



2571 & 2595 Lancaster Road

Planning Rationale
Zoning By-law Amendment and Site Plan Control Applications
September 23, 2021



Prepared for Enbridge

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1.0 Introduction

Fotenn Planning + Design has been retained by Enbridge to prepare this Planning Rationale in support of Minor Zoning By-law Amendment and Site Plan Control Applications to facilitate the proposed development on the lands municipally known as 2571 and 2592 Lancaster Road in the City of Ottawa.

1.1 Application Overview

The proposed development consists of a two-storey, multi-purpose operations centre including space for warehouse, office, outdoor storage, and maintenance shop uses. A total of gross floor area of 4,815 square metres is provided with a total of 108 parking spaces, 16 bicycle spaces, and five (5) loading bays accessed from Lancaster Road. The site design will also include a robust landscaping program, LNG fueling station for on-site fleet vehicles, and geothermal heating area.

A minor Zoning By-law Amendment has also been submitted as part of the applications package. The Subject Lands are currently split zoned with the majority of the lands including the entirety of 2571 Lancaster Road zoned Light Industrial (IL) and a portion of the rear of the lands on 2595 Lancaster Road zoned Heavy Industrial. As the Light Industrial zone does not permit Outdoor Storage uses, the Zoning By-law Amendment is requested to extend the Heavy Industrial Zone limits to the full extent of the lands to provide more flexibility in the application of that Outdoor Storage. The Zoning By-law Amendment (ZBLA) would restrict the front portion of the lot to allowing only those uses permitted in the Light Industrial Zone.

Rather than retaining the Light Industrial and Heavy Industrial zones on the merged property, the intent of the ZBLA is to harmonize the zoning requirements to recognize the property as a single development, functioning as one (1) single lot for zoning purposes in the Industrial Zone category.

The consolidated IH zoning would eliminate the need for additional changes to the By-law, and facilitate the location of parking, common access and drive-ways and drive aisles, and other provisions such as setbacks and use distribution on the lot. A site-specific zoning schedule and exceptions would delineate permitted uses and performance provisions. For instance, although the IH zone permits outdoor storage uses, the schedule and exceptions would limit that use to rear (eastern) portion of the site with the front (western) portion of the site along Lancaster Road retaining the land use permissions of the IL zone. The approach would be generally consistent with the existing zoning framework.

The schedule will be a tool to ensure that site-specific conditions and existing context are considered and respected in the Zoning By-law Amendment process, specifically regarding the treatment of the new development along Lancaster Road. In essence, the rezoning of the western portion of the lands is intended to create clarity in the process without dramatically reconfiguring the existing permitted uses on the lands.

The Minor Zoning By-law Amendment will also address non-conformance in the following areas:

- / **Permit increased parking space width to accommodate fleet vehicles in the rear parking area of 4.6 metres whereas the Ottawa Comprehensive Zoning By-law (2008-250) permits a maximum parking space dimension of 3.1 metres; and**
- / **Permit a reduced parking lot aisle width of 6.0 metre in the front vehicle parking area whereas the Ottawa Comprehensive Zoning By-law (2008-250) requires a minimum 6.7 metre drive aisle width for parking spaces angled 90 degrees.**

In addition to the ZBLA, a Site Plan Control application is also submitted to construct the building, surface parking, and landscape areas. The Site Plan Control application, supported by a range of technical studies (transportation, servicing, geotechnical, environmental) will conform with the amended zoning for the site.

2.0 Site and Surrounding Context

2.1 Site Description

The subject lands are known municipally as 2571 and 2595 Lancaster Road in the City of Ottawa. The subject lands provide 137 metres of frontage on Lancaster Road and are located within the Innes Business Park in the City of Ottawa (Ward 18 – Alta Vista). The portion of the subject lands known as 2571 Lancaster Road are currently improved with a privately run arena with associated surface parking. To the rear, the portion of the site known municipally as 2595 Lancaster Road is currently vacant and was previously utilized as a rail corridor servicing the abutting industrial properties. In all, the property comprises a lot size total of 32, 892m² with 137 metres frontage on Lancaster Road and a lot depth of 188 metres. The existing building at 2571 Lancaster Road will be demolished to accommodate the proposed development.

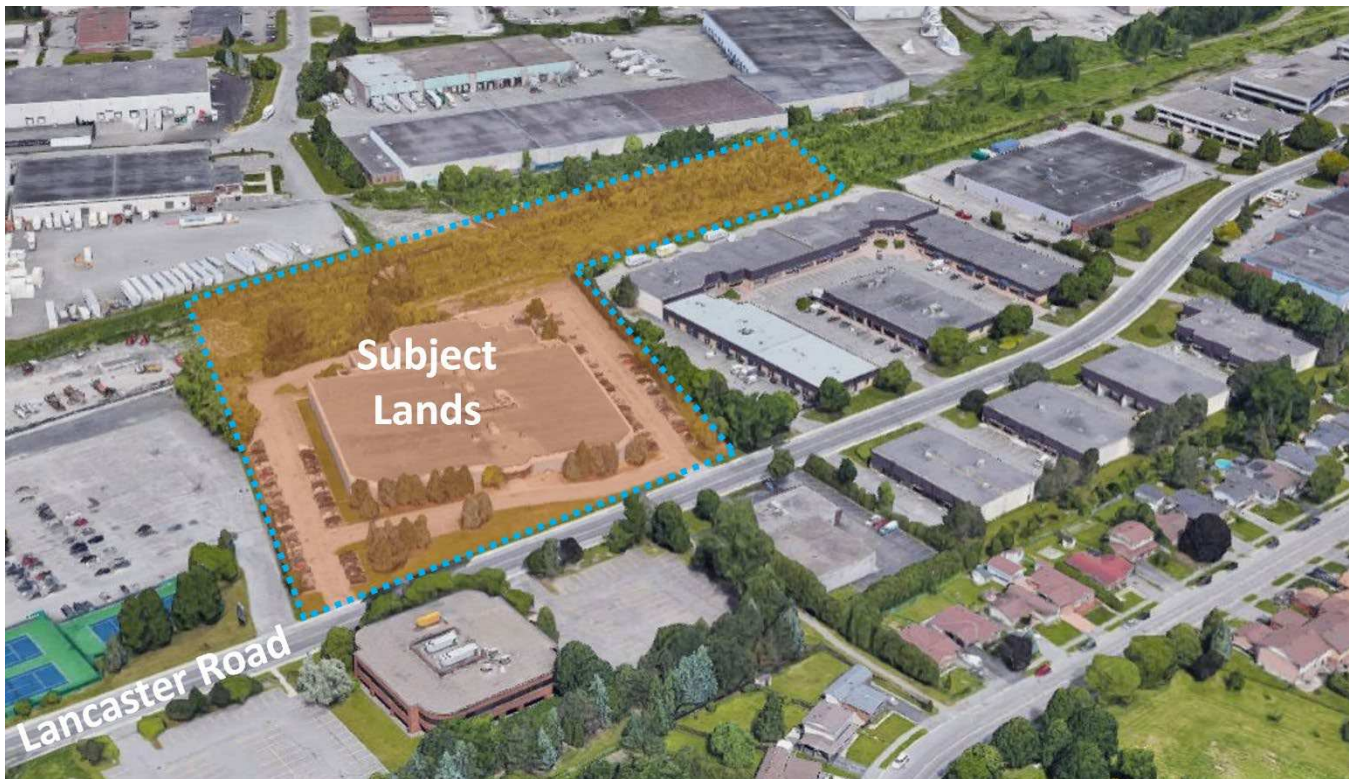


Figure 1 Study Area as part of full parcel.

