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Environmental
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Carré Saint-Louis

1050 Canadian Shield Avenue

Planning Rationale



Prepared for: Lépine Corporation

Carré Saint-Louis
1050 Canadian Shield Avenue
Ottawa, Ontario

Prepared By:

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Novatech File:120191
Ref: R-2021-092

July 9, 2021

City of Ottawa
Planning, Infrastructure and Economic Development Department
110 Laurier Ave. West, 4th Floor
Ottawa, Ontario
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**Attention: Erin O’Connell, MCIP RPP
Manager, Development Review West**

**Reference: Zoning By-law Amendment & Site Plan Control Applications
1050 Canadian Shield Avenue
Our File No.: 120191**

Novatech has prepared this Planning Rationale in relation to applications for Minor Zoning By-law Amendment and Site Plan Control for the property located at 1050 Canadian Shield Avenue. The purpose of these applications is to facilitate the development of a mixed-use building on the subject site.

The following report aims to demonstrate the suitability of the proposed development in the context of the land use policies and regulatory framework that relate to this site. The report include a review of relevant provincial and municipal planning documents, including the Provincial Policy Statement, City of Ottawa Official Plan, Kanata Town Centre Site Specific Policies, Kanata Town Centre Site Design & Architectural Guidelines and City of Ottawa Zoning By-law 2008-250, as well as the latest releases of the draft new City of Ottawa Official Plan and Kanata Town Centre Secondary Plan. This report aims to demonstrate that the proposed development represents good land use planning and is in the public interest.

Please do not hesitate to contact the undersigned should you require additional information or clarification with respect to that provided.

Yours truly,

NOVATECH



Kayla Blakely, B.E.S (PI)
Planner

Table of Contents

1.0 INTRODUCTION	1
1.1 Purpose	1
1.2 Site Location & Description	1
1.3 Surrounding Land Use and Community Context	3
2.0 DEVELOPMENT PROPOSAL	4
2.1 Description of proposed Development	4
2.2 Public Consultation Strategy	6
3.0 PROPOSED ZONING BY-LAW AMMENDMENT	7
3.1 Purpose of Existing Zoning	7
4.0 PLANNING ANALYSIS	11
4.1 Provincial Policy Statement	11
4.2 City of Ottawa Official Plan	15
4.2.1 <i>Review of Development Applications Policies</i>	17
4.3 Kanata Town Centre Special Policy Area	27
4.4 Kanata Town Centre Site Design and Architectural Guidelines	28
4.5 Draft New City of Ottawa Official Plan	29
4.6 Draft Kanata Town Centre Secondary Plan	30
5.0 PROPOSED ZONING BY-LAW AMENDMENT	31
6.0 SUPPORTING STUDIES	36
7.0 CONCLUSION	38

Appendices

Appendix A: Site Plan

Figures

Figure 1: Location Map 2

Figure 2: Existing Conditions on the subject site 2

Figure 3: Streetview of Canadian Shield Avenue (looking east)..... 3

Figure 4: Proposed Site Plan..... 5

Figure 5: Rendering view from the corner of Great Lakes Ave and Canadian Shield Ave 22

Figure 6: Rendering view from Campeau Drive..... 23

Figure 7: North Elevation 23

Figure 8: West Elevation 24

Figure 9: South Elevation 24

Figure 10: East Elevation..... 24

Figure 11: Proposed Courtyard Layout..... 25

Figure 12: Visualization of Proposed Courtyard 26

Figure 13: Draft Official Plan Schedule B8 Suburban Transect (West)..... 29

Figure 14: Excerpt of Schedule A - Designation Plan..... 31

Figure 15: Proposed Building Massing..... 35

1.0 INTRODUCTION

1.1 Purpose

Novatech has prepared this Planning Rationale in relation to applications for Minor Zoning By-law Amendment and Site Plan Control for the property located at 1050 Canadian Shield Avenue.

The following report is intended to outline the details of the proposal and provide an analysis of relevant provincial and municipal planning documents with a view of demonstrating the following:

- Consistency with the Provincial Policy Statement;
- Conformity to the City of Ottawa Official Plan;
- Conformity to the Kanata Town Centre Site Specific Policies;
- Regard for the Kanata Town Centre Site Design & Architectural Guidelines;
- Respect for the general intent of the City of Ottawa Zoning By-law 2008-250; and
- Considerations for good land use planning.

1.2 Site Location & Description

The site of the proposed mixed-use community is located in the Kanata North Ward of the City of Ottawa, in the growing Kanata Town Centre mixed-use district. The property is legally described as Block 2 on Plan 4M-1325. The total area of the site is 10,917 square metres and is bound by three streets: Campeau Drive, Great Lakes Avenue, and Canadian Shield Avenue. The site measures approximately 93 metres from north to south and 125 metres from east to west.

The subject site is currently vacant and has not been occupied previously by any buildings or structures. The topography of the site varies considerably such that the grade differs by greater than 3m, sloping downwards from Campeau Drive to Canadian Shield. While there is minimal vegetation on the site, there are rows of street trees along Great Lake Avenue and Canadian Shield Avenue planted within City rights-of-way. There are public sidewalks along Great Lakes Avenue and Canadian Shield Avenue, as well as a Multi-Use Path along Campeau Drive (Figures 1 and 2).



Figure 1: Location Map



Figure 2: Existing Conditions on the subject site

1.3 Surrounding Land Use and Community Context

The subject site is located within the area known as the Kanata Town Centre which is a developing mixed-use centre in the west end of the City of Ottawa. This area of the Kanata Town Centre is characterized by a mix of uses, including, mixed-use residential and commercial buildings, a retirement home, hotels and a number of apartment dwellings, all of which have varying mid and high-rise built forms.

The Kanata Town Centre limits include the adjacent lands to the east, south and west of the subject site. To the east of the subject site, the abutting property is occupied by a six-storey retirement home and further east is residential development beyond Maritime Way, including recently constructed mid-rise apartment buildings. The properties to the south, across Canadian Shield Avenue are occupied by a number of mid to high-rise buildings, ranging from five to twelve storeys, which make up Williams Court, and include residential apartments, small-scale retail, and an internal courtyard. Further south is a parcel planned for future high-rise residential uses, adjacent to Highway 417. The site is bordered by Great Lakes Avenue to the west, and the parcel across the street is occupied by a six-storey hotel, residence suites as well as an extended-stay hotel. Beyond this parcel there is vacant land which is subject to applications for mixed use development, and will implement the extension of Canadian Shield Avenue, connecting it to Maritime Way at Kanata Avenue and the commercial uses further west in Kanata Town Centre as well as Bill Teron Park.



Figure 3: Streetview of Canadian Shield Avenue (looking east)

Across Campeau Drive, to the north of the subject site are two-storey townhouse dwellings with rear yards and privacy fencing facing Campeau Drive, which are part of the Kanata Lakes subdivision, and are bordered by the Kanata Lakes Golf course further north.

The established built form generally transitions from taller to lower building heights approaching Campeau Drive which represents the boundary of the Town Centre. Buildings are generally oriented towards the street in a compact form with internal courtyards and parking provided primarily below grade. As demonstrated further in this report, the proposed development has been designed to fit into the developing urban context of Kanata Town Centre, in terms of compatibility with existing and planned higher-profile development, while also being sensitive to the lower-profile residential nature of the development to the north of the site on the opposite side of Campeau Drive. .

2.0 DEVELOPMENT PROPOSAL

2.1 Description of proposed Development

Lépine Corporation is an established Kanata-based firm that specializes in creating quality apartment communities in Ottawa and surrounding municipalities. This development proposal represents the eighth residential or mixed-use development by Lépine Corporation in the Kanata Town Centre community. Its rental accommodations include innovative unit designs, high-end finishes, resort-like amenities and beautifully landscaped grounds to promote appealing lifestyles for all demographics. Lépine Corporation continues to respond to evolving housing needs and intends to expand its presence in the Kanata Town Centre through the development of a new apartment community at the subject site, known as Carré Saint-Louis.

The proposed development is a terraced three to six-storey mixed-use community with facades framing Canadian Shield Avenue, Great Lakes Avenue and Campeau Drive. The development will have a total gross floor area of 25,286 square metres, comprising 244 residential units and three commercial units occupying 274 square metres of floor space at grade. Unit types will range from one to three bedrooms and are designed to accommodate evolving work-from-home needs by including dedicated workspaces with reliable high-speed internet connections.

Parking for the proposed development will be provided fully below grade on two levels and will total 348 spaces, including 10 barrier-free parking spaces and will be accessed from Canadian Shield Avenue. Bicycle parking will also be provided internal to the building with 124 spaces available.

