



Stormwater Management Report and Servicing Brief

New Two-Storey Prefabricated Building for
Dog Kennel and Personal Residence

5969 Ottawa St.
Richmond, Ontario

Prepared for:

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Richmond, Ontario
K0A 2Z0

Attention: Laurie Roberts

LRL File No.: 210341

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1 INTRODUCTION AND SITE DESCRIPTION

LRL Associates Ltd. was retained by Al Roberts to complete a Storm Water Analysis and Servicing Brief for the construction of a proposed new two (2) storey prefab building and a paved parking, located in 5969 Ottawa street, Richmond. The site is currently vacant land part of zoning RG3[385r]. The building will be used as a dog kennel as well as the caretaker's residence. The site location is shown in Figure 1.



Figure 1: Aerial view of proposed site

The subject site has an approximate frontage of 305 meters and has an approximate area of 2.22 hectares. The site is divided by a natural flow of water that runs from the front of the property to the back. Since a large portion of the property exists within floodplain boundaries, the client has planned the construction in a small portion of the site that exists out of reach of the floodplain as depicted in Figure 2.

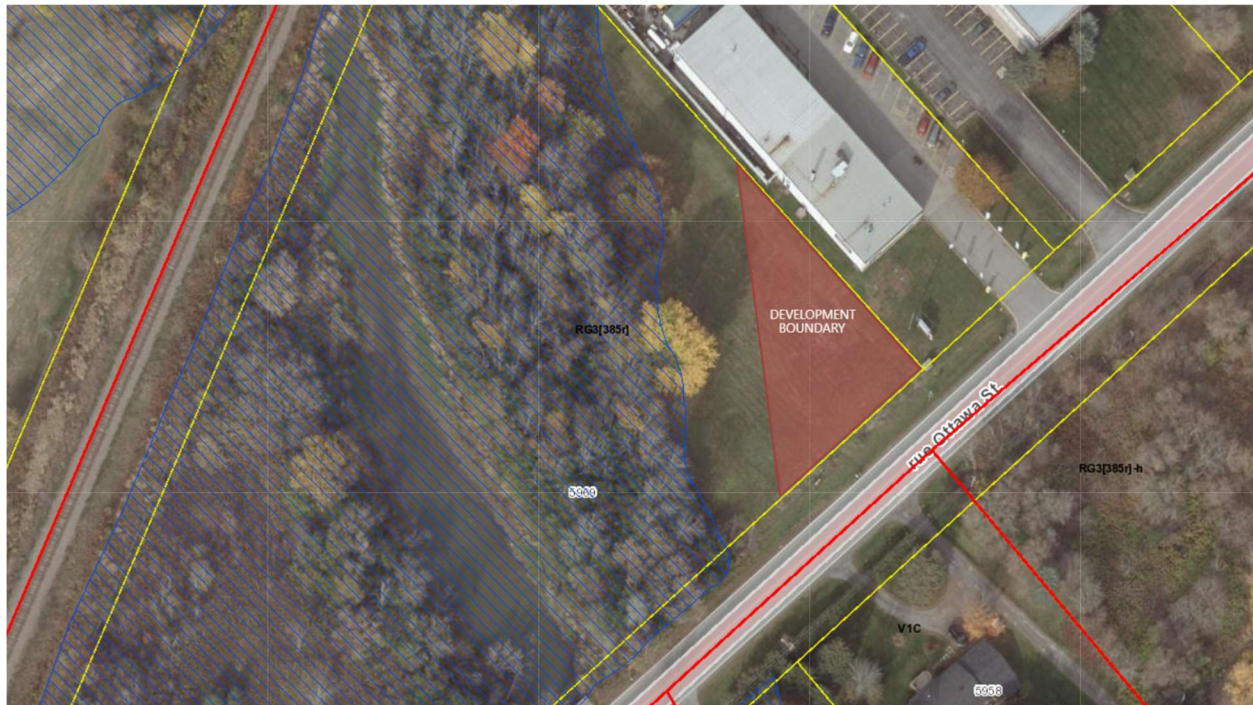


Figure 2: Aerial view of proposed development boundary

2 EXISTING SITE AND DRAINAGE DESCRIPTION

The site is 2.22 hectares in area and is currently vacant land occupied by a forested area and divided down the middle by Marlborough Creek. It is bordered by Ottawa Street to the south-east, by a railroad track to the north-west and by an automotive repair shop to the east.

There is no municipal servicing on the neighboring road. There are no sanitary lines, storm, or water pipes. There is a roadside ditch along Ottawa Street and another that runs parallel to the railroad track. There are also no private wells or septic tanks on the existing property.

3 PROPOSED DEVELOPMENT

Once developed, the site will consist of a 453 m² two-storey building to be used as a dog kennel and caretaker's residence, a paved driveway, and a parking area consisting of 6 parking spots.

The west side of the driveway and parking area is to incorporate an enhanced grass swale which leads to a stormwater detention area designed to treat collected runoff from the proposed driveway and parking area. The treated runoff is controlled and discharged to the existing roadside ditch along Ottawa St. at the southern end of the site.

Currently, the site does not include any septic system and requires one to be installed.

A private supply well already located on the site has been decommissioned and a new supply well has been proposed at the rear end of the building. A sample from this supply well was collected and tested on May 29, 2023, and proven to be adequate for the quality and demand required by the proposed development.

4 FIELD WORK

A topographic survey of the property was completed by H.A.Ken Shipman Surveying Ltd. (Ontario Land Surveyors) on July 5th, 2021. Prior to proposed site development the contractor completing the work is to confirm the location and elevation of the benchmark utilized and noted on the plans in **Appendix D**.

A hydrogeological assessment and terrain analysis study was performed by LRL Associates Ltd. Test pits were dug on July 20, 2021 to determine the general upper soil and groundwater conditions to depths of -2.75m & -3.0m; piezometers were installed in each test pit and found to be dry at the time of sampling on August 11th, 2021.

A geotechnical investigation was also performed by LRL Associates Ltd. on August 23rd, 2021; four (4) boreholes were drilled, and groundwater was carefully monitored and measured during the investigation. Immediately upon completion of drilling, groundwater was measured in all boreholes and was found to be dry. Test Pits were again dug on March 15, 2024 and stand pipes were installed to determine seasonally high groundwater elevation. Measurements taken on April 1st and April 5th determined groundwater at a depth of -1.3m and -0.75m from the existing ground.

5 STORMWATER MANAGEMENT

5.1 Existing Stormwater Infrastructure

As previously mentioned, there already exists two (2) ditches along the property boundary. The first runs parallel to the railroad track and the second runs along the frontage of the development boundary parallel to Ottawa Street. Apart from these ditches, no other stormwater infrastructure exists on site.