The subject lands are legally known as Part of Lot 25 Concession 3 (Ottawa Front) and Part of Block B, Registered Plan 4M-121, Geographic Township of Gloucester now in the City of Ottawa.

2.2 Community Context

The subject property is closely abutted by various complementary land-uses including office, wholesale, light industrial, recreational, and warehousing/storage along Lancaster Road, and within the Innes Business Park. Providing increased employment opportunities in a central location with easy access for employees from nearby residential areas is a key objective of the Official Plan. In line with the City of Ottawa's objective to promote prosperous and complete communities,

the proposed development is also in proximity to numerous established residential neighbourhoods which are located within an eight (8) kilometre radius of the subject property.



Figure 2 Community Context Aerial.



Figure 3 Subject Property aerial.

2.3 Mobility and Transportation

The nearby road and sidewalk network provides convenient and logical mobility options for employees and business activities alike. The subject lands are within 1.8 kilometres (approx. 3-minute drive) to the nearest Highway 417 interchange via Walkley Road ensuring convenient access to the greater transportation and shipping network.



Figure 4 Schedule E – Urban Road Network.

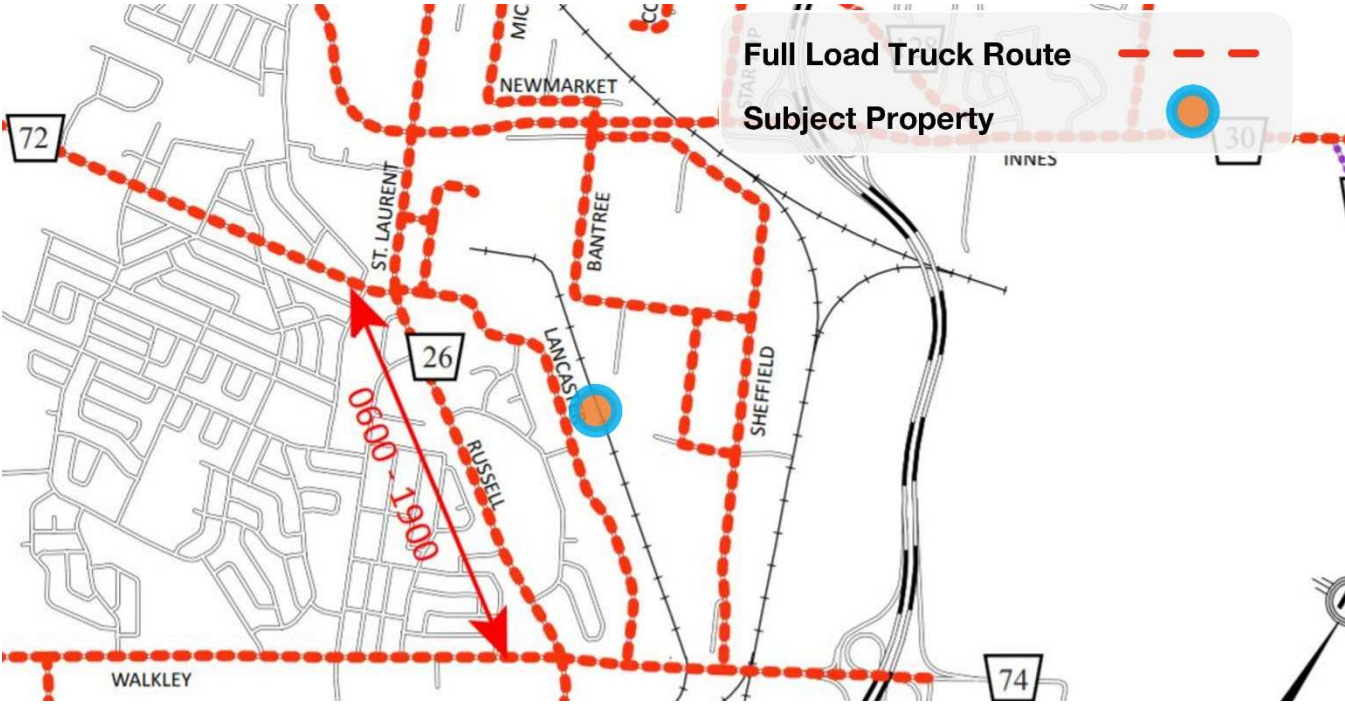


Figure 5 City of Ottawa - Urban Truck Routes Map.

Lancaster Road and Walkley Road are both designated Full Truck Load Routes and the subject lands are well situated in proximity to numerous truck routes as indicated by materials provided by the City of Ottawa (Figure 5). The existing truck route network provides access from the subject lands to important destinations within Ottawa as well as a direct route to access/egress the Highway 417 and the greater transportation network.

2.4 Transit Network

The subject lands are within 1.5 kilometres (approx.. 15 minute walk) of the future Elmvale Acres/St. Laurent bus rapid transit corridor and transit station with connections to the future Baseline BRT Corridor as indicated on Schedule D of the Official Plan. The Bus Rapid Transit service is not planned to be constructed until some time after 2031, however the policies of the Official Plan support employment intensification presently around the future station locations. Currently operating local transit routes providing connectivity to the broader network are located in close proximity of the subject lands along St. Laurent Boulevard, Walkley Road, and Southvale Crescent.

