WEST TOWER

PROPOSED GROSS FLOOR AREA :	+/- 30 179 m ²
BASEMENT G.F.A. :	+/- 0m ²
GROUND FLOOR G.F.A. :	+/- 375 m ²
RENTAL FLOORS G.F.A. (2nd to 30th floor) :	+/- 29 804 m ²
PRIVATE AMENITY AREA (G.F.A.) :	+/- 2 247 m ²
COMMUNAL AMENITY AREA :	+/- 1 045 m ²
NUMBER OF FLOORS AND BUILDING HEIGHT :	30 FLOORS + MECH. / +/- 87.50m
DWELLING UNITS :	332
PARKING STALLS :	347 (332 INSIDE / 15 VISITORS OUTSIDE)
PROVIDED BICYCLE STALLS :	166 (159 INSIDE / 7 OUTSIDE)

NUMBER OF SUITES REQUIRED TO BE BARRIER-FREE :
332 UNITS = 50 UNITS HAVE TO BE BARRIER-FREE
THEY WILL BE DISTRIBUTED BETWEEN THE 30 FLOORS

- FOR EXISTING SITE CONDITIONS, SEE SURVEY PLAN BY ANNIS, O'SULLIVAN, VOLLEBEK LTD., SUBMITTED SEPARATELY;
- FOR NEW GRADES AND SITE SERVICES, SEE CIVIL ENGINEERING PLAN BY NOVATECH ENGINEERING CONSULTANTS, SUBMITTED SEPARATELY;
- FOR PROPOSED VEGETATION AND LANDSCAPE INFORMATION, SEE LANDSCAPE PLAN BY JAMES B. LENNOX & ASSOCIATES, SUBMITTED SEPARATELY.



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SCEAU / Seal



OUVRAGE / Project
1200 MARITIME WAY (KANATA RENTAL)

EMPLACEMENT / Location
OTTAWA

NO PROJET No.
12371.00

NO	RÉVISION	DATE (aa-mm-jj)
A	FOR COMMENTS	2020.05.28
B	FOR COMMENTS	2020.06.05
C	FOR COMMENTS	2020.07.23
D	IN PROGRESS	2020.09.16
E	SITE PLAN COORDINATION	2020.12.08
F	SITE PLAN COORDINATION	2020.12.16
G	SITE PLAN COORDINATION	2021.02.22
H	PER TRANSPORTATION COMMENTS	2021.05.18

DESSINÉ PAR / Drawn by
PV

VERIFIÉ PAR / Checked
LH

DATE (aa.mm.jj)
05/28/20

ÉCHELLE / Scale
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SITE PLAN AT GROUND FLOOR LEVEL

RÉVISION / Revision
H

NO. DESSIN / Dwg Number
A203

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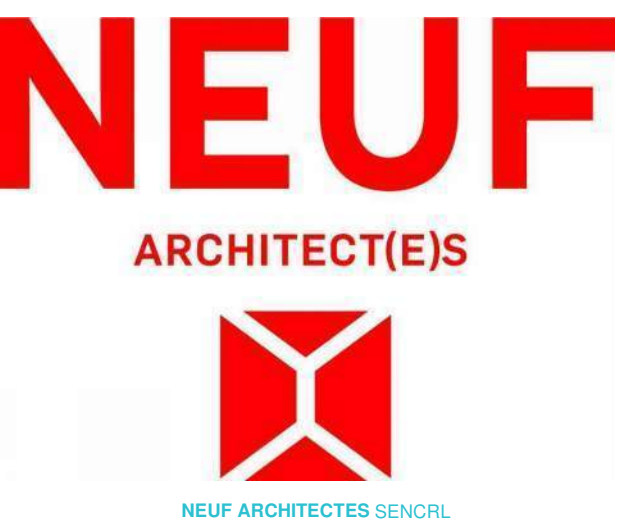
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SCEAU / Seal



CLIENT Client



OUVRAGE Project

**1200 MARITIME WAY
 (KANATA RENTAL)**

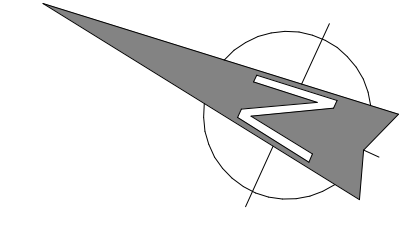
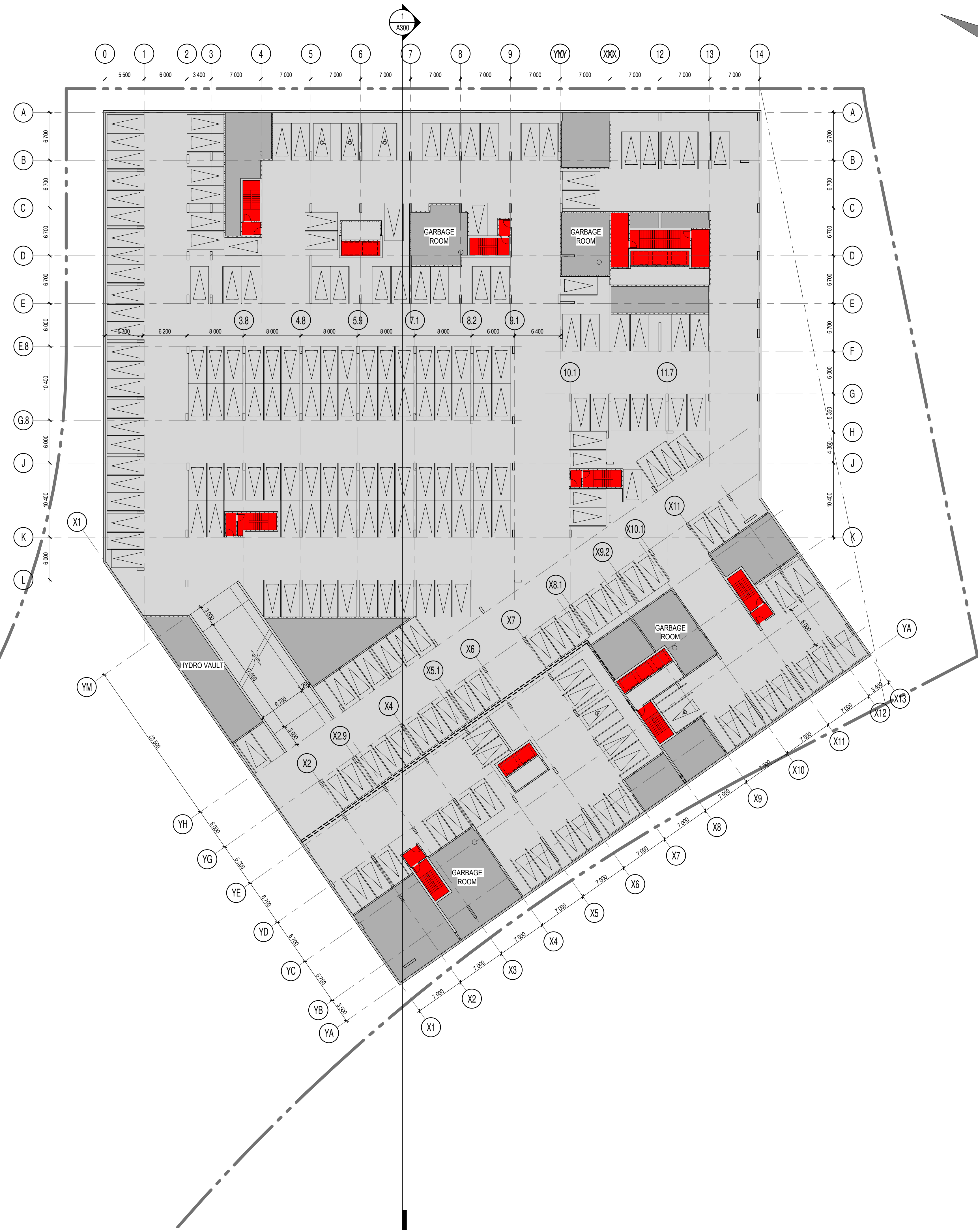
EMPLACEMENT Location NO PROJET No.
 OTTAWA 12371.00

NO	RÉVISION	DATE (aa-mm-ii)
A	FOR COMMENTS	2020.05.28
B	FOR COMMENTS	2020.07.23
C	IN PROGRESS	2020.09.16
D	SITE PLAN COORDINATION	2020.12.08
E	FOR INFORMATION	2021.05.17

DESSINÉ PAR Drawn by PV VERIFIÉ PAR Checked by LH
 DATE (aa.mm.ii) 05/28/20 ÉCHELLE Scale 1 : 300
 TITRE DU DESSIN Drawing Title

PARKING LEVEL 2

RÉVISION Revision NO. DESSIN Dwg Number
E A201



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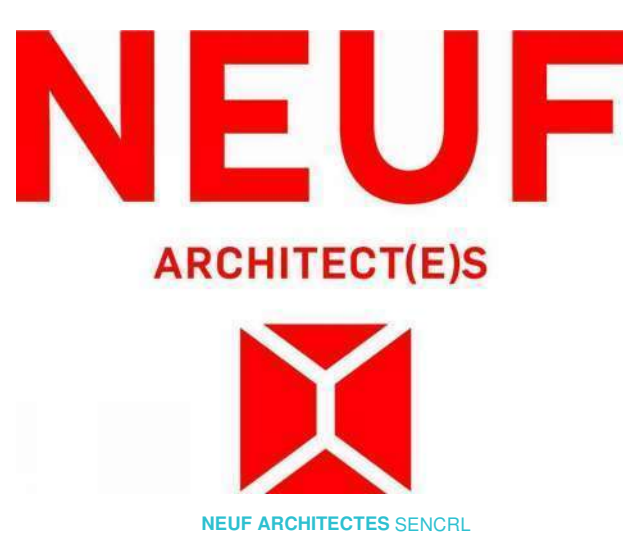
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SCEAU / Seal



OUVRAGE / Project
1200 MARITIME WAY (KANATA RENTAL)

EMPLACEMENT / Location OTTAWA
NO PROJET / No. 12371.00

NO	RÉVISION	DATE (aa-mm-ii)
A	FOR COMMENTS	2020.05.28
B	FOR COMMENTS	2020.06.05
C	FOR COMMENTS	2020.07.23
D	IN PROGRESS	2020.09.16
E	SITE PLAN COORDINATION	2020.12.08
F	FOR INFORMATION	2021.05.17

DESSINÉ PAR / Drawn by PV
DATE (aa.mm.ii) 05/28/20
TITRE DU DESSIN / Drawing Title PARKING LEVEL 1

VÉRIFIÉ PAR / Checked by LH
ÉCHELLE / Scale 1 : 300

RÉVISION / Revision **F**
NO. DESSIN / Dwg Number **A202**



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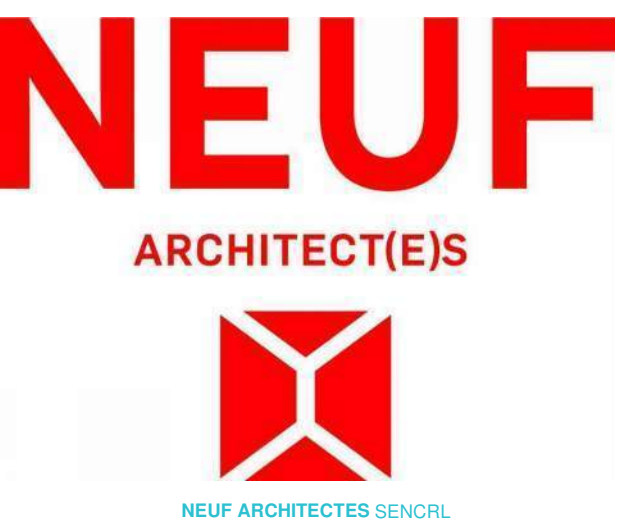
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 1877, Aunga Drive, Ottawa ON K2E 7Z7
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 T 514 847 1117 NEUFarchitectes.com

SCEAU / Seal



CLIENT Client



OUVRAGE Project

**1200 MARITIME WAY
 (KANATA RENTAL)**

EMPLACEMENT Location NO PROJET No.
 OTTAWA 12371.00

NO	RÉVISION	DATE (aa-mm-ii)
A	FOR COMMENTS	2020.05.28
B	FOR COMMENTS	2020.06.05
C	FOR COMMENTS	2020.07.23
D	IN PROGRESS	2020.09.16
E	SITE PLAN COORDINATION	2020.12.08
F	FOR INFORMATION	2021.05.17

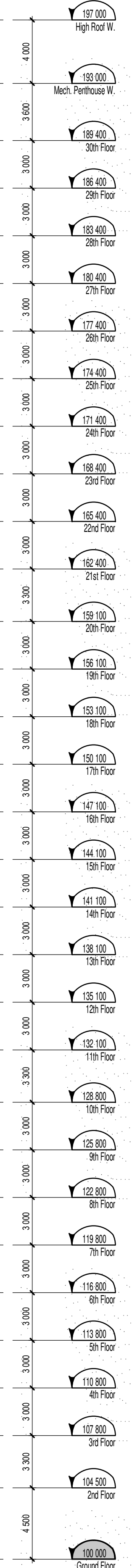
DESSINÉ PAR Drawn by PV VERIFIÉ PAR Checked by LH
 DATE (aa.mm.ii) 05/28/20 ÉCHELLE Scale 1 : 300
 TITRE DU DESSIN Drawing Title

**UPPER TYPICAL FLOOR
 (8th TO 28th / 30th)**

RÉVISION Revision NO. DESSIN Dwg Number
F A206

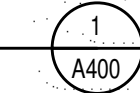
EXTERIOR FINISH LEGEND

NOTE	DESCRIPTION
A	MASONRY VENEER BRICK - BEIGE
B	MASONRY VENEER BRICK - CHARCOAL
C	SEALED THERMAL UNIT / ALUMINUM FRAME
D	GLASS SPANDREL PANEL
E	METAL PANEL CLADDING
F	ALUMINUM GUARD RAIL WITH TEMPERED GLASS
G	MASONRY VENEER ARTIFICIAL STONE
H	METAL SIDING



WEST TOWER - West Elevation

1 : 200



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SCEAU / Seal



OUVRAGE Project
1200 MARITIME WAY (KANATA RENTAL)

EMPLACEMENT Location NO PROJET No.
 OTTAWA 12371.00

NO	RÉVISION	DATE (aa-mm-jj)
A	SITE PLAN COORDINATION	2020.12.08
B	SITE PLAN COORDINATION	2020.12.16

DESSINÉ PAR Drawn by
 MH
 DATE (aa.mm.jj)
 DEC 2020

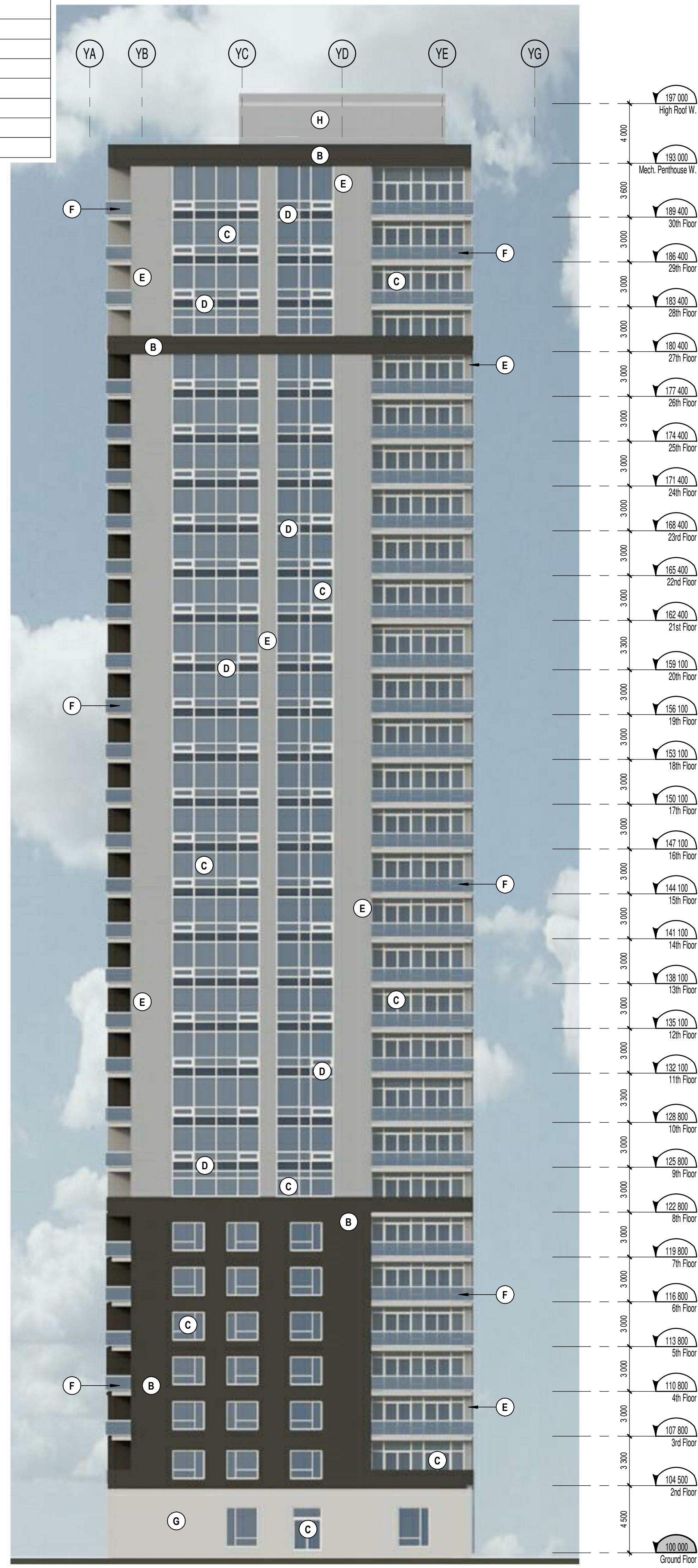
VERIFIÉ PAR Checked
 LH
 ÉCHELLE Scale
 1 : 200