5.2 Design Criteria

- As per consultation minutes with the City of Ottawa officials, the required TSS removal is set by the Rideau Valley Conservation Authority (RVCA) and the stormwater management design considers this value to be at least 80%.
- The peak flow rate post-development must match the peak flow rate pre-development as per section 8.3.6.1 of the City of Ottawa Sewer Design Guidelines (SDG).

5.3 Method of Analysis

The stormwater management criteria for this development are based on pre-consultation correspondence with the City of Ottawa and the Rideau Valley Conservation Authority, refer to **Appendix A**, as well as the Ministry of the Environment's Stormwater Planning and Design Manual, 2003 (SWMP Manual). Stormwater objectives were identified as follows:

- Pre-development and Post-development hydrological conditions are to be equal.
- Target 80% TSS removal is to be met.



5.3.1 Water Quality

It was determined that 80% TSS removal will be required for all contaminated runoff. To provide the runoff water quality control for this site, an oil-grit (sediment) separator is proposed to provide enhanced (80% minimum) TSS removal. A Stormceptor EF04 is proposed and will be located downstream of the proposed stormwater detention area. See **Appendix B** for the selection, the type, and for more information on the treatment unit.

Moreover, a low-impact development (LID) approach is also proposed for stormwater treatment. The LID Guidelines provide recommendations for enhanced grass swale that were taken into consideration, such as:

- 1) Enhanced swale side slopes are maintained as flat as possible with maximum sloping of 3:1 to provide pre-treatment and maximize swale filtering surface.
- 2) Proposed enhanced grass swales are designed at low slopes to maintain flow at a maximum velocity of 0.5 m/s, refer to **Appendix B** for enhanced grass swale design sheet.

The combined approach described above is believed to provide treatment and promote ground infiltration. Refer to **Appendix B** for more details on this LID measure.

The remaining areas of the site include uncontrolled grass areas following existing drainage paths to the existing ditch to the south along Ottawa St and to Marlborough creek to the west. Runoff generated from these areas is determined to be clean water and will not require further treatment.

5.3.2 Water Quantity

To meet the stormwater quantity control criteria, post-development release rates are required to be controlled to pre-development release rates, providing all storms up to and including 100-year design event on-site storage as required.

The proposed development, consisting of a 2-storey prefabricated structure and paved parking lot and driveway will result in an increase in the impervious surfaces, therefore quantity control measures will be implemented.

The allowable release rate, to mimic pre-development conditions, was calculated using a runoff coefficient of $C=0.30$ (determined based on pre-development land cover). The calculation of the pre-development run off coefficient is summarized in **Appendix C**. The 100-year and 5-year post-development flows will be controlled to the respective pre-development levels. To do so, an inlet control device is proposed at the outlet of a catch basin inlet structure in the stormwater detention area. Events greater than the 100-year storm will flow overland to the existing roadside ditch. Table 1 summarizes the target stormwater release rate.

Table 1: Summary of Stormwater Quantity Control Requirements

Quantity Control Parameters	5 Year Post-Development	100 Year Post-Development
Pre-Development Storm Event	5 Year	100 Year
Calculated Allowable Release Rate (L/s)	28.24	48.40

5.4 Allowable Release Rate

The 100-year and 5-year allowable release rates from the site to the roadside ditch along Ottawa Street are **48.40 L/s** and **28.24 L/s**, respectively. The release rates were calculated based on the entire watershed area of **0.325 ha**, pre-development runoff coefficient of **0.30**, and the time of concentration (T_c) 10 min. The release rates provided will be the maximum rates to which the entire site will be controlled up to during the major storm events.

5.5 Proposed Stormwater Quantity Controls

The proposed stormwater management quantity controls for this development will be accomplished by means of an Inlet Control Device (ICD) and detention in grass swales and retention areas providing the required storage volume in high-flow events.

Table 2 below summarizes the drainage areas on site in post-development conditions. Further details of the pre and post-development catchments are included in **Appendix C**.

Table 2: Stormwater Drainage Areas

Drainage Area	Area (ha)	Runoff Coefficient (C)
WS-01 (un-controlled)	0.089	0.20
WS-02 (un-controlled)	0.105	0.35
WS-03 (controlled)	0.131	0.62

The site was divided into three watersheds; two of which are uncontrolled and follow existing topography. One of these uncontrolled areas is WS-01 (0.089 ha) which consists of the grassed area at the rear of the proposed building as well as the grassed area to the west of the enhanced grass swale and retention features, this area follows existing topography and drains to Marlborough creek. The other uncontrolled watershed is WS-02 (0.105 ha) which consists of the grassed area to the east and front of the proposed building as well as half of the building rooftop drainage. This grass area drains to the existing ditch along Ottawa Street, and the half of the building rooftop will drain to the front grassed section and into the proposed swale along the East boundary which will be conveyed into the existing road-side ditch, and hence not allowing any water runoff to the neighboring property. WS-03 (0.131 ha) consists of half the proposed building rooftop and asphalt parking/driveway area which will sheet drain to the west overland towards the enhanced grass swale and retention area at the southern end of the site before reaching the existing ditch along Ottawa Street. It also consists of the flow from the rear of the property and stormwater runoff from the dog run. Any stormwater runoff from the adjacent property will be conveyed into the proposed swale on the East of the property and will eventually drain into the existing ditch along Ottawa Street.

Table 3 summarizes the release rates and storage volumes required to meet the allowable release rates for the 100-year, 5-year storm events.

Table 3: Stormwater Release Rate & Storage Volume Summary

Description	Site Area (ha)	Runoff			
		100 Year Release Rate (L/s)	100 Year Total Storage Required (m ³)	5 Year Release Rate (L/s)	5 Year Total Storage Required (m ³)
Controlled Area	0.131	12.40	35.75	12.40	11.49
Uncontrolled Area	0.194	33.92	0	15.84	0
Totals	0.325	46.32	35.75	28.24	11.49

The detailed release rate and storage calculations can be found in **Appendix C**.

The proposed stormwater quantity control measures will be achieved by using an 91 mm diameter orifice plate ICD to be installed on the proposed outlet of the catch basin inlet structure at the south end of the stormwater detention area. The ICD will serve to control flow rates to below the allowable release rates for the 100-year and 5-year storm events. From the ICD, stormwater will be conveyed through the proposed stormwater treatment unit to the existing road-side ditch, and ultimately, to Marlborough Creek. Greater detail on the proposed ICD is included in **Appendix C**.

Table 4 compares the controlled flow rates, allowable release rates, and provided storage for all storm events.

