

1125 Cyrville Road

Site Plan & Zoning Bylaw Amendment Applications
December 2022 - Revision 2



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Introduction

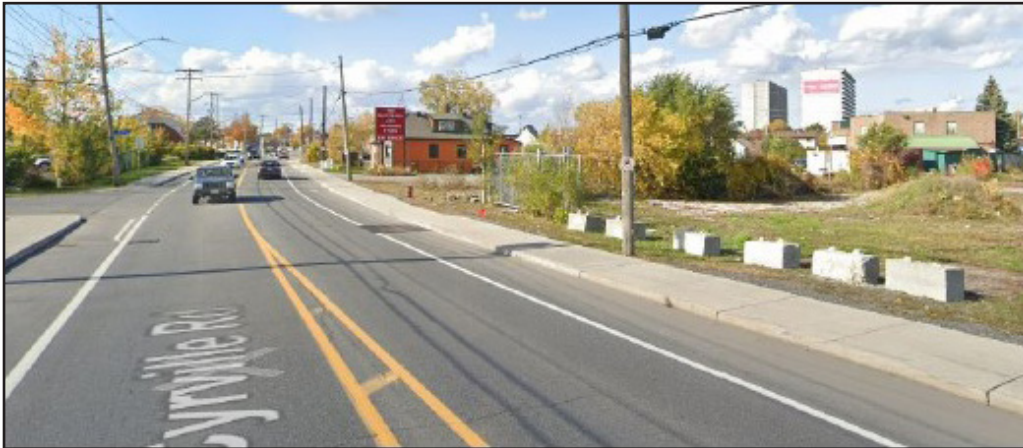
This rationale will support the Site Plan and Zoning Bylaw Amendment applications for the located at 1125 Cyrville Road. Westrich Pacific Corporation proposes to construct a multifamily development consisted of 2 buildings with total 354 dwelling units in two phases divided by underground storm water line easement. Phase one (Building A) is a 6-storey building containing 209 units; Phase two (Building B) is 20 storeys with 156 units.



PROJECT SITE

Site Context and the Surrounding Area

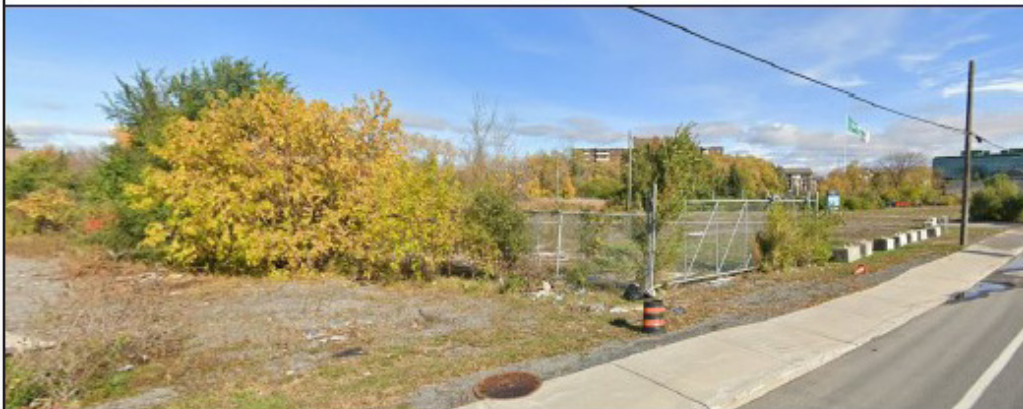
The proposed development is located on Cyrville Road by Cummings Avenue and Ogilvie Road. Within 400 metres is Cyrville O-Train East station in the Cyrville Transit Oriented Development Zone. The surrounding land uses are MC, TD2, TD3 and I1E. A future mixed-use high-rise development (LUX Place) zoned TD3 is right to the northeast on an adjacent property.