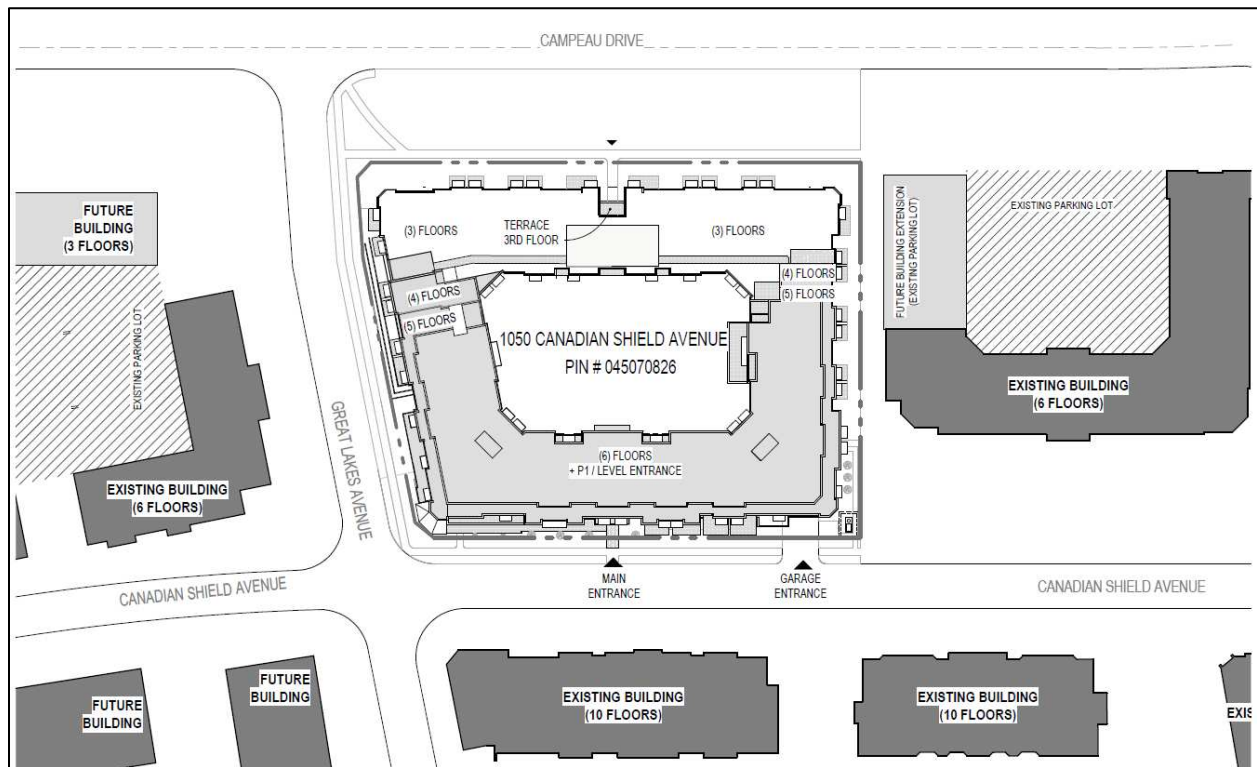


Figure 4: Proposed Site Plan

The building will be three-storeys along Campeau Drive, with building height stepping up in the direction of Canadian Shield Avenue. This gradual stepped design has been chosen to deliberately accommodate height transition between the two-storey townhouses to the north, across Campeau Drive, and the ten-storey building to the south. The topography of the site is unique in that it slopes considerably downward from north to south. As such the P1 level will be at-grade on the southern portion of the site and will accommodate three retail units in addition to several residential units and accessory amenity and administrative spaces, as well as the entrance to the parking garage.

As shown in **Figure 3** above, a series of stepbacks and rooftop terraces will be located on various floors, providing outdoor amenity space for tenants. Future residents will benefit from a large internal courtyard which will be landscaped and feature various gathering spaces for tenants, such as seating areas, a BBQ patio, fitness terrace, sun deck, and gardening plots. Additional indoor amenity areas will open onto the courtyard, including an indoor pool, a gym, and other recreational areas. Upper floor dwelling units additionally have private amenity areas in the form of balconies and roof terraces, while ground floor units will have private terraces with a direct link to the public realm or courtyard. The total proposed communal amenity area for the building is over 1,050 square metres, with an additional 2,620 square metres of private amenity area.

The areas surrounding the building, as well as the internal courtyard will be landscaped, including trees, shrubs, patios and pedestrian pathways with a fountain in the centre as a focal point of the courtyard. Waste will be stored internally to the building with the service door adjacent to the garage entrance. Residents will be able to access the building from entrances from two streets which the building fronts, with a large main lobby along Canadian Shield Avenue and an open archway courtyard entrance from the public pathway along Campeau Drive. Wide glazed openings and patio doors on the private terraces make up more than 50% of the ground floor walls facing a public street, which provides better views for the occupants and ensures “eyes on the street”.

Retail and service oriented commercial spaces are located at the intersection of Great Lakes and Canadian Shield Avenue, with access from the street distinguished by a series of arcades treated in the style of traditional architecture. These spaces will offer services to residents, visitors, and neighbours of the heart of the Town Centre community. The current 274 square metres of commercial space reflects anticipated market demand for small-scale commercial uses, however, the balance of the building abutting Canadian Shield Avenue is designed to accommodate the conversion of all floor area to commercial uses should there be future demand. A surplus of parking is provided for future commercial uses to provide flexibility for prospective tenants and the ability to expand commercial uses at-grade through the conversion of at-grade residential uses if and when demand warrants. This approach is effective in responding to the evolving nature of this suburban area.

2.2 Public Consultation Strategy

The proposal requires approval of concurrent applications for Zoning By-law Amendment (Minor) and Site Plan Control (Complex). The Zoning By-law Amendment is subject to approval by City Council in consultation with Planning Committee. The Site Plan Control application is to be processed and approved under powers delegated to the Manager of Development Review. Public notification of these applications will be provided by means of mailed notice to surrounding residents within 120m of the subject site and any registered community groups. Signage will also be posted on the property. Additional information regarding the proposed development, including electronic copies of all submission materials, will be made available on the City’s website. Any person who wishes to provide comments to staff for consideration may do so.

The proponent consulted informally with the Ward Councillor prior to submitting applications. Councillor Sudds is familiar with Lépine Corporation’s communities across the City and the level of design efforts put forward in all its development. In response to preliminary comments offered by Councillor Sudds, the design of the originally proposed residential development was revised to include a retail component at grade along Canadian Shield Avenue. Councillor Sudds was informed of the intent to seek relief with respect to certain provisions of the Zoning By-law, and raised the importance of following the direction of the Kanata Town Centre Site Specific Policies. Further consultation with Councillor Sudds will take place through the application process.

In addition to the statutory meeting that is held at the scheduled Planning Committee meeting, Councillor Sudds may wish to convene a community information meeting upon submission of the formal applications.

3.0 PROPOSED ZONING BY-LAW AMMENDMENT

3.1 Purpose of Existing Zoning

Pursuant to City of Ottawa Zoning By-law 2008-250, the subject site is zoned Mixed Use Centre Zone, Subzone 15 - MC15 [2027]. The purpose of the MC15 [2027] zone is to ensure the accommodation of a combination of transit-supportive uses such as offices, secondary and post-secondary schools, hotels, hospitals, large institutional buildings, community recreation and leisure centres, day care centres, retail uses, entertainment uses, service uses such as restaurants and personal service businesses, and high- and medium-density residential uses, as well as to allow the permitted uses in a compact and pedestrian-oriented built form in mixed-use buildings or side by side in separate buildings. This zone imposes *development standards that ensure medium to high profile development while minimizing its impact on surrounding residential areas.*

By-law 2013-86 introduced site-specific Exception 2027 for the subject site with the intent of ensuring future development conformed to the policy direction in the Official Plan for the Kanata Town Centre and aligned with the Kanata Town Centre Downtown Master Plan and Implementation Study and the Kanata Town Centre Design Guidelines. The exception includes additional provisions which together with those in the Mixed-Use Centre Zone and Subzone 15 provisions are conducive of a downtown atmosphere.

Urban Exception 2027 sets out additional zoning provisions which apply to the subject site, as shown below:

I Exception Number	II Applicable Zones	III Exception Provisions - Additional Land Uses Permitted	IV Exception Provisions - Land Uses Prohibited	V Exception Provisions - Provisions
2027 (By-law 2013-86)	MC15[2027]		<ul style="list-style-type: none"> stacked dwelling 	<ul style="list-style-type: none"> minimum yard setback from a lot line abutting Campeau Drive: 6 m maximum yard setback for the building wall of a parking garage facing Campeau Drive: no maximum maximum yard setback for any other building wall facing Campeau Drive: 6.5 m setbacks from a lot line abutting a public street other than Campeau Drive: <ol style="list-style-type: none"> no minimum maximum: <ol style="list-style-type: none"> For the building wall of a parking garage: no maximum For at least 80% of the width of any other building wall facing a public street other than Campeau Drive: <ol style="list-style-type: none"> 0.5 m; or 2 m where a patio is located between the building wall and a lot line minimum building height: 11 m maximum building height: <ol style="list-style-type: none"> for the first 3 meters back from all lot lines abutting a street other than Campeau Drive: 15m for any part of a building not within the area identified in 1 above: the lesser of 23 m or six-storeys

Table 1 provides a summary of the current MC15 [2027] zone requirements as they relate to the proposed mixed-use development and indicates where relief is required.

Table 1: City of Ottawa Zoning By-law

CITY OF OTTAWA ZONING BY-LAW 2008-250		
MC15[2027] (MIXED-USE CENTRE ZONE) PROPOSED MID-RISE MIXED-USE DEVELOPMENT		
ZONING PROVISION	REQUIRED	PROVIDED
MIXED-USE CENTRE ZONE PROVISIONS		
LOT AREA (MIN.)	NO MINIMUM	10,917m ²

LOT WIDTH (MIN.)	NO MINIMUM	±86.5m
FRONT YARD SETBACK (MIN. / MAX.) - GREAT LAKES	NO MINIMUM / MAXIMUM 0.5m FOR 80% OF THE BUILDING WALL OR 2m WHERE A PATIO IS LOCATED BETWEEN THE BUILDING WALL AND LOT LINE	3.5m MIN / 5.4m MAX *
CORNER SIDE YARD SETBACK (MIN. / MAX.) - CAMPEAU	MINIMUM 6m / MAXIMUM 6.5m	6.0m MIN / 13.3m MAX *
CORNER SIDE YARD SETBACK (MIN. / MAX.) - CANADIAN SHIELD	NO MINIMUM / MAXIMUM 0.5m FOR 80% OF THE BUILDING WALL OR 2m WHERE A PATIO IS LOCATED BETWEEN THE BUILDING WALL AND LOT LINE	3.1m MIN / 5.2m MAX *
REAR YARD SETBACK (MIN.)	6m WHERE ABUTTING A RESIDENTIAL ZONE	4.4m *
BUILDING SEPARATION (MIN.)	12m BETWEEN PORTIONS OF ANOTHER BUILDING GREATER THAN 15m IN HEIGHT	12m
FLOOR SPACE INDEX (MIN.)	0.75 FOR NON-RESIDENTIAL (192(15)(a)) OR RESIDENTIAL (192(15)(c)) USES	2.3
FLOOR SPACE INDEX (MIN.)	1.5 FOR NON-RESIDENTIAL (192(15)(a)) USES IF RESIDENTIAL (192(15)(c)) USES PROPOSED	0 *
GROSS FLOOR AREA FOR ANY STOREY ABOVE 15m (MAX.)	2,500m ²	3,000m² (5TH STOREY) * 2,220m ² (6TH STOREY)
BUILDING HEIGHT (MIN. / MAX.)	MINIMUM 11m / MAXIMUM 15m WITHIN 3m OF A LOT LINE ABUTTING A STREET OTHER THAN CAMPEAU / MAXIMUM 23m OR SIX STOREYS FOR ANY OTHER PART OF A BUILDING	11m MIN / N/A / 21.5m MAX (SIX STOREYS)

LENGTH OF GROUND FLOOR WALL CONSISTING OF WINDOWS AND/OR ENTRANCES	50% WHERE FACING PUBLIC STREET	50%
LANDSCAPED AREA (MIN.)	30%	32%
PARKING PROVISIONS (AREA C)		
PARKING SPACES - RESIDENTIAL (MIN.)	1 PER DWELLING UNIT = 244	348 (302 required)
PARKING SPACES - NON-RESIDENTIAL (MIN.)	3.4 PER 100m2 GFA RETAIL = 9	
VISITOR PARKING SPACES (MIN.)	0.2 PER DWELLING UNIT = 49	
ACCESSIBLE PARKING SPACES (MIN.)	4 TYPE A & 5 TYPE B	5 TYPE A & 5 TYPE B
BICYCLE PARKING SPACES - RESIDENTIAL (MIN.)	0.5 PER DWELLING UNIT = 122	124 (123 required)
BICYCLE PARKING SPACES - NON-RESIDENTIAL (MIN.)	1 PER 250m2 GFA RETAIL = 1	
AMENITY AREA PROVISIONS		
TOTAL AMENITY AREA (MIN.)	6m2 PER DWELLING UNIT = 1,464m2	2,620m2
COMMUNAL AMENITY AREA (MIN.)	50% OF REQUIRED TOTAL AMENITY AREA = 732m2	1,050m2

The purpose of the proposed Zoning By-law Amendment is to adjust and add site-specific provisions addressing minimum and maximum yard setbacks, minimum floor space index (FSI) for non-residential uses and maximum gross floor area for storeys above 15 metres.

