



**255 Richmond Road, 249 Richmond Road & 372
Tweedsmuir Avenue**

Planning Rationale + Design Brief
Zoning By-law Amendment + Site Plan Control
October 26, 2021



Prepared for Young Street Capital

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1.0 Introduction

Fotenn Planning + Design (“Fotenn”) has been retained by Young Street Capital to prepare this Planning Rationale and Design Brief in support of the enclosed Zoning By-law Amendment and Site Plan Control applications to facilitate the proposed mixed-use development on the lands municipally known as 249 and 255 Richmond Road; and 372 Tweedsmuir Avenue (the “subject site”) in the City of Ottawa.

1.1 Application Overview

The proposed development would redevelop and intensify the properties located at the northwest corner of the Richmond Road and Tweedsmuir Avenue intersection in the heart of Westboro, with a nine-storey mixed-use building. The proposed development would consist of consists of a nine-storey, mid-rise, mixed-use building fronting along Richmond Road, transitioning to a three-storey built form where it abuts the Tweedsmuir Avenue property to the north.

The properties at 249 and 255 Richmond Road are proposed to be demolished and replaced with a mixed-use built form that is characteristic of a Mainstreet. The property at 372 Tweedsmuir Avenue will also require demolition, redeveloping as part of the 249 and 255 Richmond Road building (lot consolidation or “one lot for zoning purposes” required). The Tweedsmuir fronting part of the building is proposed with architectural features and a residential built form that is complimentary to the residential character of the surrounding low-rise neighbourhood to the north. A total of 87 dwelling units are proposed with at grade retail space abutting Richmond Road. An underground parking garage which is accessed from Tweedsmuir Avenue provides approximately 98 vehicular parking spaces as well as 196 bicycle parking spaces. The proposed development includes high-quality indoor and outdoor amenity space complimentary to streetscape conditions within the Westboro neighbourhood including an at-grade outdoor patio at the intersection of Richmond Road and Tweedsmuir Avenue. The proposed development consists of two (2) retail units and one (1) restaurant with frontage along Richmond Road, spacious residential units, contributing to a diverse mix and type of units in the Westboro neighbourhood.

To facilitate the proposed development, concurrent Zoning By-law Amendment, and Site Plan Control Applications are being submitted. The proposed Zoning By-law Amendment proposes to amend the zoning of the entire subject property to Traditional Mainstreet, with a site-specific exception and schedule. A new site-specific zoning schedule will establish permitted building heights, required setbacks, and stepbacks while the site-specific exception will provide the necessary relief from specific provisions of the proposed zone as detailed in Section 4.7 of this report. The proposed Site Plan Control Application resolves site-specific design considerations such as landscaping, servicing locations, and building materiality.

The intent of this Planning Rationale and scoped Design Brief is to assess the proposed development against the applicable policy and regulatory framework and determine if the development is appropriate for the site and compatible with adjacent development and the surrounding community. This review also includes an analysis of how the proposed development achieves the City’s applicable design guidelines, including appropriate transition and building height along a Traditional Mainstreet, near existing higher-order transit.

Subject Site & Surrounding Context

The subject property is located on the north side of Richmond Road and the west side of Tweedsmuir Avenue in the Westboro neighbourhood of the City of Ottawa.

2.1 Subject Property

The development lands consist of a land assembly of three (3) contiguous properties municipally known as 249 Richmond Road, 255 Richmond Road and 372 Tweedsmuir Avenue. Together, these properties are generally L-shaped. The subject property has a total area of approximately 2181.15 square meters with approximately 50 metres of frontage along Richmond Road along the southern edge and 46 metres of frontage along Tweedsmuir Avenue along the eastern edge.

The subject property contains low-rise buildings and associated surface parking lots. The building located at 255 Richmond Road is currently occupied by one (1) low-rise, one-storey commercial space with surface parking along the street. The second building on the property located at 249 Richmond Road is currently occupied by one (1) low-rise, two-storey restaurant, with surface parking at the rear and accessed from Tweedsmuir Avenue. Lastly, the building located at 372 Tweedsmuir Avenue is currently occupied by one (1) low-rise, two-storey detached dwelling with surface parking along the north side of the property. A significant portion of the property is hard surfaced and generally underdeveloped given the traditional mainstreet context.

A sidewalk is provided along the Richmond Road frontage wrapping around the retail property along Tweedsmuir Avenue. A sidewalk is not provided in front of the residential property along Tweedsmuir Avenue. Combined elevated hydro distribution lines and light standards are present in the public right-of-way adjacent to the property line along Tweedsmuir Avenue. Two dedicated light standards are located in the Richmond Road right-of-way adjacent to the south side of the property.

Limited landscaping is currently provided on the property. Small street trees exist along Richmond and Tweedsmuir street edges and are limited by the overhead hydro lines. None of the trees are anticipated to be classified as distinctive.

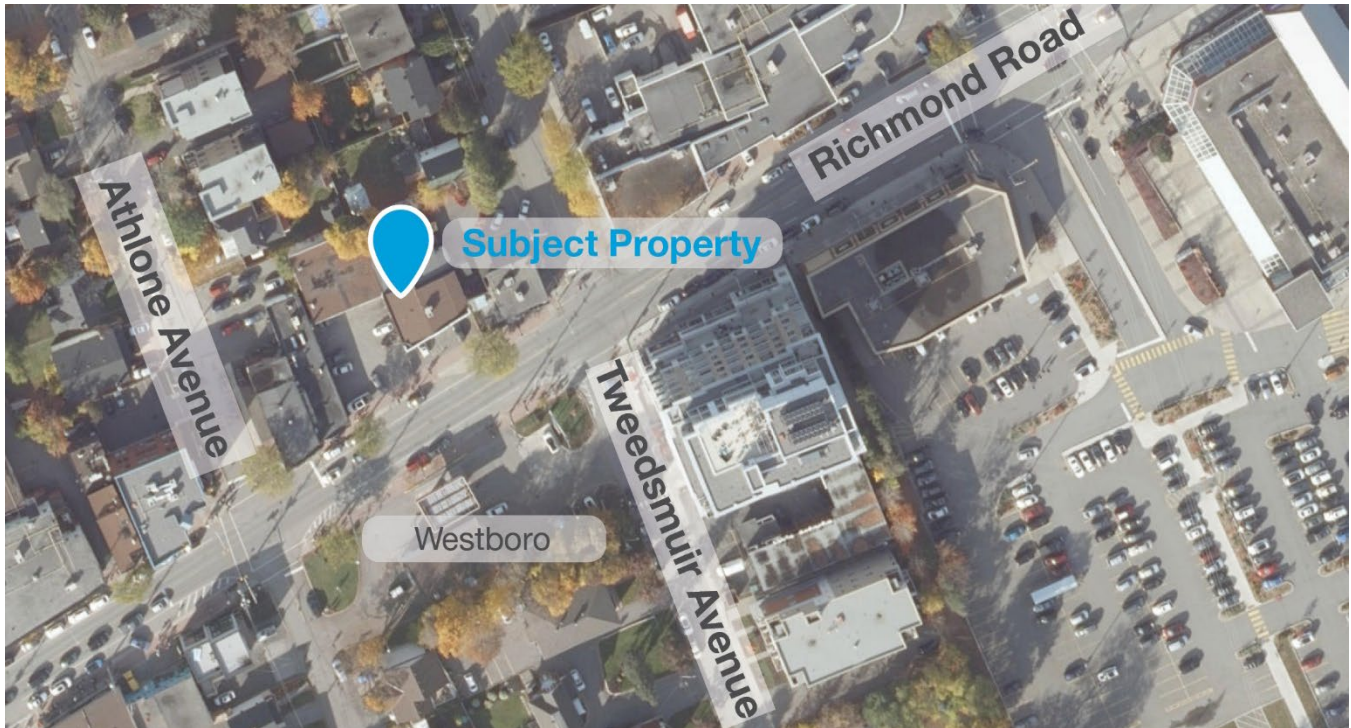


Figure 1: Context Map

The following land uses are located adjacent to the subject property and in the larger vicinity:

- / **North** of the subject property is generally characterized by a mix of low-density residential uses ranging from newly built 3 storey dwelling and low-rise apartment buildings to older 1.5-storey houses along Tweedsmuir Avenue. Generally, houses have front porches or stoops, and parking is provided as surface parking within the front yard or along the side of the dwelling. Lion's Park is also located north of the property and can be accessed from Winona Avenue which is located two blocks west of the property. The planned Westboro LRT station is also located one block north along Scott Street and is within 300 metres walking distance of the subject property.
- / **East** of the subject property, directly adjacent on the northeast corner of Tweedsmuir Avenue and Richmond Road, is a one-storey car dealership building. A recently built nine-storey mid-rise mixed-use building that includes retail at grade and residential uses above is also located adjacent to the property on the southeast corner of Tweedsmuir Avenue and Richmond Road. Further east along the Richmond Road Traditional Mainstreet, the at-grade retail, commercial uses and institutional uses including a grocery store and a school are continued generally within two- and three-storey buildings that range in character from glass and stucco to brick.
- / **South** of the subject property is a gas bar with a large paved area, beyond that is an area generally characterized by a mix of low-density residential uses ranging from newly built 3 storey dwelling and low-rise apartment buildings to older 1.5-storey houses along Tweedsmuir Avenue. Generally, houses have front porches or stoops, and parking is provided as surface parking within the front yard or along the side of the dwelling. Further south is the Byron Linear Park providing passive and active recreational opportunities.

- West of the subject property on the northeast corner of Richmond Road and Athlone Avenue are a restaurant and a mixed-use building within low-rise buildings. Further west along Richmond Road is a continuation of the traditional mainstreet environment within low- to mid-rise, mixed-use built form.



Figure 2: Aerial views surrounding the subject property

The subject property is well served by the City of Ottawa's urban road network. The subject property has direct frontage along Richmond Road, which is designated an Arterial Road pursuant to Schedule E in the City of Ottawa's Official Plan. Churchill Avenue North, which is approximately 300 metres west of the property, as well as Kirkwood Avenue, which is approximately 330 metres east of the property are also designated as Arterial Roads. Both of these roadways link to Scott Street, which acts as another east-west vehicular connection between the Westboro neighbourhood and the downtown core.

Arterial roads are intended to carry higher volumes of traffic to local and regional destinations. These roadways function as major public and infrastructure corridors that are intended to accommodate not only vehicular traffic but also pedestrians, public utilities, cyclists and public transit as well. Due to their ability to accommodate increased capacity, Arterial Roadways are generally best suited for increased activity stimulated by residential and commercial intensification.

2.4 Transit Network

The subject property is in proximity to existing rapid transit, as shown on Schedule D of the Official Plan. The entirety of the property is within an 800-metre radius of the two (2) Bus Rapid Transit stations that are planned as Light Rail Transit (LRT) Stations in the next phase of the LRT development – Westboro Station is located approximately 300 metres north of the property, while Dominion Station is located approximately 800 metres

northwest of the property (Figure 4). Westboro Station, Dominion Station and the western continuation of the Confederation Line is expected to begin service in 2025 as part of the Phase 2 Confederation Line.

Schedule D also identifies Richmond Road as a Transit Priority Corridor with isolated measures. This Transit Priority Corridor offers rapid transit connections to the greater Ottawa transportation network, including the planned Phase II of the Light Rail Transit (LRT) stations surrounding the Westboro neighbourhood.

Currently the nearest bus stop is located one (1) block east of the property along Richmond Road and is serviced by OCTranspo Route 11 providing connections between the Downtown core at its east end and Bayshore Shopping Mall at its terminus to the west. Additional bus routes #50 and #153 run north-south along Churchill Ave, providing additional connections to the greater transit network.