View 1: Looking Northwest @Cyrville Road

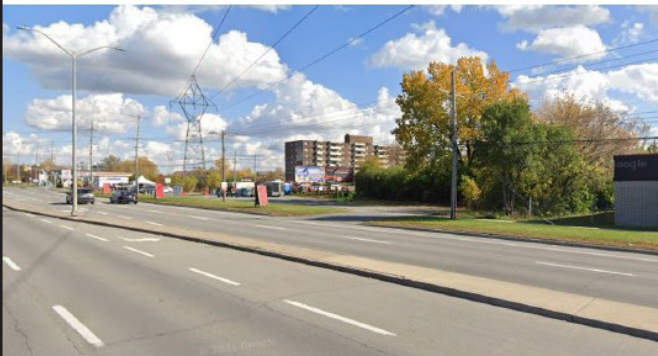


View 2: Looking East @Cyrville Road



View 3: Looking North @Cyrville Road

Site Context and the Surrounding Area



View 4: Looking South @Ogilvie Road



View 5: Looking West @Cummings Avenue

Site Context and the Surrounding Area

The Subject Site is bounded by:

NORTH - a series of multi residential sites of condominium and rental dwellings units located along Ogilvie Road just across the actual site.

EAST - across from the Subject Site, is the LUX Project and on the opposite side of Cummings Avenue is an older, eight storey apartment building and a one storey commercial building. Further to the east Richcraft has constructed higher density residential buildings.

SOUTH - one parcel at the angle of Cummings Avenue and Cyrville Road is occupied by a three (3) storey office building (Caisse Populaire).

WEST - Sites just west of the proposed development along Cyrville Road are vacant.

Westrich Pacific Corporation is submitting the following 2 applications:

Zoning By-law Amendment (ZBLA)

The ZBLA would rezone the site from Mixed-Use Centre Zone MC [1957] F (1.1) H (15) to the following zone:

- TD – Transit Oriented Development Zone – TD2 with a provision for a 7 metre setback for the west side of Building B above the 6th floor

Site Plan Application

The site plan application would allow the construction of 2 buildings with total 354 dwelling units in two phases divided by underground storm water line easement. Phase one (Building A) is a 6-storey building containing 209 units; Phase two (Building B) is 6-12 storey building with 156 units.