4.0 PLANNING ANALYSIS

4.1 Provincial Policy Statement

The *Provincial Policy Statement (PPS) 2020* provides policy direction on land use planning and development matters of provincial interest by setting the policy foundation for regulating the development and use of land as set out in Section 2 of the *Planning Act*. The decisions that affect all planning matters “shall be consistent with” relevant policy statements under the authority of Section 3 of the *Planning Act*. The following is an overall review of the applicable PPS policies:

Building Strong Healthy Communities

Section 1.1 of the *PPS* speaks to managing and directing land use to achieve efficient and resilient development and land use patterns.

Policy 1.1.1 states that healthy, liveable, and safe communities are sustained by:

- a) *promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*
- b) *accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
- c) *avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
- d) *avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent to or close to settlement areas;*
- e) *promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;*
- f) *improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;*
- g) *ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs;*
- h) *promoting development and land use patterns that conserve biodiversity; and*
- i) *preparing for the regional and local impacts of a changing climate.*

The proposed development will achieve an efficient land use pattern by using vacant land within an existing built up area for residential intensification. The proposed development will be served by existing municipal infrastructure including water, stormwater, and sanitary services. The development provides a variety of dwelling unit types in a compact form to cater to the needs of a range of individuals and families in the community, with the design of the building contributing to an enhanced urban town centre.

Section 1.1.3 speaks to settlement areas, which are urban areas and rural settlement areas and include cities, towns, villages and hamlets. The vitality and regeneration of settlement areas is critical to the long-term economic prosperity of our communities. Policy 1.1.3.1 requires that settlement areas shall be the focus of growth and development.

Policy 1.1.3.2 notes that land use patterns within *settlement areas* shall be based on densities and a mix of land uses which:

- a) *efficiently use land and resources;*
- b) *are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
- c) *minimize negative impacts to air quality and climate change, and promote energy efficiency;*
- d) *prepare for the impacts of a changing climate;*
- e) *support active transportation;*
- f) *are transit-supportive, where transit is planned, exists or may be developed.*

Policy 1.1.3.4 states that appropriate development standards should be promoted which facilitate intensification, redevelopment, and compact form, while avoiding or mitigating risks to public health and safety. The proposed residential development is situated on lands designated as a settlement area with the City of Ottawa, and further identified as a Town Centre, close to facilities and transit.

Section 1.4 of the *PPS* speaks to housing with the applicable policies. Policy 1.4.3 requires that planning authorities shall provide for an appropriate range and mix of *housing options* and densities to meet projected market-based and affordable housing needs of current and future residents of the *regional market area* by:

- b) *permitting and facilitating:*
 - 1. *all housing options required to meet the social, health, economic and well-being requirements of current and future residents, including special needs requirements and needs arising from demographic changes and employment opportunities;*
- c) *directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;*

- d) *promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed;*
- e) *requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations; and*
- f) *establishing development standards for residential intensification, redevelopment and new residential development which minimize the cost of housing and facilitate compact form, while maintaining appropriate levels of public health and safety.*

The proposed residential development will feature a mix of dwelling unit typologies to meet the needs of current and future residents and contribute positively to the availability of rental accommodations in this area. The proposed development represents a compact built form and efficiently uses land resources while also being located near services and facilities. Providing alternative housing options such as this indirectly helps to revive communities by creating movement and opportunities for individuals, families or seniors to move from other dwelling types to multi-residential forms, while remaining in their community, and allowing new residents to move into the community.

Section 1.5 of the *PPS* speaks to public spaces, recreation, parks, trails, and open space with the applicable policies. Policy 1.5.1 notes that healthy, active communities should be promoted by:

- a) *planning public streets, spaces and facilities to be safe, meet the needs of pedestrians, foster social interaction and facilitate active transportation and community connectivity;*
- b) *planning and providing for a full range and equitable distribution of publicly-accessible built and natural settings for recreation, including facilities, parklands, public spaces, open space areas, trails and linkages, and where practical, water-based resources;*

The Subject site has been designed with a variety of amenity areas and includes open space inside the courtyard, on the various terraces and, is linked to a path network which connects building access points to the public realm, and recreation features. The proposed residential development will have access to passive and active recreational green spaces located within walking and cycling distance. The overall site has been designed to minimize pedestrian and vehicular interactions where possible and promotes active transportation through the design of the site, and provision of sufficient bicycle parking.

Section 1.6.6 addresses sewage, water, and stormwater services. Policy 1.6.6.2 notes that *municipal sewage services* and *municipal water services* are the preferred form of servicing for *settlement areas* to support protection of the environment and minimize potential risks to human health and safety. Within *settlement areas* with existing *municipal sewage services* and *municipal water services*, *intensification* and *redevelopment* shall be promoted wherever feasible to optimize the use of the services. The proposed residential development will be connected to municipal infrastructure including water, stormwater, and sanitary services.

Wise Use and Management of Resources

Section 2.0 of the *PPS* speaks to conserving biodiversity and protecting the health of Great Lakes, natural heritage, water, agriculture, mineral aggregate, petroleum, cultural heritage and archaeological resources for the long-term prosperity, environmental health, and social well-being of Ontario.

Policy 2.1.1 requires that natural features and areas shall be protected for the long term. Policy 2.1.2 notes that the diversity and connectivity of natural features in an area, and the long-term *ecological function* and biodiversity of *natural heritage systems*, should be maintained, restored or, where possible, improved, recognizing linkages between and among *natural heritage features and areas*, *surface water features* and *ground water features*.

There is a history of comprehensive planning in this area which anticipates growth while taking into consideration the surrounding natural and environmental features. There are no significant natural features noted on or immediately adjacent to the subject site and no concerns with respect to resources interests in the context of the subject site.

Protecting Public Health and Safety

Section 3.0 considers the Province's long-term prosperity, environmental health and social well-being which are dependent on reducing the potential for public cost or risk to Ontario's residents from natural or human-made hazards. Development shall be directed away from areas of natural or human-made hazards where there is an unacceptable risk to public health, safety, property damage and not create new or aggravate existing hazards.

Regarding Section 3.1 (Natural Hazards), the site is not located on lands impacted by hazardous sites, erosion and/or dynamic beach hazards, or large inland lakes. The subsurface conditions on site were observed to be suitable for the proposed development, subject to implementing the recommended design and construction engineering practices.

Regarding Section 3.2 (Human-Made Hazards), there has been no development historically on the site and there are no known potential environmental concerns

associated with past and current use of adjacent lands. Public health and safety interests are not anticipated.

Based on the foregoing, the proposed development is consistent with the policy direction of the PPS.

4.2 City of Ottawa Official Plan

The City of Ottawa Official Plan was adopted by Council in May 2003 and provides a vision of the future growth of the city and a policy framework to guide its physical development to the year 2036.

The subject site is designated as a Town Centre on 'Schedule B: Urban Policy Plan' in the Official Plan. Section 3.6.2 speaks to the importance of these centres:

Town Centres and Mixed Use Centres occupy strategic locations on the Rapid-Transit network and act as central nodes of activity within their surrounding communities and the city as a whole. These centres are a critical element in the City's growth management strategy, being areas with potential to achieve high densities and compact and mixed-use development oriented to rapid transit. More jobs and housing at these locations will increase transit ridership and draw more commuter travel to these locations. In the long term the centres will become complete, liveable communities that attract people for the jobs, leisure, lifestyle, and business opportunities they provide.

Managing Growth (Section 2.2)

Section 2.2 of the Official Plan speaks to the continued growth of the City and emphasizes that growth will be distributed throughout the urban area to strengthen the city's liveable communities. The development of the currently vacant site will contribute to the completion of the Kanata Town Centre community.

Town Centres are target areas for intensification, with a target density identified in Figure 2.3 as 120 people and jobs per gross hectare in the Kanata Town Centre. The proposed development well exceeds this density at an individual site level and contributes to the overall targets for the Town Centre.

Mixed Use Centres and Town Centres (Section 3.6.2)

Kanata is one of three Town Centres in the suburban areas of Ottawa. The Official Plan describes *Town Centres, when compared with Mixed Use Centres, as more diverse and having a larger role as centres for the communities around them, offering close-to-home*

opportunities for shopping, leisure and other activities. Planning for the Kanata Town Centre was initiated in the 1970's by the Former City of Kanata and the area has evolved considerably since that time.

Policy 1 in Section 3.6.2 describes the general criteria for Mixed Use Centres and Town Centres. Criteria a), b), e) and f) are of particular relevance:

- a. Most of the centre is within an 800m walking distance of one or more rapid transit stations, and contains one or more arterial roads with all day, frequent transit service;*
- b. There is opportunity to achieve high densities of jobs and housing through intensification and redevelopment of older sites and development of vacant land;*
- c. High-Rise Buildings of 10 storeys and more can be accommodated in a manner that provides appropriate transition to the surrounding area;*
- d. Employment targets of at least 5,000 jobs can be achieved in Mixed Use Centres and at least 10,000 jobs can be achieved in Town Centres;*
- e. The area is or can become transit-oriented, as described in this section;*
- f. The area is suitable for a mix of uses and could be linked within the area's greenspace network.*

The proposal is an exemplary of development that uphold the characteristics of the Town Centre. The subject site benefits from close proximity to existing and planned transit stations and is of sufficient size to accommodate medium density mixed use development which supports multi-unit housing as well as shops and service that provide for needs of residents in the area. The development allows for appropriate transition in building height at the periphery of the Town Centre where it abuts an established low-rise area within the General Urban Area. There are also opportunities to connect this site to the sidewalk and the surrounding greenspace network.