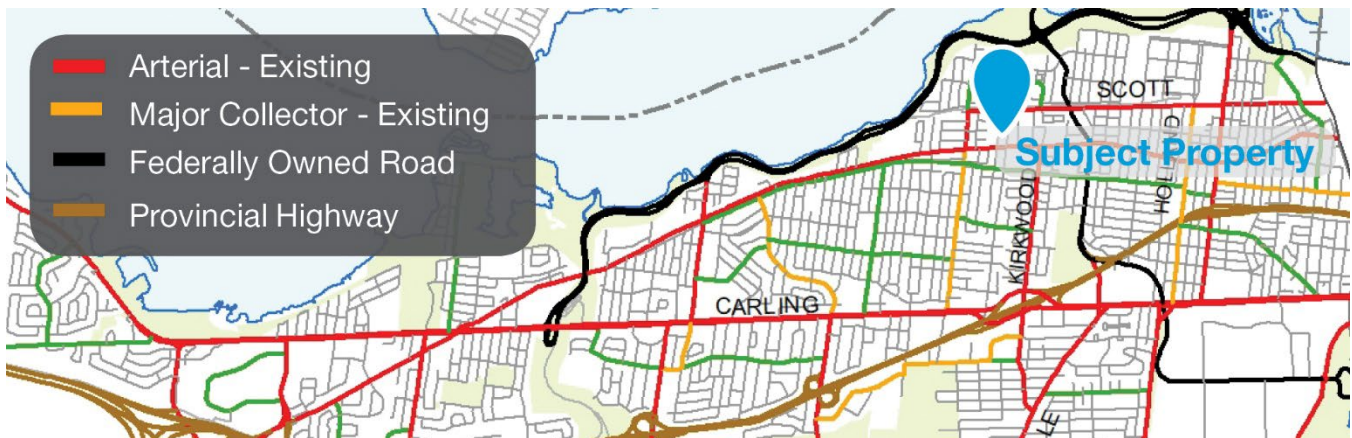


Figure 3: Excerpt from Schedule E of the Official Plan - Road Network, Official Plan.

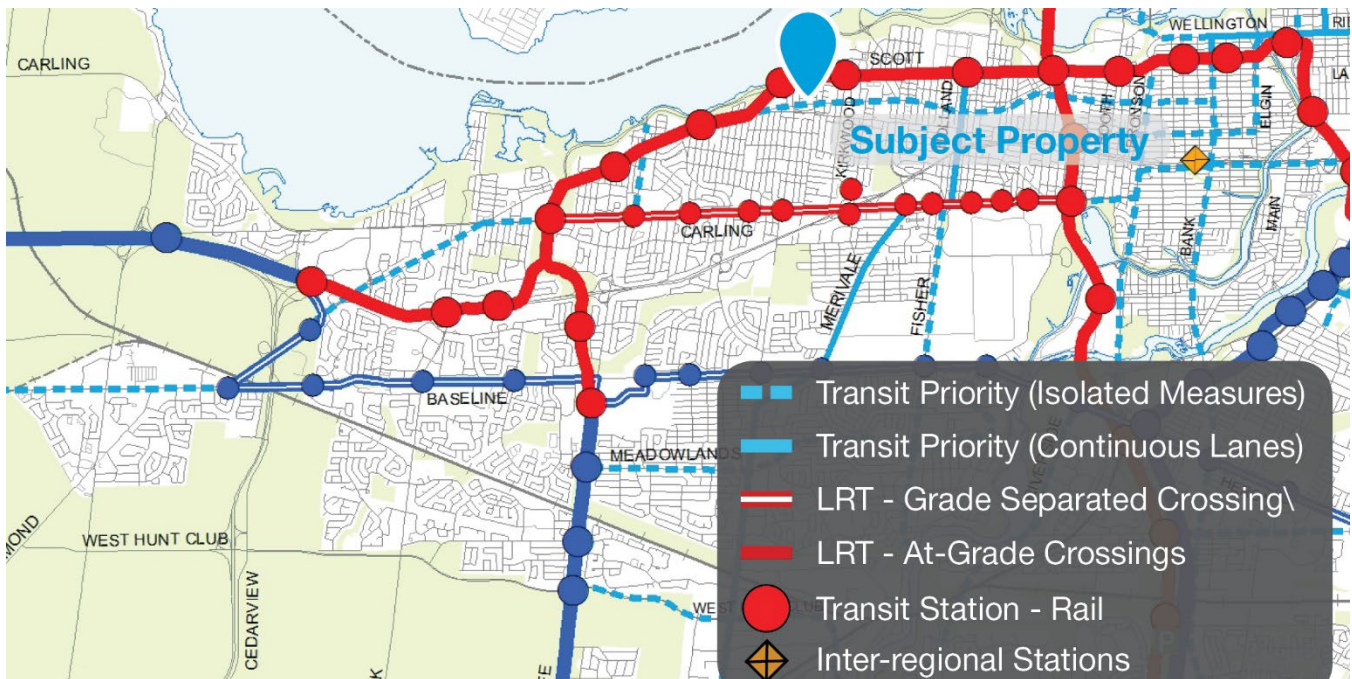


Figure 4: Excerpt from Schedule D of the Official Plan - Rapid Transit and Priority Network, Official Plan.

2.5 Cycling Network

The subject property is well-served by the City of Ottawa's urban cycling network (Figure 5). Pursuant to Schedule C (Primary Urban Cycling Network) of the Official Plan, Richmond Road is designated as a cycling spine route running in an east-west direction, connecting to designated spine routes along Churchill Avenue and Island Park Drive as well as designated cross-town bikeways along Scott Street. To the south of the subject property, a multi-use pathway along Byron Avenue provides additional connections to rapid transit, greenspaces and destinations through the city. These cycling accesses allow bicycle connections to various other routes throughout the city and rapid transit, promoting multi-modal, active transportation opportunities.

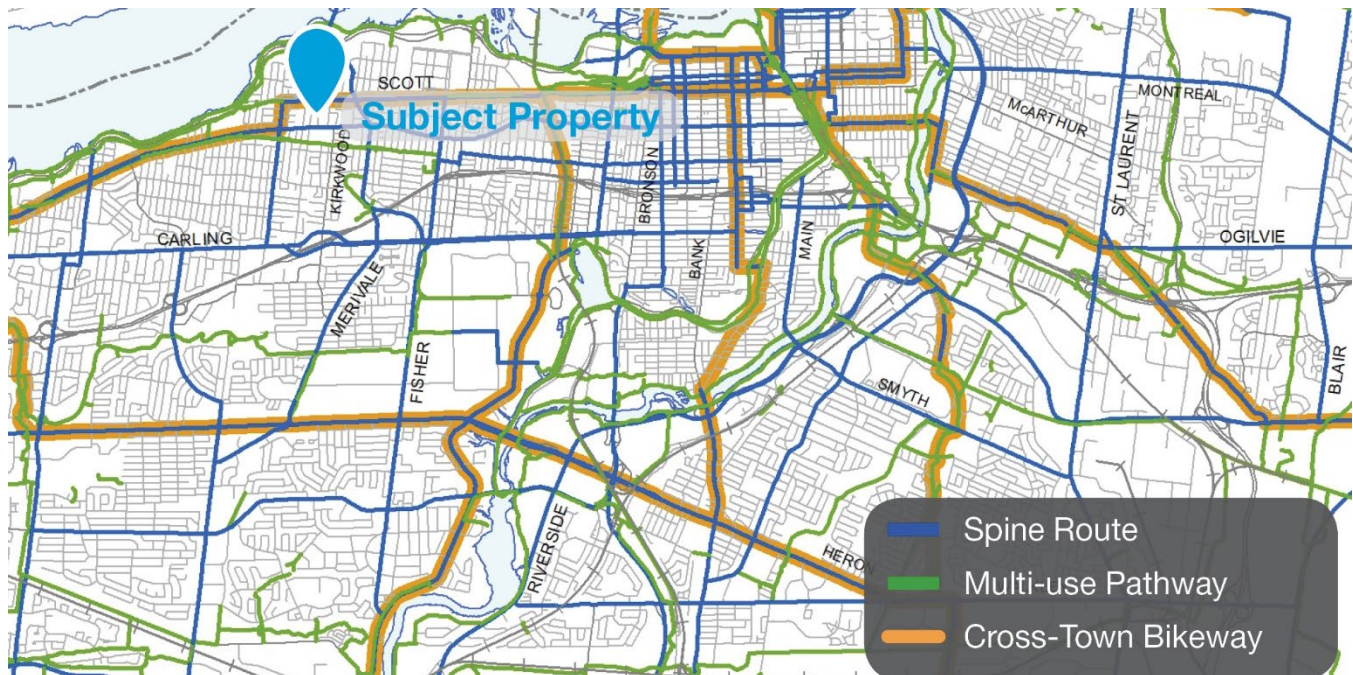


Figure 5: Excerpt from Schedule C of the Official Plan - Primary Urban Cycling Network, Official Plan.

2.6 Neighbourhood Amenities

Considering its location along an established Traditional Mainstreet, the subject site enjoys close proximity to many nearby amenities including a variety of small and locally-oriented commercial uses including restaurants, retail shops, and coffee shops. The surrounding neighbourhood also benefits from access to two large grocery stores within a 10-minute walk – Farm Boy at 317 McRae Avenue and Great Canadian Superstore at 190 Richmond Road. The site is well-served with respect to parks and community facilities being within walking distance of Lion's Park to the northeast, the Byron Linear Park to the south, and the Churchill Seniors' Recreation Centre to the west.

A non-exhaustive list of neighbourhood amenities illustrates the wide range of uses, and include:

- / Major commercial, restaurants, and retail uses in all directions throughout the Westboro neighbourhood, with particular concentrations along Richmond Road, Scott Street, and Churchill Avenue North;
- / Recreational facilities including private gyms and community fitness spaces, including the Churchill Seniors Recreation Centre, the Fisher Park Community Centre, the Westboro Community Centre and the Ottawa Gymnastics Centre;
- / Parks including Lion's Park, Byron Linear Park and Hampton Park; and

/ Schools including the Hilson Avenue Public School, Westboro Montessori School, Churchill Alternative, and the Centre Jules-Leger.

Proposed Development & Design Brief

Young Street Capital is proposing to construct a mid-rise, mixed-use building at the corner of Richmond Road and Tweedsmuir Avenue that is nine (9) storeys tall, dropping to a three (3) storey height profile along Tweedsmuir Avenue. The subject property's unique location, size, and orientation allow for an appropriate mid-rise building that will intensify an underutilized property along an important target area for intensification, while mitigating impacts resulting from the proposed building height.

A total of 87 residential units are proposed with approximately 98 underground parking spaces and 196 bicycle parking spaces. Two (2) retail units, and one (1) restaurant with a total area of 644 square metres, are located at-grade fronting Richmond Road with individual entrances on the ground floor. The primary residential entrance and lobby is located on the east side of the building, along Tweedsmuir Avenue.



Figure 6: Perspective from corner of Richmond Road and Tweedsmuir Avenue

The built form of the proposed development transitions from Richmond Road down to three storeys along Tweedsmuir Avenue and seven storeys at the west end of the Richmond Road frontage. Multiple stepbacks provide an appropriate height transition to the low-rise residential neighbourhood north along Tweedsmuir Avenue and Athlone Avenue, as well as to the evolving built form along Richmond Road.

In addition to the use of various strategically placed stepbacks, the mass of the building is further broken up through the use of differing materiality, fenestration and balconies. A one-storey podium is provided along the Richmond Road and Tweedsmuir Avenue (Figure 6) frontages and is reinforced through the high quality materials that compliment the existing streetscape.