Table 4: Comparison of Allowable Release Rates & Controlled Flow Rates

	100 Year Post Development	5 Year Post Development
Allowable Release Rate from Controlled Area (L/s)	14.48	12.40
Controlled Flow Rate Utilizing ICD (L/s)	12.40	12.40
Total Provided Storage (m ³)	36.50	

The stormwater runoff exceeding the allowable release rates will be stored on-site via the stormwater detention area. See LRL drawing C.601 included in **Appendix D**.

6 WATER SUPPLY

The supply well initially installed by the client in June 2021 was decommissioned; and a new supply well was installed by the client in May 2023 and tested by LRL Associates Ltd.

The anticipated water demand for the site is 0.132 L/s (average daily), 0.197 L/s (maximum daily), and 0.355 L/s (maximum hourly), assuming light industrial water consumption rates, daily peaking

factor of 1.5 and hourly peaking factor of 1.8. Please refer to **Appendix E** for the water service sizing details.

The revised hydrogeological assessment and terrain analysis report (March 26, 2024) states that the intended use of the new supply well meets the quantity demands of the proposed development, however, also recommends considering treatment options for the quality of the water for the parameters exceeding the ODWS and D-5-5 guidelines outlined in the study report. Refer to drawing C.301 included in **Appendix D** for the location of the proposed new supply well.

7 SANITARY SERVICE

As previously discussed, currently, there are no municipal services available in this area. A proposed Class IV septic system designed by Green Valley Environmental Engineering (GVE) will service the proposed development. A copy of the septic design is included in **Appendix F**. The septic tank shall be cleaned whenever sludge and scum occupy one-third of the tank capacity.

8 EROSION AND SEDIMENT CONTROL

During construction, erosion and sediment controls will be provided via a sediment control fence to be erected along the perimeter of the site where runoff has the potential of leaving the site. Construction and maintenance requirements for erosion and sediment controls are to comply with Ontario Provincial Standard Specification OPSS.MUNI 805. Refer to Erosion and Sediment Control Plan C101 in **Appendix D** for additional details.

9 MAINTENANCE

Maintenance is the key issue for all types of stormwater management practices. It ensures performance efficiency of the facilities and prevents undesirable consequences such as flooding or events leading to contamination to the neighboring properties.

The proposed grass swale maintenance would consist of inspecting the swale and outlet on a periodic basis as well as routine cleaning to remove sedimentation build up as deemed necessary. It is the responsibility of the owner to maintain and clean the retention area outlet and keep a log of all the maintenance activities.

The proposed quality control Stormceptor (oil and grit separator) maintenance would consist of inspecting the structure (inlet, outlet, cover) on a periodic basis as well as routine cleaning to remove sedimentation buildup as deemed necessary. It is the responsibility of the Owner to maintain and clean the treatment unit and keep a log of all the maintenance activities.

The proposed septic tank will require regular maintenance including scheduled cleaning to ensure that the sludge and scum does not occupy more than one-third of the working capacity of the tank.

10 CONCLUSION

This stormwater management and servicing report for the proposed development at 5969 Ottawa St. presents the rationale and details for the design requirements for the subject property. In

accordance with the report objectives, the servicing requirements for the development are summarized below.

Stormwater Quantity

- The site will include an outlet to the existing roadside ditch along Ottawa St. located at the southern end of the site and adheres to the following quantity control measures:
 - The post-development flows for the 5-year and 100-year storm events will be less than or equal to pre-development release rates, respectively.
 - Stormwater quantity control rates will be met with an 91mm diameter ICD orifice to limit and control the flow leaving the stormwater retention area.

Stormwater Quality

- The Stormwater quality control objectives of 80% TSS removal will be met with the installation of Stormceptor EF04 (or approved equivalent) oil and grit separator at the outlet of the stormwater detention area.
- The site design also uses low-impact development measure on the controlled watershed area and; uncontrolled watersheds are grassed areas and considered clean water.

Domestic Water

- The proposed new supply well is anticipated to adequately service the proposed dog kennel / residence.

Sanitary

- A new septic system has been designed by GVE Engineering to accommodate the sanitary discharge from the proposed dog kennel / residence.



11 REPORT CONDITIONS AND LIMITATIONS

The report conclusions are applicable only to this specific project described in the preceding pages. Any changes, modifications or additions will require a subsequent review by LRL Associates Ltd. to ensure the compatibility with the recommendations contained in this document.

In addition, this report has been prepared in consideration of the terms and conditions noted above and with the civil drawings prepared for the new development. Should there be any changes in the design features, which may relate to the stormwater considerations, LRL Associates Ltd. should be advised to review the report recommendations.

If you have any questions or comments, please contact the undersigned.

Prepared by:

LRL Associates Ltd.

Sarthak Vora

Sarthak Vora, EIT
Civil E.I.T.



Mohan Basnet, P.Eng
Civil Engineer

Appendices

Appendix A

Pre-consultation Meeting Minutes

Site Plan Pre-consultation

Meeting Date: March 18, 2021

5969 Ottawa Street

Applicant/Owner: Al Roberts

Ward- Councillor: 21 Richmond- Scott Moffatt

Proposal Summary: Construct a 446 square metre two-storey building with a kennel, personal shop, and caretaker's dwelling.

Attendees:

- Al Roberts, Applicant
- Laurie Roberts, Applicant
- Cheryl McWilliams, File lead, PIEDD, City of Ottawa
- Damien Whittaker, Project Manager, PIEDD, City of Ottawa
- Christine Reist, Infrastructure Approvals, PIEDD, City of Ottawa
- Matthew Hayley, Environmental Planner, PIEDD, City of Ottawa
- Samantha Willock, Co-op Planning Student, PIEED, City of Ottawa
- Mike Giampa, Senior Transportation Engineer, PIEDD, City of Ottawa (regrets)
- Eric Lalonde, Environmental Planner, Rideau Valley Conservation Authority (regrets)

Meeting Minutes

Proposal details

- Proposed kennel for 3-5 dogs for service dog training.
- Proposed two-storey prefab building for various uses; first floor kennel and personal shop, second storey caretaker's residence.
- One enclosed dog run at the rear of the building.