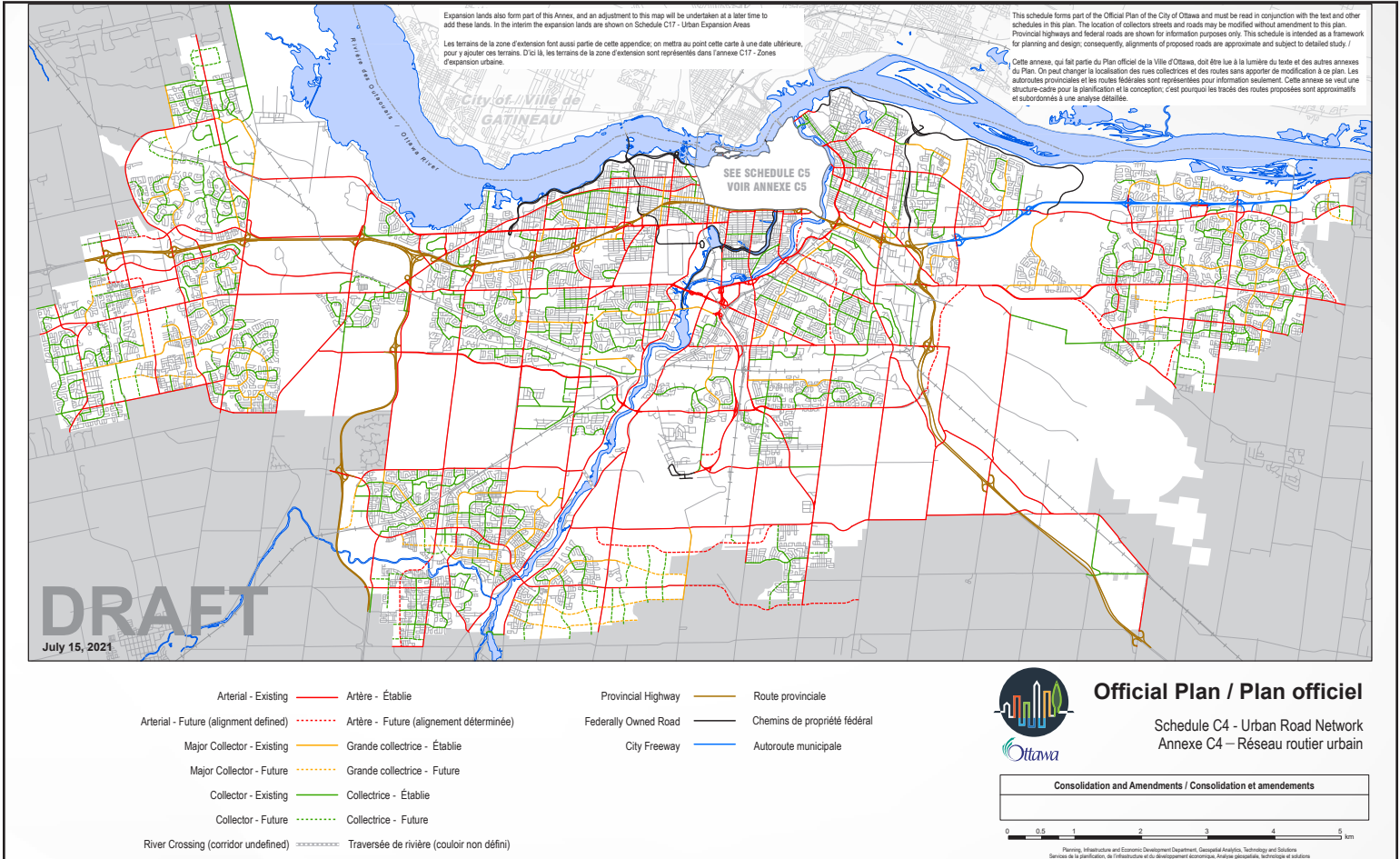
Subject Lands

The subject lands are legally known as:

Part of Lot 27, Concession 2 (Ottawa Front) Geographic Township of Gloucester, City of Ottawa

Roads

Schedule C4 – Urban Road Network of the City’s new Official Plan designates Cyrville Road west of Cummings as a Collector Roadway and Cyrville east of Cummings, Ogilvie and Cummings Avenue as Arterial roadways.



Arterial Roads are major roads which carry large volumes of traffic over longer distances. They function as major corridors in most villages providing connectivity for all modes of transportation: car and truck traffic, pedestrians, public utilities, cyclists, and transit.

Collector Roads serve neighbourhood travel needs to and from major collector or arterial roads and provide access to the local road network serving the community.

The Provincial Policy Statement (PPS) (2020)

The PPS provides policy direction on matters of provincial interest related to land use planning and development. As a key part of Ontario's policy-led planning system, the Provincial Policy Statement sets the policy foundation for regulating the development and use of land. The PPS provides for appropriate development while protecting resources of provincial interest, public health and safety, and the quality of the natural and built environment.

The Section 3 Subsection 5 of the Planning Act requires that planning decisions be consistent with the current policy statements and those decisions shall conform with the provincial plans that are in effect on the decision date.

Excerpts from the PPS (2020) are used to test the constancy and conformity of the proposed zoning and site plan.

Building Strong Healthy Communities

Policy Section 1.1.1 Healthy, liveable, and safe communities are sustained by:

- promoting efficient development and land use patterns which sustain the financial well-being of the province and municipalities over the long term; (1.1.1 (a))
- accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs. (1.1.1 (b))

Policy Section 1.1.3 Settlement Areas

- Settlement areas shall be the focus of growth and development. (1.1.3.1)
- New development taking place in designated growth areas should occur adjacent to the existing built-up area and should have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure, and public service facilities. (1.1.3.6)

Housing Policy 1.4

- Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs of current and future residents of the regional market area by:
 1. establishing and implementing minimum targets for the provision of housing which is affordable to low- and moderate-income households and which aligns with applicable housing and homelessness plans. However, where planning is conducted by an upper-tier municipality, the upper-tier municipality in consultation with the lower-tier municipalities

may identify a higher target(s) which shall represent the minimum target(s) for these lower-tier municipalities (1.4.3 (a))

2. directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;(1.4.3 (c))
3. promoting densities for new housing which efficiently use land, resources, infrastructure, and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed;(1.4.3 (d))

The proposed site plan and requested zoning are “are consistent with and conform to” the PPS (2020).

The proposed site plan supports the key objective of efficient land use patterns and densities that creates a community that will meet long term needs and will help financial sustain both the City of Ottawa and the Province of Ontario over the long term.