TITRE DU DESSIN Drawing Title
WEST TOWER WEST ELEVATION

RÉVISION Revision NO. DESSIN Dwg Number
B A400

EXTERIOR FINISH LEGEND

NOTE	DESCRIPTION
A	MASONRY VENEER BRICK - BEIGE
B	MASONRY VENEER BRICK - CHARCOAL
C	SEALED THERMAL UNIT / ALUMINUM FRAME
D	GLASS SPANDREL PANEL
E	METAL PANEL CLADDING
F	ALUMINUM GUARD RAIL WITH TEMPERED GLASS
G	MASONRY VENEER ARTIFICIAL STONE
H	METAL SIDING

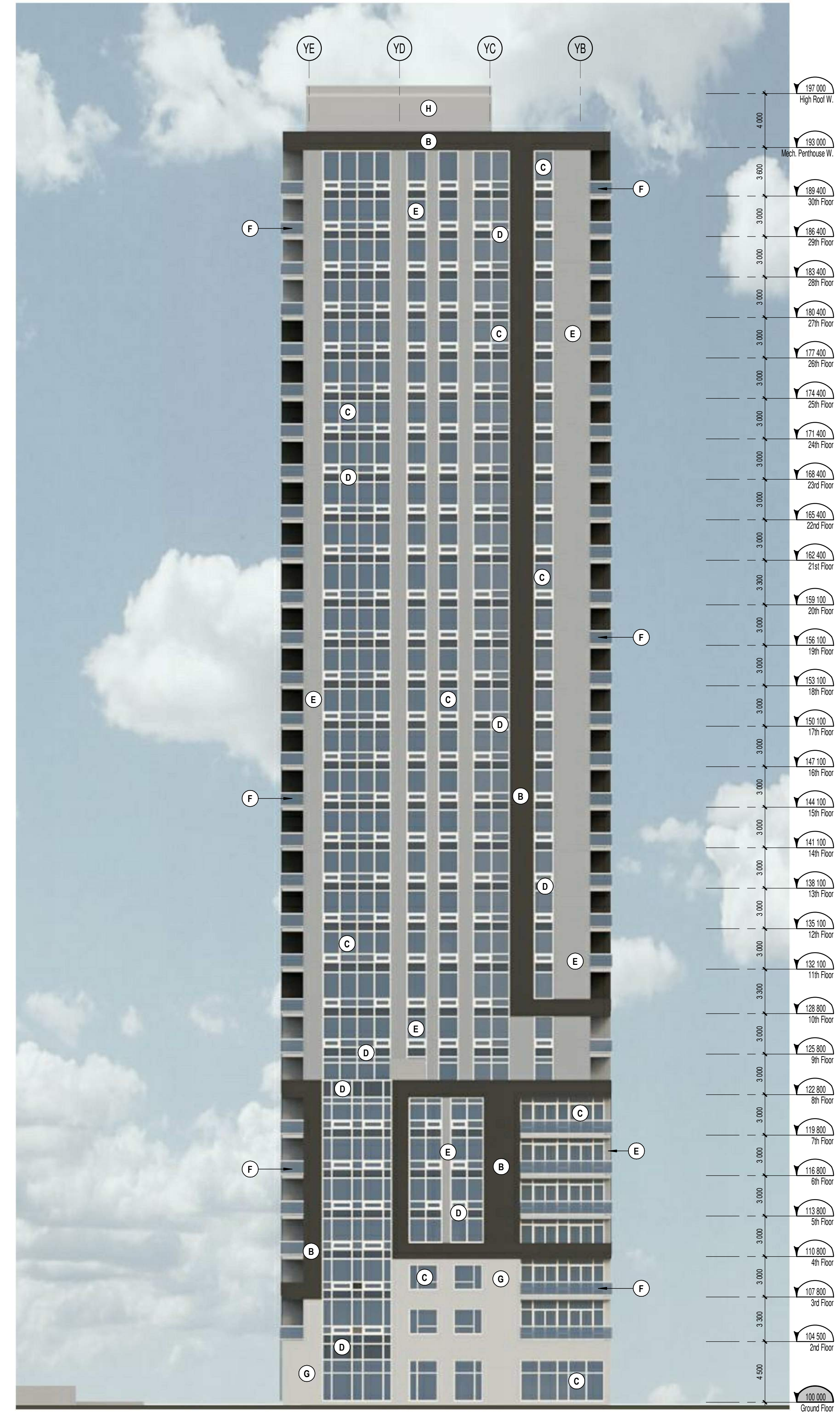


WEST TOWER - South Elevation

1 : 200

1

A401



WEST TOWER - North Elevation

1 : 200

2

A401

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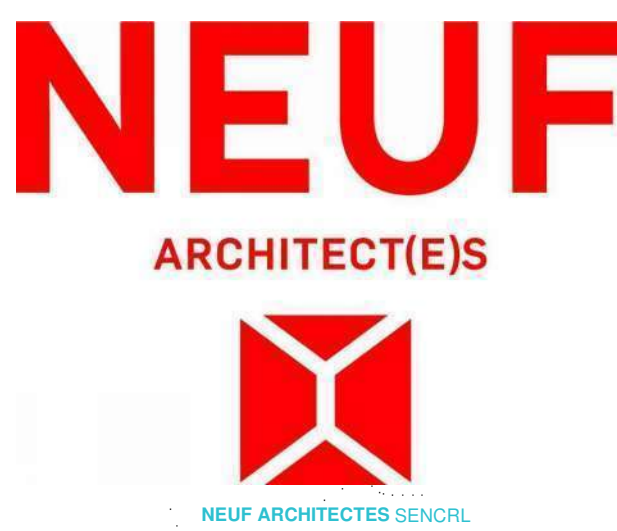
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SCEAU / Seal



OUVRAGE Project
1200 MARITIME WAY (KANATA RENTAL)

EMPLACEMENT Location NO PROJET No.
OTTAWA 12371.00

NO	RÉVISION	DATE (aa-mm-jj)
A	SITE PLAN COORDINATION	2020.12.08
B	SITE PLAN COORDINATION	2020.12.16

DESSINÉ PAR Drawn by
 MH

VERIFIÉ PAR Checked
 LH

DATE (aa.mm.jj)
 12/08/20

ÉCHELLE Scale
 1 : 200

TITRE DU DESSIN Drawing Title
WEST TOWER NORTH & SOUTH ELEV.

RÉVISION Revision NO. DESSIN Dwg Number
B A401

C:\Fichiers Revit\Locaux\12371_ARCH_OPTION 4_R20_pascalvandelee.rvt

EXTERIOR FINISH LEGEND

NOTE	DESCRIPTION
A	MASONRY VENEER BRICK - BEIGE
B	MASONRY VENEER BRICK - CHARCOAL
C	SEALED THERMAL UNIT / ALUMINUM FRAME
D	GLASS SPANDREL PANEL
E	METAL PANEL CLADDING
F	ALUMINUM GUARD RAIL WITH TEMPERED GLASS
G	MASONRY VENEER ARTIFICIAL STONE
H	METAL SIDING



WEST TOWER - East Elevation

1 : 200

1
A402

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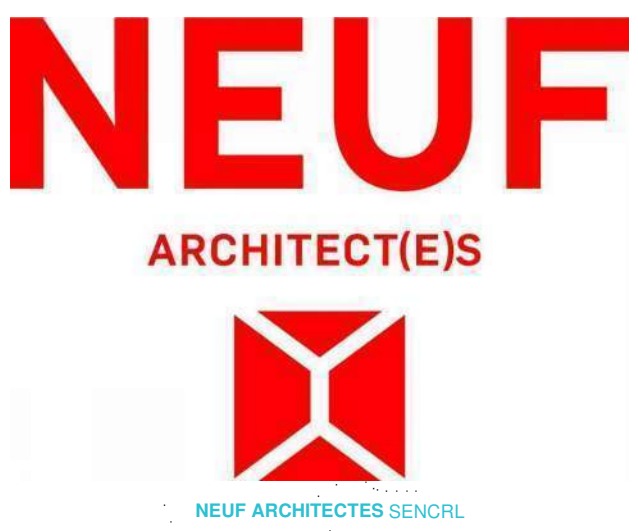
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SCEAU / Seal



OUVRAGE Project
1200 MARITIME WAY (KANATA RENTAL)

EMPLACEMENT Location NO PROJET No.
 OTTAWA 12371.00

NO	RÉVISION	DATE (aa-mm-jj)
A	SITE PLAN COORDINATION	2020.12.08
B	SITE PLAN COORDINATION	2020.12.16

DESSINÉ PAR Drawn by
 MH VERIFIÉ PAR Checked
 LH
 DATE (aa.mm.jj) 12/08/20 ÉCHELLE Scale
 1 : 200
 TITRE DU DESSIN Drawing Title

WEST TOWER EAST ELEVATION

RÉVISION Revision NO. DESSIN Dwg Number

B A402

EXTERIOR FINISH LEGEND

NOTE	DESCRIPTION
A	MASONRY VENEER BRICK - BEIGE
B	MASONRY VENEER BRICK - CHARCOAL
C	SEALED THERMAL UNIT / ALUMINUM FRAME
D	GLASS SPANDREL PANEL
E	METAL PANEL CLADDING
F	ALUMINUM GUARD RAIL WITH TEMPERED GLASS
G	MASONRY VENEER ARTIFICIAL STONE
H	METAL SIDING



EAST TOWER - West Elevation

1:200

1
A403

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CLIENT Client



OUVRAGE Project

**1200 MARITIME WAY
(KANATA RENTAL)**

EMPLACEMENT Location NO PROJET No.
 OTTAWA 12371.00

NO	RÉVISION	DATE (aa-mm-jj)
A	SITE PLAN COORDINATION	2020.12.08
B	SITE PLAN COORDINATION	2020.12.16

DESSINÉ PAR Drawn by
 MH LH
 DATE (aa.mm.jj) 12/08/20 ÉCHELLE Scale
 1 : 200

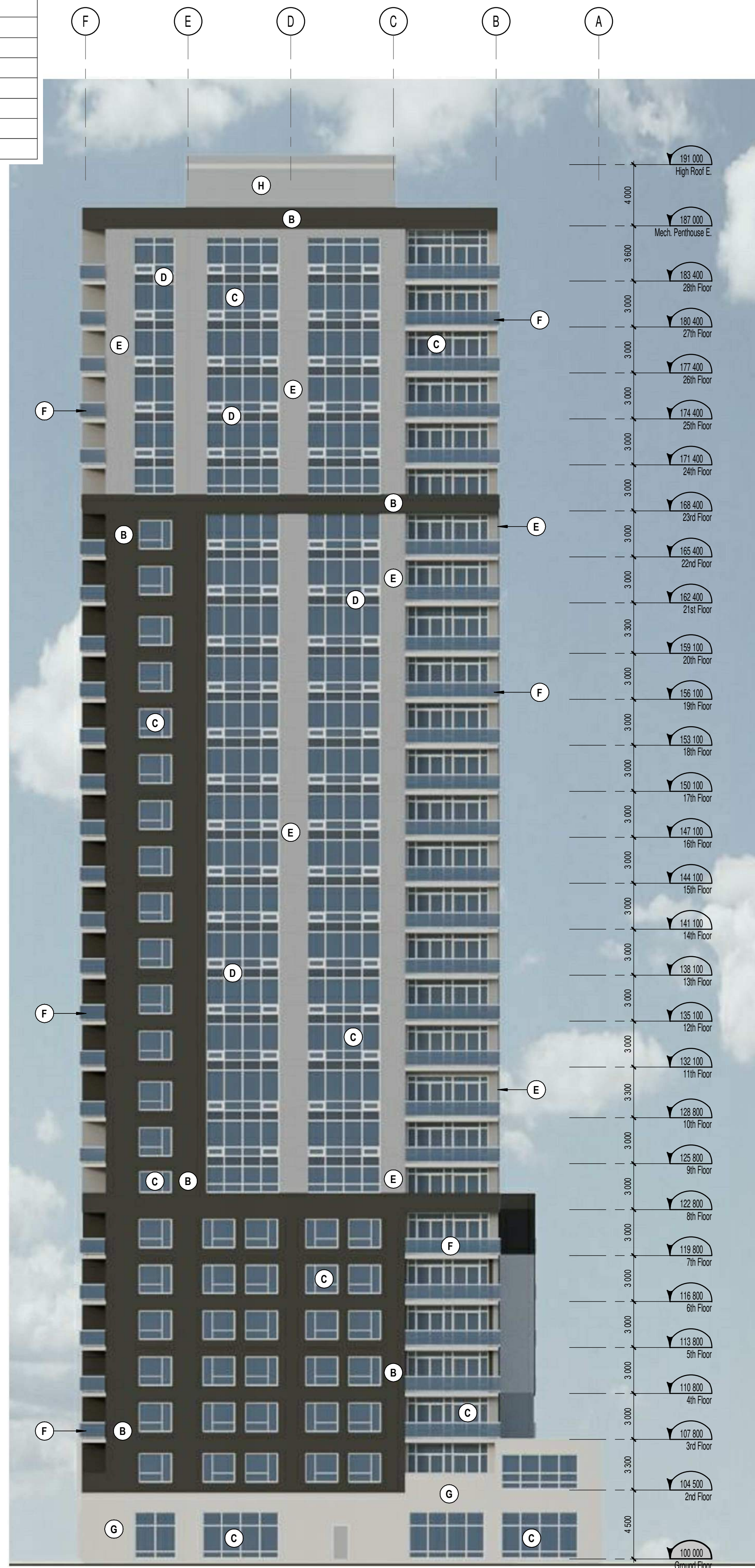
TITRE DU DESSIN Drawing Title
**EAST TOWER
WEST ELEVATION**