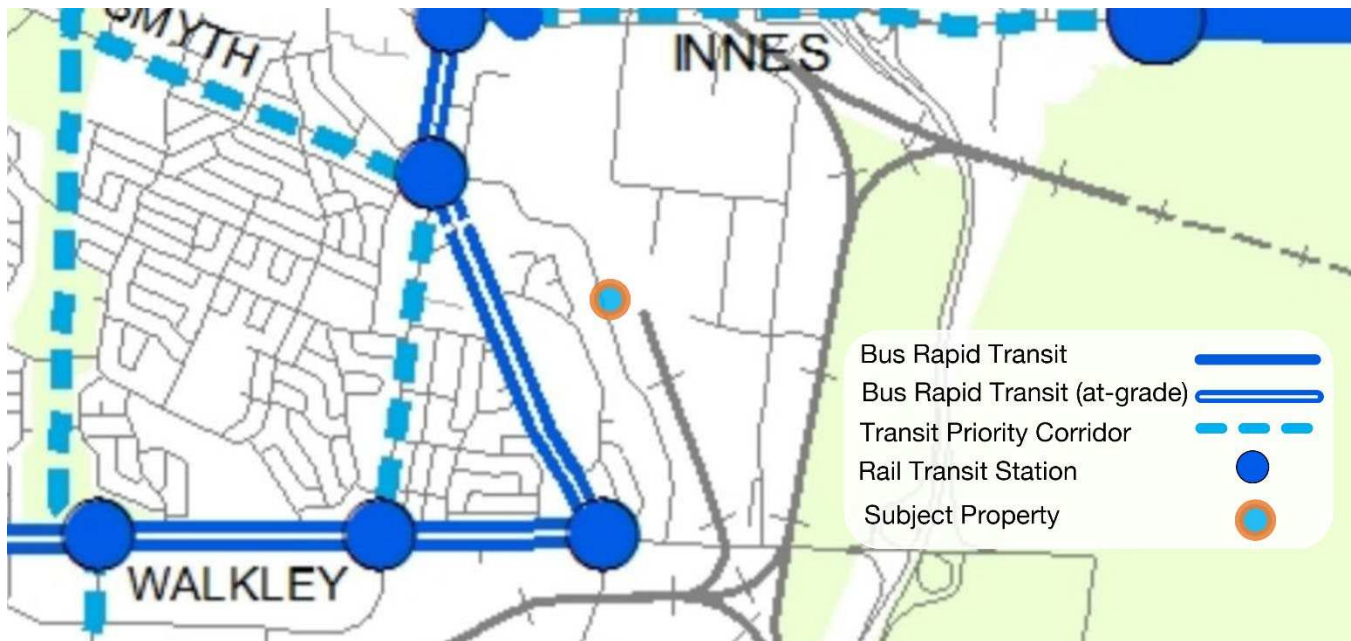


Figure 6 Schedule D - Rapid Transit Network from the Official Plan.

2.5 Active Transportation Network

The subject property is within close proximity to the St. Laurent/Russel, Smyth, and the Walkley/Heron Cycling Spine Routes as indicated on Schedule C of the Official Plan. Spine Routes are planned to provide dedicated and potentially separated bicycling infrastructure to nearby amenities as well as connections to the greater Ottawa cycling network and key destinations. Although not indicated on Schedule C, a local bicycle route with painted cycle lanes is located on Lancaster Road. As such, future employees will have access to a full spectrum of multi-modal transportation options.

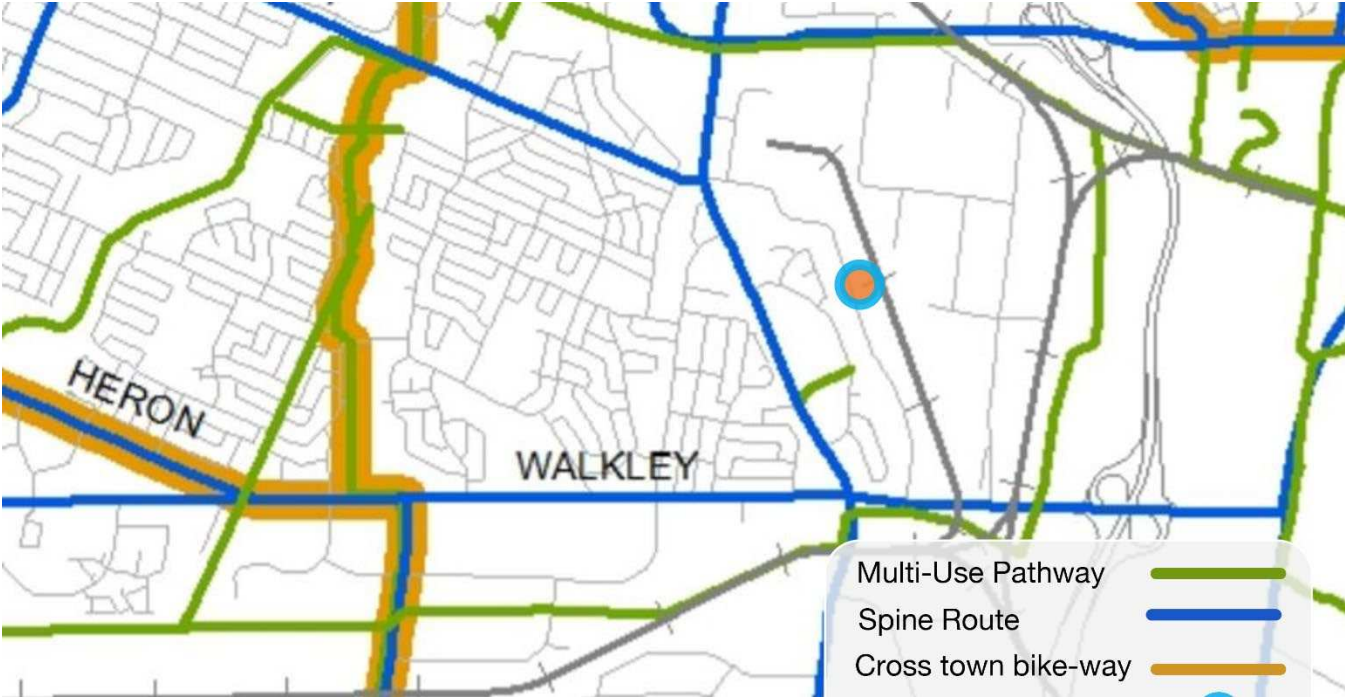


Figure 7 Schedule C – Primary Urban Cycling Network.

Proposed Development

The proposed development consists of a two-storey, multi-purpose operations centre including space for warehouse, office, outdoor storage, and maintenance and service shop uses. A total gross floor area of 4,815 square metres is provided with a total of 108 parking spaces, 16 bicycle spaces, and five loading bays accessed from Lancaster Road. The site design will also include a robust landscaping program, LNG fueling station for on-site fleet vehicles, and geothermal heating area.

The purpose of the proposed project is to develop a new operations site located in close proximity to Highway 417 via the on-ramp at Walkley Road in an established business park which is supportive of various light industrial and service uses. The proposal will provide Enbridge with convenient and required access to the region which this facility will support, create a positive and attractive working environment for current and prospective employees, embrace current sustainability practices, and provide a service yard that is suitable to meet the growth demands of the business. Through the application of the LNG fueling station, geothermal heating area, and high-quality building design, the new facility has the opportunity to support Enbridge's corporate goals to be Net-Zero Carbon by 2050.