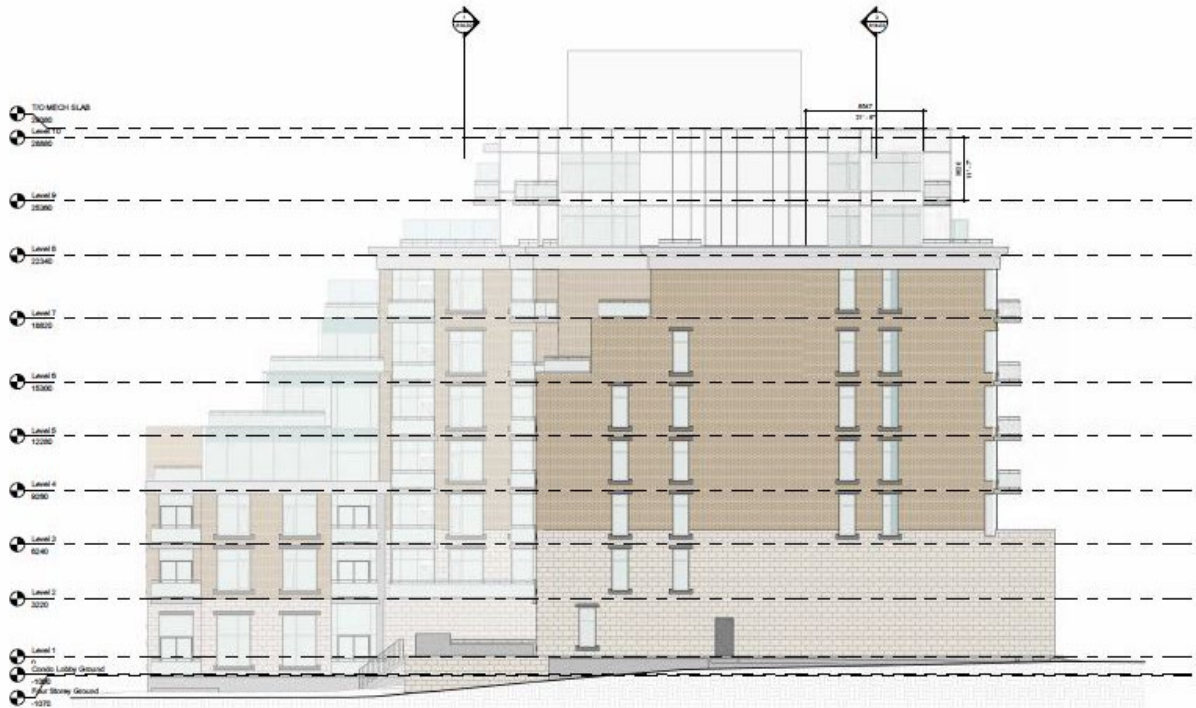


Figure 7: West Building Elevation

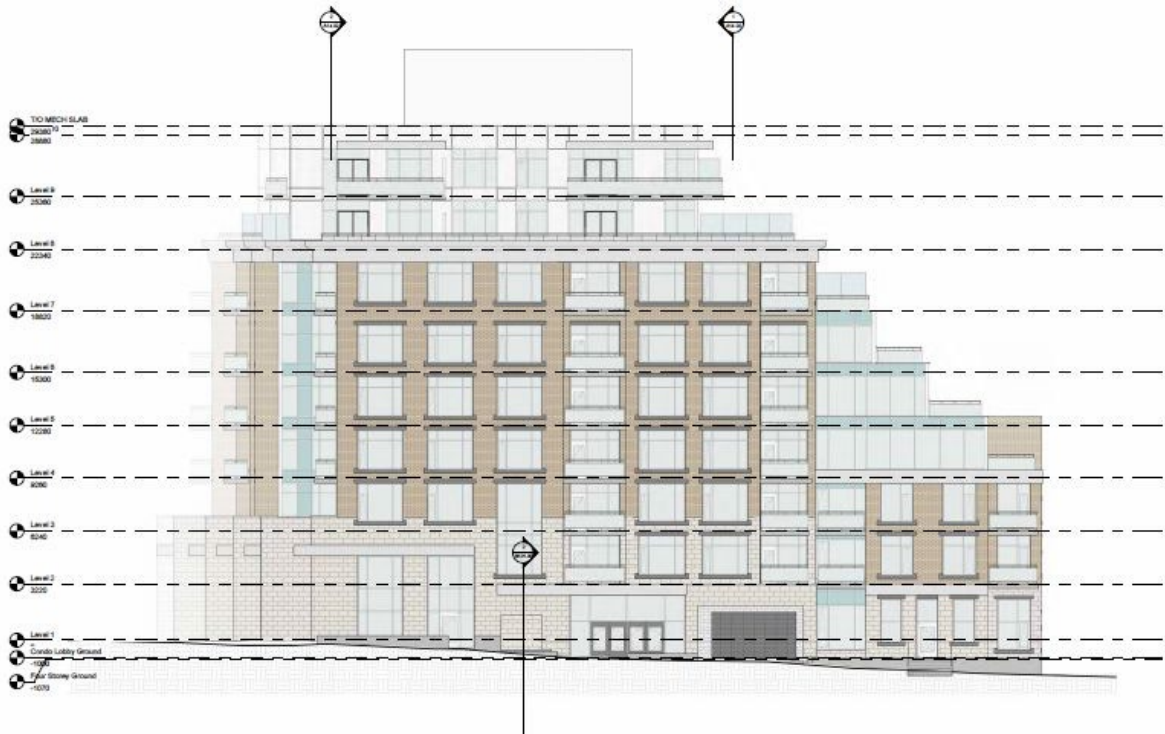


Figure 8: East Building Elevation

A 45-degree angular plane is often used as a tool to evaluate transition between a potential mid-rise building along a Traditional Mainstreet and an existing low-rise area. The proposed mid-rise portions of the building are terraced and provide gradual stepbacks to ensure an appropriate transition to existing low-rise along both Tweedsmuir Avenue and Athlone Avenue.



Figure 9: Angular Plane

Further to the stepbacks, the mass of the building is broken up using differing materiality, fenestration patterns, and balconies. The ground floor retail units along Richmond Road feature clear glazing, anchored by vertical brick and features a glass canopy to protect from the elements. The podium along Richmond Road is clad with brick and features horizontal openings to glazing and private balconies. The masonry is carried around to the east side of the building and broken up with large windows and balconies. Specific design features contribute to emphasizing the lower floor of the building and visually reducing the mass of the top portion; these include glass storefronts, a projecting soffit and Arriscraft Masonry on the base which accentuates the lower portion of the building and glass and brick masonry for the floors above. The units fronting Richmond Road are treated with a roof projection above the living rooms providing a break to the otherwise continuous roof line. The entrance to the condominium is highlighted with a vertical strip of windows which serve the bedrooms above.

3.2 Pedestrian Experience and Public Realm

The proposed development creates a series of public, private communal and private spaces. The transitions between these spaces are intended to create a richness of experience for residents, visitors, and members of the public. A total of 2,033 square metres of amenity space is provided. The proposed development includes improvements along the public right-of-way, including Richmond Road, and Tweedsmuir Avenue. The inclusion of new street trees and active at-grade uses advance the animation and pedestrian friendliness along both frontages. The treatment of the first three storeys, with generous fenestration provides visual transparency and improved safety for pedestrians in the area. The design of the access/egress to the parking garage has been located away from Richmond Road, avoiding any interruptions in the active frontage along the mainstreet while reducing conflicts between vehicles, pedestrians and cyclists.

The front or south-facing façade of the building along Richmond Road features entrances to the commercial units. The façade will also include a focal point, which is intended to accommodate a commercial patio area to further animate the pedestrian realm. Landscaping is being proposed along all frontages to reconstitute the public realm and create a more pleasant environment for pedestrians. In addition to landscaping along Richmond Road, the façades include generous glazing to improve sociability and safety through passive interaction and surveillance at the corner of the site.

The L-shaped design of the building allows for natural light and balcony space to be maximized for all units, while allowing for an internal courtyard in the base of the building. This space offers a more intimate outdoor setting for residents than the much more animated experience along Richmond Road.

3.3 Site Access and Parking

Regular vehicular access is provided off Tweedsmuir Avenue into the underground parking garage. The underground parking garage contains approximately 98 vehicular parking spaces, including 89 residential spaces and 9 visitor spaces in 3 below grade levels. Access to the underground parking garage is through a proposed two-way ramp. A total of 196 bicycle parking spaces are provided within the parking garage.

3.4 Amenities

Outdoor terraces on the stepbacks provide opportunity for efficient use of the building while offering amenity space and animating the exterior facades. The outdoor terraces are located to take advantage of sunlight exposure for users, while also reducing massing impacts for neighbouring residents. The residential component of the building also includes balconies, creating visual interest and architectural articulation, while providing private amenity space.

Internal amenity spaces are areas are also proposed for the benefit of the residents of the building. A spa business centre and a multi-purpose room have been provided for on the ground floor, with a fitness center on the seventh floor.

3.5 Massing Studies

A series of volumetric configurations were completed. One of the massing studies included the provision of an entrance to the underground parking garage along Richmond Road. This approach reduced the retail frontage and retail area dramatically and interrupted the pedestrian streetscape. Following this initial exploration, massing studies were completed and relocated the entrances to the garage, as well as the residential building along Tweedsmuir Avenue. Various options explored the location of the residential core providing flexibility on the ground floor including within the center of the plate and at the extreme edge of the plate. The studies also analyzed the existing grades along Tweedsmuir Avenue and the impact the grades would have on the grade of the entrance to the residential lobby, garage entrance, and ramp. Various elevation studies were undertaken to

address a 'corner building' and the façade treatment as the building wraps around the corner. Various configurations were also developed for the residential floor plate and its' relation to Richmond Road.

Policy and Regulatory Framework

4.1 Provincial Policy Statement (2020)

The Provincial Planning Statement (PPS) is a policy document issued under the Planning Act that provides direction on matters of provincial interest related to land use planning, growth, and development. All decisions on planning matters shall be consistent with the PPS. Generally, the PPS recognizes that “land use must be carefully managed to accommodate appropriate development to meet the full range of current and future needs, while achieving efficient development patterns”. In order to respond to current and future needs, a range of housing options is encouraged through new development and intensification.

Policies that support the development and intensification of the subject site include:

- / **1.1.1:** Healthy, liveable and safe communities are sustained by:
 - Promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term (1.1.1.a);
 - Accommodating an appropriate affordable and market-based range and mix of residential types, including multi-unit housing (1.1.1.b);
 - Promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs (1.1.1.e);

- / **1.1.3:** Identifies settlement areas as the focus of growth and development, with land use patterns in settlement areas to be based on densities and a mix of land uses which efficiently use land and resources; are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available and avoid the need for their unjustified and/or uneconomical expansion (1.1.3.2).
 - Policy 1.1.3.3 states that planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.