RÉVISION Revision NO. DESSIN Dwg Number

B A403

EXTERIOR FINISH LEGEND

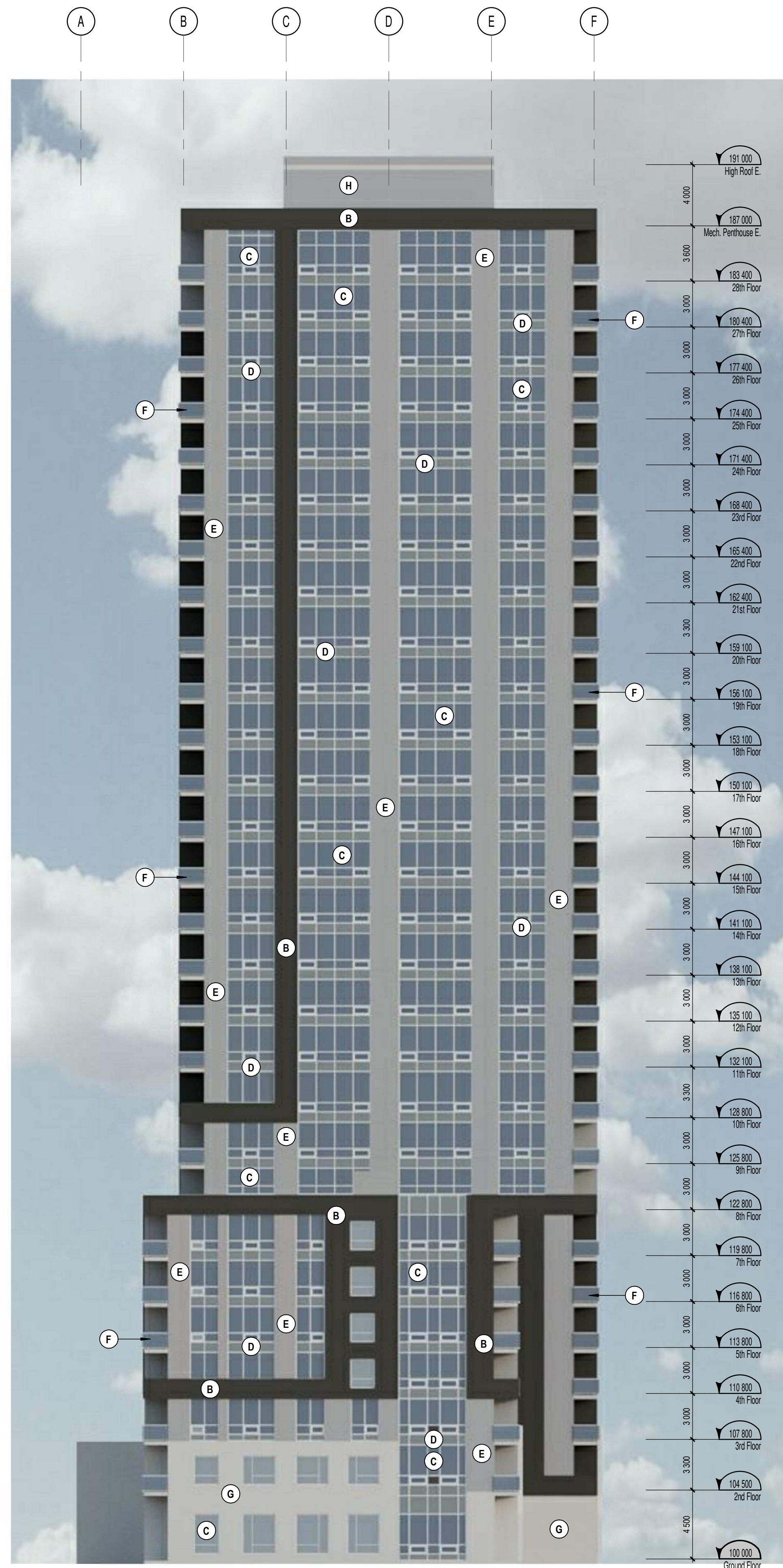
NOTE	DESCRIPTION
A	MASONRY VENEER BRICK - BEIGE
B	MASONRY VENEER BRICK - CHARCOAL
C	SEALED THERMAL UNIT / ALUMINUM FRAME
D	GLASS SPANDREL PANEL
E	METAL PANEL CLADDING
F	ALUMINUM GUARD RAIL WITH TEMPERED GLASS
G	MASONRY VENEER ARTIFICIAL STONE
H	METAL SIDING



EAST TOWER - South Elevation

1 : 200

1
A404



EAST TOWER - North Elevation

1 : 200

2
A404

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SCEAU / Seal



CLIENT Client



OUVRAGE Project

**1200 MARITIME WAY
(KANATA RENTAL)**

EMPLACEMENT Location
OTTAWA

NO PROJET No.
12371.00

NO RÉVISION

DATE (aa-mm-ii) DATE (aa-mm-ii)

A SITE PLAN COORDINATION 2020.12.08
B SITE PLAN COORDINATION 2020.12.16

DESSINÉ PAR Drawn by

MH

VÉRIFIÉ PAR Checked

LH

DATE (aa.mm.ii)
12/08/20

ÉCHELLE Scale
1 : 200

TITRE DU DESSIN Drawing Title

**EAST TOWER
NORTH & SOUTH ELEV.**

RÉVISION Revision

B

NO. DESSIN Dwg Number

A404

EXTERIOR FINISH LEGEND

NOTE	DESCRIPTION
A	MASONRY VENEER BRICK - BEIGE
B	MASONRY VENEER BRICK - CHARCOAL
C	SEALED THERMAL UNIT / ALUMINUM FRAME
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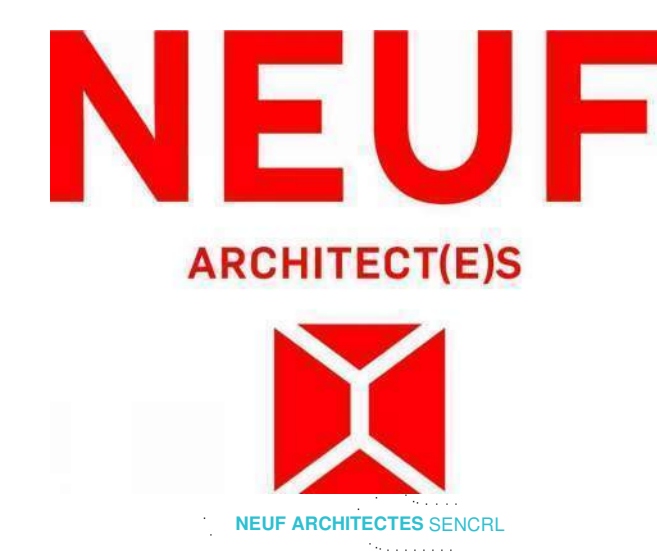
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OUVRAGE / Project
1200 MARITIME WAY (KANATA RENTAL)
 EMPACEMENT / Location
 OTTAWA
 NO PROJET No.
 12371.00

NO	RÉVISION	DATE (aa-mm-ii)
A	SITE PLAN COORDINATION	2020.12.08
B	SITE PLAN COORDINATION	2020.12.16

DESSINÉ PAR / Drawn by
 MH
 DATE (aa.mm.ii)
 12/08/20
 TITRE DU DESSIN / Drawing Title
 EAST TOWER EAST ELEVATION

VERIFIÉ PAR / Checked
 LH
 ÉCHELLE / Scale
 1 : 200

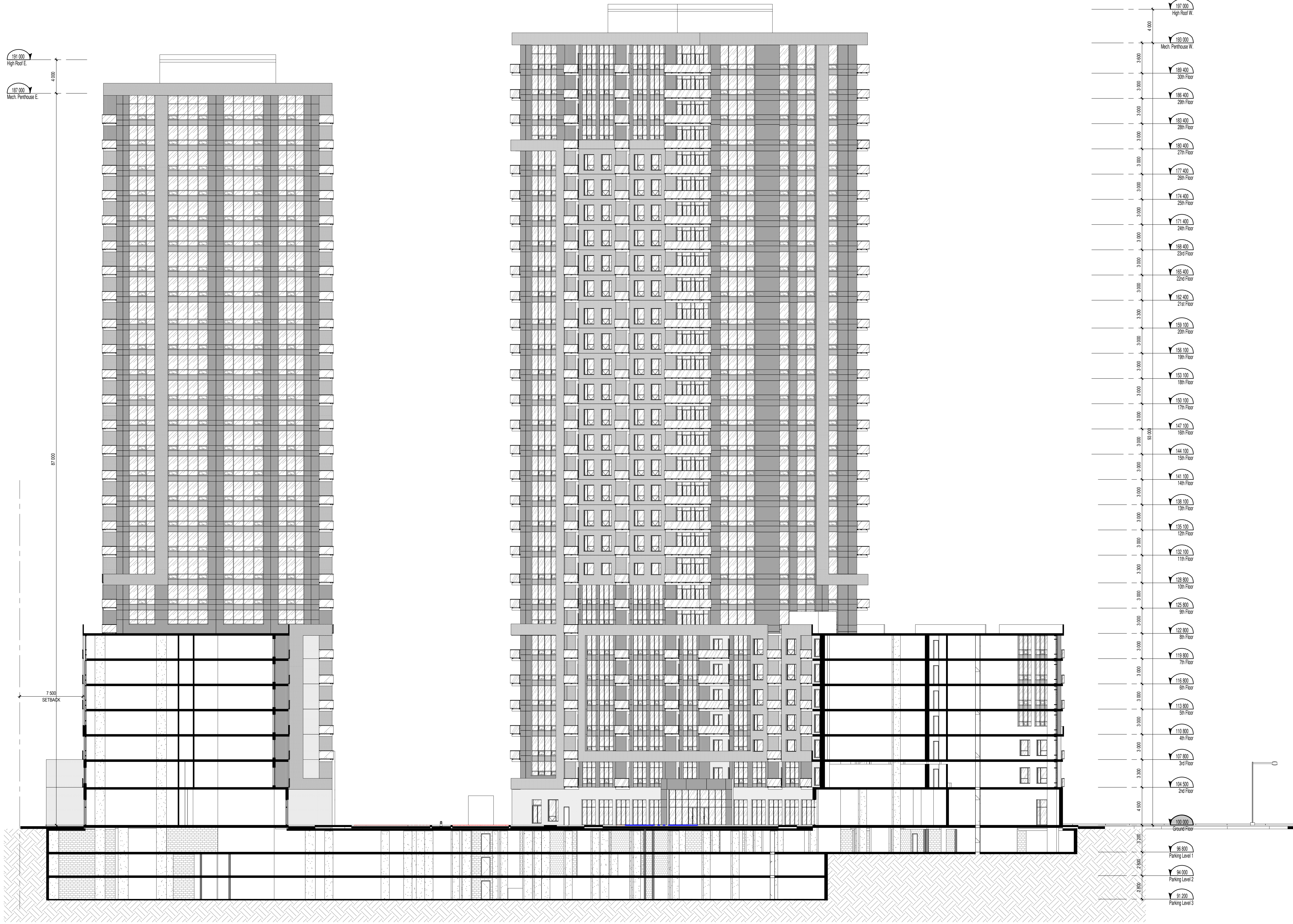
EAST TOWER - East Elevation
 1 : 200

1
 A405

RÉVISION / Revision
B

NO. DESSIN / Dwg Number
A405

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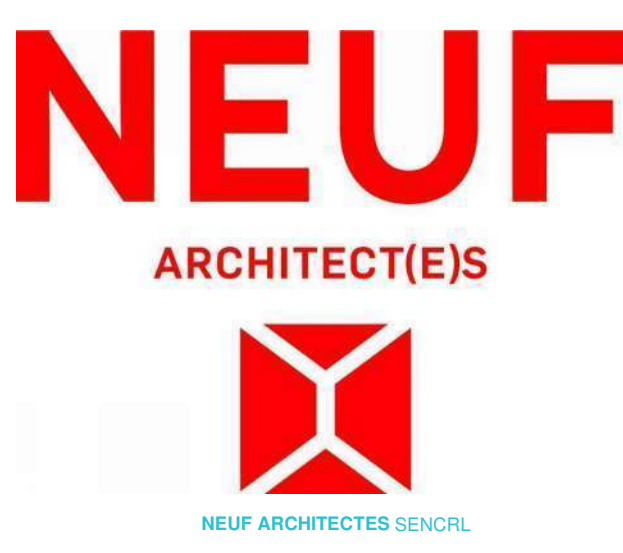
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SCEAU / Seal



OUVRAGE Project
1200 MARITIME WAY (KANATA RENTAL)

EMPLACEMENT Location
 OTTAWA

NO PROJET No.
 12371.00

NO RÉVISION DATE (aa-mm-jj)
 A FOR INFORMATION 2021.05.17

DESSINÉ PAR Drawn by
 PV

DATE (aa.mm.jj)
 NOV 2018

TITRE DU DESSIN Drawing Title
BUILDING CROSS SECTION

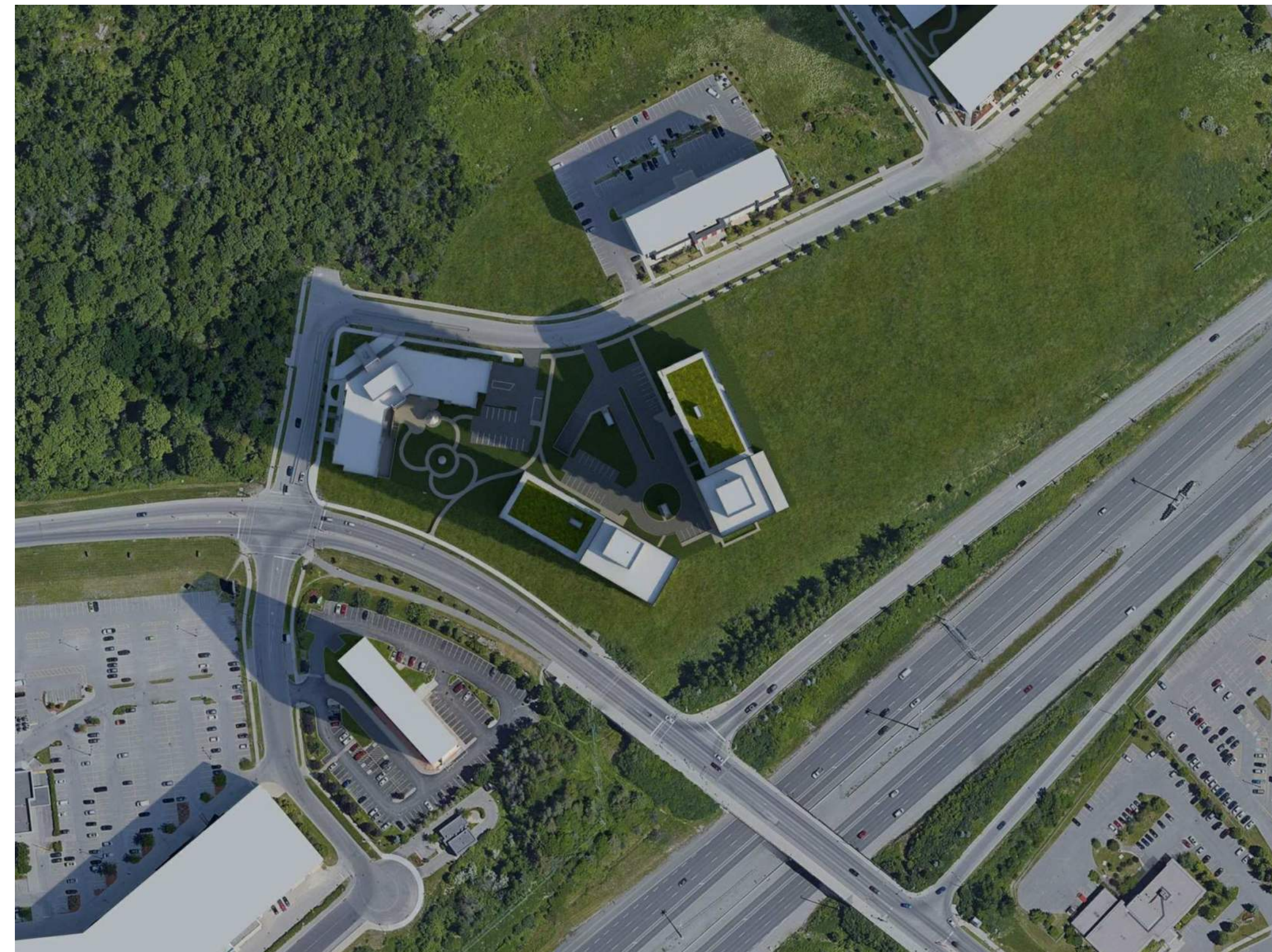
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ÉCHELLE Scale
 1 : 200