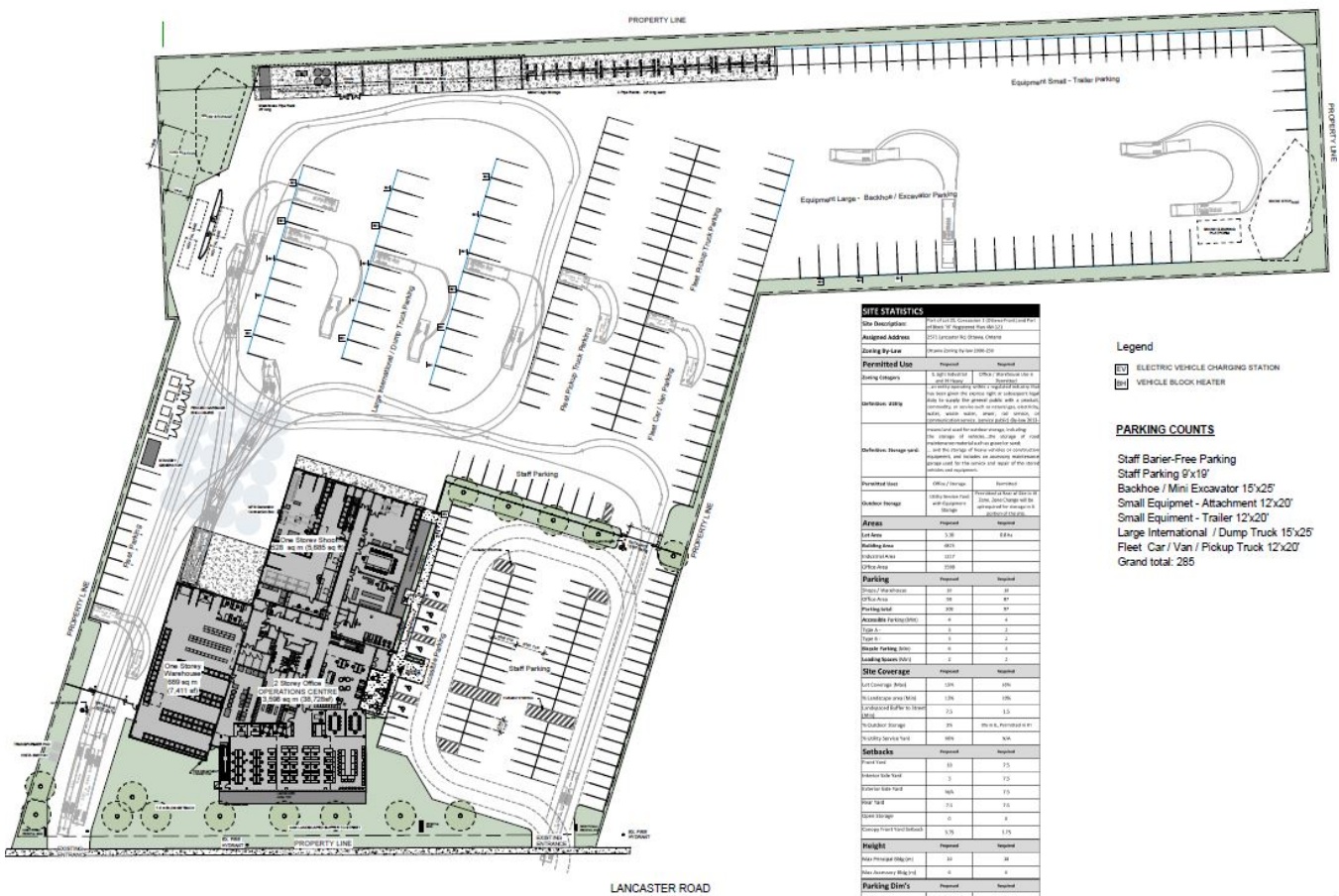


Figure 8 Proposed Site Plan.

3.1.1 Site Layout and Access

The site layout is formatted with the building addressing the street by placing it forward towards the front yard setback. The building is placed towards the northern side of the property to keep a southern exposure for the building where it faces the staff parking lot. The space between the street and building will be landscaped to soften the presence of

the facility on the public right-of-way with the fenced service yard located at the rear of the site abutting the properties zoned Heavy Industrial to the north-west.



Figure 9 Proposed North (bottom) and South (top) Elevations.

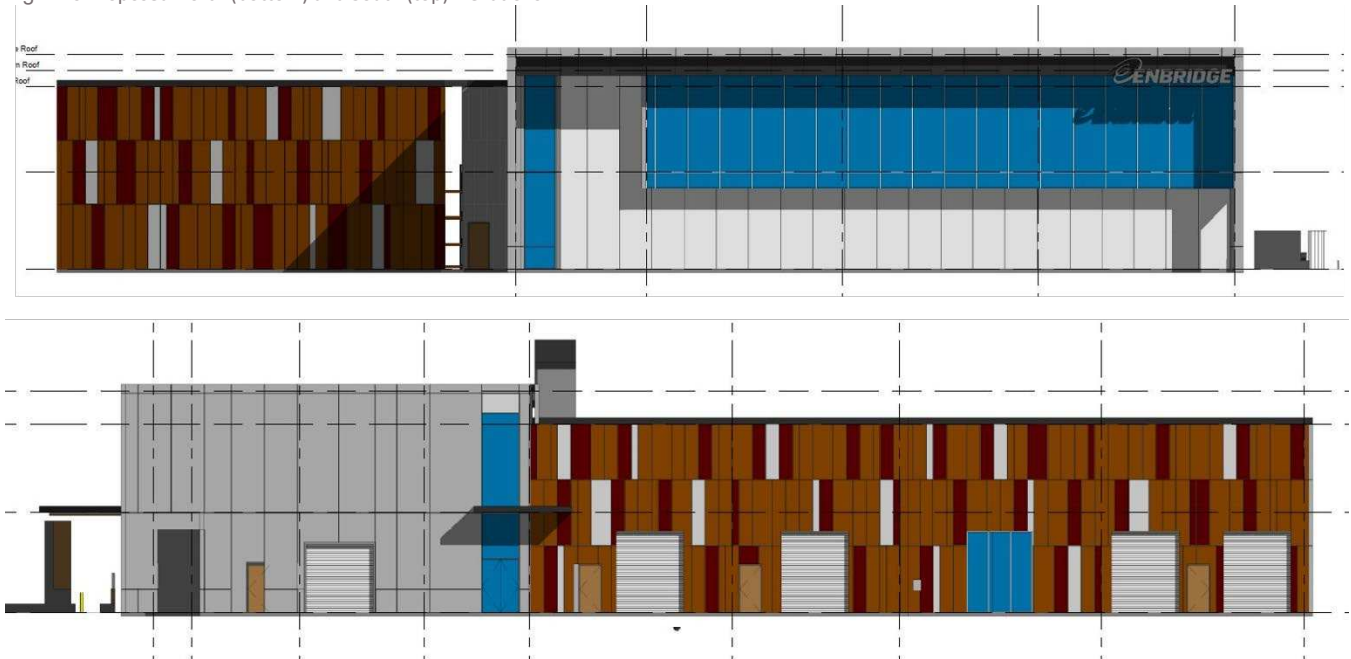


Figure 10 Proposed east (top) and west (bottom) elevations.

Site access is proposed via two existing entrances from Lancaster Road and will be maintained and enlarged in width to suit commercial fleet vehicles and employee/visitor vehicles. These two access points permit the separation of vehicular traffic entering and exiting the site. Large industrial vehicles are able to access the northern service yard via the northerly entrance, while staff and visitor vehicles access the southerly entrance. Larger industrial service vehicles can also access the yard via the southern employee entrance should the northern entrance be in-use.

The building floor plate has been arranged so that employee and amenity spaces are located on the southerly portion of the building which face the street and the parking lot. The office portion of the building is largely faced with curtainwall design to maximize views, natural daylighting for the employees and to utilize passive microclimate solar resources contributing to sustainable initiatives.