 - Policy 1.1.3.6 states that new development taking place in designated growth areas should occur adjacent to the existing built-up area and shall have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities

- / **1.4.3:** Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs or current and future residents of the regional market area by:
 - Permitting and facilitating:
 - o All housing options required to meet the social, health, economic and well-being requirements of current and future residents, including special needs requirements and needs arising from demographic changes and employment opportunities (1.4.3.b.1); and
 - o All types of residential intensification, including additional residential units, and redevelopment (1.4.3.b.2);
 - Directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs (1.4.3.c);

- Promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed (1.4.3.d);
- / **1.5.1:** Healthy, active communities should be promoted by:
 - o planning public streets, spaces, and facilities to be safe, meet the needs of pedestrians, foster social interaction, and facilitate active transportation and community connectivity;
- / Section **1.6** of the PPS provides policies for infrastructure and public service facilities. Policies require that growth be directed in a manner that optimizes the use of existing infrastructure and public service facilities, including municipal sewage and water services and transportation infrastructure.
- / **1.7.1:** Long-term economic prosperity should be supported by:
 - o Encouraging residential uses to respond to dynamic market-based needs and provide necessary housing supply and range of housing options for a diverse workforce (1.7.1.b);
 - o Maintaining and, where possible, enhancing the vitality and viability of downtowns and Mainstreets (1.7.1.d); and,
 - o Encouraging a sense of place, by promoting well-designed built form and cultural planning, and by conserving features that help define character, including built heritage resources and cultural heritage landscapes (1.7.1.e).
- / **1.8.1:** Planning authorities shall support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and preparing for the impacts of a changing climate through land use and development patterns which:
 - o Promote compact form and a structure of nodes and corridors (1.8.1.a);
 - o Promote the use of active transportation and transit in and between residential, employment (including commercial and industrial) and institutional uses and other areas (1.8.1.b); and
 - o Encourage transit-supportive development and intensification to improve the mix of employment and housing uses to shorten commute journeys and decrease transportation congestion (1.8.1.e).

The proposed development is consistent with the Provincial Policy Statement, 2020. The proposed development achieves the vision of the PPS by addressing policies related to efficient development and land use patterns, accommodating an appropriate range and mix of uses to meet long-term needs; promoting cost-effective development patterns, and supporting transit and active transportation. The proposed development conforms to PPS policies which direct development to locations that have been identified for intensification and redevelopment by the municipality. As a site located on a Traditional Mainstreet and within proximity of two rapid transit stations and future Light Rail Transit stations, the redevelopment of an underutilized property advances provincial goals of healthy, liveable and safe communities that efficiently utilizes existing infrastructure, improves the range and mix of uses and housing types, and supports transit use. Finally, the proposed development will contribute to the supply of available housing within the Westboro community in a built form that will enhance the pedestrian realm and create a new sense of place at an important intersection.

4.2 City of Ottawa Official Plan

The overarching policy document directing development, land use, and growth in the City of Ottawa, the City of Ottawa Official Plan ('Official Plan'), is composed of eight sections. Each of these addresses a different aspect of the planned function of the City as a whole. Section 2 of the Official Plan provides Strategic Directions or growth and development within the City.

The City's population is estimated to grow to 1,136,000 individuals and 489,000 households by 2031. One third of housing growth is anticipated within the greenbelt. At the same time, average household size inside the

Greenbelt is expected to decline from approximately 2.18 people in 2021 to approximately 2.12 people in 2031. Therefore, much of the anticipated demand within the Greenbelt will be for new housing in the form of smaller units such as apartments.

The City plans to meet this growth challenge by managing it in ways that support liveable communities and healthy environments. More specifically, the Official Plan pursues strategic directions in four key areas, two of which are relevant to the proposal:

1. Managing Growth
 - a. The City will manage growth by directing it to the urban area where services already exist or where they can be provided efficiently;
 - b. Growth in the urban area will be directed to areas where it can be accommodated in compact and mixed-use development, and served with quality transit, walking and cycling facilities.

2. Creating Liveable Communities
 - a. Growth will be managed in ways that create complete communities with a good balance of facilities and services to meet people's everyday needs, including schools, community facilities, parks, a variety of housing and places to work and shop; and
 - b. Attention to design will help create attractive communities where buildings, open space, and transportation work well together.

These strategic directions are developed further in the policies of Section 2.2.2 (Managing Growth) and 2.5 (Building Liveable Communities), as discussed below.

4.2.1 Land Use Designation

The subject property is designated 'Traditional Mainstreet' pursuant to Schedule B of the Official Plan. Pursuant to Section 3.6.3, Traditional Mainstreets are identified as streets that have generally developed pre-1945 and are characterized by being pedestrian-oriented and supporting small and local-oriented retail uses within buildings typically in low- and mid-rise formats. Traditional Mainstreets are intended to continue developing as pedestrian-oriented environments with sensitive intensification on underdeveloped lots and in a building format that encloses and defines the street edge with active frontages that provide direct pedestrian access to the sidewalk (Policy 10). Policy 11 supports mid-rise buildings (e.g., buildings up to nine storeys) on Traditional Mainstreets and recognizes that secondary plans may permit different building heights. A broad range of uses are permitted as detailed in Policy 5, but vehicular-oriented uses, such as automobile service stations, are generally discouraged as outlined in Policy 8.

When considering lot consolidation that includes properties along a Traditional Mainstreet, Policy 3 recognizes that the Traditional Mainstreet boundary can vary depending on site circumstances and lot configuration, including the ability to include properties on abutting side streets that exist within the same corridor.

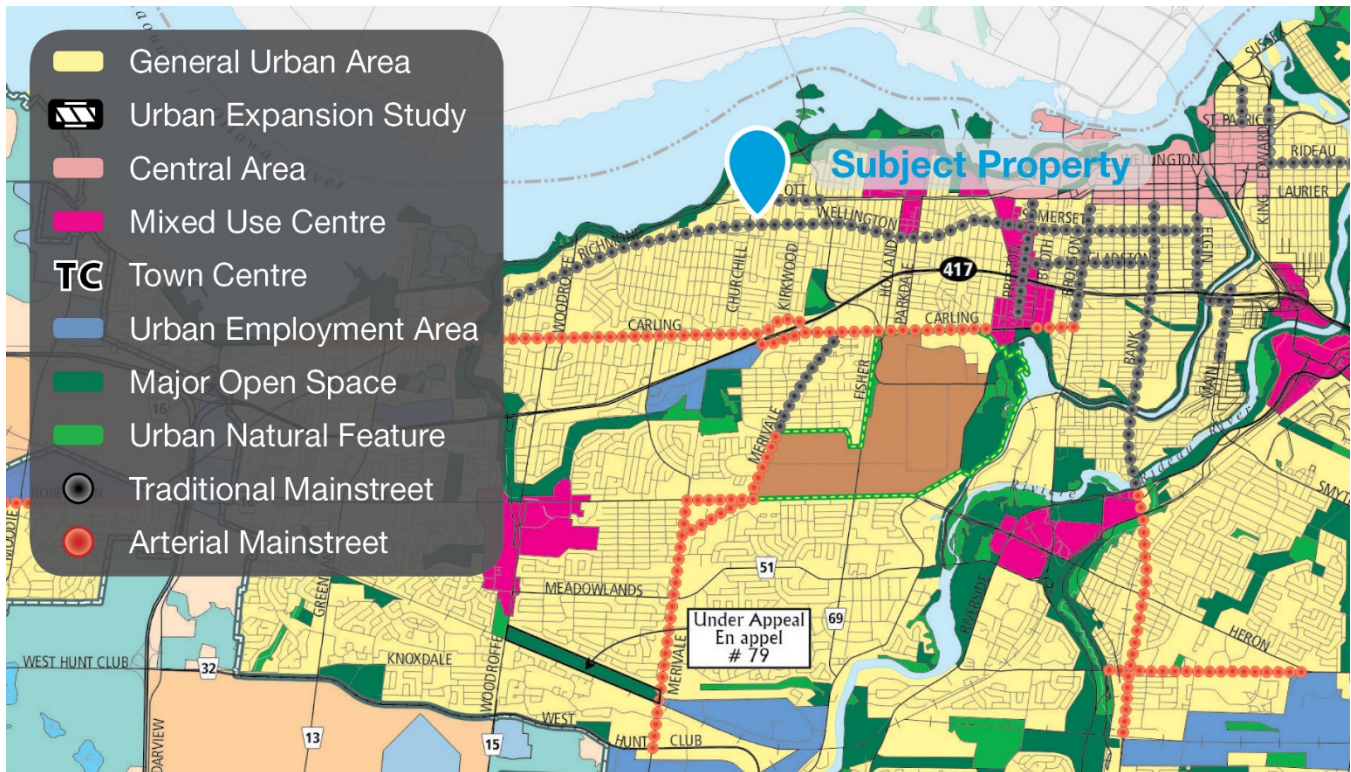


Figure 10: Excerpt from Schedule B of the Official Plan - Urban Policy Plan, Official Plan.

Policy 7 requires that development proposals on Traditional Mainstreets be evaluated in the context of Traditional Mainstreet policies, the policies set out in Section 4.11 with respect to compatibility, as well as the design objectives and principles contained within Section 2.5.1.

The proposed development meets the policies for Traditional Mainstreets by providing a building format that provides a continuous street edge, supports the pedestrian environment, and minimizes vehicular entrances. The proposed height of nine storeys is consistent with the policy intent for Traditional Mainstreets and as further discussed is also consistent with respect to the policies of the Richmond Road/Westboro Secondary Plan. Design and compatibility of the proposed development are further discussed herein. The proposed development will support the Transit Priority Corridor along Richmond Road and is within 800 metres walking distance of two Bus Rapid Transit stations that will be converted to Light Rail Transit (LRT) Stations in the next phase of the LRT development – Westboro Station is located approximately 300 metres north of the property while Dominion Station is located approximately 800 metres northwest of the property. Further, the development will encourage active transportation (walking, cycling) given the proximity of services and amenities along Richmond Road.

4.2.2 Managing Growth

The Official Plan generally supports intensification throughout the urban area where there are opportunities to accommodate more jobs and housing and increase transit use. Intensification is directed to target areas for intensification which have the potential to develop at moderate to high densities in a compact form.

Residential intensification is broadly defined in Section 2.2.2, Policy 1 as “the intensification of a property, building or area that results in a net increase in residential units or accommodation and includes the development of vacant or underutilized lots within previously developed areas and infill development”.

The proposed development intensifies an under-utilized property within the City’s urban area and in an area identified as a target area for intensification.

Traditional Mainstreets like Richmond Road are defined as “Target Areas for intensification” in Policy 3 of Section 2.2.2 of the Official Plan. Minimum density targets for many of the Target Areas are set out in Figure 2.3 of the Official Plan; however, Policy 8 of Section 2.2.2 states that for Traditional Mainstreets (such as Richmond Road), the density targets are contained in the 2006 “Residential Land Strategy”. Figure 34 of this strategy specifies a target of 1,225 new dwelling units from 2006 to 2031, with 550 of those units targeted between 2021 and 2031, and an additional 775 dwelling units post-2031. Per policy 8 of Section 2.2.2, these targets are intended to inform the community design process and other planning processes.

A central tenet of successful intensification is the quality of built environment. Well-designed public spaces and buildings are critical factors in achieving compatibility between the existing and planned built form. The Official Plan requires that intensification proposals have full regard for their immediate surroundings and the wider area.

Policy 10 of Section 2.2.2 recognizes that intensification may occur in a variety of built forms, including low-rise (1-4 storeys), mid-rise (5-9 storeys), high-rise (10-30 storeys) and high-rise 31+ (31+ storeys), provided urban design and compatibility objectives are met. Denser development, that often means taller buildings, should be in areas that support the Rapid Transit and Transit Priority networks and in areas with a mix of uses. Building heights and densities for different areas may be established through the Official Plan or a Secondary Plan and will be implemented through zoning.

Policy 11 further specifies that the distribution of appropriate building heights will be determined by:

- / The location in a Target Area for Intensification or by proximity to a Rapid Transit station or Transit Priority corridor, with the greatest density and tallest building heights being located closest to the station or corridor; and
- / The design and compatibility of the development with the surrounding context and planned function as detailed in Section 4.11 (discussed below), with buildings clustered with other buildings of similar height.

Per Policy 11 of Section 2.2.2, the Official Plan states that the distribution of appropriate building heights is by the location in a target area for intensification or proximity to transit infrastructure (a rapid transit station or a transit priority corridor) and the design and compatibility of the development as detailed in Section 4.11 (discussed below). The greatest densities and tallest buildings are to be located closest to the station or the corridor.