Policy 5 outlines the uses that Town Centres permit which comprise a broad variety of land uses at transit supportive densities, including among others high- and medium-density residential uses and mixed-use development. Generally, employment and housing are required as part of a larger mix of uses within a building or in adjacent buildings.

Policy 9 provides that: *“All development applications and Community Design Plans for Mixed-Use Centres will be reviewed in the context of this Plan and in particular:*

- 1. Will be evaluated in the context of the Design Objectives and Principles in Section 2.5.1 and the criteria set out in Section 4.11, particularly with regard to achieving a compact, mixed-use, transit-oriented, pedestrian-friendly environment and creating a place with visual interest;*
- 2. Will, where possible, contribute to a range of housing options in the area.”*

An assessment against the Design Objectives and Principles in Section 2.5.1 and the criteria set out in Section 4.11 is made in the sections below. The proposal contributes positively to housing options in the area as it provides 244 dwelling units oriented to both individual and family living, with the intent of creating an apartment community which meets the needs of all and provides high-quality long-term rental accommodations.

Policy 10) requires that: “Mixed-Use Centres will optimize the use of land through provisions for compact mixed-use development. Amongst other things, the Zoning By-law and community design plans will: Require residential uses in the form of apartments and other multiples at a medium or high density”. The proposal is consistent with this policy and contributes to a complete and livable community.

4.2.1 Review of Development Applications Policies

Section 2.5.1 of the *Official Plan – Designing Ottawa* is concerned with how buildings, landscapes, and public spaces look as well as function together. Design elements should work together to complement and enhance the unique aspects of a community’s history, landscape, and its culture. Further, it is noted that *“encouraging good urban design and quality as well as innovative architecture can also stimulate the creation of lively community places with unique and distinct character while attracting people and investment to the City”.* Compatible development speaks to enhancing the features of an established community and coexisting with existing development without causing undue adverse impacts on surrounding properties. Section 4.11 which also speaks Urban Design and Compatibility will be reviewed in conjunction with Section 2.5.1.

The following is a review of the design objectives and summary of how the proposed development is consistent with these objectives:

- 1. To enhance the sense of community by creating and maintaining places with their own distinct identity.*

The proposed development will assist in the transition between lower buildings to the north, and taller buildings to the south. The building will tie together the identities of both communities and establish itself as a focal point in the neighbourhood through thoughtful

design elements, including the street-oriented facades on all three frontages, landscaping features, as well as terracing elements.

2. To define quality public and private spaces through development.

Future residents of the proposed residential development will have access to shared outdoor amenity space in the central courtyard, as well as private balconies and terraces. Pathways will connect the site to the public realm and neighbouring developments.

3. To create places that are safe, accessible and are easy to get to, and move through.

The layout of the site will provide for a safe and navigable access along Campeau Drive, Great Lakes Avenue and Canadian Shield Avenue by minimizing potential points of conflict between vehicles, cyclists and pedestrians. Pathways through the site will connect to sidewalks and paths that abut the aforementioned roads.

4. To ensure that new development respects the character of existing areas.

The height transition elements and streetscape animating elements of the proposed development will ensure this development respects the character of the existing areas in the Kanata Town Centre.

5. To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.

The proposed residential development is located in a community which is experiencing growth and new development. The building is designed with internal spaces at-grade that can readily be converted to additional commercial floor area to respond to evolving demands for office and commercial space if and when warranted in the future. The building itself will include barrier free options to meet changing needs of its residents.

As this area is expected to be serviced by Light Rail Transit in the future as part of the City's ultimate transit network, this proposal supports the fundamentals of a Transit-supportive environment, while also accommodating underground parking to meet the needs residents in the current context of the Town Centre where local transit routes have yet to be established within this area of the Town Centre to connect to nearby major transit stations. As aforementioned a surplus of parking is provided for future commercial uses to provide flexibility for prospective tenants and the ability to expand commercial uses at-grade through the conversion of at-grade residential uses.

6. To understand and respect natural processes and features in development design.

The proposed residential development will respect the subject site's surrounding natural and environmental features.

The subject site consists of land that has already been disturbed and has remained undeveloped.

7. *To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy-use, and carbon footprint of the built environment.*

Access to transit services and pathways within the surrounding community will lead to a mixture of neighbourhood services and amenities within walking and cycling distance that will encourage future residents to reduce their overall carbon footprint.

Additionally, the applications for Zoning By-law Amendment and Site Plan Control pertaining to the subject site respect the following policies:

- Section 4.1 (Site-Specific Policies and Secondary Policy Plans) - the property is subject to the Kanata Town Centre Site Specific Policies, found in Volume 2b of the Official Plan. The policies for the Kanata Town Centre aim to achieve *an active, vibrant core area containing a diverse mix of urban uses arranged in a form and a scale, which is both human and appropriate to its function*. The property is located in the Central Business District Area and an analysis of relevant policies will follow;
- Section 4.2 (Adjacent Land-Use Designations) - the subject site is not adjacent to any land use designation or features identified in this section;
- Section 4.3 (Walking, Cycling, Transit, Roads and Parking Lots) - the subject site supports various modes of transportation, including walking, cycling, and transit. To complement these modes of transportation, bicycle parking is provided in a secure area within the building and multiple pedestrian entrances are provided at grade with direct access the adjacent sidewalks.

The proposed development is within 800m walking distance of the existing Terry Fox BRT transit station which includes an OC Transpo Park and Ride Facility and is the main transit hub for the community. This station is to be converted to an LRT station in the future once the transit service is extended west. An additional LRT transit station is also planned along Maritime Way at the pedestrian bridge over Highway 417, known as the Kanata Town Centre station. This future station will be within a 450m walking distance of the subject site and will significantly improve transit service to the Town Centre. Supplemental Transit Demand Management initiatives may be implemented as part of the development in the future once an improved level of transit service is established such that residents can rely fully on transit to commute and reach destinations throughout the City.

Campeau Drive is identified as an Arterial Road on Schedule E, with a protected right-of-way width of 40m. No road widening is required for the subject site;

- Section 4.4 (Water and Wastewater Services) – there are existing water and wastewater services in all three abutting streets. The servicing design for the development proposes connections to existing sanitary and storm sewers in Canadian Shield Avenue and an existing watermain in Great Lakes Avenue;
- Section 4.5 (Housing) – the subject site is currently vacant. An increase of 244 units intended as rental accommodations with three supporting commercial units is proposed on the property;
- Section 4.6 (Cultural Heritage Resources) – the subject site is not designated under the *Ontario Heritage Act*, nor are any adjacent lands;
- Section 4.7 (Environmental Protection) – no environmental features have been identified on the subject site. A Tree Conversation Report has been prepared and proposes to protect existing trees which are in good condition and do not conflict with the building foundation. Stormwater management, as well as erosion and sediment control during construction, will be in accordance with City of Ottawa requirements;
- Section 4.8 (Protection of Health and Safety) – the subject site is not affected by any environmental constraints identified on Schedule K. A Geotechnical Investigation has been completed to assess suitability of soil for the proposed development.

The development will incorporate noise control measures, notably installation of central-air conditioning systems where required, to mitigate noise levels associated with arterial and highway road noise and ensure comfortable interior living spaces;

- Section 4.9 (Energy Conservation Through Design) – the proponent follows energy efficient construction practices, specifies high quality products and utilizes durable, long-lasting materials to promote sustainability and longevity of all its buildings. Of particular significance is the proposed concrete superstructure which allows for repurposing or alterations over time to promote adaptability and resiliency.
- Section 4.10 (Greenspace Requirements) – parkland dedication has been fulfilled through the previous subdivision approval for the Central Business District completed by Urbandale.
- Section 4.11 of the *Official Plan* also refers to Urban Design and Compatibility and works in conjunction with the policies found in Section 2.5.1 as previously reviewed. The policies within this section focus on urban design and compatibility at a more localized scale for neighbourhoods and individual properties. Although it is important to consider all policies in the design aspects, the policies that are relevant to the proposed development are reviewed below.

Views

“Depending on its location, the mass or height of new development may enhance or impact the views visible from public viewpoints, such as public monuments, bridges, civic spaces, landforms, and other valued spaces. View corridors and view planes can be established to guide and regulate the height and mass of development within a defined area, so as to protect the public view”.

The proposed residential development will not impact the views of any public monuments, bridges, civic spaces, landforms, and other valued spaces.

Building Design

“Good building design contributes to successful neighbourhood integration and the compatibility of new development with the existing or planned character of its surroundings. The façades of buildings influence the feel and function of public spaces and define the edges of the pedestrian environment. Good building design is required throughout the city. In the City’s design priority areas and areas subject to the design priority policies, building design is intended to support the image of Ottawa as a Capital city and contribute to a positive experience for residents and visitors”.

The project offers a sophisticated architectural style using prefabricated concrete panels designed to resemble beige limestone masonry. The upper floors of the façades have a pattern recalling slate roofs and the balconies will have ornate iron railings. The proposed layout offers generous spaces looking onto a landscaped central courtyard. The terraced design of the building creates a smooth transition between the taller mid and high-rise buildings to the south and the lower two-storey homes to the north of Campeau Drive. These tapered heights will bring in more sunlight to the landscaped courtyard and promote natural light in all the dwellings.

The north elevation of the proposed building along Campeau Drive respects the maximum setback of 6.5 metres along most of the frontage. The secondary access is marked by an archway which provides a break in the façade and divides it onto two segments with townhome elements to better complement the character of the dwellings to the north across Campeau Drive.

The four corners of the building are punctuated with turrets. The building exceeds the maximum permitted setback of 2 metres from the property line along Great Lakes Avenue and Canadian Shield Avenue in order to accommodate residential access to terraces providing a more active street, with natural surveillance, as well as retaining walls and proper grading transitions on the inclined elevations. Integration of a series of stepped patios located between the building wall and the property line help mitigate the increased building setbacks.