The warehouse and service shops have been located along the northerly portion of the site as they do not have the same requirements for views, and glazing and to best screen those uses from the public realm. As a portion of the warehouse faces the street, architectural interest has been added by installing metal panels in a pattern and with high-quality finishing options.

The rear portion of the subject lands will be utilized for fleet vehicle parking, the LNG fueling area, and outdoor storage uses in-line with the Heavy Industrial zoning provisions applied to that portion of the lands. The Outdoor Storage component of the development will be screened by the proposed building and setback a significant distance from the public right-of-way.



Figure 11 Render of proposed entrance.

4.0

Policy and Regulatory Framework

4.1 Provincial Policy Statement

The 2020 Provincial Policy Statement (PPS), issued under the authority of the Planning Act, provides direction on matters of provincial interest related to land use planning and development. The Planning Act requires that decisions affecting planning matters be consistent with policy statements issued under the Act.

The PPS promotes the development of strong communities, which rely on the establishment of efficient land use and development patterns and the accommodation of an appropriate range and mix of uses.

The relevant sections of the PPS are as follows:

- 1.1.1 a)** promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;
- 1.1.3.2 a)** Land use patterns within settlement areas shall be based on densities and a mix of land uses which:
 - b)**
 - a) efficiently use land and resources;
 - b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;
- 1.3.1 a) b)** Planning authorities shall promote economic development and competitiveness by:
 - d) e)**
 - a) providing for an appropriate mix and range of employment, institutional, and broader mixed uses to meet long-term needs;
 - b) providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses;
 - c) facilitating the conditions for economic investment by identifying strategic sites for investment, monitoring the availability and suitability of employment sites, including market-ready sites, and seeking to address potential barriers to investment;
 - d) encouraging compact, mixed-use development that incorporates compatible employment uses to support liveable and resilient communities, with consideration of housing policy 1.4; and
 - e) ensuring the necessary infrastructure is provided to support current and projected needs.
- 1.3.2.1** Planning authorities shall plan for, protect and preserve employment areas for current and future uses and ensure that the necessary infrastructure is provided to support current and projected needs.
- 1.3.2.3** Within employment areas planned for industrial or manufacturing uses, planning authorities shall prohibit residential uses and prohibit or limit other sensitive land uses that are not ancillary to the primary employment uses in order to maintain land use compatibility. Employment areas planned for industrial or manufacturing uses should include an appropriate transition to adjacent non-employment areas.

The proposed development meets the applicable policies of the PPS, including:

- / Promotes the efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term.
- / Promotes cost-effective development patterns and standards to minimize land consumption and servicing costs;
- / Proposes development in a settlement area, which is the focus of growth and development;
- / Provides for an employment uses to meet long-term needs;
- / Provides opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take in account the needs of existing and future businesses; and

- / Encourages compact, development that incorporates compatible employment uses to support liveable and resilient communities.

The proposed development seeks to build new, light industrial / office uses on an existing underutilized and partially vacant portion of land. This proposal promotes a more efficient land use pattern within an existing employment area on existing municipal services. The proposed development is consistent with the policy direction of the new Provincial Policy Statement (2020).

4.2 Ottawa Official Plan

The City of Ottawa's Official Plan (OP) provides a vision and a policy framework to guide the future growth of the City of Ottawa.

4.2.1 Managing Growth

Section 2.2.2. of the Official Plan outlines policies for intensification in the urban area where there are opportunities to accommodate more jobs and housing. Policy 2 of this section defines employment intensification as the development of a property or area that results in a net increase in jobs and/or gross floor area and may occur through:

- / Redevelopment of existing employment uses at a higher density (e.g. the creation of an office building that complements previously developed land), including the redevelopment of Brownfield sites;
- / Expansion of existing employment uses;
- / Infilling of vacant or underutilized land within urban employment lands as identified in Policy 1 Section 2.2.3;
- / Replacing uses with a low number of employees with uses having a higher number of employees.

The subject lands are currently developed with a recreational facility abutting Lancaster Road and vacant land to the rear. Redevelopment of the lands with the multi-purpose light industrial facility will contribute to the expansion of commercial/employment infill on an underutilized parcel of land within the City's Urban employment lands.

4.2.2 Land Use Designation

The subject property is designated Urban Employment Area on Schedule B: Urban Policy Plan of the Ottawa Official Plan (OP). The Urban Employment Area designation is meant to ensure that, over the long term, sufficient areas of land are utilized as places of business and economic activity. The Official Plan further states that to ensure easy access to jobs, Urban Employment Areas should be centrally located or close to highways and/or transit.

As such the Official Plan states that the Urban Employment Area should permit uses such as office, manufacturing, warehousing, distribution, research, light industrial, and development facilities and utilities. As per the Official Plan, Urban Employment Areas will also permit uses that store most products outdoors and require large land areas devoted to external storage, sale or service of goods or for vehicle sales and service.

Urban Employment Areas are designated to enable a variety of functions:

- / Noxious industrial uses that impose constraints on other uses locating nearby and require a buffer between these and other uses;
- / Uses that, while not noxious, are incompatible with other uses because of noise, lights and around the clock operation, etc.;
- / Prestigious uses (usually office or combinations of office/clean industrial) with a signature address and a desire to locate among other similar uses.