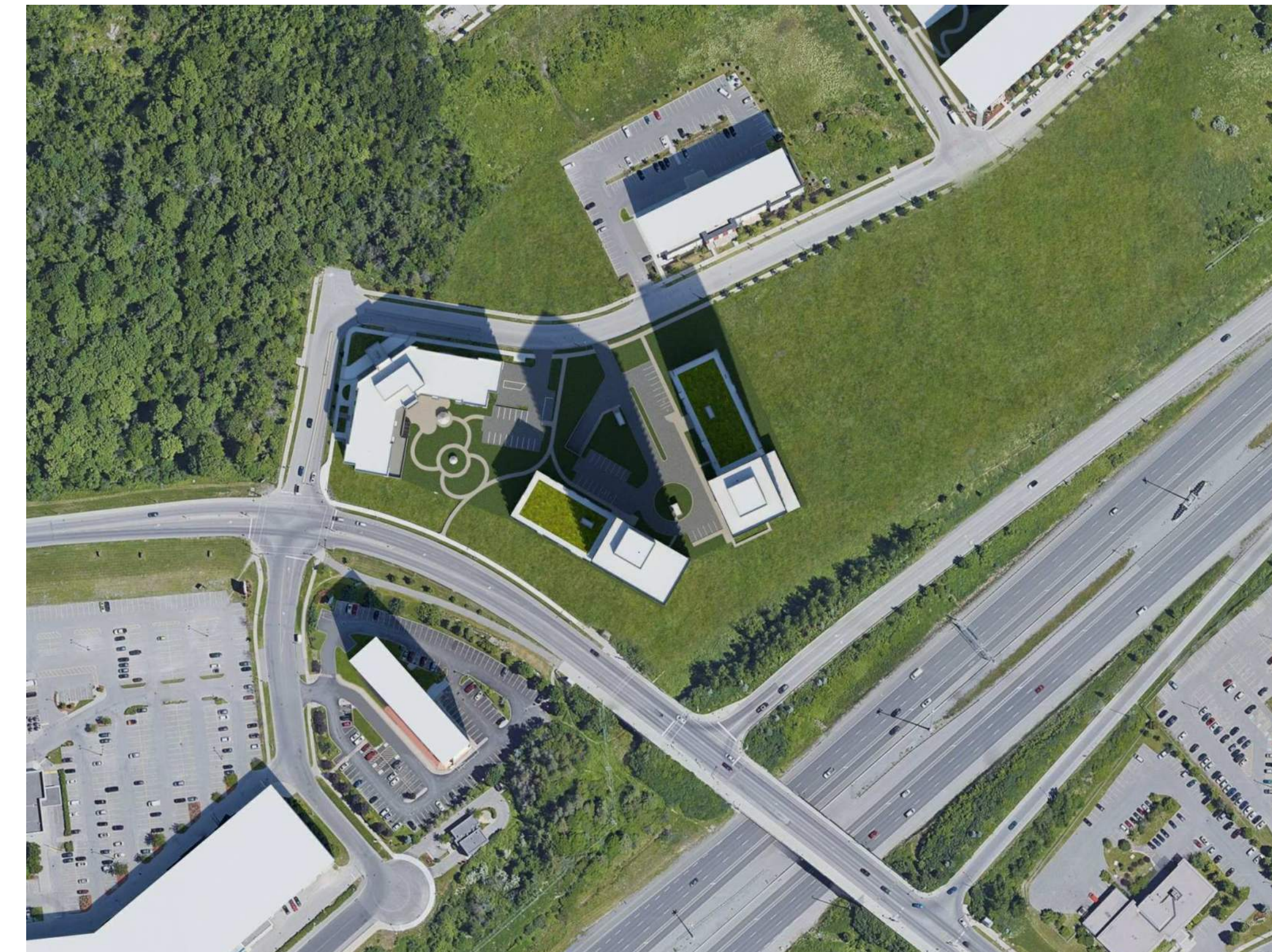
RÉVISION Revision
A

NO. DESSIN Dwg Number
A300

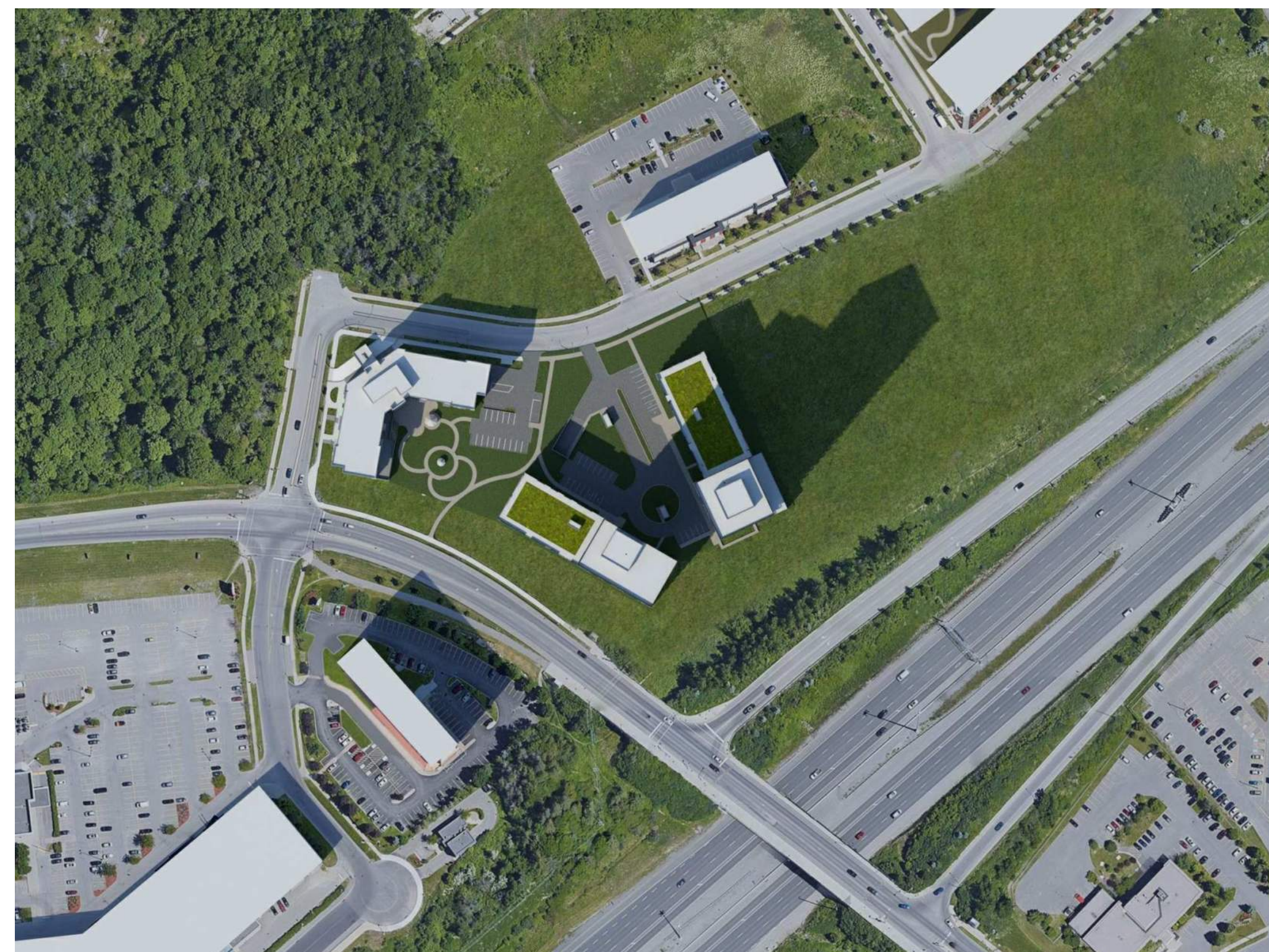
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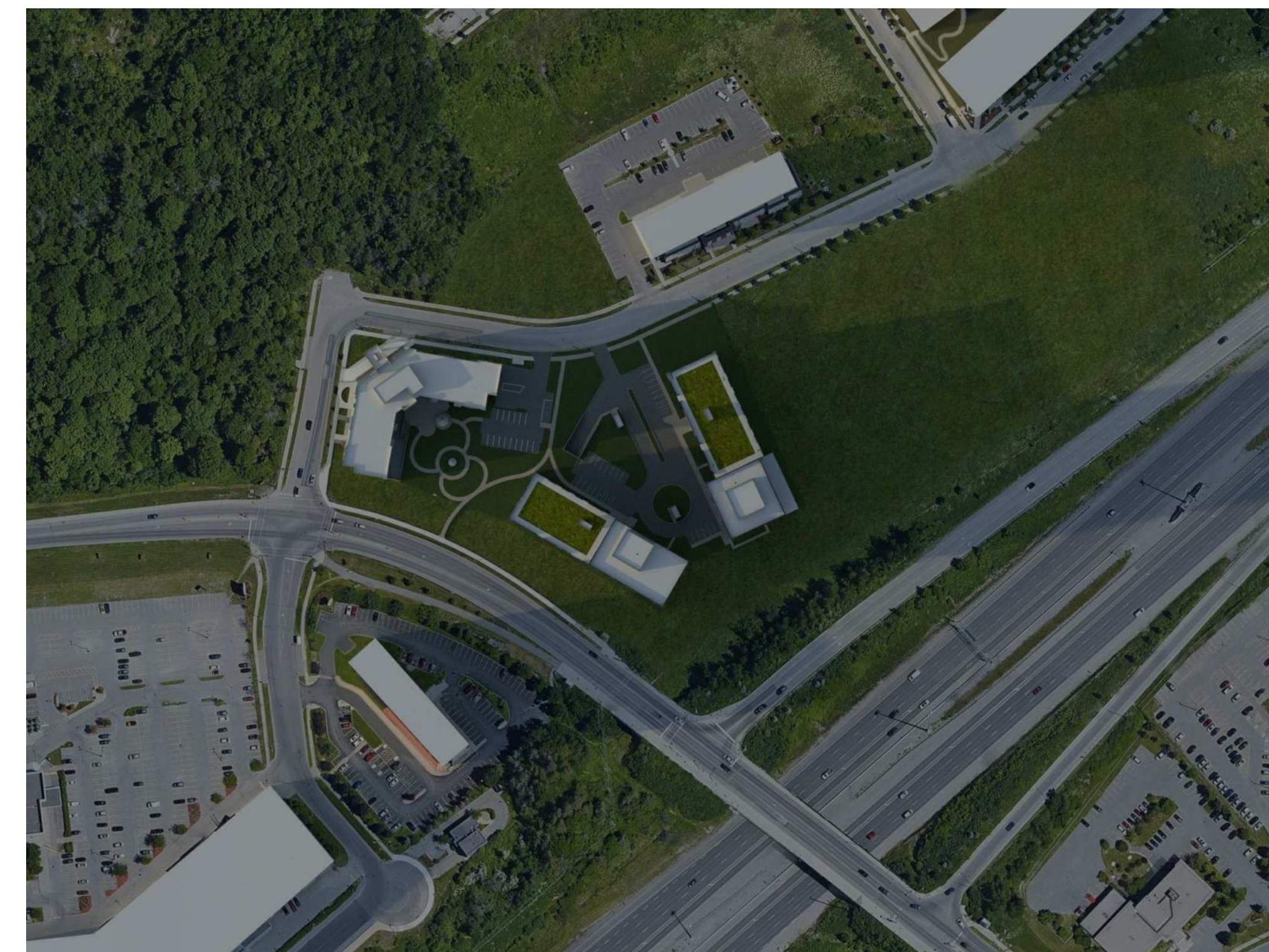
MARCH 21st 9:00 am



MARCH 21st 12:00 pm



MARCH 21st 3:00 pm



MARCH 21st 6:00 pm

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ARCHITECTURE DE PAYSAGE Landscape Architect
James B. Lennox & Associates
3332 Carling Avenue, Ottawa ON K2N 5J6
T 613 722 5168 jbla.ca

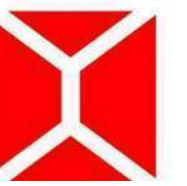
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240, Michael Cowpland Drive, Suite 200, Ottawa ON K2M 1P6
T 613 234 9843 novatech-eng.com

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NEUF

ARCHITECT(E)S



NEUF ARCHITECTES SENCRL

CLIENT Client



OUVRAGE Project

**1200 MARITIME WAY
(KANATA RENTAL)**

EMPLACEMENT Location NO PROJET No.
OTTAWA 12371.00

NO RÉVISION DATE (aa-mm-jj)

NO	RÉVISION	DATE (aa-mm-jj)

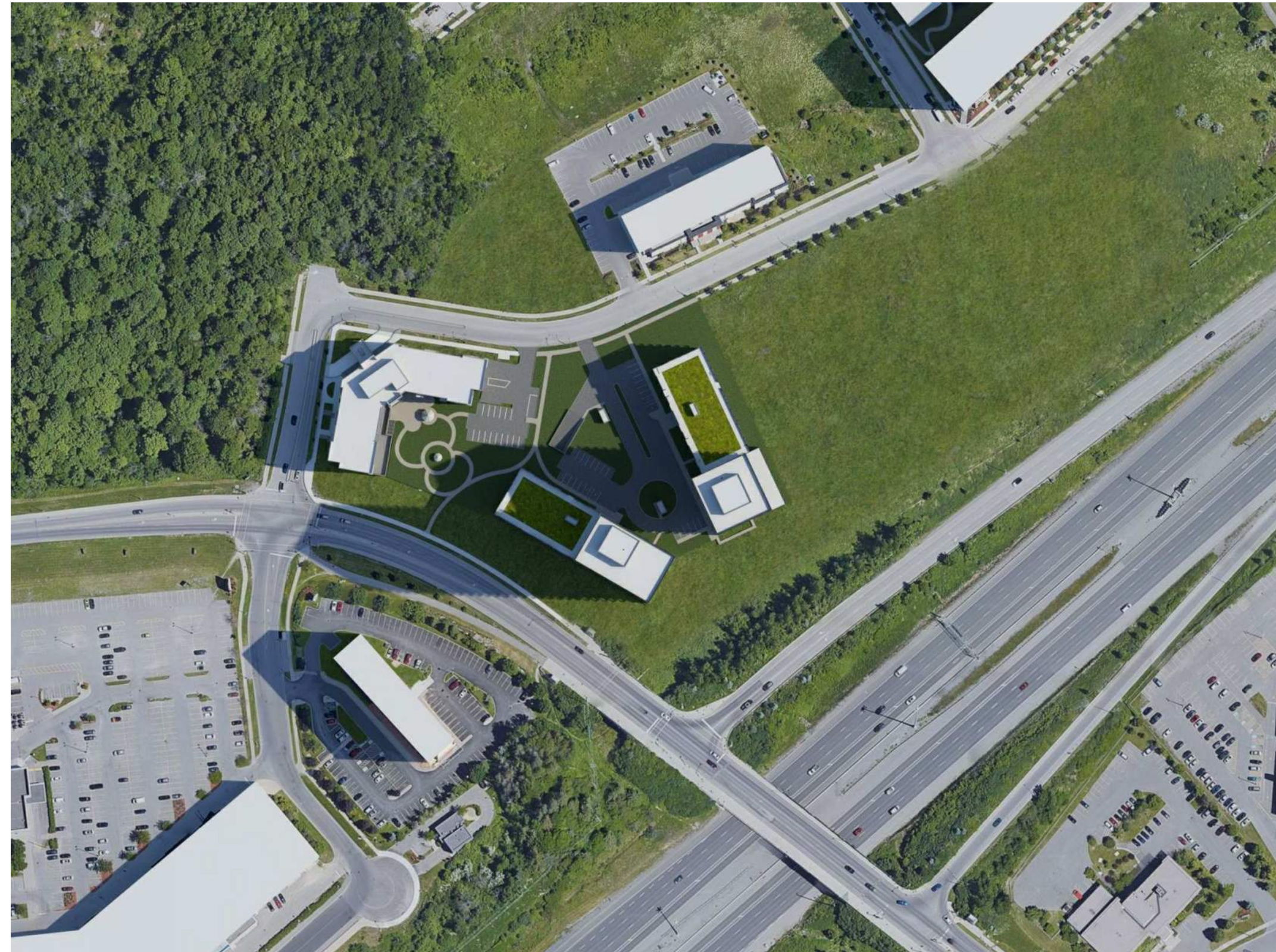
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PI LH
DATE (aa.mm.jj) ÉCHELLE Scale
12/11/20