Planning Comments (Provided by Cheryl McWilliams)

- City staff recommend hiring a consulting firm with engineering, environmental and planning staff who can assist you through this process and prepare the plans and reports required.
- A **site plan** is required, illustrating the proposed building, septic, well, fencing, landscaping, parking and the distance from the dog run to the nearest residence. This plan must show the compliance to the [Zoning By-law](#) in a table format- noting the requirement (e.g. setbacks and parking) with a column for requirement and a column for your actual proposed.
- The Zoning By-law requires a kennel to be set back 100 metres from the nearest house for 4 or less dog runs and 210 metres for more than 4 runs.
- The proposed uses will require 4 parking spaces- 1 space for less than 4 dog runs, 1 space for the caretaker unit, and 2 spaces for the shop bay.
- The Zoning setback required for the rail line is a minimum of 30 metres
- A minimum setback of 30 metres is required from Marlborough Creek.
- Landscaping along Ottawa Street must be shown on the site plan- include trees as is possible, shrubs and other perennials, and potentially other features such as stone fence or post and rail fence as decorative elements.
- Provide **building elevation drawings** (colours, materials, etc). Given your building has been ordered and there is very limited opportunity to ensure that the building meets the intent of the new building design guidelines for the Village of Richmond (expanded on below), and there may be limited space for trees, adding in decorative elements may assist in meeting the intent of the buildings guidelines for Richmond. Alternatively, consider faux architectural details on the front of the building- as you indicated there will be no windows. The Community Design Plan (CDP) contains some examples of heritage styles and potential façade changes to existing more modern buildings to be more in keeping with the village character.
- From the [Village of Richmond Community Design Plan \(ottawa.ca\)](#): "Guidelines 1. New buildings and additions should be of their own time and should harmonize with the existing heritage and rural character in the Village. They should:

Prepared by Samantha Willock
Date: March 25, 2021

- a. Use design elements inspired by buildings of heritage interest in the Village of Richmond or by buildings and landscape features in other villages and towns in Eastern Ontario as reflected in Section 7.3.1
- b. Reflect the existing colours and materials in the village such as clay brick, grey stone/blocks or wood siding or use high-quality, modern and colourful materials that complement the village character".
- Please demonstrate that you have access for equipment (such as drilling) to the well in the rear of the building.
- Please ensure that the kennel run fenced area does not include the well, nor any part of the existing treed area.
- Fence off the floodplain and show it on the plans (including the details of the type of fence, height, etc). It is suggested to use a tighter weave for the bottom 60cm of the fence to protect blanding's turtles from the dogs.
- A **Planning rationale** will be required, which is essentially a cover letter noting how you will comply to zoning, the Richmond CDP and their village heritage feel, and the engineering, transportation and environmental issues. This does not have to go into depth, this report is essentially an overview that can be written by the owner.
- The City suggests contacting the Councillor's office for his information on your proposal.

Environmental Comments (Provided by Matthew Hayley)

- Next to Marlborough Creek provides important habitat including for the Blanding's turtle.
- A Blanding's turtle sighted within 2km- meaning there is regulated habitat on the property along Marlborough Creek, defined as 30m from the edge of the watercourse and then an additional 200m under the Endangered Species Act and Ministry of Environment, Conservation and Parks (MECP). See attached for the discussion on what is considered Blanding's turtle habitat.
- The presence of Blanding's turtle habitat triggers the requirement for an **Environmental Impact Statement (EIS)** to demonstrate that your application will not have a negative impact on the Blanding's habitat. Since it is regulated habitat, MECP needs to sign off on the EIS or the Province may require a permit under the Endangered Species Act. Since the category 3 blanding's turtle habitat is meant for allowing turtles to travel to other areas of habitat, it is feasible to have a development approved in the area outside of the 30m from the watercourse with mitigations since your site is not functioning in that manner.
- The EIS will need to be scoped to address species at risk and include consultation/ signoff with the MECP.
- The attached Excel file lists EIS consultants who provide service in Ottawa. The list is not an approved list, it is collected to provide a list of professionals who provide the service.

Transportation Engineering Comments (Provided by Mike Giampa)

- You need to comply with the Private Approach By-law with a maximum entrance width of 9 metres.
- Please show all required parking and loading spaces on the **site plan**.
- A **stationary noise study** will be required for the kennel- if only one dog run is proposed then it is required if you are looking to have the kennel run located closer than 100 metres from the nearest dwelling.
- A **noise and vibration study** is required for the residential use in proximity to the rail corridor.

Engineering Comments (Provided by Damien Whittaker and Christine Reist)

- A **Survey monument** will be required, shown and annotated, with sufficient information provided to enable a layperson to locate it.
- There are no water pipes in the direct area. Municipal water could be extended to connect to the address, though this could be costly. Alternatively, a drilled well could be used, though re-testing would be required. It will need to be demonstrated that there is access outside of the floodplain for the proposed well.
- There are no sanitary pipes in the direct area and the sanitary pump station is at capacity presently. A **septic design** will be required- with investigation of the greatest groundwater elevation and percolation results determined.
- Please note that sensitive marine clays are anticipated in the area of the proposal and, if so, enhanced **geotechnical investigation and analysis** will be necessary. Investigation of clays should be undertaken with vane shear, Atterberg limits, shrinkage, size, grade raise restriction, consolidation, sensitivity, and liquefaction analysis, amongst others.
- Note that there are considerations for trees in proximity to foundations in sensitive marine clays. In sensitive marine clays, trees in proximity to foundations can cause foundation damage.
- As the application is near the watercourse, slope stability may be a concern; please have a consultant provide **slope stability analysis** or state (with rationale) that it is not required.
- A **hydrogeological report** will be required for the well and for septic (with comprehensive discussion of dog wastes). Enhanced discussion is required as there are suspected thin soils in the area. Well water may be released under artesian pressure and discussion of control will be required if that is the case.
- There are no storm sewers in the Right-of-Way (ROW). If it is proposed to discharge storm water to the existing ditches in the ROW, the ditches will need to be shown to be continuous. If a direct discharge to the watercourse (that has been recorded at least once as Marlborough Creek (or the Richmond By-pass drain)) is proposed, it

would require a direct type of Environmental Compliance Approval (ECA) application submission to the MECP- and the approval timeline is anticipated to be 6-12 months.

- Groundwater is anticipated to be high and the level is to be derived from long-term analysis, with the high groundwater anticipated, the City advises against basements for the development. An **annual groundwater elevation**, from a long-term study will be required.
- There is an existing rail corridor adjacent to the property. Prior to approval of the site plan application, consultation with the railway line owner/operator is required to obtain their approval of the rail safety design.
- A **noise and vibration study** is required to look at the impact from the adjacent rail corridor on the proposed development. In addition, if there are outdoor kennel runs proposed within 100m of a residence, the noise study will also need to include a discussion of the impacts from the proposed kennel runs on the neighbouring residence.
- A **rail safety barrier** will be required in accordance with the study prepared, jointly, by the Railway Association of Canada (RAC) and the Federation of Canadian Municipalities (FCM) titled Guidelines for New Development in Proximity to Railway Operations, published in 2013. The barrier/berm cost is anticipated to be severe. Also, there will likely be challenges associated with the barrier/berm because a large portion of the property is within the floodplain. If floodplain capacity is reduced (e.g. by construction of a rail safety barrier/berm within the floodplain), floodplain compensation volume must be provided elsewhere, however, there is limited area available on the property to provide floodplain compensation. Note that any placing of material in the floodplain (e.g. a rail safety barrier/berm) is regulated by the Conservation Authority and requires approval from RVCA.
- **Stormwater management** quality criteria shall be set by the RVCA and is anticipated to be 80% TSS removal. Reporting of TSS removal shall be extensive and if peer reviewed and published papers are relied on for conclusions, the conclusions shall be patently clear and the report shall show overwhelming agreement.
- The stormwater management quantity criteria for the development is that the post development peak flow rate must match the pre-development peak flow rate as per section 8.3.6.1 of the SDG. The watercourse setback shall be determined (as amended), agreed upon, provided, marked out and shown on drawings.
- Schedule G of the current Official Plan shows Ottawa Street in the location under review to be a rural collector, and, as per annex 1 of the Official Plan a ROW of 26m is required. This appears to have been taken.
- Please refer to the City of Ottawa Private Approach By-law 2003-447 for the entrance design.