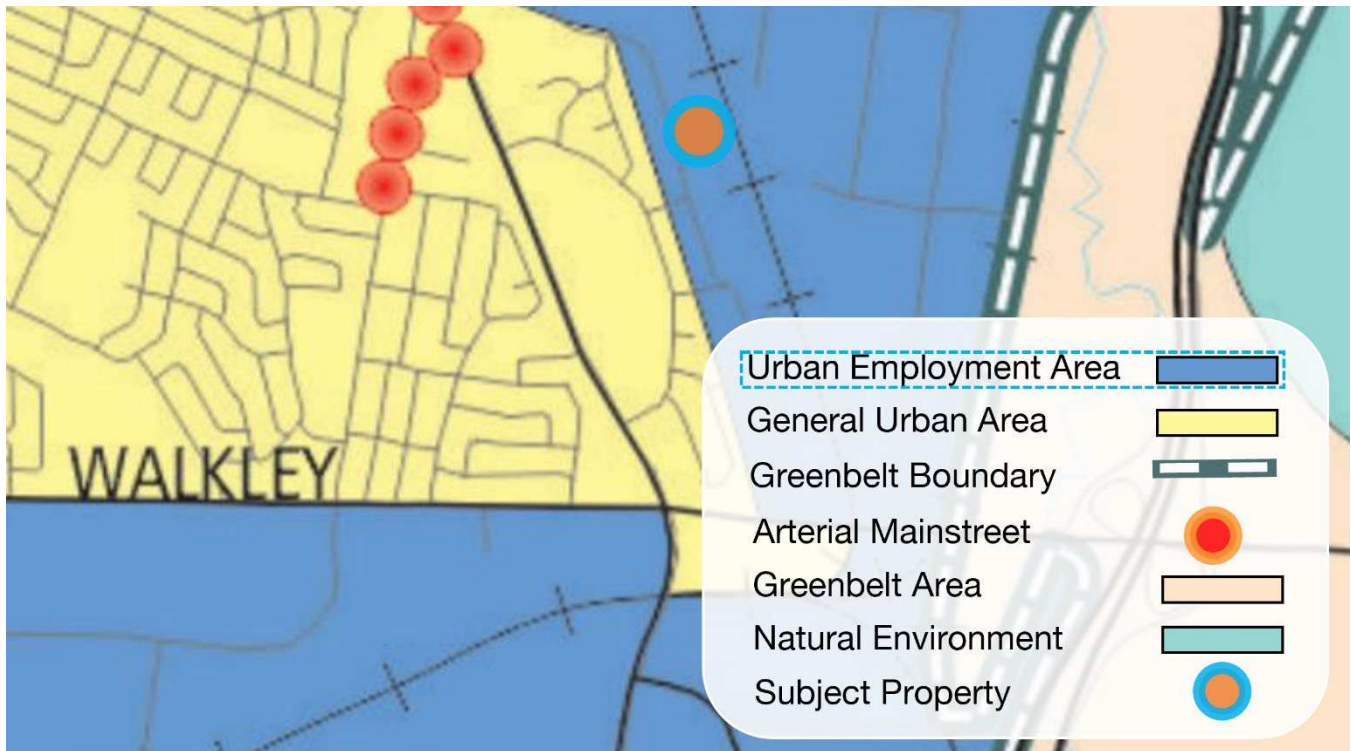


Figure 12 Schedule B: Urban Policy Plan.

Specifically, relevant policies under the Urban Employment Area Designation include:

Policy 2.2.3.5:

The City encourages intensification and renewal of employment uses within the Urban Employment Areas located inside the Greenbelt. Due to their central location these Employment Areas provide access to goods movement corridors and major transportation routes for workforce access. These locations also create stable operating environments for a wide variety of traditional industrial uses, which helps to diversify the local economy.

Policy 3.6.5.1:

- / Urban Employment Areas are designated on Schedule B and are intended to be established clusters of business and economic activity at a board spectrum of job densities;
- / Urban Employment Areas shall be distributed throughout the urban area to help provide access to jobs throughout the city; and
- / Employment areas must also be located so that they have designated truck route access.

Policy 3.6.5.2:

In Urban Employment Areas, the Zoning By-law will:

- / Permit traditional industrial uses;
- / Permit uses that store most products outdoors and require large land areas of land;
- / Permit office uses, including, research & development and emergency services;
- / Permit sample and showroom uses,;
- / Permit a variety of ancillary uses, such as recreational, health and fitness uses, child care, and service commercial uses;
- / Distinguish uses with characteristics that are likely to impact negatively on surrounding areas from those uses that are likely to have negligible such impacts;

The proposed site redevelopment will provide additional employment opportunities in the community. As Ottawa continues to grow, this form of employment infill development will address the Official Plan's objectives of compatible growth while ensuring employment opportunities in close proximity to existing residential areas and in areas already characterized by existing light industrial, manufacturing, production, and employment built form while benefiting from the pre-established and critical infrastructure including municipal services and convenient access to the broader transportation network.

The subject lands are well suited to accommodate additional employment density for a number of reasons. As mentioned previously, the subject property is within close proximity to access/egress to Highway 417, existing residential areas for future employees, future Bus Rapid Transit Infrastructure, and designated trucking routes. Importantly, the subject lands are located within the Innes Business Park which is a well-established employment area purposefully designed and situated to accommodate industrial employment uses and associated vehicle traffic.

4.2.3 Urban Design and Compatibility

Section 2.5.1 provides guidance on how to appropriately incorporate compatible development into new building projects. Compatible development is defined in the Official Plan as development that enhances and coexists with existing development without undue adverse impacts. It 'fits well' within its physical context and 'works well' among those functions that surround it.

Section 2.5.1 contains design objectives intended to guide development. The applicable guidelines are discussed below:

To define quality public and private spaces through development	The proposed development will enhance the public realm by providing additional landscape treatment on the street edge while proposing a well-designed building façade with well articulated and high-quality materiality in close proximity to the public realm.
To create places that are safe, accessible and are easy to get to, and move through	The proposed development has been arranged to allow for safe and convenient vehicle movements that will not impact on the adjacent streets. Pedestrian, cyclist, and vehicular movements have been defined and separated appropriately, with additional consideration for fire routes as well as internal circulation of buildings. Importantly, no new vehicle access/egress points are proposed.
To ensure that new development respects the character of existing areas	The proposed building respects the character of the Lancaster Road and the Innes Business Park and will make a positive contribution to the employment area.

The Official Plan states that compatibility is achieved through a design that appropriately addresses impacts generated by infill or intensification. Section 4.11 of the Official Plan provides criteria that can be used to objectively evaluate the compatibility of a proposed development. The policies are grouped into topics and the relevant policies are discussed below.

Policies 5 through 9 speak to building design. Specifically, policy 5 states that the compatibility of new buildings with their surroundings is achieved in part through the design of the portions of the structure facing other buildings or the public realm. Specifically, new development should consider setbacks, heights and transition, facade and roofline articulation, colours and materials, architectural elements, etc.

Policy 6 states that the principal facade and main entrance should be oriented towards the street, and that windows should be incorporated into the facades facing the street. Building entrances should be accentuated through architecture.

Finally, policy 8 states that, to maintain a high quality, obstacle free pedestrian environment, all loading and service areas should be internalized or integrated into the building wherever possible.

The immediate area is defined by existing employment, office, light industrial, production, and service uses. The proposed development reflects and promotes the character of the surrounding area by proposing a modern and well-designed building program along the street frontage while proposing an additional warehouse, outdoor storage, and heavy-vehicle parking to the rear of the site and screened from the public realm. The proposed landscape plan will positively frame the public right-of-way and contribute to maintaining the existing character of the area.

The loading and parking will be internal to the site to be partially screened from the view of the street. The proposed use of the subject lands will not generate noise, fumes, odours, or other hazardous or obnoxious impacts and will satisfy development standards to ensure compatibility between uses and minimize the negative impact of the uses on adjacent lands.

The proposed development will not generate undue adverse impact on the neighbouring properties and fulfills the compatibility objectives and principles outlined in section 2.5.1 and the compatibility criteria of Section 4.11. The proposed development conforms to the Official Plan.

4.3 Official Plan Update

The City of Ottawa has released the draft version of the New Official Plan, set to be heard by the Joint Committee (PC/ARAC) on October 14th, 2021. The below provides a brief summary of the proposed policy changes applicable to the site, though it is noted that none of the changes have any effect on the proposed use as the site is and continues to be located within an Employment Area.

- / The subject property is located within the “Mixed Industrial” land use designation on Schedule B5 of the Draft New Official Plan. The “Mixed Industrial” designation is characterized by a broad mix of uses including small-scale office, light industrial, wholesale, small contractors and other uses that would otherwise not be permitted on lands designated “Industrial and Logistics.”
- / Permitted uses include low-impact industrial uses, trades and contractors and small-scale office uses (typically less than 10,000 square metres).