TITRE DU DESSIN Drawing Title

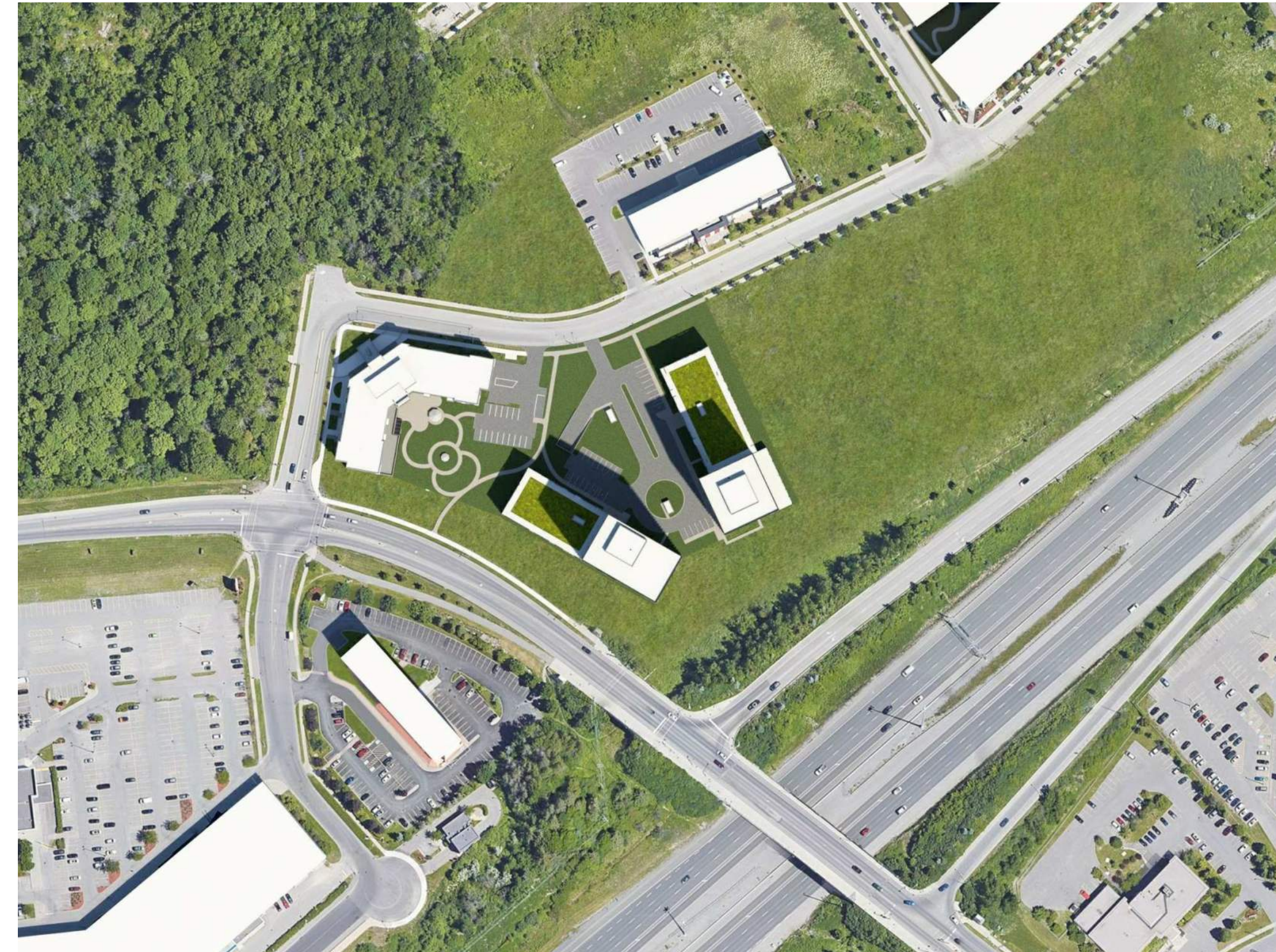
SUN STUDY - MARCH

RÉVISION Revision NO. DESSIN Dwg Number

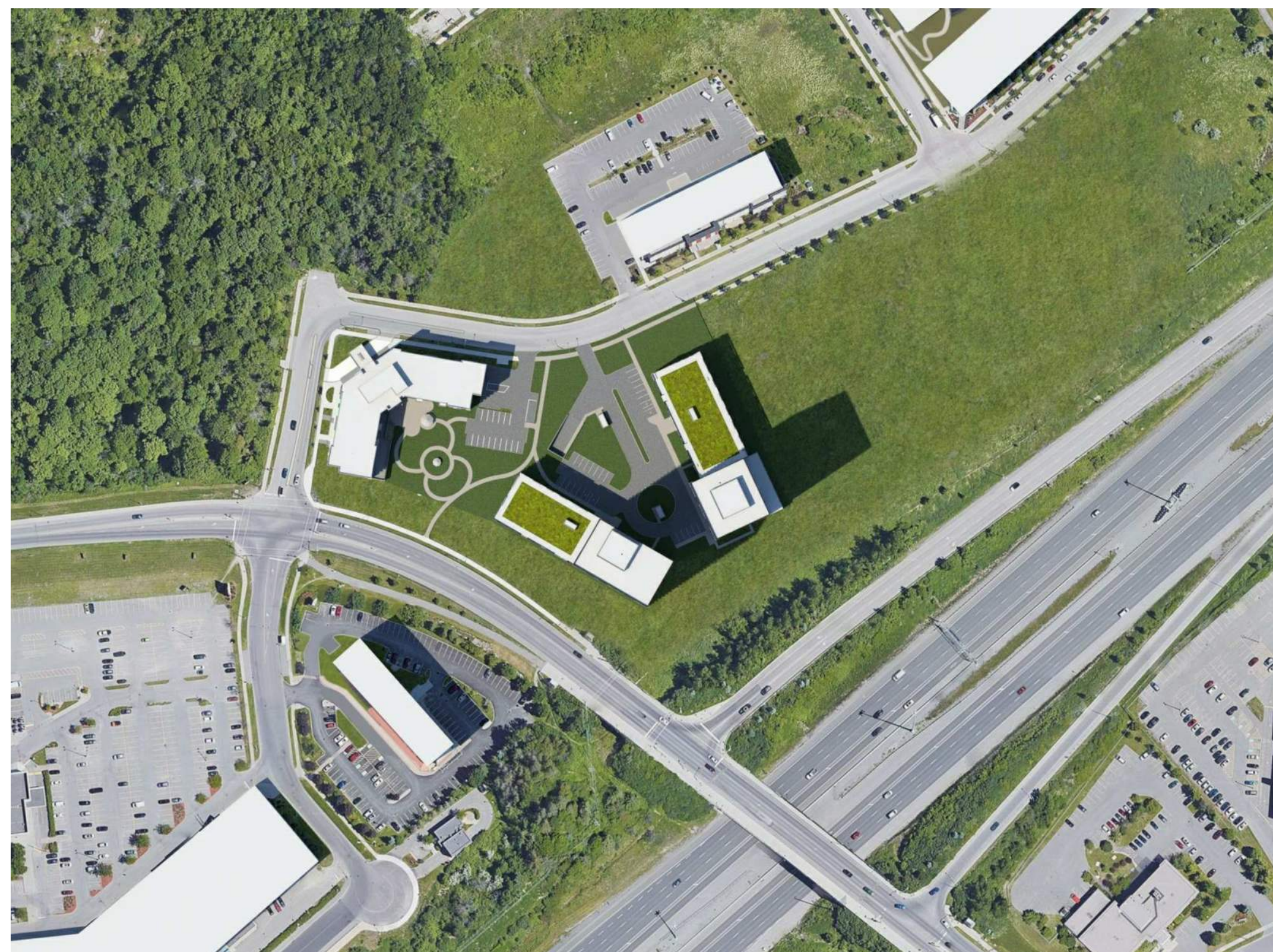
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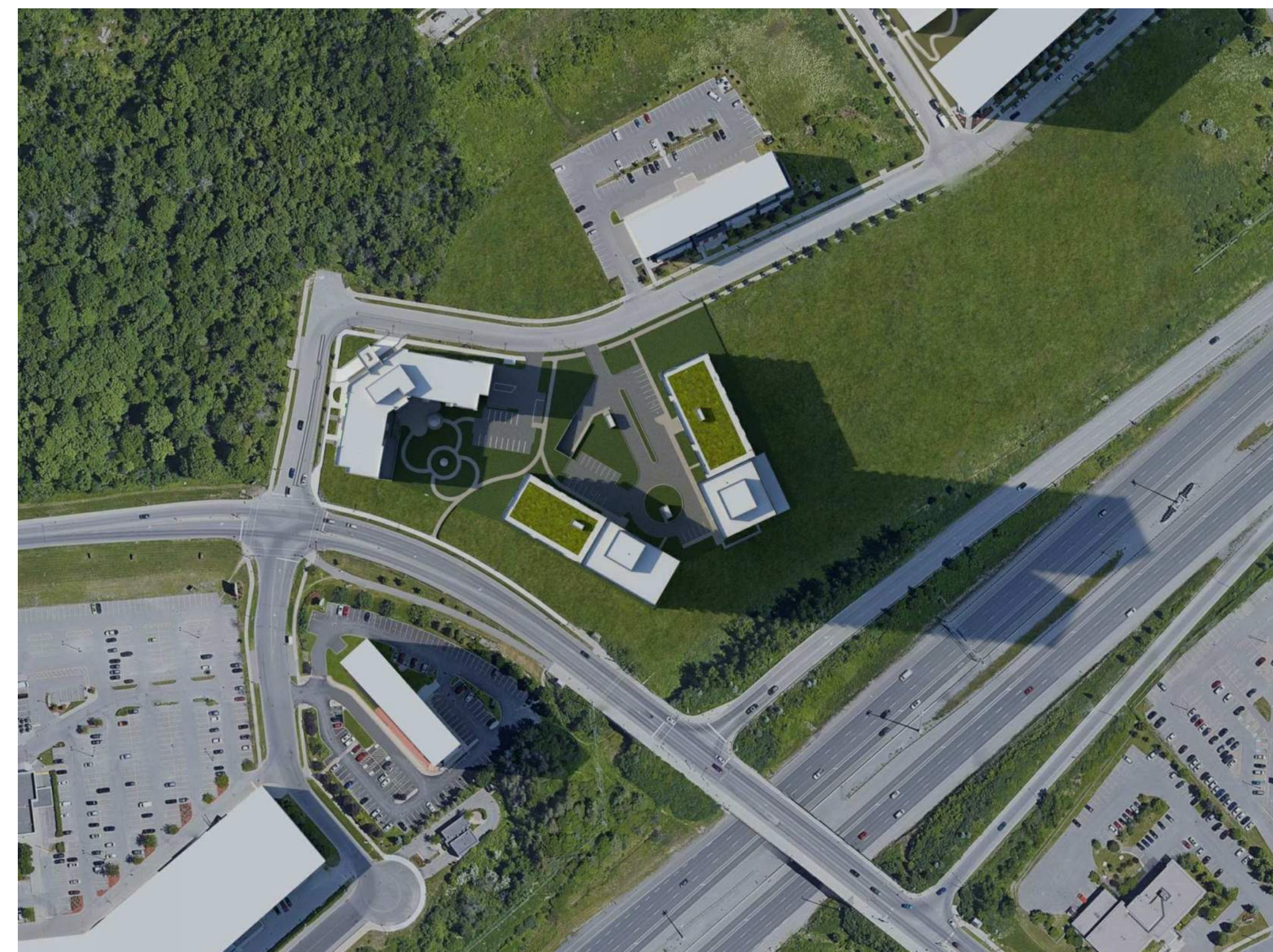
JUNE 21st 9:00 am



JUNE 21st 12:00 pm



JUNE 21st 3:00 pm



JUNE 21st 6:00 pm

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CLIENT Client



OUVRAGE Project

**1200 MARITIME WAY
(KANATA RENTAL)**

EMPLACEMENT Location NO PROJET No.
OTTAWA 12371.00

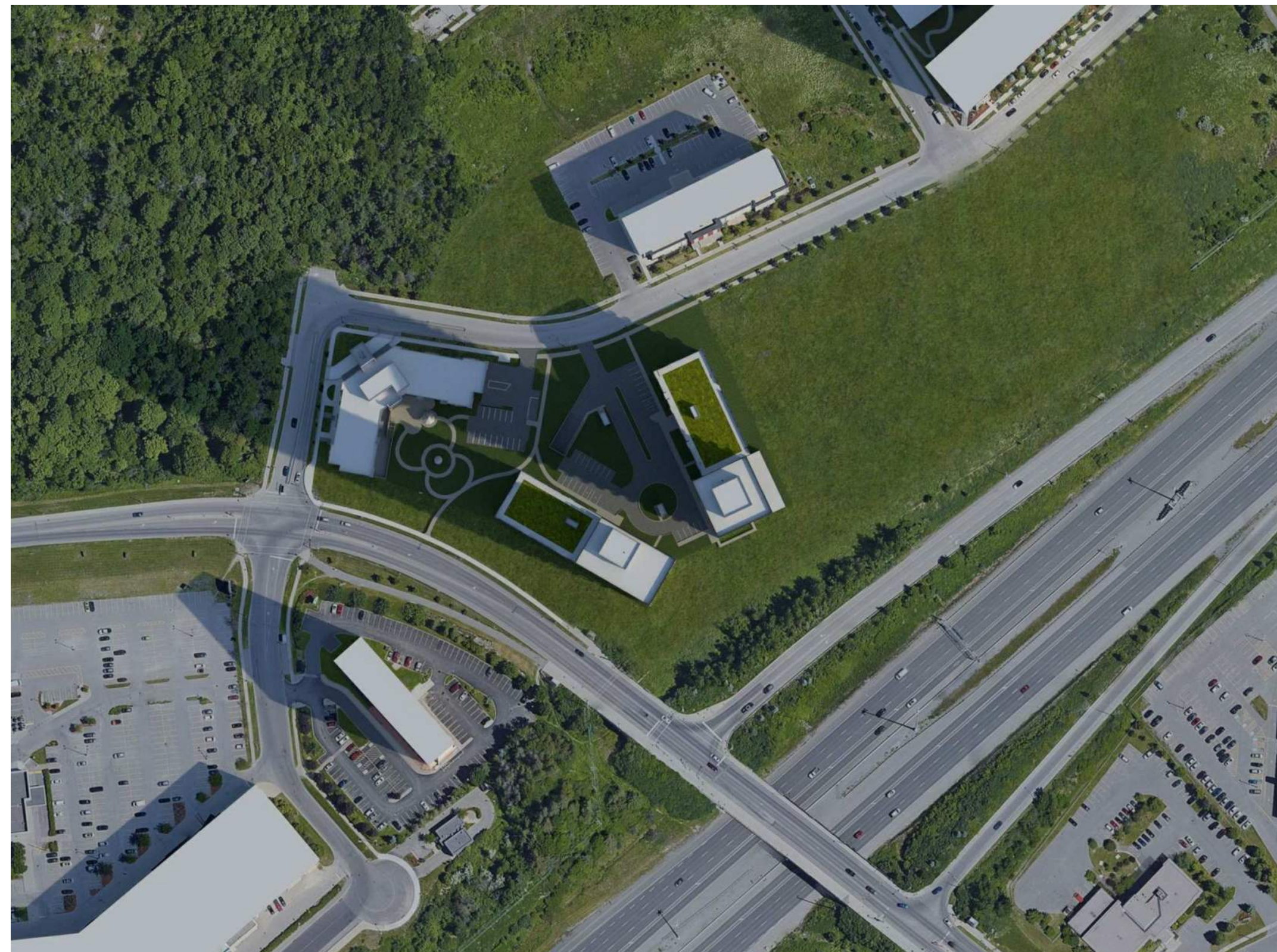
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PI LH