City of Ottawa Official Plan (2003, Consolidated)

For the purposes of this rationale, the policies of the old and new City of Ottawa Official Plan have been reviewed with respect to the proposed development.

The subject lands are designated Mixed Use Centre on Schedule B – Urban Policy Plan. Although the subject lands are discussed in detail in the Cyrville TOD Plan which are outlined later in this rationale Policy 3.6.2 – Mixed Use Centre and Town Centres of the Official Plan does provide general guidance and support for development of the subject lands.

Mixed Use Centres are a critical element in the City’s growth management strategy, being areas with potential to achieve high densities and compact and mixed-use development oriented to rapid transit. OP Page 3-16

The Plan contemplates that sites within an 800m walking distance of one or more rapid transit stations are opportunities to achieve high densities of housing through intensification and redevelopment of older sites and development of vacant land. It also contemplates High-Rise Buildings. The subject site is with 800 metre walking distance of the Cyrville rapid transit station.

Section 2.51 of the Official Plan speaks to the issue of community design.

The proposal for subject lands must be sensitive, flexible, complimentary and must fit well into the wider community context.

- Introducing new development and higher densities into existing areas that have developed over a long period of time requires a sensitive approach and a respect for a community’s established characteristics.
- Allowing for some flexibility and variation that complements the character of existing communities is central to successful intensification.

- compatible development means development that, although it is not necessarily the same as or similar to existing buildings in the vicinity, can enhance an established community through good design and innovation and coexists with existing development without causing undue adverse impact on surrounding properties.

The site plan which shows the building massing at 6 stories and the larger 12 storey building to the rear complimenting the abutting LUX development and does not overwhelm community. The fact that the Cyrville Streetscape is 6 storey brings a human scale to the development.

The Official Plan states in Section 4.11 that the compatibility of a development application will be evaluated in the context of this Section. Section 4.11 provides guidance on compatibility against which the subject lands are tested:

1. Development Scale
 - The scale of the proposed development is similar to the surrounding community in that it comprises medium and high-rise residential development, pathways and dedications to new roads. The density is different from the lands to the west, but those lands are expected to redevelop to similar density.
2. Traffic
 - Stantec has prepared a Community Transportation Study (CTS) which concludes that the development will not adversely impact the surrounding transportation network.
3. Access
 - The concept plan shows an access to Cyrville Street. As a result, the CTS does not identify any concerns with respect to the access to the subject lands.
4. Outdoor Amenity Area
 - Outdoor amenity areas will be provided in and around the subject property. In addition, each unit will have private outdoor balcony space.
5. Lighting
 - Standard Street lighting is proposed and will not have an impact on abutting properties.
6. Noise and Air Quality
 - The proposed residential community will not generate any adverse noise impacts.
7. Sunlight
 - The building typology is medium and high rise, and a sun shadow study has been provided which shows minimal blockage or impedance of sun light on abutting amenity areas or abutting land uses.
8. Microclimate
 - Wind, snow drifting and temperature impacts on adjacent properties are not anticipated.

TRANSIT-ORIENTED DEVELOPMENT (TOD) Plans Lees, Hurdman, Tremblay, St. Laurent, Cyrville and Blair - January 29, 2014

The Cyrville Transit-Oriented Development plan set the stage for future transit-supportive or intensified land development in priority areas located near future Confederation Line stations.

Section 10.5 identifies approximately 99 ha. as the Cyrville Transit-Oriented Development Plan area. Figure 67 of the Cyrville TOD plan shows the subject lands with the TOD density code of TD2 and proposes a maximum height limit of 20 storeys.

The Plan is provided as an Annex to this rationale.