The primary pedestrian entrance is located along Canadian Shield Avenue with a double-height lobby overlooking the landscaped courtyard. The generously recessed top storey of the building offers private terraces. The projected building height along Canadian shield avenue is six storeys above the parking level. Vehicular parking for residents and visitors is all located below grade.

The development of the residential community, including commercial elements and greenspace will foster the conditions for a pleasant and healthy living environment for visitors, residents and business owners in the area. As per the proposed **Figures 8, 9 and 10**, the integrated concept of the development of the new apartments ensures that this building will maintain its appearance and value for the decades to come.



Figure 5: Rendering view from the corner of Great Lakes Ave and Canadian Shield Ave



Figure 6: Rendering view from Campeau Drive

Massing and Scale

“Complementary to building design, the massing and scale of new development also contributes to successful neighbourhood integration and the compatibility of new development with the character of the surrounding community. Massing and scale describe the form of the building, how tall it is, how much of the lot it occupies and how it is positioned in relation to the street and surrounding buildings”.

As shown in the elevations, the massing and scale of the proposed dwellings is compatible with the diverse character of the surrounding community.



Figure 7: North Elevation



Figure 8: West Elevation



Figure 9: South Elevation



Figure 10: East Elevation

Outdoor Amenity Areas

“Applications to develop residential or mixed-use buildings incorporating residences will include well-designed, usable amenity areas for the residents that meet the requirements of the Zoning By-law, and are appropriate to the size, location and type of development. These areas may include private amenity areas and communal amenity spaces such as: balconies or terraces, rooftop patios, and communal outdoor at-grade spaces (e.g. plazas, courtyards, squares, yards).”

The proposal includes private and communal areas which have been designed to accommodate a variety of leisure activities, in the form of the communal indoor areas, terraces, balconies and the central courtyard, as shown in **Figures 10 and 11** below.

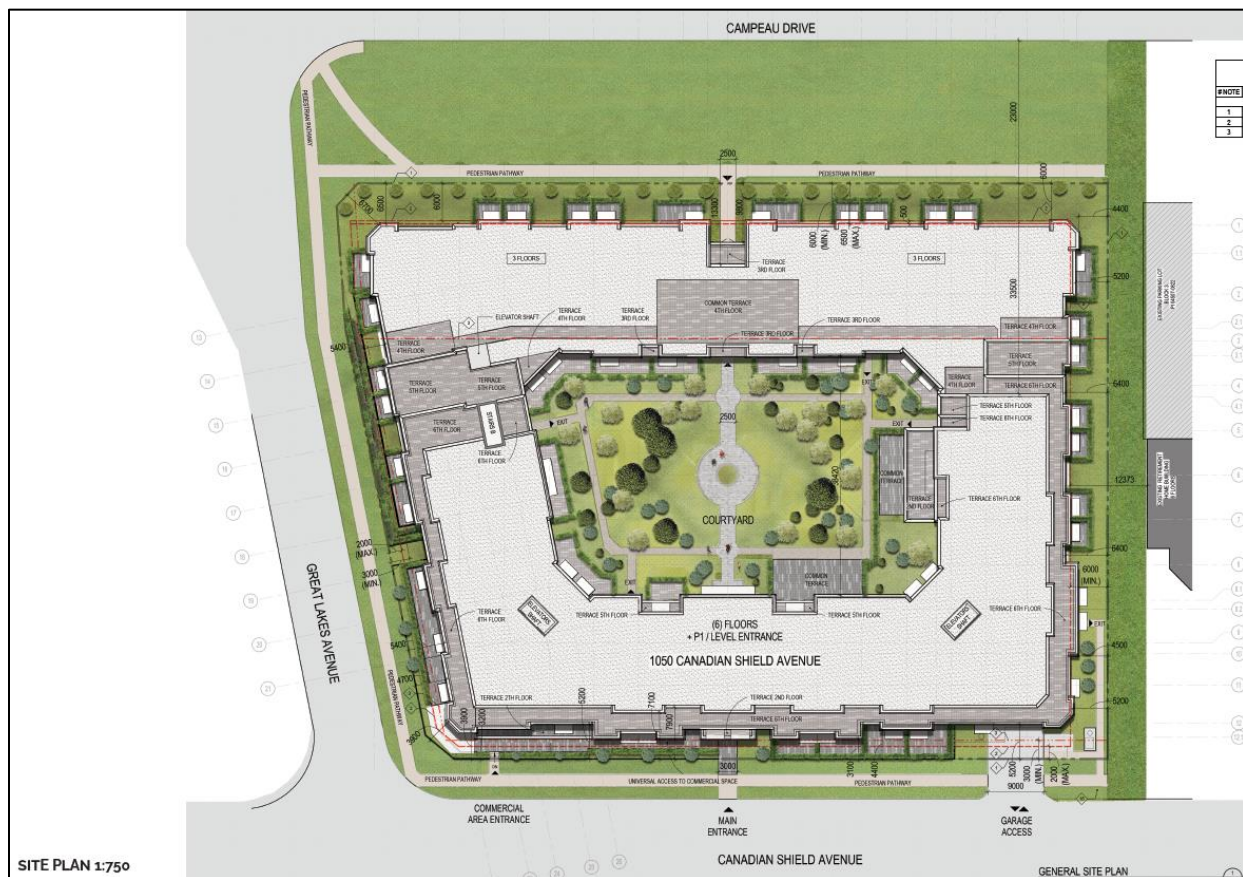


Figure 11: Proposed Courtyard Layout



Figure 12: Visualization of Proposed Courtyard

The proposed amenity areas are largely focused internal to the site in order to minimize impacts on adjacent development. Balconies and patios are present on the building's three street faces which help to animate the street and bring the building closer to the lot line.

Design Priority Areas

The City has identified target areas for intensification and other prominent areas which are significant destinations in the city and recognized them as design priority areas in Section 2.5.1 of this Plan. The site has been designed with consideration for these policies.

Considerable emphasis has been placed on integrating the design of the building into the neighbourhood context, and height-sensitive design which provides a deliberate transition between neighbouring developments. Private and public spaces are well-designed and outlined by buildings with active entrances and glazing, opening onto pedestrian areas and greenspaces to create an attractive and inviting environment.

Based on the foregoing the proposed development conforms to all the policies of the Official Plan as they relate to development within the Town Centre designation and upholds urban design and compatibility principles.

4.3 Kanata Town Centre Special Policy Area

The Kanata Town Centre planning framework was prepared prior to amalgamation and transferred to the new City of Ottawa. The policies carry forward the original Kanata Town Centre planning process that began in the 1970s. The plan's vision statement is that: *"The Kanata Town Centre will be an active, vibrant core area containing a diverse mix of urban uses arranged in a form and a scale, which is both human and appropriate to its function. It will be a uniquely identifiable place that caters to residents and attracts visitors and will be a constant source of pride for future generations"*

Section 5.7.4 speaks to Broad Issues, and describes residential development as follows: *"As the City grows and matures, demographic indicators suggest the need for residential development, which caters to smaller household size, lower incomes, and households which are less dependent on the private automobile."* A range of 2,500 to 3,000 dwellings is anticipated within the CBD.

The proposal addresses this issue by providing 1, 2- and 3-bedroom apartments which are inherently more affordable than larger ground-based dwellings, within 800m of an existing BRT stop that will become an LRT station in the future.

Land Use designations are mapped in Schedule B1 and described at Section 5.7.5. The subject site is designated 'CBD' (Central Business District). It is the intent of this designation that these lands become: *"an urban place that provides for employment uses, complemented by higher density residential uses...The lands designated Central Business District are conveniently located adjacent to the central and west rapid-transit stations providing access to pedestrians and cyclists, as well as motorised vehicles."*

The policies for the 'CBD' contemplate higher density residential development (generally 65 - 100 units per net hectare and higher) if included in a mixed-use building or development. It is expected that residential developments within the Central Business District may occur sooner than office and commercial developments given market conditions. As per the phasing policies in Section 5.7.5.1, this is permitted provided the capacity to achieve the 10,000 job target within the Central Business District and Regional Shopping Centre designations is retained.

The policy further dictates that the area of Central Business District which abuts Campeau Drive will be predominantly residential and be developed in a form which does not overshadow residential development in the Kanata Lakes community.

The subject site is situated at the boundary of the 'CBD' where it abuts the Town Centre Residential designation to the east and established low-rise residential to the north. While a mixed employment and residential character is envisioned for the CBD, the development maintains a residential character along Campeau Drive, with complementary commercial uses at-grade facing Canadian Shield Avenue, recognizing that office or similar employment uses can more appropriately be accommodated on

lands within the “CBD’ in closer proximity to existing non-residential uses and rapid-transit stations, along the future Kanata Avenue Mainstreet, or within underutilized portions of the regional shopping centre which are likely to attract companies that specialize in commercial development. Accordingly, the target of 10,000 jobs is not undermined.

Schedule B2 and Section 5.7.5.8 set out building heights. The subject site is designated for mid-rise buildings up to a maximum of six storeys, subject to providing building height transition. For development facing this stretch of Campeau Drive (between the City lands to the west and Gray Crescent), “the maximum building height is three storeys for any development within 25 metres of the lot line abutting Campeau Drive” and “to maintain a landscaped buffer and parkway character between the edge of the Town Centre and residential neighbourhoods to the north, yard setbacks from a lot line abutting Campeau Drive will generally be 6 metres”. The proposal is a three to six storey building, with two levels of underground parking, and respects both the transition and setback requirements.

The development of the subject site with a mid-rise mixed-use building is appropriate in use, form and scale and will contribute to the evolution of the ‘CBD’ as a vibrant urban community while responding to changing market conditions and community needs. The development conforms to the policies for the Kanata Town Centre.

4.4 Kanata Town Centre Site Design and Architectural Guidelines

The Kanata Town Centre Site Design and Architectural Guidelines were developed in 2000 to serve as a reference for all participants in the development of the Kanata Town Centre. The guidelines build on several previous studies for the area, including the Kanata Town Centre Downtown Master Plan and Implementation Study.