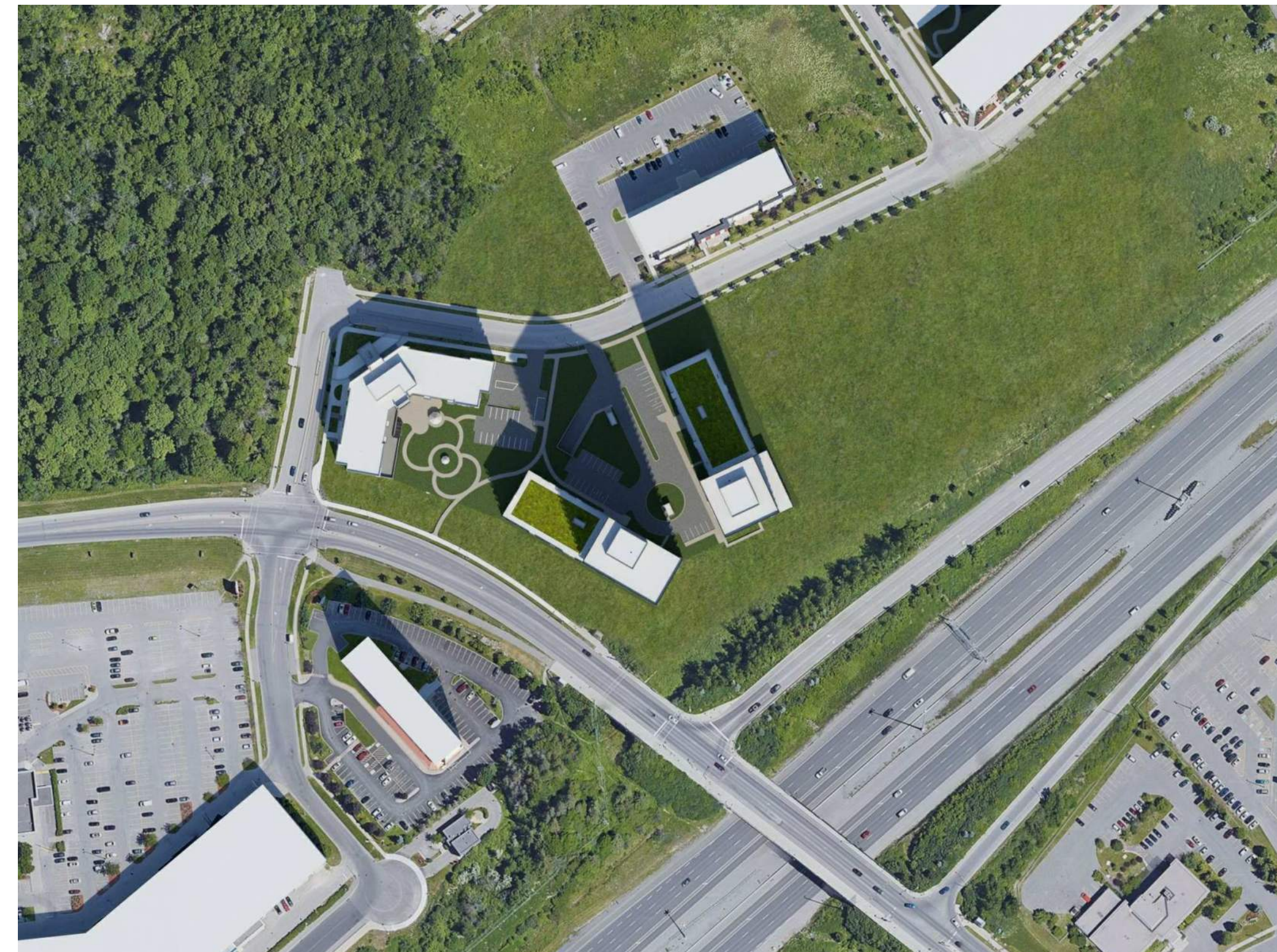
DATE (aa-mm-ii) ÉCHELLE Scale
12/11/20

TITRE DU DESSIN Drawing Title
SUN STUDY - JUNE

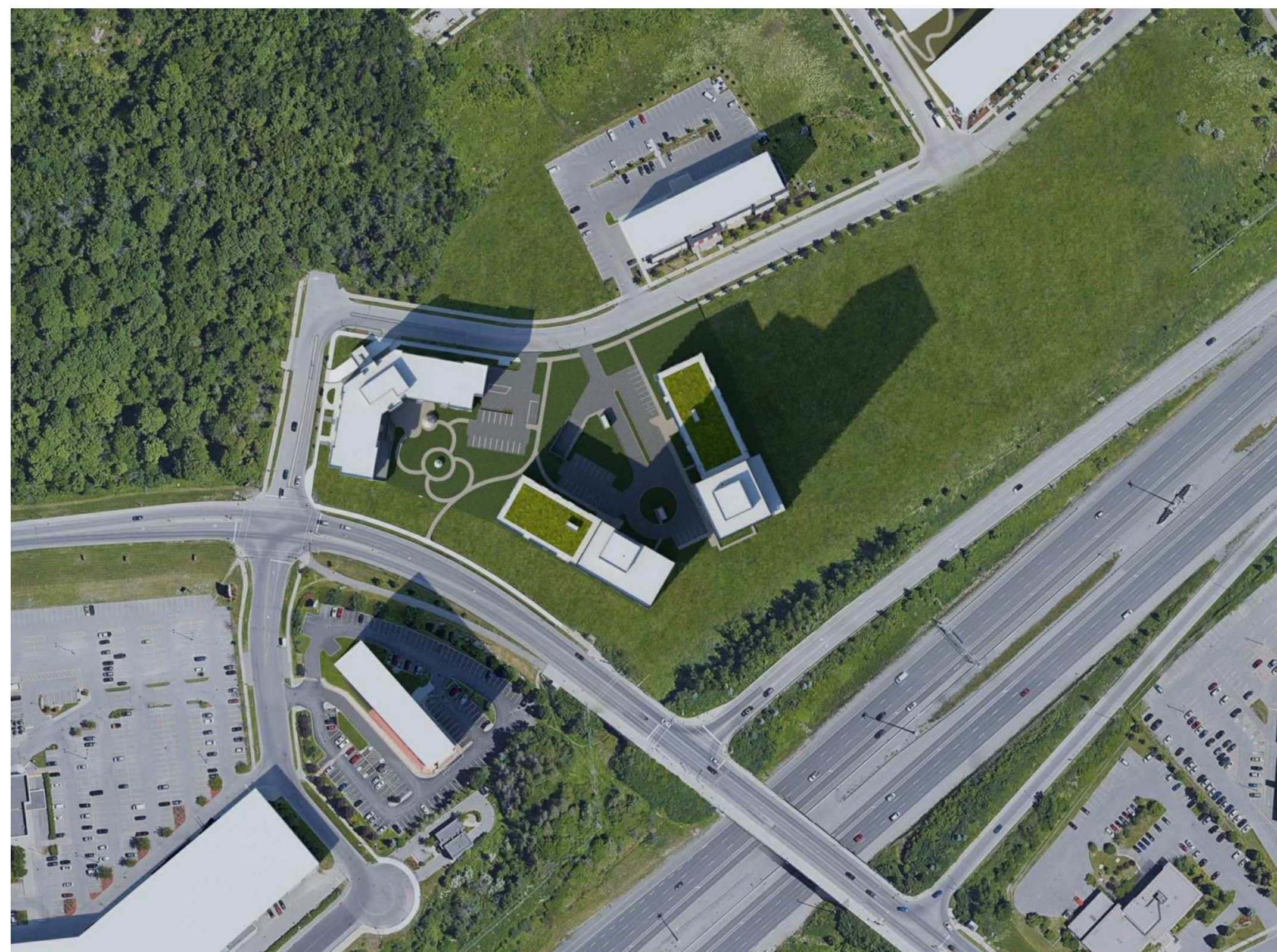
RÉVISION Revision NO. DESSIN Dwg Number
A1001



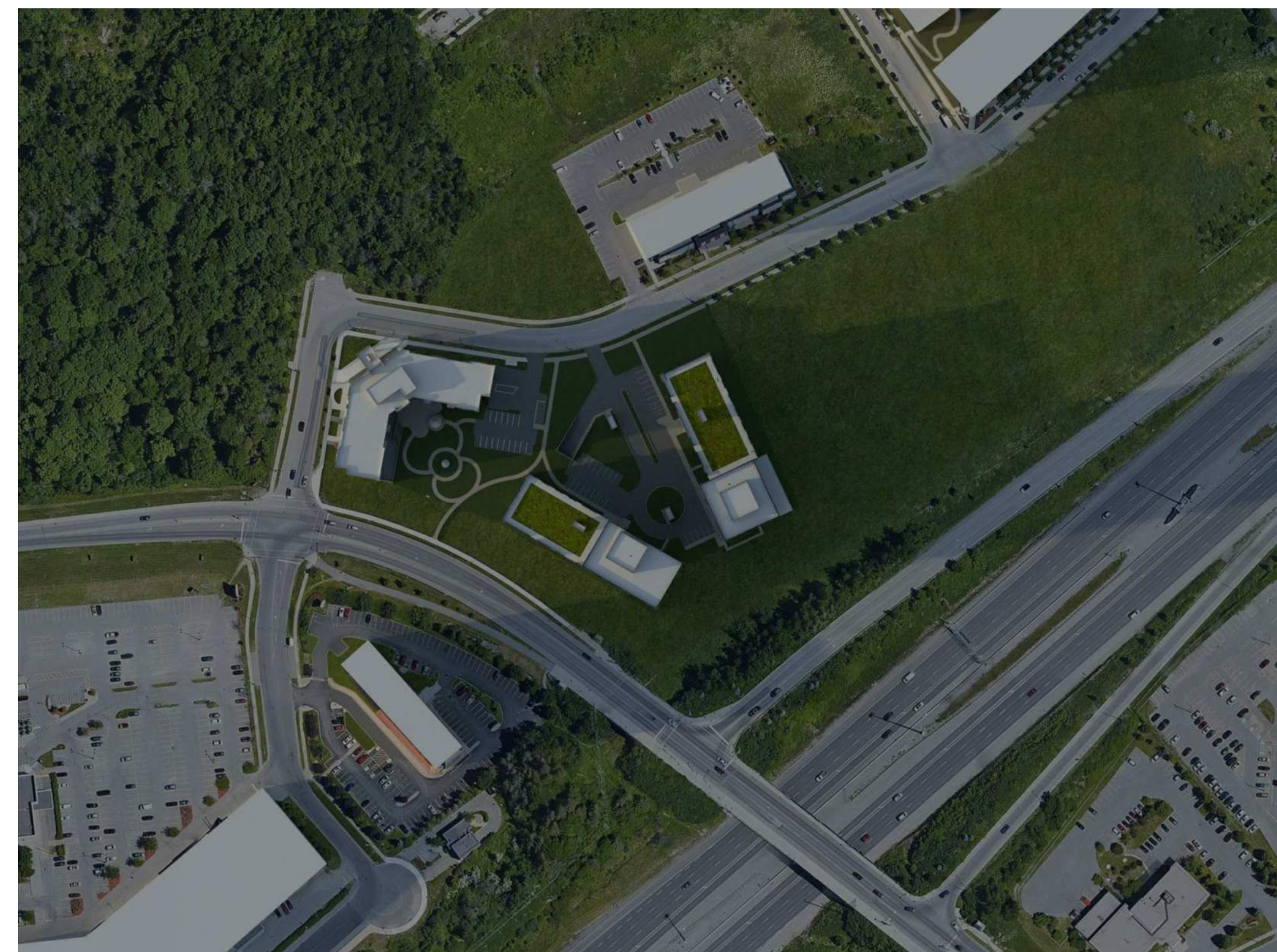
SEPTEMBER 21st 9:00 am



SEPTEMBER 21st 12:00 pm



SEPTEMBER 21st 3:00 pm



SEPTEMBER 21st 6:00 pm

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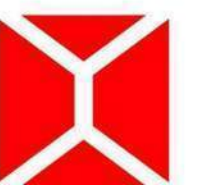
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OUVRAGE Project
**1200 MARITIME WAY
 (KANATA RENTAL)**

EMPLACEMENT Location NO PROJET No.
 OTTAWA 12371.00

NO RÉVISION DATE (aa-mm-jj)

NO	RÉVISION	DATE (aa-mm-jj)