The proposed development is located within a target area for intensification, is located along a Transit Priority Corridor, and satisfies the urban design and compatibility criteria as noted below.

4.2.3 Urban Design and Compatibility

High-quality urban design is critical for all aspects of development on the subject property, given its location within an identified Design Priority Area. Two sections of the Official Plan provide design guidance on achieving high-quality urban design and compatibility within the existing and planned built context. Section 2.5.1 of the Official Plan sets out seven high-level design objectives which are broadly applicable on a city-wide basis. Section 4.11 of the Official Plan sets more specific criteria for certain classes of development.

Section 2.5.1 addresses community design, setting high level objectives to encourage good urban design and high-quality architecture. The section applies particularly to new, higher-density infill in existing urban areas. Development must be sensitive to, and compatible with, existing communities that have developed over long periods of time. Compatible development is identified as development that, although not necessarily the same as or similar to existing buildings in the vicinity nonetheless enhances an established community and coexists

with existing development without causing undue adverse impact on surrounding properties. Compatible development 'fits well' within the physical context and 'works well' among those functions that surround it.

The proposed development responds to the identified Design Objectives in the following ways:

1. To enhance the sense of community by creating and maintaining places with their own distinct identity.

The proposed development enhances the Traditional Mainstreet character and function of Richmond Road, by providing active, at-grade uses, and adding residential densities to support existing commercial uses and amenities. The one (1) storey podium responds to the mainstreet character while the mid-rise built form marks the intersection of Richmond Road and Tweedsmuir Avenue with a sensitive and appropriate design that does not result in undue adverse impacts on adjacent properties.

This redevelopment of the underutilized property within the vibrant Westboro community has the potential to establish this intersection as a prominent contribution to the continuing evolution of this area.

2. To define quality public and private spaces through development.

The proposed development animates the street edge with a one (1) storey podium that features an appropriately scaled 6 metre ground floor. The street edge will be animated with large panels of glazing, active entrances to the sidewalk, and active uses along the street. The storeys above the podium have been setback to maintain the scale along the mainstreet.

The proposed development creates a series of public and private spaces that serve members of the neighbourhood as well as residents of the proposed development and their visitors. Within the building, the various amenity areas will provide a high-quality communal amenity spaces for residents and their guests including rooftop patios at the fourth and sixth levels as well as a private at-grade courtyard space. These communal spaces are complemented by private balconies for residents.

3. To create places that are safe, accessible and are easy to get to, and move through.

The proposed development has been designed to improve the existing pedestrian environment along Richmond Road and Tweedsmuir Avenue by enclosing the street edge with buildings and fronting the mainstreet with active retail uses. A high percentage of glazing and active entrances along the frontage will ensure eyes on the street for safety. Canopies will provide for protection from the elements which is missing along this stretch of Richmond Road.

Vehicular access is consolidated to the underground parking garage access from Tweedsmuir Avenue along the eastern periphery of the property to minimize the impact on the pedestrian environment and the Traditional Mainstreet character of the Richmond Road frontage.

4. To ensure that new development respects the character of existing areas.

The proposed development incorporates several design features to respect the existing and planned character of Richmond Road. The one (1) storey podium along the front with the mid-rise form setback will provide a pedestrian scale along the street. The one-storey, tall retail and commercial floors at-grade will also contribute positively to the vibrant retail street. The floorplates of the eighth and ninth storeys have been sized and located strategically to mitigate impacts on the adjacent low-rise areas.

5. To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.

The proposed development adds apartment-style dwelling units ranging from studios to large two bedrooms to the Westboro neighbourhood. The proposed development will increase the housing options within the community, in a walkable, pedestrian-oriented, inner-urban neighbourhood in proximity to existing transit and amenities.

6. The proposal understands and respects natural processes and features in development design.

The proposed development improves and/or adds landscaped elements including soft and hard landscaping on the ground-floor along all the south and east frontages, an outdoor courtyard at the ground level, rooftop terrace at the 4th and 6th floors, all of which will help reduce the heat island effect. The proposal also incorporates stormwater management infrastructure to properly collect and discharge surface runoff from all parts of the subject property.

7. To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.

This proposal includes an active land use in proximity to a rapid transit station, creating opportunities to meet daily needs by alternative modes of transportation. Landscape elements are proposed throughout the site and will contribute to and a reduced urban heat island effect. The development also provides a supply of bicycle parking spaces, to facilitate bicycle use by residents and visitors. Car charging stations are being considered in the parking garage.

Policy 2 of Section 2.5.1 identifies Traditional Mainstreets as Design Priority Areas within the City and per policy 4, projects within these areas are subject to review by the Urban Design Review Panel (UDRP). The UDRP is instructed to review developments against the criteria set out in Section 4.11 of the Official Plan. Requests for additional height and density are also evaluated against criteria set out in this section of the Official Plan. This planning rationale incorporates the requirements of Design Brief.

Section 4.11 of the Official Plan provides guidance to ensure that new development is compatible with existing areas with respect to specific issues such as noise, parking, light spillover and shadowing.

The following table explains how the proposed development responds to the applicable policies of Section 4.11 currently:

Policy	Proposed Development
Building Design	
<p>5. Design of the parts of the structure adjacent to existing buildings and facing the public realm will achieve compatibility through design of:</p> <ul style="list-style-type: none"> a. Setbacks, heights and transition; b. Façade and roofline articulation; c. Colours and materials; d. Architectural elements including windows, doors and projections; e. On site grading; and f. Elements and details that reference common characteristics of the area. 	<p>The proposed development is providing a consistent street wall along Richmond Road, and Tweedsmuir Avenue. The architectural articulation and stepbacks to the building façade create visual interest and an appropriate building transition to surrounding development in the neighbourhood.</p> <p>The proposal has been designed with consideration for the existing grade change affecting the site.</p> <p>Architectural treatments such as materiality, colours, and projections have been carefully chosen to be compatible with the surroundings while contributing to high-quality design. The building design creates visual interest in the area and reduces the impact of massing.</p>
<p>6. Orient the principle façade and entrances to the street, include windows on elevations adjacent to public spaces, and use architectural elements, massing and landscaping to accentuate entrances.</p>	<p>Principal entrances are oriented towards the abutting public streets, improving interaction with the public realm. The main building entrance is accentuated through one-storey treatments and additional fenestration and an interesting corner treatment. All</p>

Policy	Proposed Development
	building walls are fenestrated to better interface with the street and improve safety through passive surveillance.
7. The intersections of arterial and collector roads can serve as gateways into communities and can support high levels of pedestrian and vehicular traffic, the greatest density of housing, and other land uses and services, and commercial services and other land uses that are focal points for a community.	The subject property is located at the northwest corner of Richmond Road and Tweedsmuir Avenue. At this corner, the design includes a restaurant as well as a patio space to promote community gathering and identification of the site.
8. To maintain a high quality, obstacle free pedestrian environment, all servicing, loading areas, and other required mechanical equipment and utilities should be internalized and integrated into the design of the base of the building where possible. If they cannot be internalized these services are to be screened from public view (i.e. trees, landscaping, decorative walls and fences etc.) and are to be acoustically dampened where possible. The location and operation these areas and equipment should be designed to maintain a pedestrian friendly environment and not impede public use of the sidewalk.	Servicing, loading areas, and mechanical equipment are generally located internal to the site and away from the public realm.
9. Roof-top mechanical or telecommunications equipment, signage, and amenity spaces should be incorporated into the design and massing of the upper floors of the building.	The rooftop mechanical equipment has been incorporated into the building.
Massing and Scale	
10. Where a secondary planning process establishes criteria for compatibility of new development or redevelopment in terms of the character of the surrounding area, the City will assess the appropriateness of the development using the criteria for massing and scale established in that Plan. Where there are no established criteria provided in an approved Plan, the City will assess the appropriateness of the proposal relying upon its approved Design Guidelines, as applicable, and the following criteria: <ul style="list-style-type: none"> a. Building height, massing and scale permitted by the planned function of adjacent properties as well as the character established by the prevailing pattern of abutting development and development that is across the street; b. Prevailing patterns of rear and side yard setbacks, building separation and landscaped open spaces and outdoor amenity areas as 	<p>The Richmond Road/Westboro Secondary Plan provides general policies that direct a transition in height and density in a north-south direction. This is further elaborated upon in Section 4.3 of this report. Higher building heights are permitted at a corner location where the development provides a community focus.</p> <p>The proposed development provides a height transition from the highest point along Richmond Road down to three storeys along Tweedsmuir Avenue in keeping with the low-rise residential neighbourhood.</p>

Policy	Proposed Development
<p>established by existing zoning where that pattern is different from the existing pattern of development;</p> <p>c. The need to provide a transition between areas of different development intensity and scale as set out in policy 12 of this section.</p>	
<p>11. Transition refers to the integration of buildings that have greater height or massing than their surroundings. Transition is an important building design element to minimize conflicts when development that is higher or has greater massing is proposed abutting established or planned areas of Low-Rise development. Proponents for developments that are taller in height than the existing or planned context or are adjacent to a public open space or street shall demonstrate that an effective transition in height and massing, that respects the surrounding planned context, such as a stepping down or variation in building form has been incorporated into the design.</p>	<p>As discussed above, the proposed development provides a height transition from Richmond Road down to three storeys along Tweedsmuir Avenue in keeping with the low-rise residential neighbourhood. The height transitions to five storeys along the northwest corner of the building in keeping with the low-rise residential neighbourhood along Athlone Avenue.</p>
<p>12. Building height and massing transitions will be accomplished through a variety of means, including:</p> <p>d. Incremental changes in building height (e.g. angular planes or stepping building profile up or down);</p> <p>e. Massing (e.g. inserting ground-oriented housing adjacent to the street as part of a high-profile development or incorporating podiums along a Mainstreet);</p> <p>f. Building setbacks and step-backs.</p>	<p>Transition has been provided via increased setbacks at each corner of the building, along with building stepbacks, including a 45-degree angular plane to the residential neighbourhood to the north.</p>
<p>Outdoor Amenity Areas</p>	
<p>19. Applicants will demonstrate that the development minimizes undesirable impacts on the existing private amenity spaces of adjacent residential units through the siting and design of the new building(s). Design measures include the use of transitions or terracing and the use of screening, lighting, landscaping, or other design measures that achieve the same objective.</p>	<p>Balconies in the development are sensitively located to mitigate issues of overlook and privacy. Amenity areas for residents are being provided in the form of balconies and rooftop patios at both the fourth and sixth levels. The L-shaped building maintains a significant setback space that allows for the development of an at-grade outdoor courtyard space at the rear providing a soft buffer between the proposal and the low-rise pattern to the north along Tweedsmuir Avenue and Athlone Avenue.</p>
<p>20. Applications to develop residential or mixed-use buildings incorporating residences will include well-designed, usable amenity areas for the residents that meet the requirements of the Zoning By-law, and are appropriate to the size,</p>	<p>Amenity space is provided via a combination of private balconies and rooftop terraces at the fourth and sixth levels. A commercial patio, small urban plaza at the corner of Richmond Road and Tweedsmuir Avenue as</p>

Policy	Proposed Development
<p>location and type of development. These areas may include private amenity areas and communal amenity spaces such as: balconies or terraces, rooftop patios, and communal outdoor at-grade spaces (e.g. plazas, courtyards, squares, yards). The specific requirements for the private amenity areas and the communal amenity spaces shall be determined by the City and implemented through the Zoning By-law and site plan agreement.</p>	<p>well as an internal courtyard space are to be developed to promote community gathering and interaction.</p>
Design Priority Areas	
<p>22.The portion of the building(s) which are adjacent to the public realm will be held to the highest building design standards by incorporating specific building design features.</p>	<p>The site is located along the Richmond Road Traditional Mainstreet Design Priority Area. The proposal has been designed to meet high design standards, including building materials, continuous building lines, articulation, and fenestration, while helping to define and improve this section of Richmond Road. Sidewalks and landscaping elements are also provided adjacent to the building. The massing and scale of the proposed development is designed to define and enclose public and private spaces.</p>
<p>23.The portion of the development which impacts the public realm will be held to the highest site design standards and should incorporate enhanced public realm improvements.</p>	<p>As part of the development, streetscape improvements will be made including new street trees and the creation of a small urban plaza with permanent seating features.</p>
<p>24.The massing and scale of development will define and enclose public and private spaces (e.g. streets, parks, courtyards, squares) using buildings, structures and landscaping; and relate to the scale and importance of the space they define (e.g. street width to height ratios).</p>	<p>An internal courtyard is proposed as part of the development. This shape is in keeping with the backyards of the residential neighbourhoods to the north along Tweedsmuir Avenue and Athlone Avenue and maintains sufficient space for a variety of activities.</p>

The proposed development conforms to the policy direction of Section 4.11. The proposed development will positively contribute to the Richmond Road Traditional Mainstreet and the surrounding neighbourhood through streetscape improvements and a high-quality design. The development has been designed in a manner that will minimize impacts to surrounding properties by providing appropriate height transition, internalizing traffic and waste, and respecting residential rear yards through the I-shaped design.