The subject site is identified within Town Centre – Central Business District Area 5 (fronting Campeau and the Esplanade) in the Kanata Town Centre Site Design and Architectural Guidelines. The Guidelines establish a vision for this part of the Kanata Town Centre as an urban core, characterized by higher density mixed-use planning aimed at providing a diverse, pedestrian-oriented form of development typically found in Canada’s older established city centres. Area 5 envisions medium-rise office and/or residential buildings set close to the street with well-treed landscaping and accommodation for retail uses along Canadian Shield Avenue (formerly shown as the cross-town Esplanade) The proposed development has regard for the following site design guidelines, and architectural guidelines:

SITE DESIGN GUIDELINES

- 1.1.1 Landscape at or near property line to ‘finish’ the public realm.*
- 1.1.2 Provide clear identification and emphasis of pedestrian over vehicular.*
- 1.1.5 Enhance street corners.*
- 1.1.7 Enclose the street by building on or close to property line.*
- 1.1.9 Building address should be on major streets.*
- 1.1.10 Allow for phased development.*

ARCHITECTURAL GUIDELINES

- 1.2.1 Materials and architectural features should reflect urban character.
- 1.2.2 Promote a simple, unaffected architectural style.
- 1.2.3 Use lighting to respond to the street environment.
- 1.2.4 Control signage and promotion.
- 1.2.5 Consider long-distance views to buildings.
- 1.2.6 Clearly identify mixed use.
- 1.2.7 Create a safe, attractive pedestrian domain and interest.
- 1.2.8 Design parking structures to the same level of appearance as buildings.

4.5 Draft New City of Ottawa Official Plan

The City of Ottawa's Official Plan provides a vision for the future growth of the city and a policy framework to guide the city's physical development. In 2019, the City of Ottawa began a multi-year process to develop a new Official Plan. While the draft new Official Plan does not currently have any status under the Planning Act, the proposed policies have been reviewed to demonstrate conformity to future policy direction.

The subject site is designated as Town Centre Hub, with a Transforming Neighbourhood Overlay on Schedule B5 of the Draft Official Plan. This Hub designation also corresponds with a Protected Major Transit Station Area shown on Schedule C1. The Site is located within the Suburban (West) Transect.

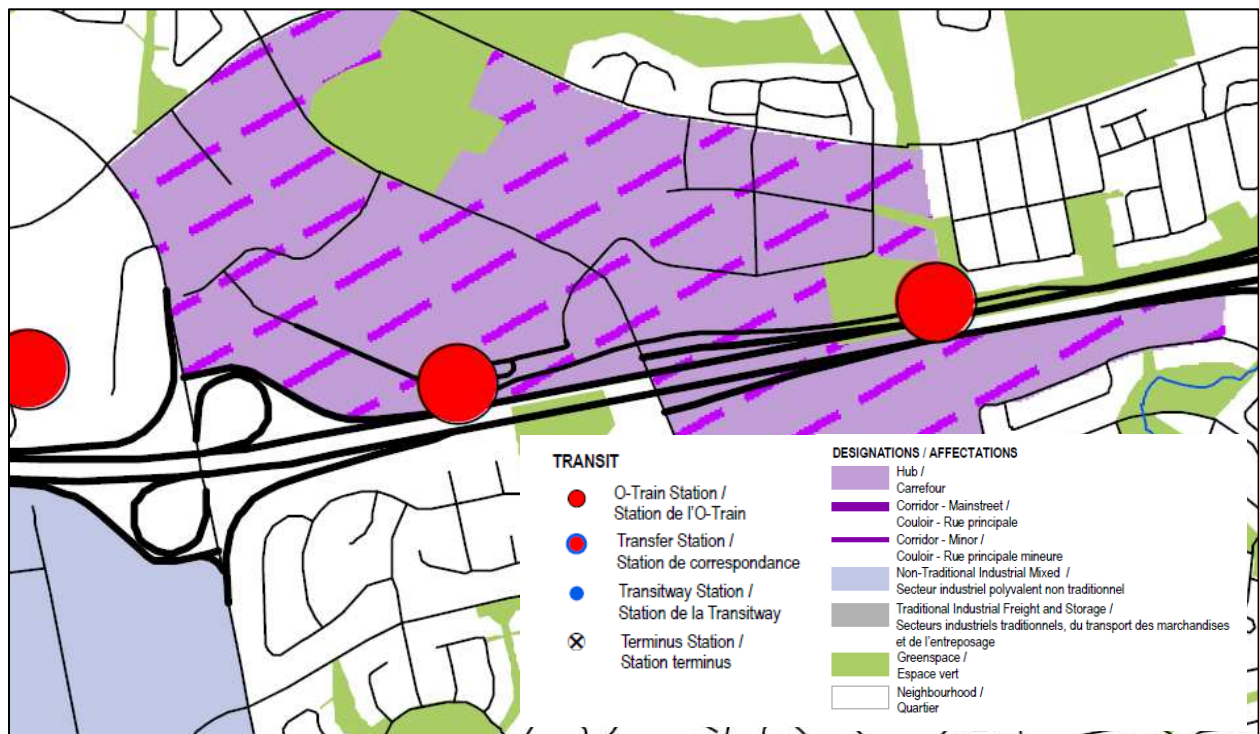


Figure 13: Draft Official Plan Schedule B8 Suburban Transect (West)

Hubs are areas centred on planned or existing rapid transit stations and/or frequent street transit stops. The planned function of Hubs is to concentrate a diversity of functions, a higher density of development, a greater degree of mixed land-use, and a higher level of public transit connectivity than the areas abutting and surrounding the hub. Hubs are also intended as major employment centres. The Draft Official Plan proposes to increase the minimum density of people and jobs per gross hectare for the Kanata Town Centre to 160 over its planning horizon to ensure the critical mass essential to make transit viable.

Section 5.4.3 of the Draft Official Plan provides direction to the Hubs and Corridors located within the Suburban Transect. The policy states that Town Centres are intended to be the most important and largest Hub of their Suburban community and are planned for at least 10,000 jobs each. They are the preferred location for any office-based employers and Major Office Development seeking to locate in Suburban communities. In addition to this, Town Centres also seek uses which support a 15-minute neighbourhood. Development within Hubs shall incorporate residential uses throughout the hub, however, large employment, commercial or institutional uses may be directed to locate close to the transit station. There are no significant changes to the policy context for the subject site in terms of general permitted use and built form.

Development is directed to locate highest density closest to transit stations within Hubs. In keeping with the policies of Section 6.1.1, the subject site is located 800 metres walking distance from the current Terry Fox rapid transit station and 450m walking distance from the future proposed Kanata Town Centre LRT station and, as such, proposes a mid-rise built form. It includes underground parking to meet the transportation needs of residents in the short-terms before the LRT station is constructed, while being transit-supportive long-term. The proposed development will offer commercial and service uses on the ground floor alongside the otherwise residential uses above grade in order to support the 15-minute neighbourhood concept, and a transit-supportive community.

The proposal is in keeping with the Draft Official Plan policies relating to development within the proposed Hub designation.

4.6 Draft Kanata Town Centre Secondary Plan

Along with the new Draft Official Plan, the current Kanata Town Centre Site-Specific Policies are to be repealed and replaced with a Secondary Plan for the Kanata Town Centre that has been updated to be consistent with the new Official Plan.

The Central Business District designation, on Schedule A - Designation Plan, will continue to apply to the subject site, pursuant to the Draft Secondary Plan. This land has the marketing asset of good Queensway exposure and high accessibility. It is the intent of this designation that these lands become an urban place that provides for employment uses, complemented by higher-density residential uses. Higher-density residential development is permitted if included in a mixed-use building or development.

That portion of the Central Business District which abuts Campeau Drive, including the subject site is planned to be residential in nature and to be developed in a form which does not overshadow residential development in the Kanata Lakes community. The subject site is identified as having a maximum permitted height of six storeys in Schedule B – Maximum building heights.

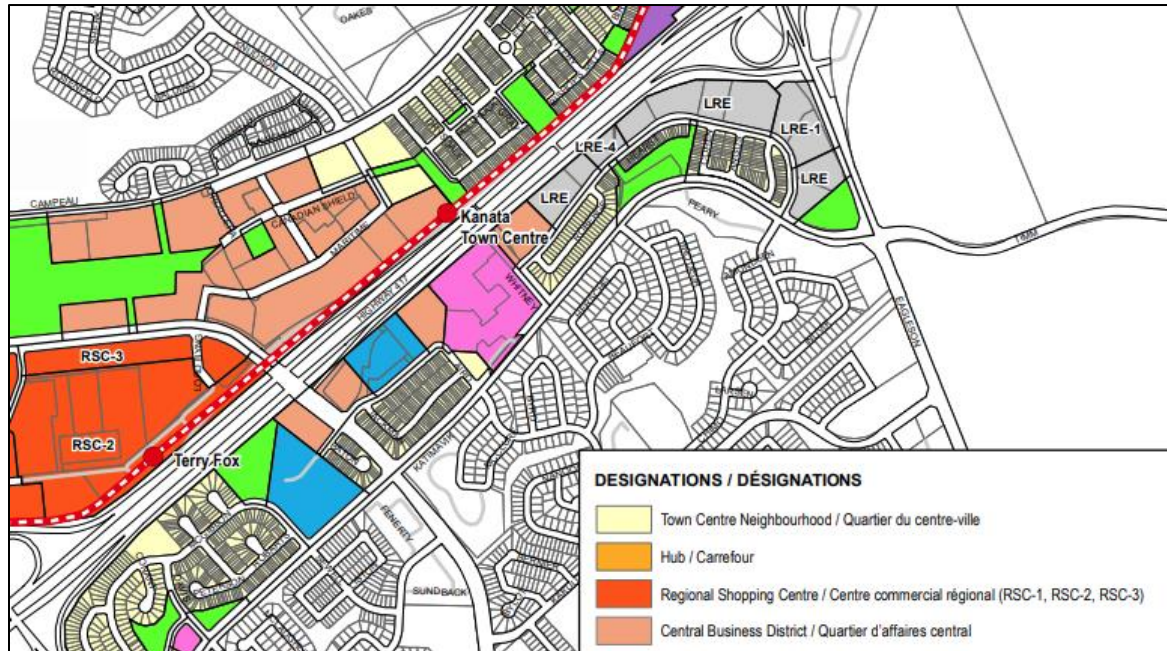


Figure 14: Excerpt of Schedule A - Designation Plan

There are no significant changes to the policy direction set out in the Draft Secondary Plan for the Kanata Town Centre as it relates to the subject site.

5.0 PROPOSED ZONING BY-LAW AMENDMENT

An amendment to Zoning By-law 2008-250 is proposed in order to modify the existing MC15[2027] zone. The purpose of the amendment is to adjust and add site-specific provisions addressing minimum and maximum yard setbacks, minimum floor space index (FSI) for non-residential uses and maximum gross floor area for storeys above 15 metres.