Comments provided by RVCA (Provided by Eric Lalande)

- The conservation authority has already issued a permit and has no issues with what was proposed.
 - Please note that any changes to the submission previously approved by the conservation authority may require additional review.
-

ADDITIONAL COMMENTS

Planning Comments

Official Plan: Village

Secondary Plan and/or Community Design Plan: Richmond Secondary Plan/ Village of Richmond CDP

Zoning By-law: Rural General Industrial Zone Subzone 3. Rural Exception 385 (RG3[385r])

1. Parking:
All parking must comply with Part 4 (Sections 100-114) of the Zoning By-law.

Site Plan Approval Process

[Key Steps in the Site Plan Control Approval Process](#)

- File all submissions (plans and reports) noted in the PDF list electronically to Cheryl McWilliams (cheryl.mcwilliams@ottawa.ca). Include the completed Site Plan application form. From there a file number will be assigned and instructions on how to pay will be shared. This is a rural small site plan application so only the fee itself is payable now. This is not subject to formal public consultation though the application is available once filed on the City website through [Search Results - Development Applications Search \(ottawa.ca\)](#).
- City staff will review the submission to deem it complete then circulate it to technical agencies, internal and external contacts (including the Councillor) when we have everything.
- At this point comments are provided to you with any changes needed to the plans and reports, which can then be resubmitted.

- Once all issues/ changes are completed, we prepare a Delegated Authority Report with conditions of approval for your and the Councillor's concurrence, which is then finalized.
- After concurrence, the site plan agreement is prepared by legal service. If works and costs are minimal (to be determined through the process) it may be possible to go through with a letter of undertaking instead that we as staff handle.
- Once the agreement is in place and other permits are obtained, we would issue the commence work for the site works and can then support a building permit being issued. Please note that any work completed in advance of this step is at your own risk and may require changes once the site plan is approved.

Costs

- City direction is that all development must pay for itself. In addition to the fees for plans and reports and the application fee required- once approved the following would be applicable.
 - Cash-in-lieu of parkland (if not already paid previously through planning application such as severance)- estimate 2% of the value of the land before site plan approval
 - Legal fees for preparing and registering the agreement
 - Engineering design review and inspection fee- based on 2% of the value of those on-site works.
- A cost estimate will be required for the exterior works on the site- things like fences, driveway, landscaping grading, ditching, etc. This excludes the building, well, and septic costs. We will then require security (based on 50% of the value of those site works) be provided to ensure the works are completed as approved. This security will be released back once the inspector is satisfied that they are complete.

Building

- Building permits are separate forms and fees, which can be found [here](#).
- Building permits can only be issued once the site plan agreement is registered and any other needed approvals are issued (ex. RVCA permits, MECP permits or stormwater approvals).

Engineering and Hydrogeology Comments:

Water and Sanitary:

- An oil and grit separator should be used where vehicles are maintained, cleaned, or potential for discharge of contaminants.

Storm Water Management:

- Water quality design requirements will be determined by the Rideau Valley Conservation Authority.
- All stormwater management determinations shall have supporting rationale.

Permits and Approvals:

- If a direct stormwater discharge to the watercourse is proposed, an Environmental Compliance Approval (ECA) will be required from the Ministry of the Environment, Conservation and Parks (MECP). Typically, approval timelines for direct submission to the MECP are between 9-11 months.
- Please contact the Ministry of the Environment, Conservation, and Parks (MECP) and Rideau Valley Conservation Authority (RVCA), amongst other federal and provincial departments/agencies, to identify all the necessary permits and approvals required to facilitate the development: responsibility rests with the developer and their consultant for obtaining all external agency approvals. The address shall be in good standing with all approval agencies. Copies of confirmation of correspondence will be required by the City of Ottawa from all approval agencies that a form of assent is given. No construction shall commence until after a commence work notification is given.

Ministry of the Environment, Conservation, and Parks	Rideau Valley Conservation Authority
Contact Information: Jena Leavoy, Senior Environmental Office 613-521-3450 ext.236 Jena.leavoy@ontario.ca	Contact Information: Eric Lalande Eric.lalande@rvca.ca

Plan submission requirements for engineering:

- Layout and Servicing plan
- Erosion and Sediment Control plan
- Grade Control and Drainage Plan

All identified required plans are to be submitted on standard A1 size sheets as per [City of Ottawa Guide to Preparing Studies and Plans](#) and shall note the survey monument used to establish datum on the plans with sufficient information to enable a layperson to locate the monument.

Report submission requirements for engineering:

- Servicing report
- Hydrogeological report
- Geotechnical report
- Slope Stability study (or adequate justification from an engineering consultant to support that slope stability isn't an issue on this site)
- Stormwater Management Brief
- Noise and Vibration study (for impact from adjacent rail corridor and a discussion of the impact from the proposed kennel runs on the neighbouring residence if within 100m of a residence).
- Rail Safety report.

To request City of Ottawa plan(s) or report information please contact the City of Ottawa Information Centre:

InformationCentre@ottawa.ca <<mailto:InformationCentre@ottawa.ca>> OR (613) 580-2424 ext. 44455

As per section 53 of the Professional Engineers Act, O. Reg 941/40, R.S.O. 1990, all documents prepared by engineers must be signed and dated on the seal.

Application Submission Information

Application Type: **Site Plan Control-Rural, Small**

For information on Zoning By-law Amendment Applications and Site Plan Control Applications, including fees, please visit: <https://ottawa.ca/en/city-hall/planning-and-development/information-developers/development-application-review-process/development-application-submission/fees-and-funding-programs/development-application-fees>

The application processing timeline generally depends on the quality of the submission. For more information on standard processing timelines, please visit: <https://ottawa.ca/en/city-hall/planning-and-development/information-developers/development-application-review-process/development-application-submission/development-application-forms#site-plan-control>

Prior to submitting a formal application, it is recommended that you pre-consult with the Ward Councillor.