The proposed development achieves the intent of the draft policies of the New Official Plan as it proposes new, office, and light industrial uses on Lancaster Road, an area characterized by existing similar uses. The proposal therefore is compatible with the existing and planned function of the area and planned future framework as per the Draft new Official Plan.

4.4 City of Ottawa Zoning By-law (2008-250)

The subject property is dual zoned with the western portion of the lands directly abutting Lancaster Road zoned Light Industrial (IL) and a small portion to the east and at the rear of the lands zoned Heavy Industrial (IH) in the City of Ottawa Zoning By-law (2008-250).



Figure 13 Zoning Map.

The purpose of the Light Industrial (IL) Zone is to permit a wide range of low impact industrial uses, as well as office in a consolidated industrial park setting. The IL Zone will also allow a variety of complementary uses such as recreational, health and fitness uses and service commercial (e.g. convenience store, personal service business, restaurant, automobile service station and gas bar), occupying small sites to serve the employees of the area. The IL Zone prohibits uses which are likely to generate noise, fumes, odours, or are hazardous or obnoxious; and will provide development standards to ensure compatibility between uses and minimize the negative impact of the uses on adjacent non-industrial areas.

The purpose of the Heavy Industrial (IH) Zone is to permit a wide range of industrial uses, including those which, by their nature, generate noise, fumes, odours, and are hazardous or obnoxious. As with the IL zone, the IH zone will also permit a variety of complementary uses such as recreational, health and fitness uses and service commercial (e.g. convenience store, personal service business, restaurant, automobile service station and gas bar), to serve the employees of the area and provide development standards that would ensure that the industrial uses would not impact on the adjacent non-industrial areas.

4.4.1 Zoning Provisions Table

The following provisions of the Zoning By-law apply to the subject property:

ZONING MECHANISMS	Required Provision	Provided	Conformance
(a) Minimum lot area	IL: 2,000 m ² IH: 4,000 m ²	32,892 m²	Yes

(b) Minimum lot width		IL: No Minimum IH: No Minimum	137 metres	Yes
(c) Maximum lot coverage		IL: 65% IH: N/A	15%	Yes
(d) Minimum front yard and corner side yard setback		IL: 7.5 m IH: 7.5 m	10 metres	Yes
(e) Minimum interior side yard setback	(i) for uses listed in subsection 203(1) abutting a residential or institutional zone	IL: 15 m IH: N/A	N/A	N/A
	(ii) all other cases	IL: 7.5 m IH: 7.5 m	>7.5 metres	Yes
(f) Minimum rear yard setback	(i) for uses listed in subsection 203(1) for light industrial or 201(1) for heavy industrial abutting a residential or institutional zone	IL: 7.5m IH: N/A	N/A	N/A
	(ii) abutting hydro or railway right-of-way	IL: 3.5 m IH: N/A	N/A	N/A
	(iii) all other cases	IL: 7.5 m IH: 7.5 m	>7.5 metres	Yes
(g) Maximum floor space index		IL: 2 IH: 2	0.15	Yes

(h) Maximum building height	(i) within 20 metres of a residential zone	IL: 11m IH: N/A	N/A	N/A
	(ii) in all other cases	IL: 18 m IH: 22m	10.5 metres	Yes
(i) Minimum width of landscaped area	(i) abutting a residential or institutional zone	IL: 3 m; may be reduced to one metre if a 1.4 metre high opaque screen is provided IH: 7.5 m	N/A	N/A
	(ii) abutting a street	IL: 3 m IH: N/A	10.4 metres	Yes
	(iii) in all other cases	IL: No minimum IH: 3 m	>3 metres	Yes
Vehicle Parking (Area C):		<p>Industrial Use: 0.8 per 100 m² of gross floor area = 4</p> <p>Office Use: 2.4 per 100 m² of gross floor area = 87</p> <p>Warehouse: 0.8 per 100 m² for the first 5000 m² of gross floor area = 5</p> <p>= 97 spaces</p>	108 spaces	Yes
Loading Space per GFA		<9999m ² : 1	2	Yes
Oversized Vehicle Loading Spaces Required:		1 space for first 5,000 square metres plus all required spaces for gross	Yes	Yes

	<p>floor area exceeding 5,000 square metres.</p> <p>Refer to table 113B-Regulations for Vehicle Loading Spaces dimension requirements.</p>		
Parking Space Dimensions	<p>Standard size:</p> <ul style="list-style-type: none"> • Min.width 2.6 m x min length 5.2 m • Max. W 3.1 metres 	<p>Yes</p> <p>>4.6 metres</p>	<p>Yes</p> <p>No</p>
Drive Aisle Dimensions	Min: 6.7 metres	6.0 metres in some locations.	No
Driveway Dimensions	Min: 6.0 metres for a double traffic lane;	9.0 metres	Yes
Location of Garbage Storage Area	<p>All outdoor refuse collection and refuse loading areas contained within or accessed via a parking lot must be:</p> <ul style="list-style-type: none"> / at least 9.0 metres from a lot line abutting a public street; / at least 3.0 metres from any other lot line; / screened from view by an opaque screen with a minimum height of 2.0 metres. 	<p>>9.0 metres from public street.</p> <p>>3.0 metres from lot line.</p> <p>>2.0 screen</p>	Yes

<p>Bicycle Parking</p>	<p>Industrial: 1 per 1000 m² of gross floor area = 0</p> <p>Office: 1 per 250 m² of gross floor area = 16</p> <p>Warehouse: 1 per 2000 m² of gross floor area = 0</p> <p>= 16</p>	<p>16</p>	<p>Yes</p>
<p>Accessory Structures</p>	<p>Maximum Permitted Size:</p> <p>Minimum Required Setback from an Interior Side Lot Line or Rear Lot Line not abutting a street:</p> <p>Other accessory buildings or structures: 0 m</p>	<p>No restriction.</p> <p>0 metres from projecting roof and 3 metres for main structures</p>	<p>Yes</p> <p>Yes</p>

The submitted proposal adheres to the vast majority of the existing zoning provisions and overall intent of the existing zoning framework.

5.0 Required Applications

5.1 Minor Zoning By-law Amendment

The proposed Zoning By-law Amendment proposes to amend the zoning on the entirety of the lands from XXXX to “Heavy Industrial, Exception XXXX, Schedule YYYY (IH [XXXX], SYYYY)”. As the zoning amendment request is to effectively extend the existing Heavy Industrial Zone to an abutting portion of land, the application is deemed a Minor Zoning By-law Amendment.

A site-specifics schedule and exception will provide the necessary permissions of the current zone as detailed in this report. The site-specific zoning Schedule and Exception will be considered to appropriately manage the expansion of the Heavy Industrial Zoning provisions on the lands to ensure the proposed uses do not generate undesirable impacts on the community and are in keeping with the objectives and intent of the City of Ottawa’s planning regulatory and policy framework.