DESSINÉ PAR Drawn by
 PI

DATE (aa.mm.jj)
 12/11/20

TITRE DU DESSIN Drawing Title
**SUN STUDY -
 SEPTEMBER**

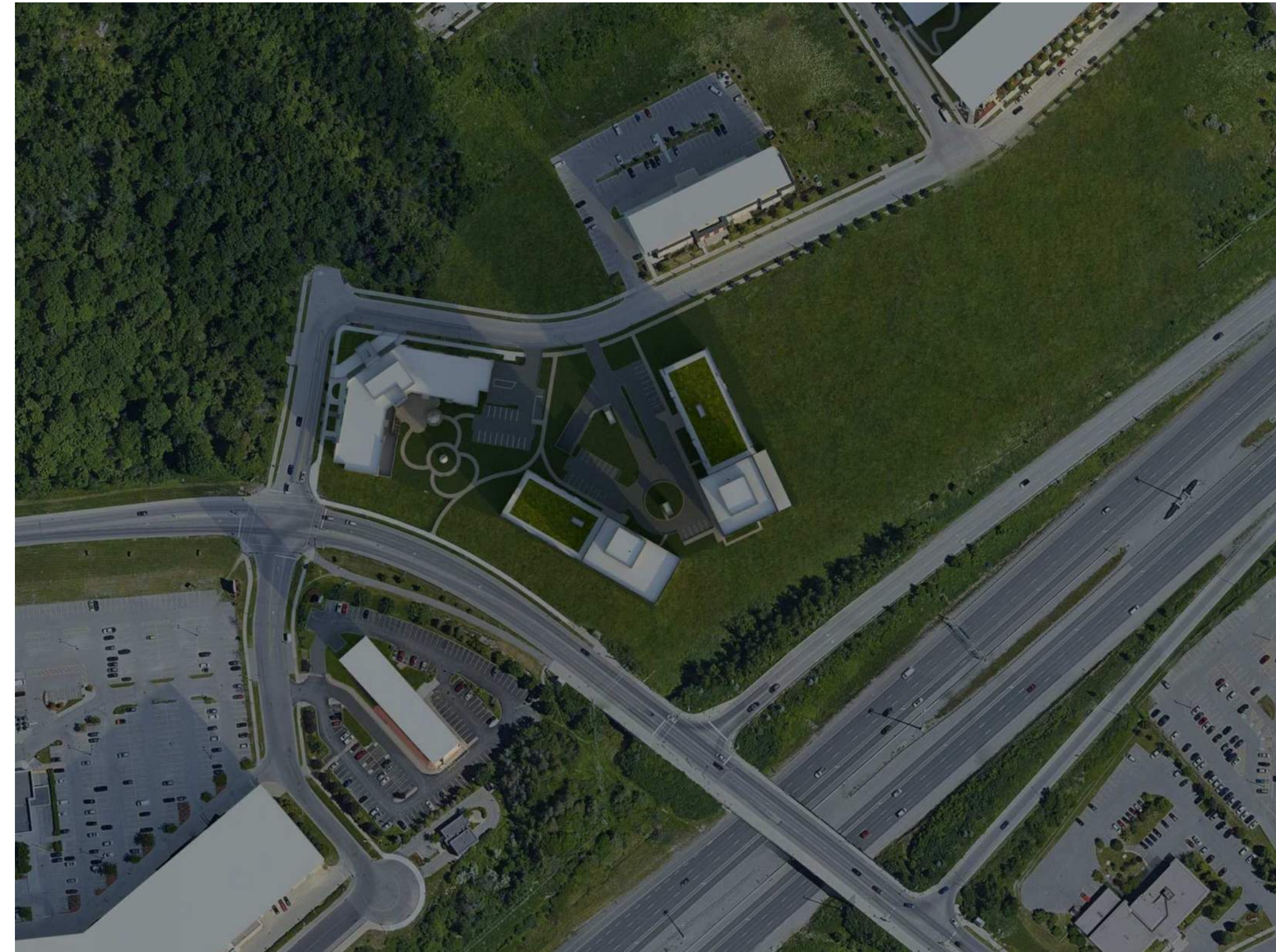
NO. DESSIN Dwg Number
A1002

VERIFIÉ PAR Checked
 LH

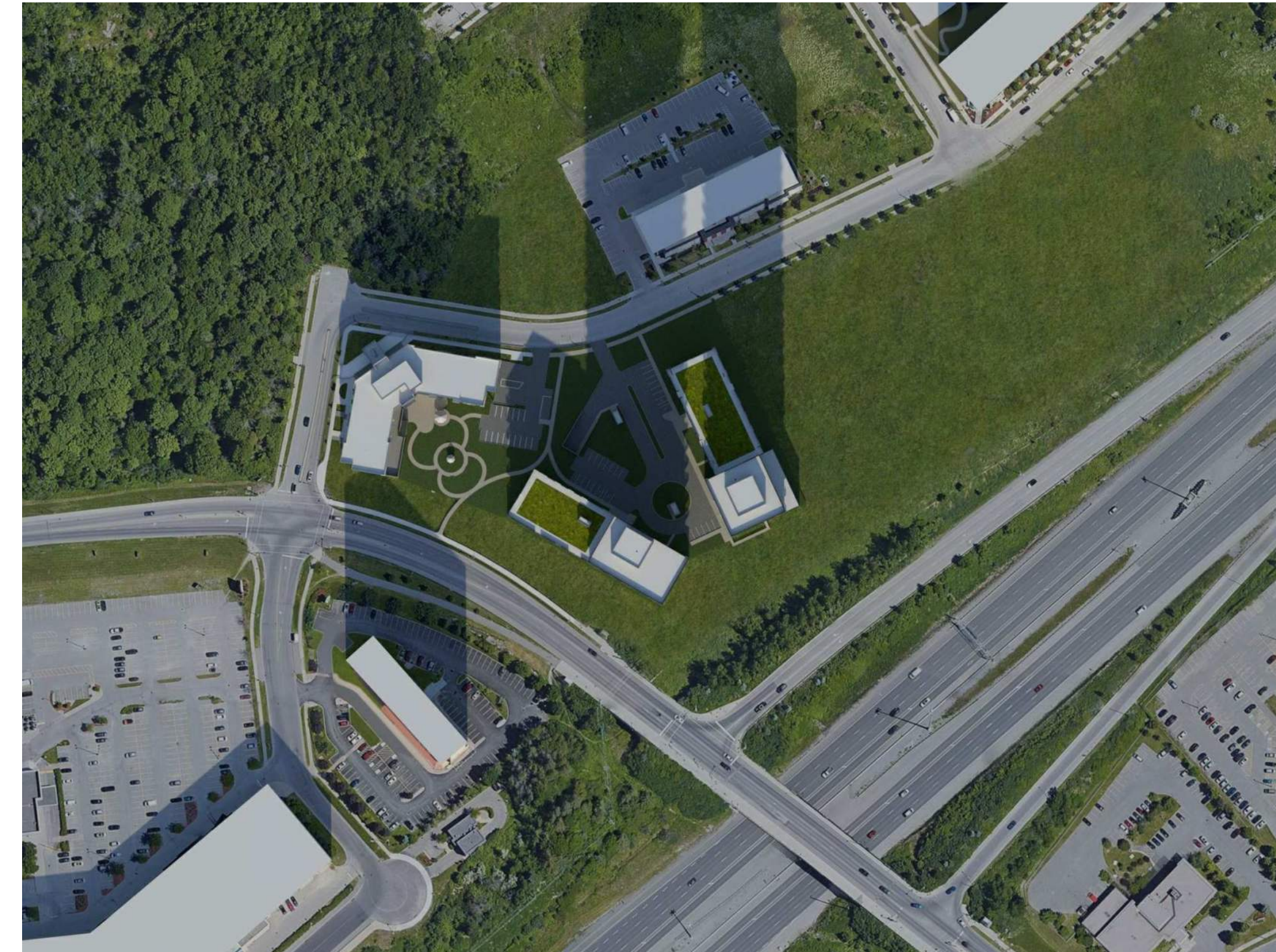
ÉCHELLE Scale

RÉVISION Revision

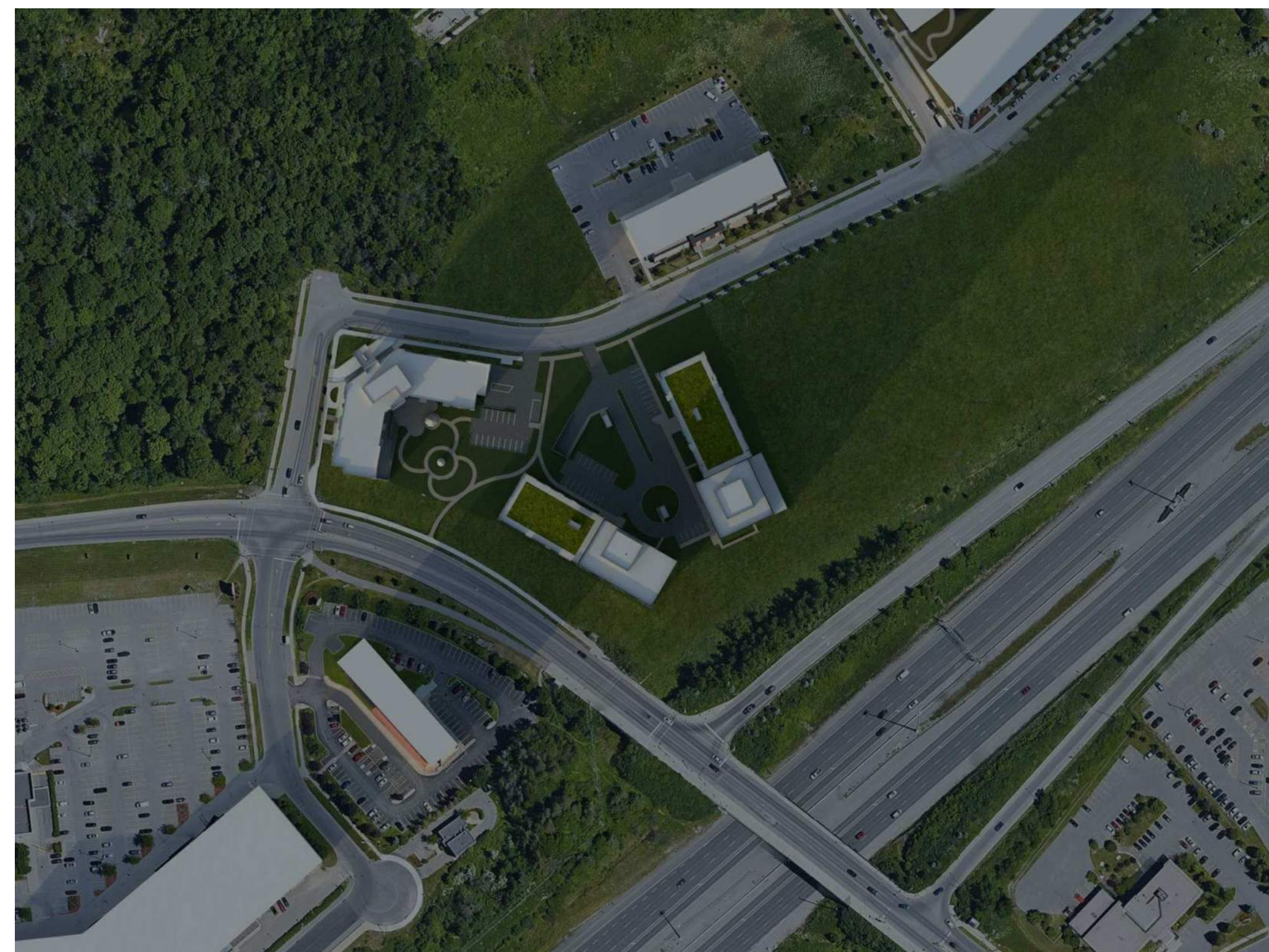
NO. DESSIN Dwg Number
A1002



DECEMBER 21st 9:00 am



DECEMBER 21st 12:00 pm



DECEMBER 21st 3:00 pm

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CLIENT / Client



OUVRAGE / Project

**1200 MARITIME WAY
 (KANATA RENTAL)**

EMPLACEMENT / Location
 OTTAWA NO PROJET No.
 12371.00

NO RÉVISION DATE (aa-mm-jj)

NO	RÉVISION	DATE (aa-mm-jj)

DESSINÉ PAR / Drawn by
 PI VERIFIÉ PAR / Checked by
 LH
 DATE (aa-mm-jj) 12/11/20 ÉCHELLE / Scale

TITRE DU DESSIN / Drawing Title

SUN STUDY - DECEMBER

RÉVISION / Revision NO. DESSIN / Dwg Number

A1003



BIRDS EYE VIEW LOOKING SOUTH

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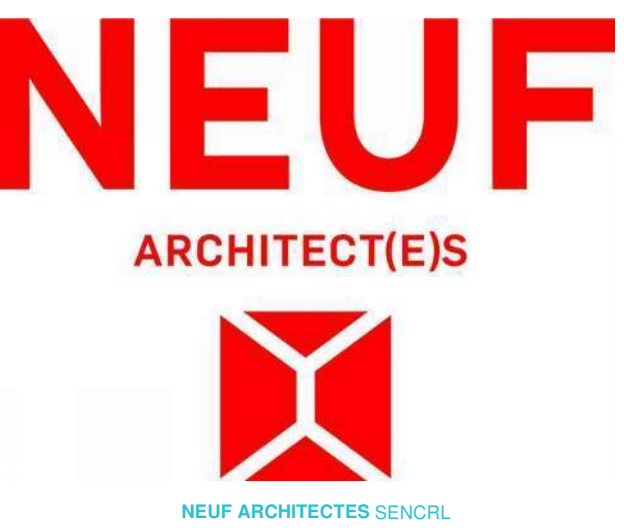
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CLIENT Client



OUVRAGE Project
**1200 MARITIME WAY
 (KANATA RENTAL)**

EMPLACEMENT Location NO PROJET No.
 OTTAWA 12371.00

NO	RÉVISION	DATE (aa-mm-ii)
A	SITE PLAN COORDINATION	2020.12.16
B	FOR INFORMATION	2021.05.17

DESSINÉ PAR Drawn by
 PI VERIFIÉ PAR Checked
 DATE (aa.mm.ii) 12/16/20 ECHELLE Scale

TITRE DU DESSIN Drawing Title
3D RENDERING

RÉVISION Revision NO. DESSIN Dwg Number
B A1100

#18348

D07-12-21-0017



STREETSCAPE LOOKING EAST

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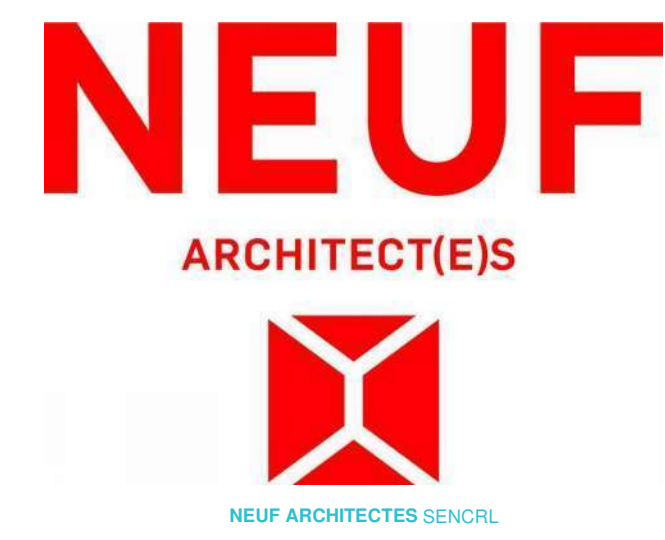
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OUVRAGE / Project
1200 MARITIME WAY (KANATA RENTAL)

EMPLACEMENT / Location NO. PROJET / No.
 OTTAWA 12371.00

NO	RÉVISION	DATE (aa-mm-ii)
A	FOR INFORMATION	2021.05.17

DESSINÉ PAR / Drawn by: PI
 DATE (aa.mm.ii): 05/26/21
 TITRE DU DESSIN / Drawing Title: **3D RENDERING**

VERIFIÉ PAR / Checked by: LH
 ECHELLE / Scale

RÉVISION / Revision: **A**
 NO. DESSIN / Dwg Number: **A1101**
#18348



STREETSCAPE LOOKING WEST

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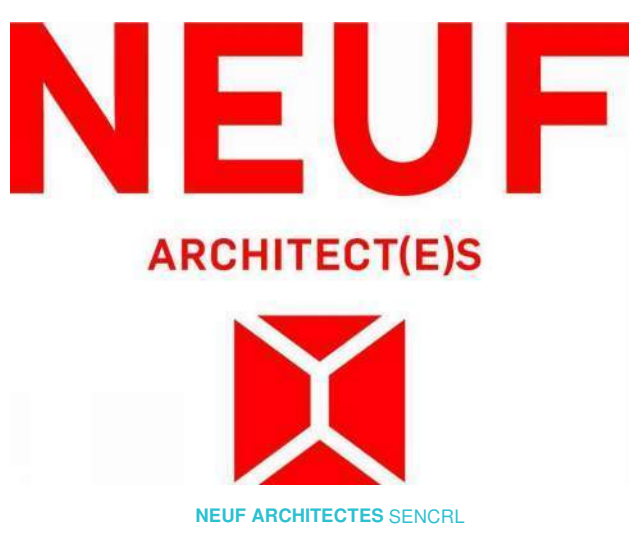
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OUVRAGE / Project

**1200 MARITIME WAY
(KANATA RENTAL)**

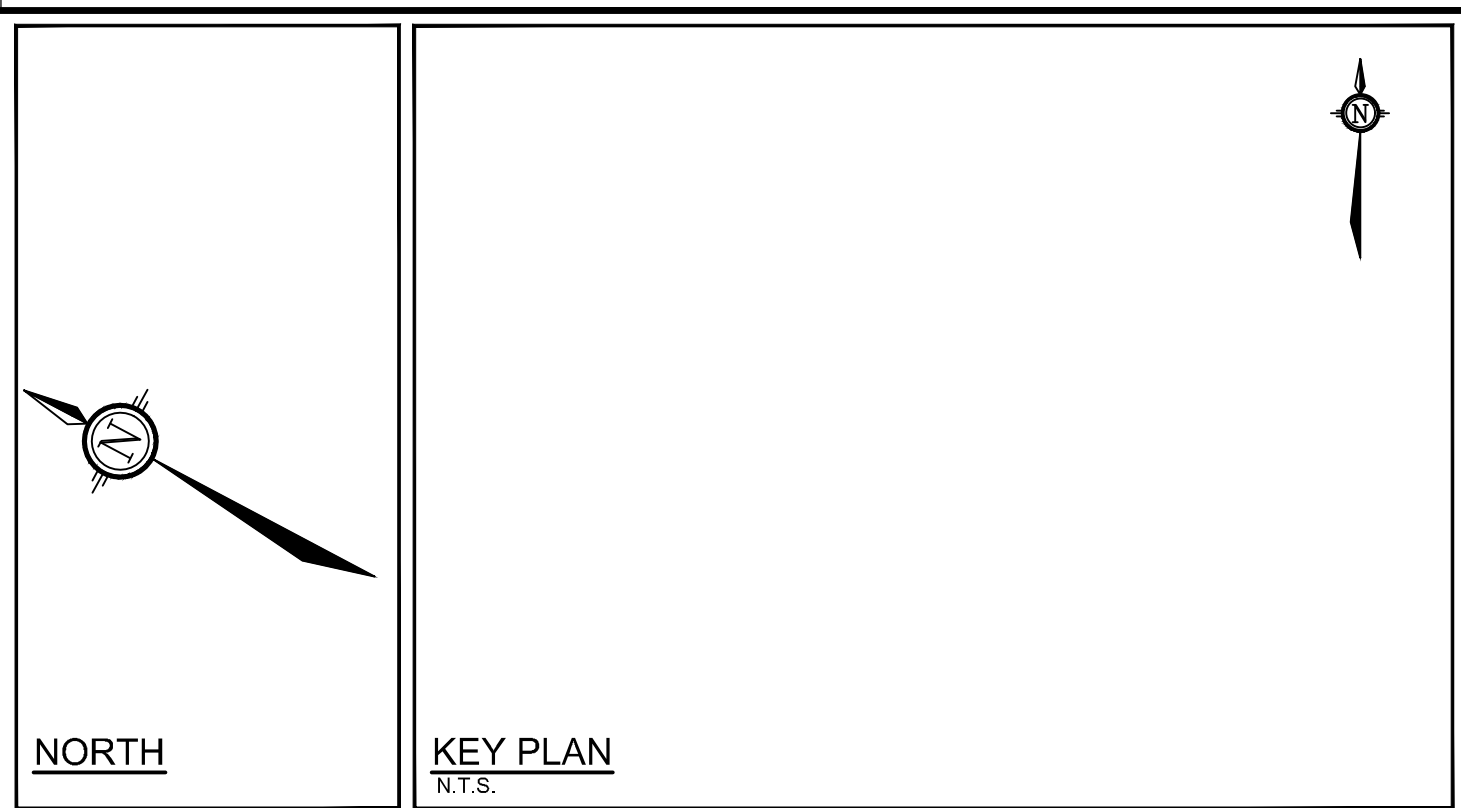
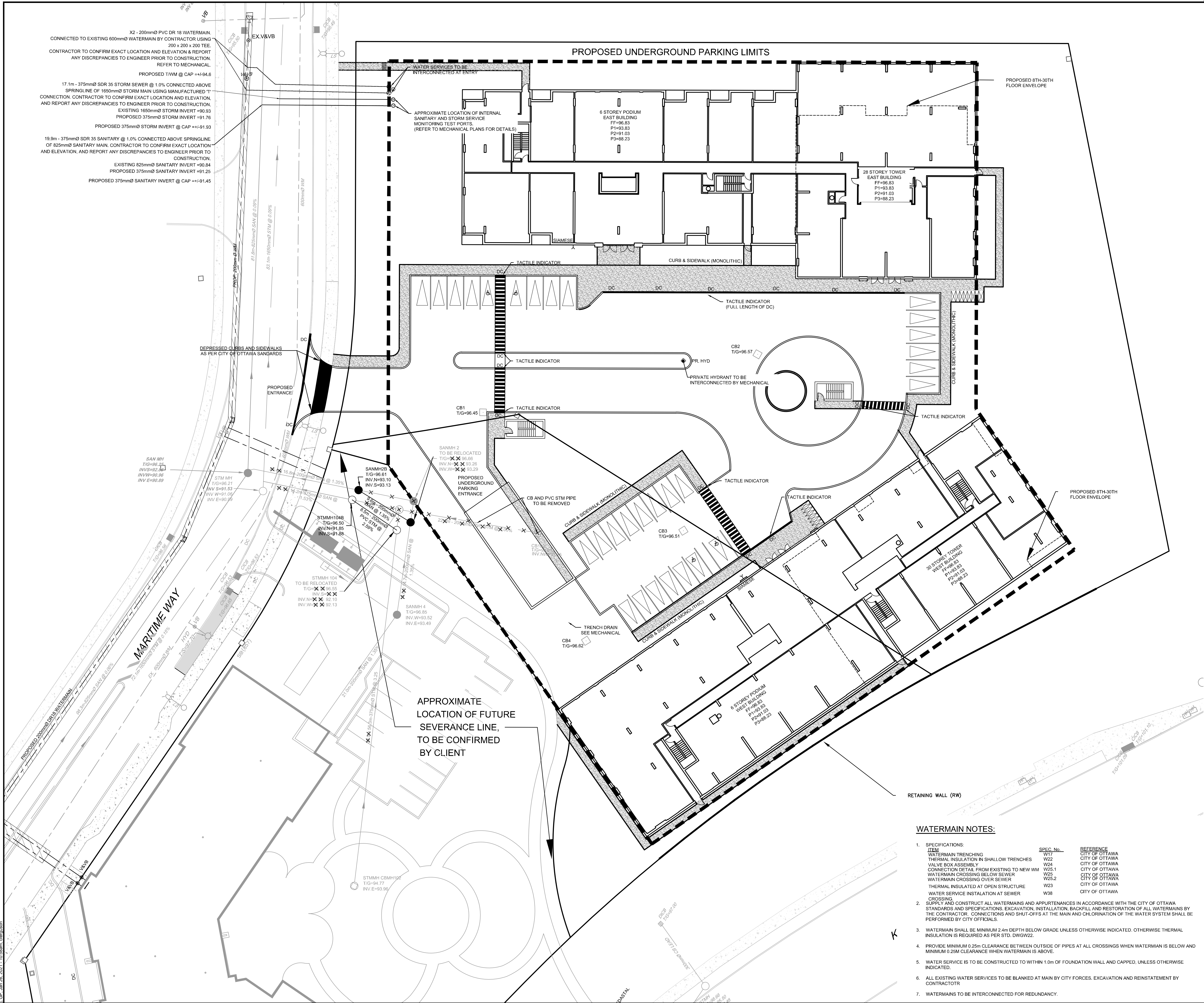
EMPLACEMENT / Location NO PROJET / No.
 OTTAWA 12371.00