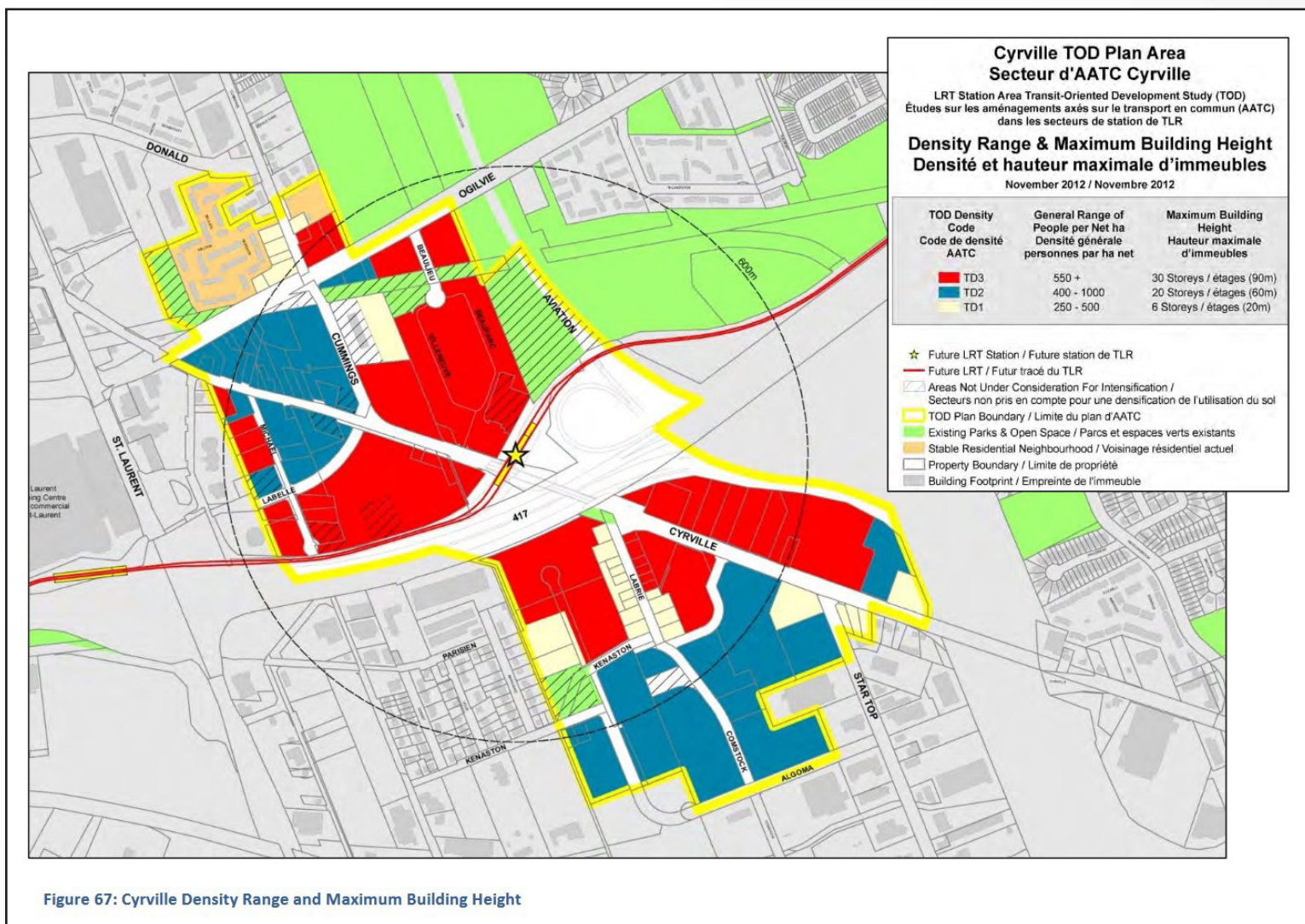


Figure 67: Cyrville Density Range and Maximum Building Height

The proposed zoning for the site is TD2 and the maximum height of the proposed buildings is 12 storeys conforming to the approved Cyrville TOD plan.

New City of Ottawa Official Plan (2021)

The subject lands are designated hub and evolving neighbourhood on Schedule B2 - Inner Urban Transect and Protected Major Transit Station Areas (PMTSA) on Schedule C1 - Protected Major Transit Station Areas (PMTSA).

PMTSAs permit a range of mid- and high-density housing types as well as a full range of non-residential functions and mandates minimum building heights and lot coverage requirements within PMTSAs as specified by the Inner East Lines 1 and 3 Secondary Plan.

The goals of the Inner Urban Transect are to enhance the urban pattern of built form, site design and prioritize walking, cycling and transit. The Hub designation provides focus for major residential and non-residential origins and destinations within easy walking access of rapid transit stations. Hubs also establish higher densities than surrounding areas conditional on an environment that prioritizes transit users, cyclists and pedestrians.