4.3 Richmond Road/Westboro Secondary Plan

The subject property is located within the Richmond Road/Westboro Secondary Plan area and, specifically, within the 'Westboro Village' and 'McRae and Churchill Avenue' planning sectors pursuant to Schedule A of the Secondary Plan. General policies for the entire Secondary Plan area are detailed, as well as policies for each planning sector.



Figure 11: Richmond Road/Westboro Secondary Plan Area

The general policies for the entire Secondary Plan area contemplate redevelopment and infill along both Richmond Road and Scott Street Traditional Mainstreets to optimize the use of land through increased building height and density. Generally, building heights in the range of four to six storeys are permitted, however, greater building heights will be considered in any of the following circumstances as outlined in Policy 1.3.3 without the need for an Official Plan Amendment:

- / Specific building heights are established in the Zoning By-law based on the Richmond Road/Westboro Community Design Plan or other Council-approved study;
- / The proposed building height conforms with prevailing building heights or provides a transition between existing buildings;
- / The development fosters the creation of a community focus where the proposal is on a corner lot, or at a gateway location or at a location where there are opportunities to support transit at a transit stop or station;
- / The development incorporates facilities, services or matters as set out in Section 5.2.1 of the Official Plan with respect to the authorization of increases in height and density that, in the opinion of the City, significantly advance the vision for Mainstreets; and
- / Where the application of the provisions of Section 2.5.1 and Section 4.11 of the Official Plan determine that additional height is appropriate.

General maximum building height ranges for the Secondary Plan area are outlined in Schedule C of the Secondary Plan. The subject property is contemplated for development at heights between four and six storeys as shown in the figure below.



Figure 12: Schedule C2 – Richmond Road/Westboro Secondary Plan (subject property in blue)

The policies for each planning sector are provided in Policy 1.3.4 including those for the Westboro Village planning sector (Sector 3), which address maintaining a sense of human scale by providing mixed-use buildings generally in the four- to six-storey range with a minimum of two storeys. Buildings are to be located close to the street. Buildings in the seven- to nine-storeys range are also permitted as shown on Schedule C where block sizes and the street network aid in minimizing impacts on low-rise residential neighbourhoods. Mixed-use development is encouraged in order to provide a continuity of retail and restaurant uses on the ground floor.

While heights in the four to six storey range are contemplated for the portion of the subject property fronting onto Richmond Road, Policy 1.3.3 of the Secondary Plan permits consideration for greater building heights without the need for an Official Plan Amendment. Pursuant to Policy 1.3.3., the proposed development provides a community focus at the corner of Richmond Road and Tweedsmuir Avenue, while also supporting transit due to its proximity to both the Westboro and Dominion transit stations. It is also important to note that the southeast corner of Richmond Road and Tweedsmuir Avenue is developed with a nine-storey building due to the increased depth of the lot. Not only do the subject lands provide a similar depth due to recent consolidation of ownership but they will also satisfy another criteria within Section 1.3.3 related to transition of similar area heights. Lastly, the depth of the subject lands enables a gradual and sufficient transition to the existing low-rise heights to the north. Through intentional stepbacks, setbacks and the incorporation of a courtyard amenity space at the ground floor,

4.4 New Ottawa Official Plan (Anticipated 2021-2046)

The City of Ottawa is currently undertaking a comprehensive review of their Official Plan (OP), which will result in a brand-new OP that will plan for a 25-year time horizon (2021 to 2046). A draft Official Plan was released on November 20th, 2020. The final draft was released periodically between July 2021 and September 2021. This final draft version is anticipated to be adopted by City Council on October 27, 2021. Following adoption, the final new Official Plan will undergo review by the Ministry of Municipal Affairs and Housing (MMAH) prior to final ministerial approval, anticipated in February 2022. While this Official Plan is not yet in-force, the policy directions recommended by the Joint Planning and Agricultural and Rural Affairs Committee made October 18, 2021 have been reviewed as they relate to the proposed development.

The subject site is proposed to be located within the “Inner Urban Transect” and designated a “Mainstreet Corridor” with the “Evolving Overlay” applied, as shown in Schedule B2- Inner Urban Transect (Figure 5).

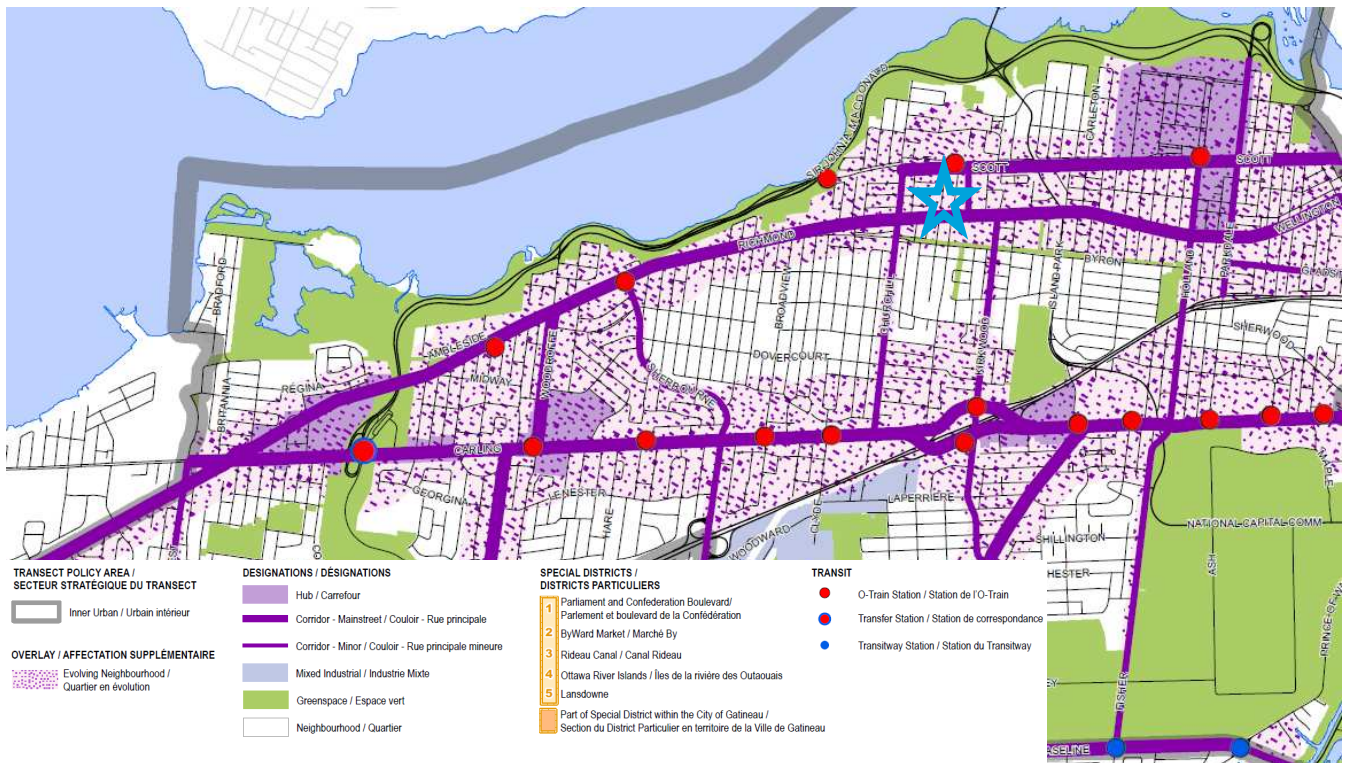


Figure 13: Schedule B2, Inner Urban Transect Area, subject property indicated

4.4.1 Inner Urban Transect Area

The Inner Urban Transect includes the pre-World War II neighbourhoods that immediately surround the Downtown Core and is characterized by both urban and suburban elements. The Inner Urban Transect is to continue to develop as a mixed-use environment, where a full range of services are located within a walking distance from home to support the growth of 15-minute neighborhoods. This will be achieved by:

- / Minimum density of 120 units per net hectare with a minimum of 5% (and target of 10%) of the units on lots with a wide of 15 metres or more much be large dwellings units (2+ bedrooms).
- / Generally mid- to high-density development;

- / Prohibiting new automobile-oriented land uses and development forms, including drive-through facilities; and,
- / Prioritize walking and cycling.

4.4.2 Mainstreet Corridor Designation

The Mainstreet Corridor designation's planned function combines a higher density of development, greater degree of mixed land-use and a higher level of street transit serve than abutting Neighbourhoods, but lower densities than nearby Hubs.

- / The designation applies to a maximum depth of 220 m from the centreline of the street identified as a Mainstreet;
- / Where a side street intersects with a Corridor, the Corridor designation may include one or more lots on the side street so as to extend the Corridor designation along the side street to the average depth of the Corridor designation along the rest of the Corridor block;
- / The forthcoming zoning by-law may require buildings to be mixed-use with uses along the ground floor that will create a stretch of continuous active frontage along the Mainstreet.

4.4.3 Maximum Permitted Building Heights

- / Along Mainstreets, permitted building heights are as follows, subject to appropriate height transitions, stepbacks, and angular planes:
 - o On sites that front on segments of streets whose right-of-way (after widening requirements have been exercised) is 30 metres or greater, and where the parcel is of sufficient size to allow for a transition in built form massing not less than 2-storeys and up to high-rise;
 - *Note: This portion of Richmond Road **does not** have a right-of-way of 30 metres*
 - o On sites that front on segments of streets whose right-of-way is narrower than 30 metres, generally up to **nine storeys except where a secondary plan or area-specific policy specifies different heights**; and,
 - o In all cases:
 - The wall heights directly adjacent to a street, and the heights of the podiums of high-rise buildings, where permitted, shall be proportionate to the width of the abutting right-of-way, and consistent with the objectives in the urban design section on mid-rise and high-rise built form in Section 4.6.6(8) and 4.6.6(9); and,
 - The height of such buildings may be limited further on lots too small to accommodate an appropriate height transition.”

4.4.4 Corridor Development

Development within the Corridor designation shall establish buildings that **locate the maximum permitted building heights and highest densities close to the Corridor**, subject to building stepbacks where appropriate. Further, development:

- / Shall ensure appropriate transitions in height, use of land, site design and development character through the site, to where the Corridor designation meets abutting designations;

4.4.5 Evolving Overlay

The Evolving Overlay is applied to areas within 150 metres of Hubs and Corridors to signal a gradual evolution over time that will see a change in character to support intensification, largely a change in character from suburban to urban.