Urban Exception 2027 is proposed to be amended as outlined in **Table 2** below.

Table 2: Proposed amended and additional MC15[2027] Provisions

I Exception Number	II Applicable Zone	Exception Provisions		
		III Additional Land Uses Permitted	IV Land Uses Prohibited	V Provisions
2027 (By-law 2013-86)	MC15[2027]		stacked dwelling	<ul style="list-style-type: none"> • minimum yard setback from a lot line abutting Campeau Drive: 6 m • maximum yard setback for the building wall of a parking garage facing Campeau Drive: no maximum • maximum yard setback for any other building wall facing Campeau Drive: 6.5 m • maximum yard setback for at least 80% of the width of the building wall facing Campeau Drive – 6.5 m • setbacks from a lot line abutting a public street other than Campeau Drive: <ol style="list-style-type: none"> 1. no minimum 2. maximum: <ol style="list-style-type: none"> 1. For the building wall of a parking garage: no maximum 2. For at least 80% of the width of any other building wall facing a public street other than Campeau Drive: <ol style="list-style-type: none"> i. 0.5 m; or ii. 2 m where a patio is located between the building wall and a lot line 2. For at least 80% of the width of any other building wall facing a public street other than Campeau Drive:

				<ul style="list-style-type: none"> i. 6.0 metres on Great Lakes Avenue ii. 5.5 metres on Canadian Shield Avenue <ul style="list-style-type: none"> • minimum building height: 11 m • maximum building height: <ol style="list-style-type: none"> 1. for the first 3 metres back from all lot lines abutting a street other than Campeau Drive: 15m 2. for any part of a building not within the area identified in 1 above: the lesser of 23 m or six-storeys <p>Despite the provisions of Section 192(15), the following provisions apply to a mixed-use building</p> <ol style="list-style-type: none"> 1. A dwelling unit is permitted without the FSI being equal to or greater than 1.5 for non-residential uses listed in clause 192(15)(a) <p>Despite the provisions of Table 192C, the following provisions apply to a mixed-use building:</p> <ol style="list-style-type: none"> 1. The minimum rear yard setback shall be 4.0 metres <p>Despite the provisions of Table 192C, the following provisions apply to a mixed-use building:</p> <ol style="list-style-type: none"> 1. The maximum gross floor area for the fifth storey above 15m is 3,000m²; and 2. The maximum gross floor area for the sixth storey above 15m is 2,250m².
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Yard Setbacks

Rear (East) Yard

The lot is configured in such a way that the technical rear (east) yard functions essentially as an interior yard, while all other yards face a road frontage. The proposed reduced rear yard of no less than 4.0m applies to only certain short lengths of the building wall, particularly at the corners, while other portions are set back at the required 6 metre minimum. The building maintains an adequate setback from the existing adjacent building to the east and achieves the minimum 12-metre building separation required by the Zoning By-law. All of the amenities which would typically be provided in a rear yard are provided internally to the site with the courtyard. As such, the reduced yard setback is appropriate as an interior yard condition and meets the intent of the Zoning by-law.

Abutting Great Lakes and Canadian Shield Avenues

A maximum setback of 0.5 metres, or 2 metres where a patio is located between the building wall and a lot line, is required for at least 80% of the width of any building wall facing Great Lakes Avenue and Canadian Shield Avenue. There is no minimum setback abutting these streets. In the context of the subject site, maximum setbacks varying from 0.5 to 2.0 metre are not feasible given the desire to provide large terraces wrapping the building, while accommodating requirements for retaining walls along inclined elevations. The proposed minimum setbacks of approximately 3.5 metres and 3.0m, and increased maximum setbacks of 6.0 metres and 5.5 metres along Great Lakes Avenue and Canadian Shield Avenue, respectively, align more appropriately with existing adjacent building and will allow for an enhanced streetscape and pedestrian environment. While portions of the façade are further set back allow for building articulation, the facades feature balconies which give the appearance of bringing the building towards the street. The proposed zoning is designed to have 80% of the façade at the proposed maximum setbacks, while the other 20% will be set back further to accommodate terracing. The intent of the By-law is maintained by providing a distinct edge to the street and features which animate the street.

Abutting Campeau Drive

The Zoning By-law requires a minimum 6 metre setback, and maximum 6.5 metre setback to ensure appropriate setback from Campeau Drive while maintaining a relationship with the multi-use pathway. While the design generally respects the required setbacks for the majority of the façade, this requirement provides little opportunity for meaningful building articulation along the façade of larger buildings such as this. The entryway from Campeau to the courtyard is set back significantly to avoid a tunnel like view and to break up the façade of the building in order to capture the rhythm of the neighbouring townhouses across Campeau. The setback deviation occurs this this central break in the façade as well as at the corners where the architect has proposed an angled turret design, as shown

below in Figure 14. Designing the building with a maximum setback of 6.5m for over 80% of the façade maintains the intent of the By-law.



Figure 15: Proposed Building Massing

Minimum Commercial Floor Space Index

Relief is being sought from the provisions of Section 192 of the Zoning by-law to eliminate the requirement for a minimum office or employment uses FSI on the subject site. In the current pandemic and coming post-pandemic environment, many individuals are now working from home and many enterprises are changing their attitudes and direction towards remote working arrangement. As a consequence, there is no demand in the current market for commercial office space. The dwelling units are to be designed with upgrades for working from home and telecommuting opportunities.

Should office space demand recover in the coming years, there are other more appropriate lands in the Town Centre to accommodate large scale employment uses, particularly in closer proximity to transit stations. The proposed predominantly residential with small scale retail uses is more in keeping with the character of this immediate area. The vision for the Town Centre has anticipated significant employment uses, however much of it has evolved with a residential focus, and given market demand it is anticipated that office and employment uses will follow when services, housing and transit are established to support these uses. As such the proposal has been designed to support the intent of the by-law by locating retail opportunities and housing appropriately among similar uses and contributing to a density that will support future employment uses in the Town Centre. The portion of the building along Canadian Shield is designed to accommodate evolving demands for retail or small-scale office uses at-grade, with

residential space that could be readily converted to commercial use if market demand should justify it. This will further support the identity of the Town Centre, while responding to the policies which mandate residential uses abutting Campeau Drive.

Gross Floor Area Above 15 Metres

The proposal includes a gross floor area of 3,000 square metres at the 5th storey which exceeds that which is permitted in the Zoning By-law. Given the size of the property, this GFA limit would result in over-terracing and disproportionate setback which would reduce the ability to bring the building to the street. All efforts have been made to balance this increase by further setting back the 6th storey and reducing its gross floor area to 2,250 square metres which is less than the requirement. This approach achieves gradual building setbacks to define the top of the building and avoid a boxy built form at the top floors. Given the size of the site and the built form, the increase is virtually indistinguishable on the 5th storey. Further, height transition is achieved without the need to limit the gross floor area of upper storeys to a maximum of 2,500 square metres. The impact of this increase at the street level is negligible and matches the height of the existing abutting building to the east.

6.0 SUPPORTING STUDIES

This Planning Rationale should be read in conjunction with the following supporting studies:

Transportation Impact Assessment

A Transportation Impact Assessment (TIA) has been prepared by Novatech to address transportation-related considerations for the proposed development. The residential component of the proposed development is estimated to generate 98 person trips (including 40 vehicle trips) during the AM peak hour and 100 person trips (including 54 vehicle trips) during the PM peak hour. The proposed commercial trip generation is relatively low. The surrounding road and transit networks as well as pedestrian and cycling facilities are adequate to support the proposed development.

Environmental Noise Control Study

Paterson Group conducted an Environmental Noise Control Study in relation to the proposed development to review noise levels associated with arterial and highway road traffic. The study recommends that all units on the exterior elevations be designed with the provision of central air conditioning systems to ensure comfortable indoor living areas. Standard building materials are expected to provide adequate soundproofing, however, warning clauses are also recommended in all sale and lease agreements to advise of potential increased noise levels for certain units and outdoor living areas.

Phase I Environmental Site Assessment

A Phase I Environmental Site Assessment (ESA) has been conducted by Paterson Group to investigate any potential environmental concerns associated with past or current uses on or near the subject site. The surrounding area has historically been used for primarily residential purposes. The Phase I ESA did not identify any potential contaminating activities or other environmental concerns that may have affected the groundwater, soil and/or sediment on the subject site. Accordingly, it was concluded that no further assessment is required.

Geotechnical Investigation

A Geotechnical Investigation has been carried out by Paterson Group to identify the general subsurface soil and groundwater conditions at the subject site and provide geotechnical recommendations for the design and construction of the project. The subject site appears to have undergone topsoil removal in the past and consists of fill overlying brown to grey silty clay deposit underlain by compact glacial till and/or bedrock. It is concluded that the subject site is considered satisfactory for the proposed development from a geotechnical perspective. A foundation support system is required dependent on the design building loading and depth of foundation. It is expected that bedrock removal will be required for portions of the underground parking levels of the proposed structure and foundation waterproofing is recommended.

Servicing and Stormwater Management Report

A Site Servicing and Stormwater Management Report for the proposed development has been prepared by Novatech. The servicing design proposes connections to an existing watermain in Great Lakes Avenue and existing sanitary and storm sewers in Canadian Shield Avenue. The stormwater management system will provide quantity control of stormwater prior to outletting to the City storm sewer system. Quality control of stormwater will be provided by the existing end of pipe stormwater management facility (wet pond) along Maritime Way.

The report references the original Servicing Brief prepared by J.L. Richards & Associates for the Kanata Town Centre - Central Business District (KTC-CBD) Subdivision which provided servicing design information for the business park and assessed the availability of servicing capacities at the time of subdivision of land. It is anticipated that there is adequate capacity within the existing system to service the proposed development.

7.0 CONCLUSION

This Planning Rationale has been prepared in relation to applications for Minor Zoning By-law Amendment and Site Plan Control for the development of a proposed mixed-use community at 1050 Canadian Shield Avenue.

It is our assessment that the proposed development is consistent with the *Provincial Policy Statement*, by promoting residential intensification within an existing built up area where municipal services, facilities and transportation choices are available to support the increased provision of housing in the Kanata Town Centre with no concerns for environmental or public health and safety impacts.

The proposed development conforms to the *City of Ottawa's Official Plan* by achieving transit-supportive residential densities in a form which is compatible and provides transition among adjacent development. The proposal contributes to the diversity of uses to ensure a complete and livable community.