Application Submission Requirements

For information on the preparation of Studies and Plans and the City's requirements, please visit: <https://ottawa.ca/en/city-hall/planning-and-development/information-developers/development-application-review-process/development-application-submission/guide-preparing-studies-and-plans>

Please provide electronic copy (PDF) of all plans and studies required.

All plans and drawings must be produced on A1-sized paper and folded to 21.6 cm x 27.9 cm (8½" x 11").

Note that many of the plans and studies collected with this application must be signed, sealed and dated by a qualified engineer, architect, surveyor, planner or designated specialist.

Appendix B

Stormwater Quality Control

Imbrium® Systems

ESTIMATED NET ANNUAL SEDIMENT (TSS) LOAD REDUCTION

04/17/2024

Province:	Ontario
City:	Ottawa
Nearest Rainfall Station:	OTTAWA CDA RCS
Climate Station Id:	6105978
Years of Rainfall Data:	20

Project Name:	5969 Ottawa St.
Project Number:	210341
Designer Name:	Jessica Steffler
Designer Company:	Forterra Pipe & Precast
Designer Email:	jessica.steffler@RinkerPipe.com
Designer Phone:	519-239-6958
EOR Name:	Sarthak Vora
EOR Company:	LRL Engineering
EOR Email:	svora@lrl.ca
EOR Phone:	613-915-7633

Site Name:	5959 Ottawa Street, Ottawa
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Drainage Area (ha):	0.11
Runoff Coefficient 'c':	0.69

Particle Size Distribution:	Fine
Target TSS Removal (%):	80.0

Required Water Quality Runoff Volume Capture (%):	90.00
Estimated Water Quality Flow Rate (L/s):	2.45
Oil / Fuel Spill Risk Site?	Yes
Upstream Flow Control?	Yes
Upstream Orifice Control Flow Rate to Stormceptor (L/s):	11.00
Peak Conveyance (maximum) Flow Rate (L/s):	
Influent TSS Concentration (mg/L):	200
Estimated Average Annual Sediment Load (kg/yr):	93
Estimated Average Annual Sediment Volume (L/yr):	76

Net Annual Sediment (TSS) Load Reduction Sizing Summary	
Stormceptor Model	TSS Removal Provided (%)
EFO4	98
EFO6	100
EFO8	100
EFO10	100
EFO12	100

Recommended Stormceptor EFO Model: EFO4
Estimated Net Annual Sediment (TSS) Load Reduction (%): 98
Water Quality Runoff Volume Capture (%): > 90



Stormceptor® **EF** Sizing Report

THIRD-PARTY TESTING AND VERIFICATION

► **Stormceptor® EF and Stormceptor® EFO** are the latest evolutions in the Stormceptor® oil-grit separator (OGS) technology series, and are designed to remove a wide variety of pollutants from stormwater and snowmelt runoff. These technologies have been third-party tested in accordance with the Canadian ETV **Procedure for Laboratory Testing of Oil-Grit Separators** and performance has been third-party verified in accordance with the **ISO 14034 Environmental Technology Verification (ETV)** protocol.

PERFORMANCE

► **Stormceptor® EF and EFO** remove stormwater pollutants through gravity separation and floatation, and feature a patent-pending design that generates positive removal of total suspended solids (TSS) throughout each storm event, including high-intensity storms. Captured pollutants include sediment, free oils, and sediment-bound pollutants such as nutrients, heavy metals, and petroleum hydrocarbons. Stormceptor is sized to remove a high level of TSS from the frequent rainfall events that contribute the vast majority of annual runoff volume and pollutant load. The technology incorporates an internal bypass to convey excessive stormwater flows from high-intensity storms through the device without resuspension and washout (scour) of previously captured pollutants. Proper routine maintenance ensures high pollutant removal performance and protection of downstream waterways.

PARTICLE SIZE DISTRIBUTION (PSD)

► The **Canadian ETV PSD** shown in the table below was used, or in part, for this sizing. This is the identical PSD that is referenced in the Canadian ETV **Procedure for Laboratory Testing of Oil-Grit Separators** for both sediment removal testing and scour testing. The Canadian ETV PSD contains a wide range of particle sizes in the sand and silt fractions, and is considered reasonably representative of the particle size fractions found in typical urban stormwater runoff.

Particle Size (µm)	Percent Less Than	Particle Size Fraction (µm)	Percent
1000	100	500-1000	5
500	95	250-500	5
250	90	150-250	15
150	75	100-150	15
100	60	75-100	10
75	50	50-75	5
50	45	20-50	10
20	35	8-20	15
8	20	5-8	10
5	10	2-5	5
2	5	<2	5