4.5 Richmond Road / Westboro Secondary Plan

The subject site is proposed to remain subject to the Richmond Road / Westboro Secondary Plan upon adoption of the New Official Plan. For the most part the policy direction remains unchanged from the Plan currently in place (detailed in Section 4.3 above), however the following should be noted;

- / The outlining when building heights over 6 storeys will be considered along Richmond Road was revised to:

Redevelopment is supported on Richmond Road and Scott Street, which are designated as Mainstreet Corridors in the Official Plan, in order to optimize the use of land through increased building height and density. Development proposals will be evaluated based on the objectives of this secondary plan. This secondary plan supports building heights generally in the range of four to six storeys. Greater building heights will be considered in any of the following circumstances:

- a) *Specific building heights are permitted by the Zoning By-law based on the Richmond Road / Westboro Community Design Plan or other Council-approved studies; and*
- b) *The proposed building height is compatible in scale with, or provides a transition between existing buildings; and*
- c) *The development provides a cultural asset, and is located on a corner lot, at a transit stop or station, or is located at a prominent location, such as a major destination, an important public space, the termination of a vista or view, or a unique natural setting; and*
- d) *The development incorporates facilities, amenities, or services that support the goals for Mainstreet Corridors as per the Official Plan; and*
- e) *Where the application of the provisions of Section 4.6 of the Official Plan determine that additional height is appropriate; and*
- f) *Provide wider sidewalks where appropriate*

- / The “Maximum Building Heights” identified on Schedule C were removed for the subject site (no longer indicated as “4-6 Storeys”).

While this proposed development will be evaluated under the current Official Plan it is important to note that the proposed project is supported by the policies of the new Official Plan. The location of the subject site, within the Inner Urban Transect along a Mainstreet Corridor, in proximity to rapid transit is where the City wishes to direct intensification to meet the growth management objectives of this new Plan. The proposed development contributes to a land use pattern that is consistent with the 15-minute neighbourhood which is a key goal of the future Official Plan.

4.6 Urban Design Guidelines for Development along Traditional Mainstreets

The Urban Design Guidelines for Development along Traditional Mainstreets were approved by City Council in 2006 and serve to “provide urban design guidance...to assess, promote and achieve appropriate development along Traditional Mainstreets”. The objectives of the guidelines are:

- / To promote development that will enhance and reinforce the recognized or planned scale and character of the street;
- / To promote development that is compatible with, and complements its surroundings;

- / To achieve high-quality built form and strengthen building continuity along Traditional Mainstreets;
- / To foster compact, pedestrian-oriented development that is linked to street-level amenities; and
- / To accommodate a broad range of uses including retail, services commercial uses, offices, residential and institutional uses where one can live, shop and access amenities.

These objectives are achieved by meeting the various guidelines, including the following which are applicable to the proposed development:

- / The proposed development maintains a setback along Richmond Road that is consistent with the prevailing character of a minimal front yard setback (**Guideline 1**);
- / The proposed development maintains the current streetscape design, which includes a sidewalk and treed boulevard (**Guideline 3**);
- / A patio space will be developed at the corner of Richmond Road and Tweedsmuir Avenue. This will serve as an interesting corner within the neighbourhood and will provide opportunities for seating, gathering, and use of the space for larger events along Richmond Road (**Guideline 6**);
- / The building form respects the rhythm and pattern of Richmond Road by providing active street entrances (**Guideline 8**);
- / The building is L-shaped and transitions in height down to three storeys along Tweedsmuir Avenue. This serves to complement the existing rear-yard pattern of the residential neighbourhood along Tweedsmuir Avenue while also providing light and privacy (**Guideline 9**);
- / The building design includes street-level windows and retail uses along Richmond Road to promote an active streetfront (**Guideline 11**);
- / A building setback is provided at the first and seventh storeys along the Richmond Road frontage (**Guideline 12**);
- / The corner of Richmond Road and Tweedsmuir Avenue will include a patio space together with a restaurant to highlight this intersection in the community (**Guideline 16**).

The proposed development responds to the Urban Design Guidelines for Development Along Traditional Mainstreets by providing a building that includes a high level of architectural detail, including glazing, active entrances, and building setbacks.

4.7 Transit-Oriented Development Guidelines

The Transit-Oriented Development Guidelines, which were approved by City Council in 2007, are to be applied throughout Ottawa for all development within a 600 metre walking distance of a rapid transit station or stop. As the proposed development is within 600 metres of one transit station, these guidelines apply. The following is a summary of how the proposed development meets the various guidelines for transit-oriented development.

- / The proposed development provides residential intensification, as well as new commercial opportunities, on an underdeveloped lot within close proximity to two rapid transit stations (**Guideline 1**);
- / The commercial uses within the development are small allowing them to be oriented to local businesses and encourage pedestrian travel over vehicle travel. The Zoning By-law Amendment associated with the proposed development does not seek to introduce any new uses to the Traditional Mainstreet zoning applicable to the subject property (**Guideline 2**);
- / The proposed development is to be mixed-use and include both residential uses with a variety of units sizes, and ground-floor retail space. Further to this, the proposed development incorporates a corner plaza to encourage pedestrian activity and opportunity for seating (**Guideline 3**);

- / The proposed development creates an active streetfront along this block of Richmond Road, which connects to Tweedsmuir Avenue and the greater street network supporting connections to the rapid transit network and stations (**Guideline 7**);
- / The building includes setbacks at the first and seventh storeys along the Richmond Road frontage (**Guideline 11**);
- / The proposed development includes a patio space at the corner of Richmond Road and Tweedsmuir Avenue (**Guideline 12**);
- / The ground level units include a high level of fenestration to provide an active streetfront (**Guidelines 14 and 15**).

The proposed development responds to the Transit-Oriented Development Guidelines by providing a building that provides intensification as well as a range of housing choices and retail opportunities within close proximity of one transit station.

4.8 Zoning By-law 2008-250

The subject property is currently subject to three zone categories as follows:

- / **249-255 Richmond Road:** 'Traditional Mainstreet, Maximum Building Height of 15 metres' (TM H(15)). The TM zone permits a wide range of uses that support the Traditional Mainstreet, including retail, restaurant and personal service businesses, as well as residential uses. The maximum building height of 15 metres permits approximately a four-storey building consisting of a commercial ground floor and three residential floors above.
- / **372 Tweedsmuir Avenue:** 'Residential Fourth Density, Subzone UB' (R4UB). The R4 zone permits a maximum of height of 11 metres within a low-rise apartment building form.

The site is located within the **Mature Neighbourhoods Overlay**. The intent of the overlay is to regulate the character of low-rise development including front door location, driveway width, and parking. While the provisions of the overlay do not generally apply beyond low rise built form, some of the characters of the overlay, such as walkway width and driveway width, are required to be met.

4.8.1 Traditional Mainstreet Zone Provisions and Analysis

As permitted through the Official Plan, the Traditional Mainstreet land use designation can be extended to properties on abutting side streets when they are to be developed within the Traditional Mainstreet corridor. As such, the proposed Zoning By-law Amendment for the subject property is to rezone it entirely to a Traditional Mainstreet zone with site-specific provisions to address the proposed development. Further to this, the amendment includes exempting the site from the Mature Neighbourhoods Overlay.

Table 1 provides a summary of the Traditional Mainstreet zoning provisions as detailed in Zoning By-law 2008-250 and how the proposed development meets the provisions.

Table 1: Zoning Summary

Provision	Required	Provided	Compliance
Minimum Lot Area	No minimum	2181.15 m ²	✓
Minimum Lot Width	No minimum	50 m	✓

Provision	Required	Provided	Compliance
Uses	No residential or office uses are permitted within 6 metres of the front facade	Retail uses exclusively are proposed fronting Richmond Road	✓
	Residential uses may occupy a maximum of 50% of the ground floor area of a building that faces Richmond Road	17% of the ground floor is occupied by residential units and guest suite	✓
Maximum Front Yard setback <i>Richmond Road</i>	2 m Except for any part of a building above 15 metres, for which a min. front yard setback of 2 m must be provided	0m	✓ Front yard of 0 m is provided for majority of Richmond frontage (except where noted) ✓ Building above 15 metres
Minimum Corner Side Yard setback <i>Tweedsmuir</i>	3 m Except for any part of building >15 m for which an additional 2 m setback is required	0 m 2.6 m setback above 23.7m	✗ ✗
Minimum Interior Side Yard Setback	3m from residential uses	7.5m	✓
	0m from non-residential uses	0.4m	✓
Transparent Glazing	Min. 50% ground floor façade to a height of 4.5 m facing the main street must comprise transparent windows and active entrances.	> 50%	✓
Minimum Rear Yard Setback	Abutting a residential zone: 7.5 m Other cases: No minimum	Abutting Tweedsmuir: 1.2 m Abutting Athlone property: .07 m	✗ ✗
	Min. 6.7m for distance of 20 m from front lot line	Height varies	✓
Building Height	Max. 15 metres	31 m	✗
	No part of a building abutting an R4 zone may project above a 45° angular plane measured at a height of 15 m from a point 7.5 metres from	Proposed development does not meet the 45° angular plane as required abutting the R4 zone	✗

Provision	Required	Provided	Compliance
	the rear lot line, projecting upwards towards the front lot line		
Min. width of landscaped area	When abutting a residential area: 3m – may be reduced to 1 m where a min. 1.4m high opaque fence is provided All other cases: No minimum	Tweedsmuir: Min. 1.2 m with fence Athlone: 0.07 m	✓ ✗
Active Entrances	Min. one active entrance per each residential or non-residential use on ground floor	3 retail units abutting Richmond Road with individual active entrances	✓
Amenity Area 6m ² per unit Minimum 50% communal; One area at least 54m ² in area	522m ² total 261m ² communal	Total: 2,033m ² Communal: 1,089m ²	✓
Permitted Projections (canopies and awnings)	A distance equal to ½ the depth of a front, rear or corner side yard but not closer than 0.6 m to a lot line	1m beyond the property line	✗

Minimum Vehicular Parking Area Y on Schedule 1A Residential: 0.5/unit after first 12 units, 10% reduction when all parking is underground Residential Visitor: 0.1/unit after first 12 units Retail Units less than 500m ² : None required. Anything above 500m ² requires 1.25 per 100m ² of GFA Restaurant units less than 350m ² : None required. Anything above 350m ² requires 5 per 100m ² .	Residential: 36 spaces Visitor: 8 spaces Retail: 0 spaces Total: 44 spaces	Residential: 90 Visitor: 8 Retail: 0 Total: 98	✓
Parking Space Dimensions	Minimum: 2.6m wide x 5.2m long Up to 40% may be reduced to 2.4m wide and 4.6m long	Minimum: 2.6m wide x 5.2m long, with 40% reduced	✓
Aisle and Driveway Provisions	Driveway Minimum width: 6m Aisle Minimum width: 6m	Driveway Minimum width: 6m Aisle Minimum width: 6m, with a pinchpoint 5.9m	✗

Accessible Parking (per By-law 2017-301)	1 space min 3.66m wide	4 spaces	✓
Minimum Bicycle Parking Residential: 0.5/unit Retail: 1/250m ² of GFA Max 50% vertical spaces Minimum 25% indoors	Residential: 44 Retail: 0 spaces Total: 44 spaces Maximum: 22 vertical Minimum: 11 spaces indoors	Residential: 198 spaces Horizontal: All Total of 2 outdoors and 196 indoors	✓
Access Aisle for Bicycle Parking Spaces	1.5m minimum	>1.5m	✓

The proposal meets the general intent of the provisions of the Traditional Mainstreet zone, however, relief will be required from the zoning provisions as detailed in the following section.