NO RÉVISION DATE (aa-mm-ii)
 A FOR INFORMATION 2021.05.17

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 PI LH
 DATE (aa.mm.ii) ECHELLE / Scale
 05/26/21

TITRE DU DESSIN / Drawing Title
3D RENDERING

RÉVISION / Revision NO. DESSIN / Dwg Number
A A1102
#18348



SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
---	PROPOSED PROPERTY LINE	VC	EXISTING WATERMAIN VALVE CHAMBER
---	PROPOSED WATERMAIN	VB	EXISTING WATERMAIN SHUT-OFF VALVE BOX
○	PROPOSED VALVE BOX	W	EXISTING WATERMAIN
○	PROPOSED STORM MANHOLE	VB	EXISTING HYDRANT C/W LEAD & SHUT OFF VALVE BOX
---	PROPOSED SANITARY SEWER	GV	EXISTING GAS VALVE
---	PROPOSED FOUNDATION DRAIN	---	EXISTING GAS MAIN
---	UNDERGROUND PARKING LIMITS	---	EXISTING ABANDONED GAS MAIN
---	PROPOSED STORM SEWER	---	EXISTING BELL CONDUIT
---	PROPOSED STORM TRENCH DRAIN	HP/LP	EXISTING OVER HEAD WIRE
---	PROPOSED CAP	---	EXISTING HYDROUTILITY POLE
---	PROPOSED BACKWATER VALVE	---	EXISTING GUY WIRE
---	EXISTING LEGAL ADJACENT LINE	TAM	EXISTING TRAFFIC MANHOLE
---	EXISTING STANDARD IRON BAR / CONTROL POINT	---	EXISTING JOINT USE STREET LIGHT
---	EXISTING FLOW DIRECTION OF SEWERS	---	EXISTING STREET LIGHT
---	EXISTING SANITARY MANHOLE & SEWER	---	EXISTING TRAFFIC HAND HOLE
---	EXISTING STORM MANHOLE & SEWER	---	EXISTING TRAFFIC SIGN
---	EXISTING CATCH BASIN	DCB	EXISTING DITCH INLET CATCH BASIN
---	EXISTING BUILDING ENVELOPE	EX HYD	EXISTING HYDRANT
---		PR HYD	PROPOSED HYDRANT
---		---	PROPOSED SIAMESE CONNECTION

- GENERAL NOTES:**
- COORDINATE AND SCHEDULE ALL WORK WITH OTHER TRADES AND CONTRACTORS.
 - DETERMINE THE EXACT LOCATION, SIZE, MATERIAL AND ELEVATION OF ALL EXISTING UTILITIES PRIOR TO COMMENCING CONSTRUCTION. PROTECT AND ASSUME RESPONSIBILITY FOR ALL EXISTING UTILITIES WHETHER OR NOT SHOWN ON THIS DRAWING.
 - OBTAIN ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF OTTAWA BEFORE COMMENCING CONSTRUCTION.
 - BEFORE COMMENCING CONSTRUCTION OBTAIN AND PROVIDE PROOF OF COMPREHENSIVE ALL RISK AND OPERATIONAL LIABILITY INSURANCE FOR \$5,000,000.00. INSURANCE POLICY TO NAME OWNERS, ENGINEERS AND ARCHITECTS AS CO-INSURED.
 - RESTORE ALL DISTURBED AREAS ON-SITE AND OFF-SITE, INCLUDING TRENCHES AND SURFACES ON PUBLIC ROAD ALLOWANCES TO EXISTING CONDITIONS OR BETTER TO THE SATISFACTION OF THE CITY OF OTTAWA AND ENGINEER.
 - REMOVE FROM SITE ALL EXCESS EXCAVATED MATERIAL, ORGANIC MATERIAL AND DEBRIS UNLESS OTHERWISE INSTRUCTED BY ENGINEER. EXCAVATE AND REMOVE FROM SITE ANY CONTAMINATED MATERIAL. ALL CONTAMINATED MATERIAL SHALL BE DISPOSED OF AT A LICENSED LANDFILL FACILITY.
 - ALL ELEVATIONS ARE GEODETIC.
 - REFER TO GEOTECHNICAL REPORT (No. PB5281-1, DATED JULY 16TH, 2020), PREPARED BY PATerson, FOR SUBSURFACE CONDITIONS, CONSTRUCTION RECOMMENDATIONS, AND GEOTECHNICAL INSPECTION PRIOR TO PLACEMENT OF THE GRANULAR MATERIAL.
 - REFER TO ARCHITECTS AND LANDSCAPE ARCHITECT'S DRAWINGS FOR BUILDING AND HARDSURFACE AREAS AND DIMENSIONS.
 - REFER TO STORMWATER MANAGEMENT REPORT PREPARED BY NOVATECH ENGINEERING CONSULTANTS LTD.
 - SAW CUT AND KEY GRIND ASPHALT AT ALL ROAD CUTS AND ASPHALT TIE IN POINTS AS PER CITY OF OTTAWA STANDARDS (R10).
 - PROVIDE LINE-PARKING PAINTING.
 - CONTRACTOR TO PROVIDE THE CONSULTANT WITH A GENERAL PLAN OF SERVICES INDICATING ALL SERVING AS-BUILT INFORMATION SHOWN ON THIS PLAN. AS-BUILT INFORMATION MUST INCLUDE: PIPE MATERIAL, SIZES, LENGTHS, SLOPES, INVERT AND TIG ELEVATIONS, STRUCTURE LOCATIONS, VALVE AND HYDRANT LOCATIONS, TWM ELEVATIONS AND ANY ALIGNMENT CHANGES, ETC.

- SEWER NOTES:**
- SPECIFICATIONS:

ITEM	SPEC. No.	REFERENCE
CATCH BASIN (600x600mm)	705.010	OPSD
STORM / SANITARY MANHOLE (1200)	701.010	OPSD
CB, FRAME & COVER	400.020	OPSD
STORM / SANITARY MH FRAME & COVER	401.010	OPSD
SEWER TRENCH - BEDDING (GRANULAR A)	S6, S7, W17	CITY OF OTTAWA / OPSD
COVER (GRANULAR A OR GRANULAR B TYPE I, WITH MAXIMUM PARTICLE SIZE=25mm)		
STORM SEWER	PVC DR 35	
SANITARY SEWER	PVC DR 35	
CATCH BASIN LEAD	PVC DR 35	
SEWER SERVICE CONNECTION - RIGID PIPE	S11	CITY OF OTTAWA
SEWER SERVICE ABANDONMENT	S11.4	CITY OF OTTAWA
 - INSULATE ALL PIPES (SAN/STM) THAT HAVE LESS THAN 1.5m COVER WITH 50mmX1200mm H-40 INSULATION. PROVIDE 150mm CLEARANCE BETWEEN PIPE AND INSULATION.
 - SERVICES ARE TO BE CONSTRUCTED TO 1.0m FROM FACE OF BUILDING AT A MINIMUM SLOPE OF 1.0%.
 - PIPE BEDDING, COVER AND BACKFILL ARE TO BE COMPACTED TO AT LEAST 98% OF THE STANDARD PROCTOR MAXIMUM DRY DENSITY. THE USE OF CLEAR CRUSHED STONE AS A BEDDING LAYER SHALL NOT BE PERMITTED.
 - FLEXIBLE CONNECTIONS ARE REQUIRED FOR CONNECTING PIPES TO MANHOLES (FOR EXAMPLE KORAN-SEAL, PSX POSITIVE SEAL AND DURASEAL). THE CONCRETE CRADLE FOR THE PIPE CAN BE ELIMINATED.
 - THE OWNER SHALL REQUIRE THAT THE SITE SERVICING CONTRACTOR PERFORM FIELD TESTS FOR QUALITY CONTROL OF ALL SANITARY SEWERS. LEAKAGE TESTING SHALL BE COMPLETED IN ACCORDANCE WITH OPS 410.07.16, 410.07.16.04 AND 407.07.24. DYE TESTING IS TO BE COMPLETED ON ALL SANITARY SERVICES TO CONFIRM PROPER CONNECTION TO THE SANITARY SEWER MAIN. THE FIELD TESTS SHALL BE PERFORMED IN THE PRESENCE OF A CERTIFIED PROFESSIONAL ENGINEER WHO SHALL SUBMIT A CERTIFIED COPY OF THE TEST RESULTS.
 - STORM MANHOLES AND CBMHs ARE TO HAVE 300mm SUMPUS UNLESS OTHERWISE INDICATED.
 - CONTRACTOR TO TELEVISION (CCTV) ALL PROPOSED SEWERS, 200mm OR GREATER PRIOR TO BASE COURSE ASPHALT. UPON COMPLETION OF CONTRACT, THE CONTRACTOR IS RESPONSIBLE TO FLUSH AND CLEAN ALL SEWERS & APPURTENANCES.
 - FULL PORT BACKWATER VALVES ARE REQUIRED ON THE SANITARY SERVICES, INSTALLED AS PER THE MANUFACTURER'S RECOMMENDATIONS AND A BACKWATER VALVE IS REQUIRED ON THE STORM SERVICES / FOUNDATION DRAINS FOR EACH BUILDING, INSTALLED AS PER STD. DWG514
 - REINSTATE ALL EXISTING PAVEMENT, CURB AND BOULEVARDS AS PER CITY OF OTTAWA R10.
 - ALL EXISTING SANITARY AND STORM SERVICES ARE TO BE CAPPED AT THE PROPERTY LINE TO THE SATISFACTION OF THE CITY OF OTTAWA'S SEWER OPERATION.
 - MONITORING TEST PORTS FOR BUILDING SERVICES TO BE INSTALLED IN PARKING GARAGE.

- WATERMAIN NOTES:**
- SPECIFICATIONS:

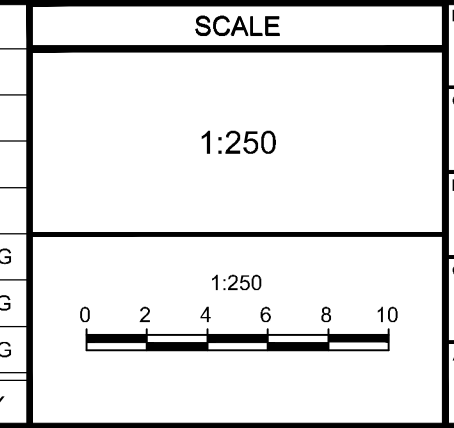
ITEM	SPEC. No.	REFERENCE
WATERMAIN TRENCHING	W17	CITY OF OTTAWA
THERMAL INSULATION IN SHALLOW TRENCHES	W22	CITY OF OTTAWA
VALVE BOX ASSEMBLY	W24	CITY OF OTTAWA
CONNECTION DETAIL FROM EXISTING TO NEW WM	W25.1	CITY OF OTTAWA
WATERMAIN CROSSING BELOW SEWER	W25.2	CITY OF OTTAWA
WATERMAIN CROSSING OVER SEWER	W25.2	CITY OF OTTAWA
THERMAL INSULATION AT OPEN STRUCTURE	W23	CITY OF OTTAWA
WATER SERVICE INSTALLATION AT SEWER	W38	CITY OF OTTAWA
 - CROSSING: SUPPLY AND CONSTRUCT ALL WATERMANS AND APPURTENANCES IN ACCORDANCE WITH THE CITY OF OTTAWA STANDARDS AND SPECIFICATIONS. EXCAVATION, INSTALLATION, BACKFILL AND RESTORATION OF ALL WATERMANS BY THE CONTRACTOR. CONNECTIONS AND SHUT-OFFS AT THE MAIN AND CHLORINATION OF THE WATER SYSTEM SHALL BE PERFORMED BY CITY OFFICIALS.
 - WATERMAIN SHALL BE MINIMUM 2.4m DEPTH BELOW GRADE UNLESS OTHERWISE INDICATED. OTHERWISE THERMAL INSULATION IS REQUIRED AS PER STD. DWG22.
 - PROVIDE MINIMUM 0.25m CLEARANCE BETWEEN OUTSIDE OF PIPES AT ALL CROSSINGS WHEN WATERMAIN IS BELOW AND MINIMUM 0.25m CLEARANCE WHEN WATERMAIN IS ABOVE.
 - WATER SERVICE IS TO BE CONSTRUCTED TO WITHIN 1.0m OF FOUNDATION WALL AND CAPPED, UNLESS OTHERWISE INDICATED.
 - ALL EXISTING WATER SERVICES TO BE BLANKED AT MAIN BY CITY FORCES. EXCAVATION AND REINSTATEMENT BY CONTRACTOR.
 - WATERMANS TO BE INTERCONNECTED FOR REDUNDANCY.

NOTE: THE POSITION OF ALL POLE LINES, CONDUITS, WATERMANS, SEWERS AND OTHER UNDERGROUND AND OVERGROUND UTILITIES AND STRUCTURES IS NOT NECESSARILY SHOWN ON THE CONTRACT DRAWINGS, AND WHERE SHOWN, THE ACCURACY OF THE POSITION OF SUCH UTILITIES AND STRUCTURES IS NOT GUARANTEED. BEFORE STARTING WORK, DETERMINE THE EXACT LOCATION OF ALL SUCH UTILITIES AND STRUCTURES AND ASSUME ALL LIABILITY FOR DAMAGE TO THEM.

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K2P 0Y6.



No.	REVISION	DATE	BY
3	SUBMITTED WITH ZONING/SITE PLAN APPLICATIONS	JAN25/21	JAG
2	RE-ISSUED FOR COORDINATION	JAN 22/21	JAG
1	ISSUED FOR COORDINATION	JAN15/21	JAG



FOR REVIEW ONLY	DATE	BY
DESIGN	JAG	
CHECKED	GJM	
DRAWN	CJF	
CHECKED	JAG	
APPROVED	GJM	



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LOCATION CITY OF OTTAWA 1200 MARITIME WAY	PROJECT No. 120144
DRAWING NAME GENERAL PLAN OF SERVICES	REV # 3
DRAWING No. 120144 - GP	