Hub policy encourages development within with the zone to:

- Place the highest density close to the transit station or stop so that transit is the most accessible means of mobility to the greatest number of people.
- Require minimum building heights in terms of number of storeys to ensure multi-storey structures where uses can be mixed vertically within the building.
- Shall establish safe, direct and easy-to-follow public routes for pedestrians and cyclists between transit stations and the proposed development.
- Create a high-quality, comfortable public realm that prioritizes active transportation.

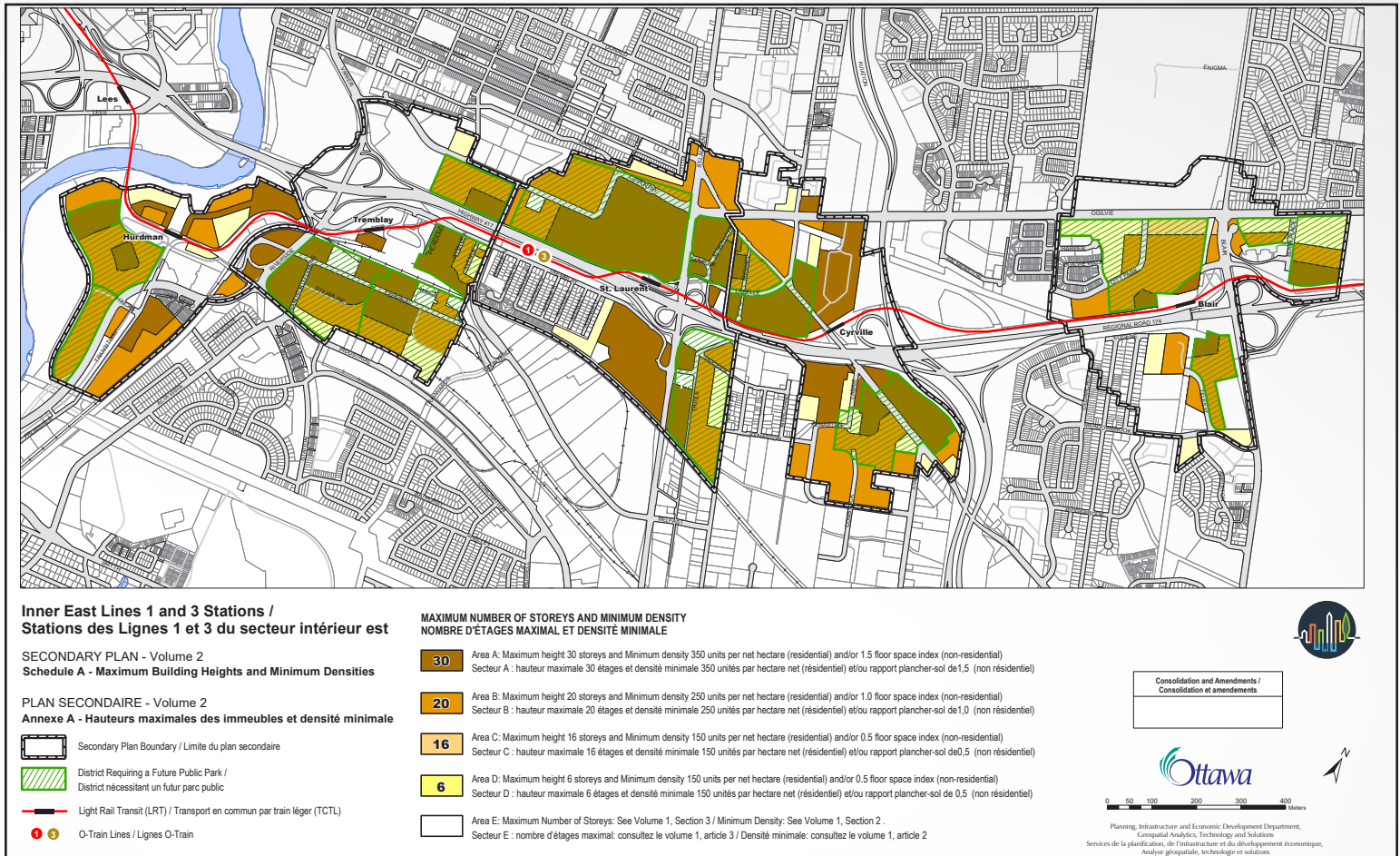
Buildings with a Hub designation should define and enhance the public realm through building placement, entrances, fenestration, signage and building facade design. Principal entrances should be situated to provide convenient pedestrian access to the transit station and the public realm.