Further, the development conforms to the Kanata Town Centre site-specific policies by contributing to a diversity of uses within the Central Business District in a manner that responds to the envisioned built form for this site, which ranges from three to six-storeys and maintains the existing residential nature along Campeau Drive. The proposal also has regard for the Kanata Town Centre Design and Architectural Guidelines.

While the City's Draft New Official Plan, and Draft Kanata Town Centre Secondary Plan are not yet in effect and have no formal status, the proposal aligns with the future policy direction for the Town Centre which is intended to continue to support medium and high density, multi-unit housing in a mixed-use development within proximity to planned LRT stations.

The proposed amendment to the *Zoning By-Law 2008-250* to accommodate site-specific provisions for yard setbacks, maximum GFA for storeys above 15 metres, and minimum commercial FSI is in keeping with the intent of the by-law by achieving a desirable built form in a compact and pedestrian oriented manner which integrates with surrounding medium and high-density uses yet minimizes the impacts on adjacent low-rise residential areas.

This planning rationale and design brief, along with the associated technical studies, supports the proposed development by illustrating appropriate site development with respect to compatibility with surrounding land use context and fit with the evolving character of the Kanata Town Centre. The commendable architecture provides active uses and framing along all street frontages and incorporate appropriate terracing and stepbacks. The mixed-use community will provide high-quality rental housing options with neighbourhood retail along Canadian Shield Avenue and additional floor area that can readily be converted to commercial uses should there be demand in the future as this area of the Town Centre evolves.

In conclusion, the proposed development is a desirable addition to this community, is in the public interest and represents good land-use planning.

Sincerely,

NOVATECH

Prepared By:



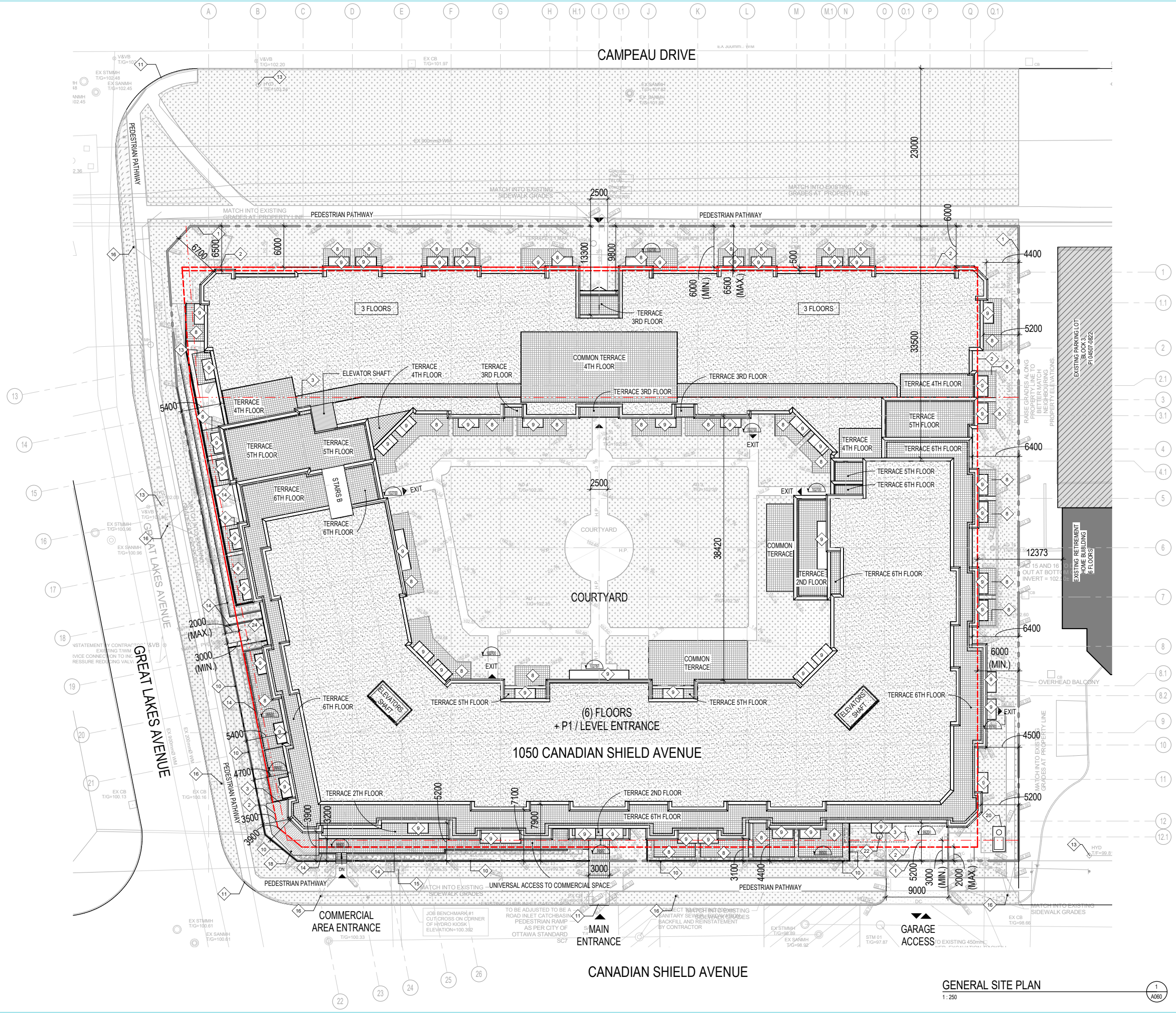
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Planner**

Reviewed By:



**Kayla Blakely, B.E.S (PI)
Planner**

Appendix A



GENERAL NOTES	
#NOTE	DESCRIPTION
1	PROPERTY LINE
2	SETBACKS
3	HEIGHT LIMIT MAX. 23m OR 6 STOREYS
4	GROUND FLOOR - OUTLINE
5	2ND FLOOR - OUTLINE
6	POOL - OUTLINE
7	COMMERCIAL TERRACE
8	PRIVATE TERRACE
9	PRIVATE BALCONY
10	FENCE
11	DEPRESSED CURB
13	EXISTING FIRE HYDRANT
14	RETAINING WALL
15	HYDRO KIOSK
16	LAMP STANDARD
17	ELEVATOR PIT
18	CANOPY
19	CANOPY FOOTPRINT
20	TRANSFORMER
21	WATER TANK ACCESS
22	P2 LEVEL - OUTLINE
23	SIGNAGE
24	FIRE DEPARTMENT CONNECTION

SURFACE LEGEND	
[Pattern]	PAVING (ON-SLAB) LARGE PATIO TILES
[Pattern]	PAVING 2 (ON-GRADE) 300 x 600 FLAME GRANITE, NEWTON BROWN
[Pattern]	ASPHALT
[Pattern]	CONCRETE ON-GRADE
[Pattern]	STONE BALLAST ROOF
[Pattern]	LANDSCAPING
[Pattern]	EXISTING BUILDING

SITE STATISTICS	
LOT AREA	10 917 m ²
FOOTPRINT	6 261 m ²
GROSS BUILDING AREA ABOVE GRADE	30 968 m ²
GROSS BUILDING AREA BELOW GRADE (BUILDING & PARKADE)	15 871 m ²
GROSS FLOOR AREA (ABOVE + BELOW GRADE)	46 839 m ²

NUMBER OF UNITS	
P1 FLOOR UNITS AT STREET LEVEL	6 UNITS
FIRST FLOOR UNITS AT STREET LEVEL	49 UNITS
2ND FLOOR UNITS ON A TYPICAL LEVEL	55 UNITS
3RD FLOOR UNITS ON A TYPICAL LEVEL	54 UNITS
4TH FLOOR UNITS ON A TYPICAL LEVEL	31 UNITS
5TH FLOOR UNITS ON A TYPICAL LEVEL	30 UNITS
6TH FLOOR UNITS ON A TYPICAL LEVEL	19 UNITS
TOTAL	244 UNITS

NUMBER OF PARKING SPACES	
1.0 Residents (parking space / dwelling unit)	244
0.2 Visitors (parking space / dwelling unit)	49
Commercial parking	10
TOTAL PARKING REQUIRED	303
TOTAL PARKING PROVIDED (INCLUDED BARRIER FREE PARKING)	348
MINIMUM BARRIER FREE PARKING REQUIRED	PROVIDED 10

ZONE PROVISIONS BLOCK 2 - 1050 CANADIAN SHIELD		
ZONING BY-LAW 2008 250		
CURRENT ZONING: MC15 (2027)		
	REQUIRED	PROPOSED
PERMITTED USES	RESIDENTIAL	RESIDENTIAL
FRONT YARD SETBACK (GREAT LAKES AVENUE (WEST LOT LINE))	MAX. 0.5m OR 2m WHERE A PATIO IS PROVIDED	3.5m MIN / 5.4m MAX
SIDE YARD SETBACK (CAMPEAU DRIVE (WEST LOT LINE))	6.0 m MIN. 6.5 m MAX.	6.0 m MIN. 13.3 m MAX.
SIDE YARD SETBACK (CANADIAN SHIELD AVENUE (SOUTH LOT LINE))	MAX. 0.5m OR 2m WHERE A PATIO IS PROVIDED	3.1m MIN / 5.2m MAX
INTERIOR REAR YARD SETBACK (EAST LOT LINE)	6.0 m	4.4 m
BUILDING HEIGHT	23.0 m MAX.	6 STOREYS + P1 / LEVEL ENTRANCE
WINDOWS ON WALL FACING STREET	MIN 50% OF LENGTH	CAMPEAU DRIVE: 52% CANADIAN SHIELD AVE: 30% GREAT LAKES AVE.: 52%
LANDSCAPE AREA	30% MIN. (3 275 m ²)	32.4 % (3 541 m ²)
MINIMUM BICYCLE PARKING	0.5 PER DWELLING UNIT 122 SPACES	124
TOTAL PRIVATE AMENITY SPACE	6 m ² PER DWELLING UNIT 1 464 m ²	2 622 m ²
TOTAL COMMUNAL AMENITY AREA	55% OF PRIVATE AMENITY SPACES 735 m ²	686 m ² (INDOOR) 373 m ² (OUTDOOR) 1059 m ² (TOTAL)

NOTES GÉNÉRALES / General Notes

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CARRÉ SAINT-LOUIS

EMPLACEMENT Location NO PROJET No. 12461
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NO RELEASE / VERSION DATE (aa-mm-jj)
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 DATE (aa-mm-jj) **06/04/20** ÉCHELLE Scale **Comme indiqué**

SITE PLAN

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