5.0 Requested Zoning Amendment

A zoning amendment in by way of an exception and schedule is proposed for the site to clearly identify and delineate the maximum building heights, as well as the required building setbacks and stepbacks. The height schedule will be confirmed as the project moves forward and will follow the building envelope as shown on the Site Plan associated with this application. The Zoning Amendment will address the following site-specific zoning provisions:

- / **Maximum Building Height:** As shown on the site plan and described herein, the maximum proposed height is 31 metres with a number of height transitions incorporated within the building design to provide appropriate setbacks and stepbacks to the surrounding neighbourhood. The proposed 31 m height conforms to the Official Plan and Secondary Plan policies and is an appropriate height for the site. The zoning provisions also require that no part of a building abutting an R4 zone may project above a 45° angular plane measured at a height of 15 m from a point 7.5 metres from the rear lot line, projecting upwards towards the front lot line. As mentioned above, several intentional stepbacks have been incorporated into the design ensuring that any impacts to neighbouring residential properties are mitigated. Moreover, the angular plane appears to be less due to the grade change sloping up along Tweedsmuir Avenue towards Richmond Road.

- / **Corner yard setback:** The zoning provisions require that a minimum 3 metre setback is provided along Tweedsmuir Avenue, however the proposed development requires relief to decrease the setback. The design of the built form and massing makes a reduced setback is appropriate along this frontage, it provides a transition from the Traditional Mainstreet to the residential neighbourhood along Tweedsmuir Avenue. It is also important to note that the sidewalk along this portion of Tweedsmuir Avenue is approximately 7 metres wide thereby mitigating any impacts the reduced setback may have on any surrounding properties to the east or the creation of a canyon effect.

- Rear Yard Setback:** The Zoning By-law requires a rear yard setback of 7.5 metres. The applicant seeks to reduce the rear yard setback to 1.5 metres abutting the residential properties along Tweedsmuir Avenue and 0.07 metres abutting the residential properties along Athlone Avenue. To mitigate any impacts on the neighbouring low-rise residential properties along Tweedsmuir, the building has a maximum building height of three (3) storeys along the north corner and floors above have been terraced ensuring a gradual transition with stepbacks. It is important to note that as a result of the location of the garage ramp, a terrace condition is created at the first floor. This terrace space is not occupied by residential units and provides a passive amenity space for residents. Essentially, it provides a privacy wall between the proposed building and neighboring low-rise buildings along Athlone Avenue.

- / **Minimum width of landscaped area:** The zoning provisions require that a minimum 3 metre landscaped area is provided between the low-rise residential developments where no opaque fencing is provided. Along the Athlone Avenue property a landscaped area of 0.07 metres is provided. As noted above, this space is occupied by an outdoor terrace at the first floor essentially providing a privacy wall for residents..

- / **Permitted Projections:** The zoning provisions require that permitted projections be located no closer than 0.6 metres from the lot line, whereas the glass overhang above the first storey along Richmond Road and Tweedsmuir Avenue projects up to 1 metre beyond the property lines. These canopies will provide protection from the elements while in the public realm surrounding this development. It is understood that an Encroachment Agreement with the City will also be required.

- / **Drive-Aisle Width:** The zoning provisions require that a minimum 6 metre drive-aisle width is provided within the underground parking garage, however the proposed development requires relief to decrease the width to 5.9 metres at a pinchpoint. This amendment is minimal as it is only at one location and affects a total of four (4) cars.

6.0 Supporting Studies

Transportation Impact Assessment

Novatech Engineering prepared the Transportation Impact Assessment for the proposed development through Step 4. The study examined existing traffic conditions, travel demands of the proposed development, background conditions (including an assumed annual traffic growth without the development), transportation demand management measures, and intersections.

Serviceability Report

IBI Group prepared the Adequacy of Public Services report to outline the required services, including water, stormwater, and wastewater needed to support the redevelopment of the subject property. The report identifies that there are existing services available to develop the proposal.

It is assumed based on the size of the connecting sewer and proximity of the collector that the existing downstream sanitary sewer system can accommodate the proposed development, City confirmation would be required prior to SPA.

Incorporating into the detail design lot level controls and conveyance controls will result in effective treatment of surface stormwater runoff from the site.

Final detail design will be subject to governmental approval prior to construction,

Geotechnical Study

Paterson Group prepared a Geotechnical Investigation for the subject property as part of the current application. The study concludes that the subject property is a suitable location for the proposed redevelopment and notes several considerations for the construction of the project. Finally, the report recommends that all excess soils, with the exception of engineered crushed stone fill, generated by construction activities that will be transported on-site or off-site should be handled as per Ontario Regulation 406/19: On-Site and Excess Soil Management.

Wind Study

Gradient Wind Engineers & Scientists (Gradient) prepared a Pedestrian Level Wind Study to evaluate wind conditions at grade surrounding the proposed development. The study also considered wind conditions on outdoor communal amenity areas provided on the proposed rooftop terrace.

The methodological approach evaluates wind conditions for human comfort using five activities as thresholds. Under “sitting” conditions, wind speeds are less than 10 km/per hour at least 80% of the time. Standing is comfortable in slightly higher wind speeds, followed by strolling and walking. If mean wind speeds are projected to exceed 20 km/hour more than 20% of the time, conditions are considered uncomfortable. Different outdoor areas are expected to meet different standards; for example, transit stops should be comfortable for sitting and standing, whereas public sidewalks are only expected to meet the comfort criteria for strolling.

The study found that based on site exposures, and favourable massing of the study building itself, wind conditions at grade level are expected to be comfortable for a wind range of pedestrian uses year-round. The main condo building entrance on Tweedsmuir Avenue is expected to experience calm wind conditions year-round. Retail entrances on Richmond Road are also expected to be suitable for regular pedestrian access, (standing, strolling and walking activities). Private terraces and balconies are expected to experience comfortable conditions during the three warmer seasons of the year and acceptable wind comfort during the winter months. The study concluded that no dangerous wind conditions are expected to occur over the site.

Noise Study

Gradient also prepared a noise impact study report for the proposed development in order to evaluate the noise impacts from the adjacent road traffic on the proposal as well as the impact of the proposal on nearby noise sensitive receptors.

A detailed noise assessment of the proposed Development was completed by modelling the noise impact from road traffic on the Development. The assessment and review demonstrate that the noise levels predicted due to roadway traffic exceed the criteria listed in ENCG for building components and upgraded building components will be required for the north and east facades. The results of the current analysis indicate that noise levels will range between 45 and 68 dBA during the daytime period (07:00-23:00) and between 57 and 61 dBA during the nighttime period (23:00-07:00). The highest noise level (68 dBA) occurs at the south façade, which is nearest and most exposed to Richmond Road. Building components with a higher Sound Transmission Class (STC) rating will be required where exterior noise levels exceed 65 dBA.

Results of the calculations also indicate that the development will require central air conditioning, which will allow occupants to keep windows closed and maintain a comfortable living environment. A Warning Clause1 will also be required in all Lease, Purchase and Sale Agreements.

Noise levels at the OLA located at the northeast corner of the building (receptor 6) are expected to approach 57 dBA during the daytime period. Since noise levels exceed 55 dBA at this location, it is recommended to provide mitigation where it is administratively and technically feasible. However, since noise levels do not exceed 60 dBA, mitigation is not required.

Phase One Environmental Site Assessment

Paterson Group completed the Phase I Environmental Site Assessment (ESA) for the subject property, researching the past and current use of the site and study area to identify any environmental concerns. Based on a review of available historical information, the subject site was first developed with residential uses, circa 1930.

Based on the review, evaluation, and interpretation of the information obtained from the records review, interviews and site reconnaissance, the current use of this property is considered to represent an APEC with respect to the subject site.

Based on the results of this assessment, it was concluded that a Phase II –Environmental Site Assessment was required for the subject site.

Phase Two Environmental Site Assessment

Further to the identification of Areas of Potential Environmental Concern (APEC) on the site as per the Phase I ESA, Paterson Group completed the Phase II ESA for the subject property.

The Study concluded that additional investigative, remedial, and/or risk assessment work will be required prior to filing a Record of Site Condition. However, it is understood that this will be mitigated as all soil at the site will be excavated as part of the proposed redevelopment.

Tree Conservation Report

IFS Associates prepared a Tree Conservation Report for the proposed development by highlighting the condition of any existing trees on site, the impact of the proposed development and measures recommended to preserve and minimize impact. The report makes several recommendations for the construction of any redevelopment on the lands.

The methodological approach evaluates trees of 10 centimetres in diameter or greater. In accordance with the Tree Protection By-law No. 2020-340, a protection fence is to be erected at vegetation that is to be preserved. The protection fence shall be maintained throughout all phases of the development. No work is to be done within the tree protection fence.

Public Consultation Strategy

In partnership with the City of Ottawa, all public engagement activities will comply with Planning Act requirements, including circulation of notices and the Statutory Public Meeting. The following Public Engagement steps and activities have already been undertaken in preparation of this application submission or will be undertaken in the following months after the application has been submitted.

- / Pre-Application Consultation Meeting
 - A Pre-Application Consultation Meeting was held with City Staff and the applicant team on June 14, 2021. A member of the local community association was present and provided comments.

- / Notification of Ward Councillor, Councillor Jeff Leiper
 - The Ward Councillor will be notified of the proposed development for the subject property prior to the application being submitted.

- / Community “Heads Up” to local registered Community Associations (City of Ottawa)
 - A ‘heads up’ notification to local registered community associations will be completed by City of Ottawa during the application process

- / Community Information Session
 - If requested by the Ward Councillor, a community information session will be held to discuss the proposed development.
 - It is anticipated that, due to current COVID-19 restrictions, the community information session would be held in an online webinar format organized and moderated by the Ward Councillor and their staff members.

- / Planning Committee Meeting Advertisement and Report Mail out to Public
 - Notification for the statutory public meeting will be undertaken by the City of Ottawa.

- / Statutory Public Meeting for Zoning By-law Amendment – Planning Committee
 - The statutory public meeting will take place at the City of Ottawa Planning Committee.

8.0 Conclusions

It is our professional opinion that the proposed Zoning By-law Amendment and concurrent Site Plan Control Application to permit a nine-storey building on the subject property constitutes good planning and is in the public interest. As outlined in the preceding sections:

- / The proposed development is **consistent with the Provincial Policy Statement (2020)** by providing residential development that will provide increase choices for housing within an existing and established neighbourhood that is close to transit and is walkable.
- / The proposed development will allow greater intensification and the addition of residential density to a target area for intensification, **helping to implement the growth management policies of Section 2.2. of the Official Plan, while also conforming to the policies for urban design and compatibility.**
- / The proposed development **conforms to the policies for increased height within the Richmond Road/Westboro Secondary Plan** by providing appropriate transition of height and a community focused use at the corner of Richmond Road and Tweedsmuir Avenue.
- / The proposed development **conforms to the new Official Plan's vision** for managing growth in the Downtown Core transect and Neighbourhood designation.
- / The proposed development **responds strongly to the Urban Design Guidelines for Development along Traditional Mainstreets** by enhancing the public environment and streetscape, as well as providing transition to the adjacent low-rise residential neighbourhood.
- / The proposed **development responds to the Transit-oriented Design Guidelines** by introducing intensification and broader range of housing options within 800 metres of two (2) rapid transit stations.
- / The proposed development **generally aligns to the applicable requirements of the Traditional Mainstreet zone** in the Comprehensive Zoning By-law 2008-250. The requested amendments are appropriate allowing for the redevelopment of an underutilized site in a target area for intensification while respecting the area context.
- / The proposed development is **supported by technical studies** submitted as part of this application.

Sincerely,



Lisa Dalla Rosa, RPP, MCIP
Associate



Ghada Zaki, RPP, MCIP
Senior Planner