Stormceptor® EF Sizing Report

Upstream Flow Controlled Results

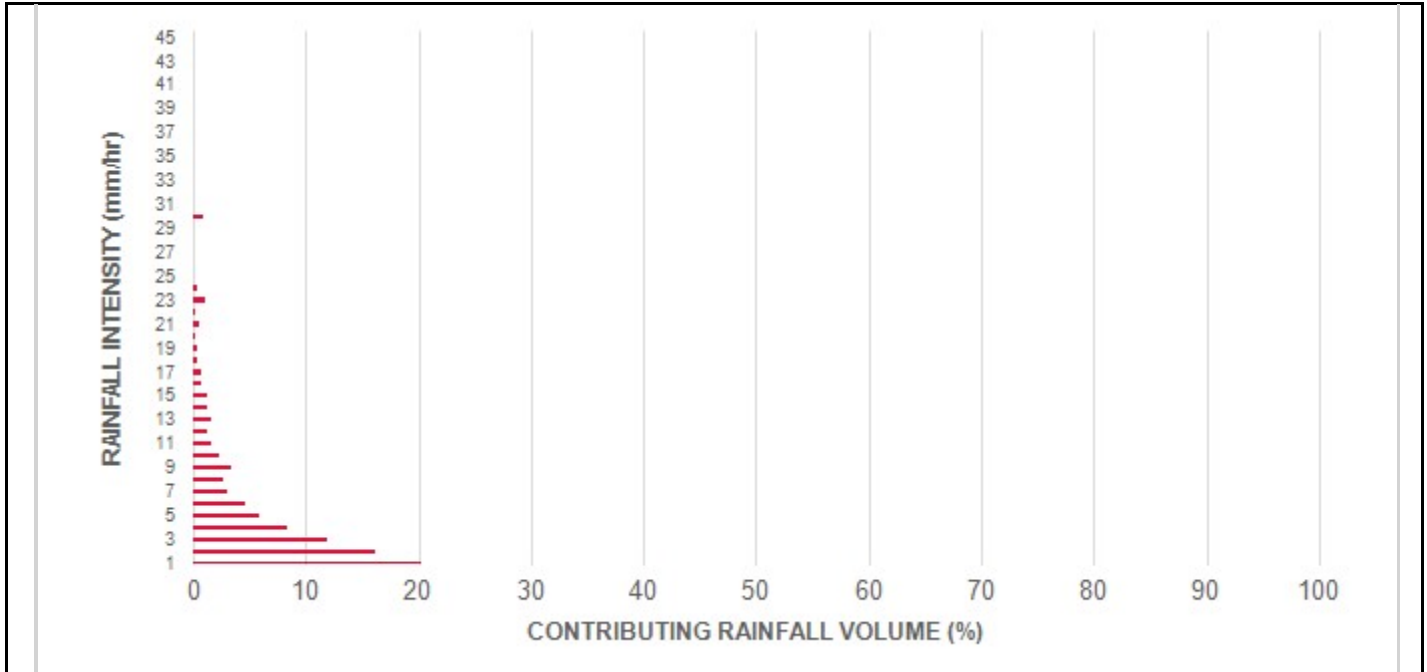
Rainfall Intensity (mm / hr)	Percent Rainfall Volume (%)	Cumulative Rainfall Volume (%)	Flow Rate (L/s)	Flow Rate (L/min)	Surface Loading Rate (L/min/m ²)	Removal Efficiency (%)	Incremental Removal (%)	Cumulative Removal (%)
0.50	8.6	8.6	0.11	6.0	5.0	100	8.6	8.6
1.00	20.3	29.0	0.21	13.0	11.0	100	20.3	29.0
2.00	16.2	45.2	0.42	25.0	21.0	100	16.2	45.2
3.00	12.0	57.2	0.63	38.0	32.0	100	12.0	57.2
4.00	8.4	65.6	0.84	51.0	42.0	100	8.4	65.6
5.00	5.9	71.6	1.06	63.0	53.0	100	5.9	71.6
6.00	4.6	76.2	1.27	76.0	63.0	100	4.6	76.2
7.00	3.1	79.3	1.48	89.0	74.0	100	3.1	79.3
8.00	2.7	82.0	1.69	101.0	84.0	98	2.7	82.0
9.00	3.3	85.3	1.90	114.0	95.0	97	3.2	85.2
10.00	2.3	87.6	2.11	127.0	106.0	96	2.2	87.4
11.00	1.6	89.2	2.32	139.0	116.0	95	1.5	88.9
12.00	1.3	90.5	2.53	152.0	127.0	93	1.2	90.1
13.00	1.7	92.2	2.74	165.0	137.0	92	1.6	91.7
14.00	1.2	93.5	2.95	177.0	148.0	91	1.1	92.8
15.00	1.2	94.6	3.17	190.0	158.0	89	1.0	93.8
16.00	0.7	95.3	3.38	203.0	169.0	88	0.6	94.5
17.00	0.7	96.1	3.59	215.0	179.0	86	0.6	95.1
18.00	0.4	96.5	3.80	228.0	190.0	84	0.3	95.4
19.00	0.4	96.9	4.01	241.0	200.0	83	0.3	95.8
20.00	0.2	97.1	4.22	253.0	211.0	83	0.2	96.0
21.00	0.5	97.5	4.43	266.0	222.0	82	0.4	96.3
22.00	0.2	97.8	4.64	279.0	232.0	82	0.2	96.5
23.00	1.0	98.8	4.85	291.0	243.0	81	0.8	97.3
24.00	0.3	99.1	5.06	304.0	253.0	81	0.2	97.6
25.00	0.9	100.0	5.28	317.0	264.0	80	0.8	98.3
30.00	0.9	100.9	6.33	380.0	317.0	78	0.7	99.0
35.00	-0.9	100.0	7.39	443.0	369.0	75	N/A	98.3
40.00	0.0	100.0	8.44	506.0	422.0	73	0.0	98.3
45.00	0.0	100.0	9.50	570.0	475.0	71	0.0	98.3
Estimated Net Annual Sediment (TSS) Load Reduction =								98 %