Hub designation also seeks to place parking, loading, vehicle access, service entrances and similar facilities in locations that minimize their impact on the public realm.

Inner East Lines 1 and 3 Stations Secondary Plan (2021)

The Inner East Lines 1 and 3 Stations Secondary Plan establishes policy on maximum building heights and minimum densities within the Cyrville area. These building heights and densities are identified on Schedule A - Maximum Building Heights and Minimum Densities.

Schedule A shows the subject lands as part of Area B: Maximum height 20 storeys and Minimum density 250 units per net hectare (residential) and/or 1.0 floor space index (non-residential).



Bill 23, More Homes Built Faster Act, 2022

Bill 23 impacts the proposed zoning and site plan applications in several ways.

Zoning: The City of Ottawa is now required, by the Act, to implement “as-of-right” zoning for transit supportive densities in specified areas around transit stations such as the proposed TD2 zone for 1125 Cyrville Road. The Act requires the City of Ottawa to update their zoning by-laws by November 5, 2023. The proposed zoning for the development of 1125 Cyrville is ready for approval by the City of Ottawa.

Site Plan: Site Plan approvals have changed as the Act **does not allow** the City of Ottawa to regulate **architectural details and landscape design**.

Parkland Charges: The Act has changed such that the maximum amount that can be required for higher density developments would be updated to:

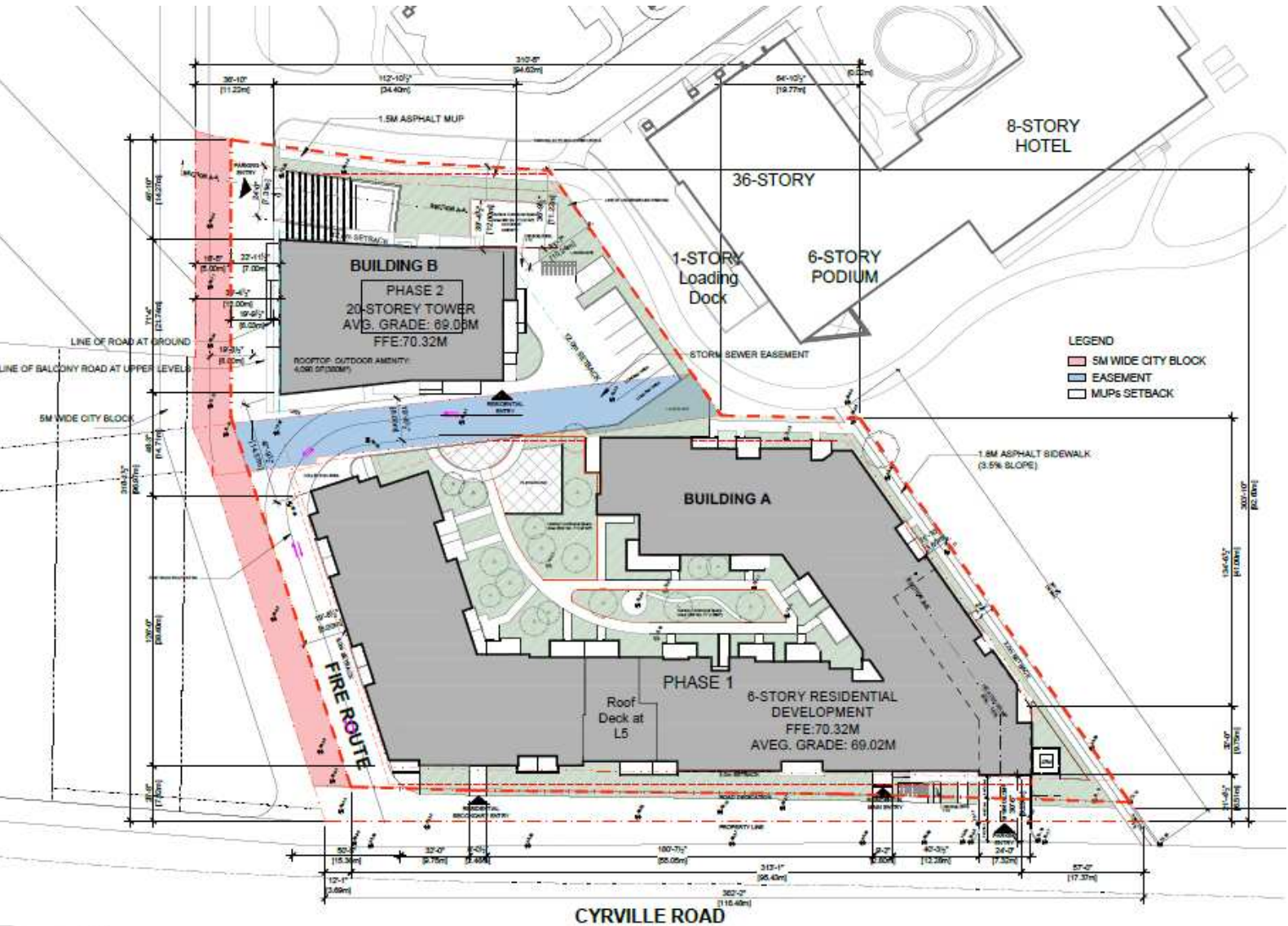
For the purposes of land conveyed, from the current rate of one hectare for each 300 dwelling units to one hectare for each 600 dwelling units; and