5.1.1 Zoning Amendment from an IL to an IH Zone:

- / Lancaster Road and the properties to the rear of the site directly abutting 2592 Lancaster Road can be characterized by their industrial nature and existing zoning framework. The proposed expansion of the Heavy Industrial Zone and associated permitted Outdoor Storage use is supported within the overarching planning and regulatory framework including the Official Plan, and existing zoning context.
- / The proposed Heavy Industrial Zone and associated Outdoor Storage Use will be screened and buffered by the proposed building from the Lancaster Road right-of-way and other more sensitive lands uses. The existing character to the rear of the subject lands and along the decommissioned rail line is compatible with the proposed use which will not create any detrimental impacts to abutting properties.
- / Given the current environment and planned function of the subject lands, the abutting lands, and the broader community, the Heavy Industrial Zone with Site Specific Exceptions and Schedule is considered the most appropriate zoning for the subject lands.

The Minor Zoning By-law Amendment will also address non-conformance in the following areas:

- / Permit increased parking space width of 4.6 metres to accommodate larger fleet vehicles in the rear parking area whereas the Ottawa Comprehensive Zoning By-law (2008-250) permits a maximum parking space width dimension of 3.1 metres; and
- / Permit a reduced parking lot aisle width of 6.0 metre in the front vehicle parking area whereas the Ottawa Comprehensive Zoning By-law (2008-250) requires a minimum 6.7 metre drive aisle width for parking spaces angled 90 degrees.

The existing maximum parking space width of 3.1 metres within the Zoning By-law is intended to avoid the over provision of paved surface areas relative to typical vehicle size and currently does not adequately contemplate the needs of larger service vehicles in industrial areas. The request to permit an increased parking space width in this development is directly related to the operational needs of the proposed use with the larger parking spaces only proposed in the rear of the site which is separated and screened from the public realm as well as the employee/visitor parking area.

Regarding the reduced drive aisle width requested, it should also be noted that the requested 6 metre aisle widths are becoming more common in municipal development in Ottawa. This width is currently the minimum indoor parking standard included within the Zoning By-law which demonstrates that it has been deemed reasonable by the City. The proposed development includes separate vehicle access points for the fleet and service vehicles which provides a larger drive aisle width to accommodate those vehicles.

5.2 Site Plan Control Application

The Site Plan Control Application for the proposed development is required and submitted to resolve site-specific design considerations such as site access, landscaping, servicing locations, and building materiality.

6.0 Supporting Studies

6.1 Phase I and Phase II Environmental Site Assessment

BluMetric Environmental Inc. was retained to conduct a Phase 1 Environmental Site Assessment (ESA). The purpose of the Phase 1 ESA is to research and study the past and current use of the subject property and to identify any environmental concerns with the potential to have impacted the subject property. The report identified several areas of potential environmental concern on the subject lands. The report concludes that a Phase 2 ESA is required for the subject property which will inform the submission of a Record of Site Condition to the Province. The Phase 2 ESA further investigated the sources of contamination and provided recommendations for the appropriate remediation process for the subject lands.

6.2 Geotechnical Investigation

Malroz Engineering Inc. was retained to conduct a Geotechnical Investigation. The purpose of the study is to determine the subsoil and groundwater conditions at the site by means of boreholes, and provide geotechnical recommendations for the design of the proposed development including construction considerations which may affect the final design. The report concludes that from a geotechnical perspective, the site is suitable for the proposed development and provides recommendations for the future construction of the proposed commercial / industrial building.

6.3 Functional Servicing and Stormwater Management Report

Walterfedy Inc. was retained to prepare a Functional Servicing and Stormwater Management Report for this application. The report provides an overview of the servicing and stormwater considerations for the site. The report concludes that the existing municipal water distribution system and sanitary sewage flow rate is acceptable and adequate for the proposal, and the stormwater flow off the site is also acceptable given the pre- and post-development flow rates.

6.4 Transportation Impact Assessment

WSP was retained to assist with design options and to prepare the Transportation Impact Assessment. The scoping report up to **Step 3, Forecasting** was submitted to staff (P. McMahon) and is currently under review.

6.5 Noise and Vibration Report

SLR Consulting Inc. was retained to prepare an Environmental Noise and Vibration Assessment Report for this application. The report provides an overview of the potential for noise effects and mitigation strategies from the proposed development. Based on the results of the study, the following conclusions have been reached:

- / Noise effects from the proposed development equipment were assessed on surround noise sensitive buildings.
- / The ENCG/MECP default NPC-300 Class 1 Area criteria were applied as a conservative assessment of facility effects.
- / Stationary noise effects from the development's equipment noise are predicted to meet NPC-300 Class 1 guideline limits in surrounding noise sensitive buildings. Noise mitigation measures are not required for the proposed development equipment.

6.6 Tree Conservation Report

IFS Associates was retained to prepare a Tree Conservation Report for this application. This report details a pre-construction tree conservation report (TCR) for the property.

Tree conservation reports are required for all properties subject to site plan control applications on which trees of 10 centimetres in diameter or greater are present. The inventory in the report details the assessment of all individual trees on and directly adjacent to the subject property.

The report concludes that all trees currently on the subject property conflict with the proposed building and parking areas and so are slated for removal. The removal of trees on adjacent private property is not considered necessary in terms of constructing the proposed development.

Public Consultation Strategy

All public engagement activities will take place in accordance with the City's Public Notification and Consultation Policy and will comply with Planning Act notification requirements. The following steps and activities have already been undertaken in preparation of the application submission or will be undertaken in the following months after the applications have been submitted:

- / Pre-Application Consultation Meeting
 - o A Pre-Application Consultation Meeting was held with City Staff, and the applicant team on April 1st, 2021;
- / Notification of Ward Councillor, Jean Cloutier
 - o The Ward Councillor was notified of the proposed development for the subject site prior to the Site Plan Control Application being submitted.
- / Community Information Session
 - o If requested by the Ward Councillor, a community information session will be held to discuss the proposed development.
 - o It is anticipated that, due to current COVID-19 restrictions, the community information session would be held in an online webinar format organized and moderated by the Ward Councillor and their staff members.

8.0

Conclusion

It is our professional planning opinion that the proposed Site Plan Control Application represents good planning and is in the public interest for the following reasons:

- / The proposed development is consistent with the Provincial Policy Statement, 2020 through the intensification of a property within the inner urban area where existing infrastructure and public service facilities are available, and where active transportation and transit will be supported and encouraged. The proposal also promotes the objectives of the Provincial Policy Statement through providing employment uses in a designated Employment Area and promoting economic prosperity in an area identified for such uses.
- / The proposed development conforms to the City of Ottawa Official Plan policies regarding intensification, managing growth, and the land use policies for the Urban Employment Area designation.
- / The proposed development conforms to urban design objectives and compatibility criteria established in Sections 2.5.1 and 4.11 of the Official Plan.
- / The proposed development complies with the general intent of the Zoning By-law, including the provisions of the Light Industrial and Heavy Industrial Zones.
- / The extension of the Heavy Industrial Zone to the entirety of the lands will provide for logical and efficient use of the lands while continuing to screen and buffer the proposed Outdoor Storage Use from the public realm and other more sensitive land uses. The application represents the extension of an existing zoning boundary to reflect and consolidate the planned function of the lot with that of the existing zoning to the rear.
- / The proposed development is supported by technical studies submitted as part of this application.

Sincerely,



Timothy Beed, MCIP RPP
Planner