For the purposes of cash payment in lieu of land, from the current rate of one hectare for each 500 dwelling units to one hectare for each 1000 dwelling units.

No more than 10 per cent of the amount of developable land (or equivalent value) could be required for parks or other recreational purposes for 1125 Cyrville Road.

To achieve the development envisioned by the CDP and Secondary Plan will require the submission of two planning applications, a Site Plan and Zoning By-law Amendment.

Site Plan



Site Plan



The site plan has been prepared to respond with the following design intent:

- Building masses have been prudently placed to define the street edge, create safe public and private spaces.
- Creating unique building character by introduction of contemporary architectural expression and attractive landscaped courtyards.
- All parking facilities are located within underground parkade to minimize their impact on the public realm.
- Outdoor amenity areas will be provided in the landscaped courtyard and roof deck open space
- Respecting the character of existing areas and future development urban fabric to integrate this development into the surrounding context by similar architectural articulation.
- provide 3m setback along Cyrville Road to create active frontage street as public realm

A full design brief is submitted as an Annex to this rationale.

Zoning Bylaw Amendment

Consistent with the Inner East Lines 1 and 3 Stations Secondary Plan the ZBLA would rezone the lands from Mixed-Use Centre Zone - MC [1957] F (1.1) H (15) to the following zoning classification:

- **Transit Oriented Development Zone - TD2 Subzone**
- **With an amendment to allow for 7 metre setback above the 6th floor for building B**

The required setback above the 6th floor is 12 metres however in this situation a 5 metre mup will separate the abutting property. Therefore, the true separation is 12 metres and in context of the proposed development with there being no development to the west of the site the 7 metre setback is appropriate and will have no impacts on abutting properties.

Included as part of the approvals process is an overview of the public consultation process to ensure the development process is transparent and informative for the community, and the larger public.

Number of Pre-Application Consultation meetings took place with the following City staff:

- William Curry
- Shoma Murshid
- Randolph Wang

After the filing of the necessary development applications the following public consultation is planned:

- Contact with the Ward Councillor to introduce the project.
- An email to the owners of abutting properties outlining the project.

The Public Meeting will occur at Planning Committee when the application is heard and interested parties can provide their input on the merits of the proposed development. The Planning Committee's recommendations then would be considered by City Council.

Conclusions

Based on the information presented throughout this Planning Rationale, it is my opinion that the proposed site plan and Zoning By-law Amendment applications are appropriate when evaluated against the applicable Official Plan and CDP policies as they relate to the subject lands.

In conclusion, the zoning proposed confirms to both the 2014 TRANSIT-ORIENTED DEVELOPMENT (TOD) Plans for Cyrville and the follow on Inner East Lines 1 and 3 Stations Secondary Plan and represents good urban development.

Respectfully Submitted,

Peter Hume

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