Climate Station ID: 6105978 Years of Rainfall Data: 20

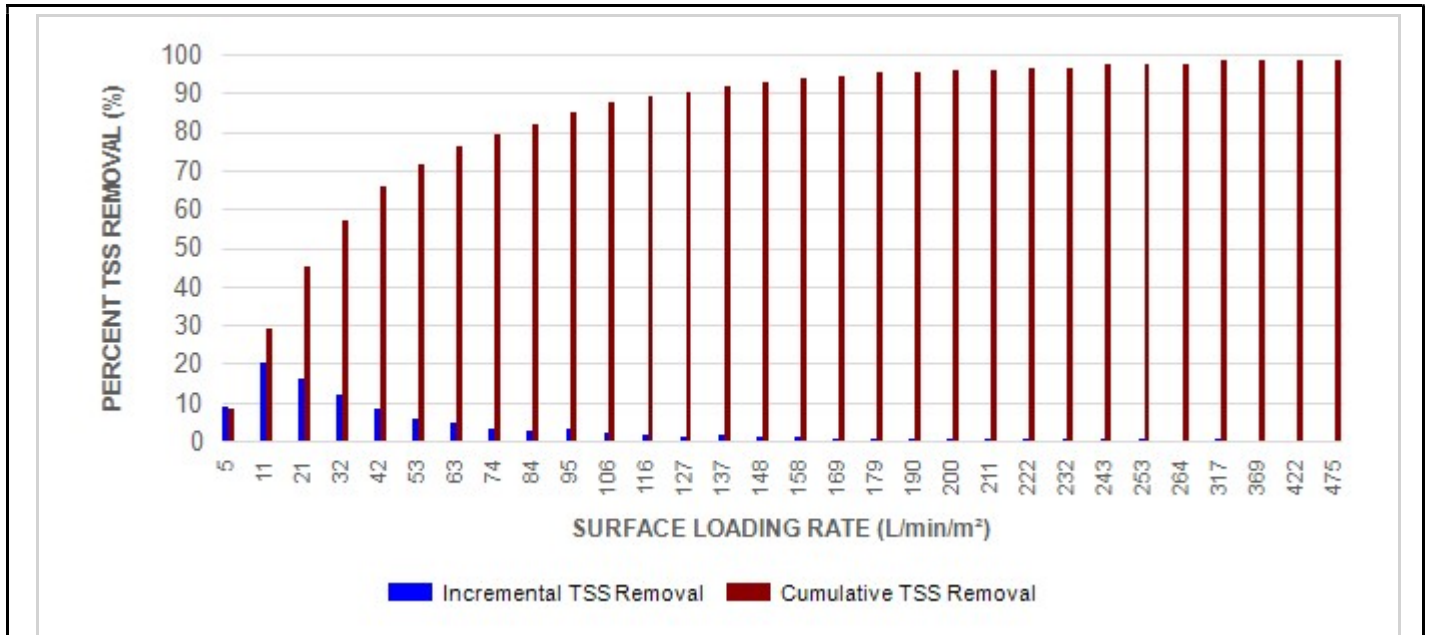


Stormceptor® EF Sizing Report

RAINFALL DATA FROM OTTAWA CDA RCS RAINFALL STATION



INCREMENTAL AND CUMULATIVE TSS REMOVAL FOR THE RECOMMENDED STORMCEPTOR® MODEL



Stormceptor® **EF** Sizing Report

Maximum Pipe Diameter / Peak Conveyance

Stormceptor EF / EFO	Model Diameter		Min Angle Inlet / Outlet Pipes	Max Inlet Pipe Diameter		Max Outlet Pipe Diameter		Peak Conveyance Flow Rate	
	(m)	(ft)		(mm)	(in)	(mm)	(in)	(L/s)	(cfs)
EF4 / EFO4	1.2	4	90	609	24	609	24	425	15
EF6 / EFO6	1.8	6	90	914	36	914	36	990	35
EF8 / EFO8	2.4	8	90	1219	48	1219	48	1700	60
EF10 / EFO10	3.0	10	90	1828	72	1828	72	2830	100
EF12 / EFO12	3.6	12	90	1828	72	1828	72	2830	100

SCOUR PREVENTION AND ONLINE CONFIGURATION

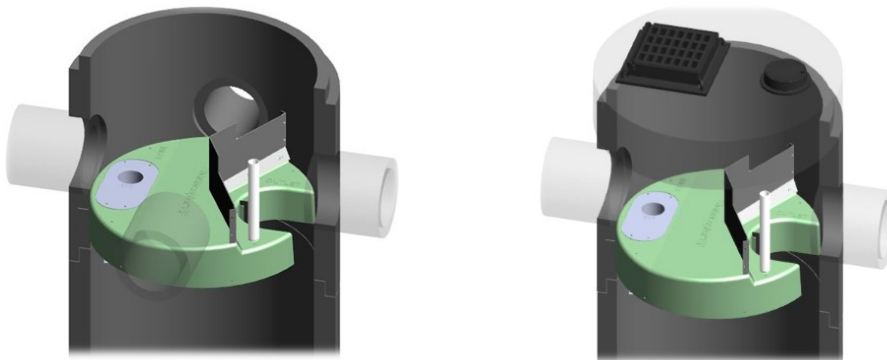
► Stormceptor® EF and EFO feature an internal bypass and superior scour prevention technology that have been demonstrated in third-party testing according to the scour testing provisions of the Canadian ETV **Procedure for Laboratory Testing of Oil-Grit Separators**, and the exceptional scour test performance has been third-party verified in accordance with the ISO 14034 ETV protocol. As a result, Stormceptor EF and EFO are approved for online installation, eliminating the need for costly additional bypass structures, piping, and installation expense.

DESIGN FLEXIBILITY

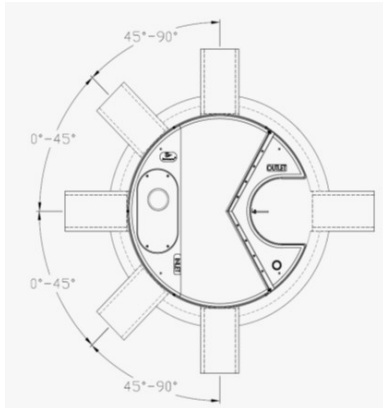
► Stormceptor® EF and EFO offers design flexibility in one simplified platform, accepting stormwater flow from a single inlet pipe or multiple inlet pipes, and/or surface runoff through an inlet grate. The device can also serve as a junction structure, accommodate a 90-degree inlet-to-outlet bend angle, and can be modified to ensure performance in submerged conditions.

OIL CAPTURE AND RETENTION

► While Stormceptor® EF will capture and retain oil from dry weather spills and low intensity runoff, Stormceptor® EFO has demonstrated superior oil capture and greater than 99% oil retention in third-party testing according to the light liquid re-entrainment testing provisions of the Canadian ETV **Procedure for Laboratory Testing of Oil-Grit Separators**. Stormceptor EFO is recommended for sites where oil capture and retention is a requirement.



Stormceptor® EF Sizing Report



INLET-TO-OUTLET DROP

Elevation differential between inlet and outlet pipe inverts is dictated by the angle at which the inlet pipe(s) enters the unit.

0° - 45° : The inlet pipe is 1-inch (25mm) higher than the outlet pipe.

45° - 90° : The inlet pipe is 2-inches (50mm) higher than the outlet pipe.

HEAD LOSS

The head loss through Stormceptor EF is similar to that of a 60-degree bend structure. The applicable K value for calculating minor losses through the unit is 1.1.

For submerged conditions the applicable K value is 3.0.

Pollutant Capacity

Stormceptor EF / EFO	Model Diameter		Depth (Outlet Pipe Invert to Sump Floor)		Oil Volume		Recommended Sediment Maintenance Depth *		Maximum Sediment Volume *		Maximum Sediment Mass **	
	(m)	(ft)	(m)	(ft)	(L)	(Gal)	(mm)	(in)	(L)	(ft ³)	(kg)	(lb)
EF4 / EFO4	1.2	4	1.52	5.0	265	70	203	8	1190	42	1904	5250
EF6 / EFO6	1.8	6	1.93	6.3	610	160	305	12	3470	123	5552	15375
EF8 / EFO8	2.4	8	2.59	8.5	1070	280	610	24	8780	310	14048	38750
EF10 / EFO10	3.0	10	3.25	10.7	1670	440	610	24	17790	628	28464	78500
EF12 / EFO12	3.6	12	3.89	12.8	2475	655	610	24	31220	1103	49952	137875

*Increased sump depth may be added to increase sediment storage capacity

** Average density of wet packed sediment in sump = 1.6 kg/L (100 lb/ft³)

Feature	Benefit	Feature Appeals To
Patent-pending enhanced flow treatment and scour prevention technology	Superior, verified third-party performance	Regulator, Specifying & Design Engineer
Third-party verified light liquid capture and retention for EFO version	Proven performance for fuel/oil hotspot locations	Regulator, Specifying & Design Engineer, Site Owner
Functions as bend, junction or inlet structure	Design flexibility	Specifying & Design Engineer
Minimal drop between inlet and outlet	Site installation ease	Contractor
Large diameter outlet riser for inspection and maintenance	Easy maintenance access from grade	Maintenance Contractor & Site Owner

STANDARD STORMCEPTOR EF/EFO DRAWINGS

For standard details, please visit <http://www.imbriumsystems.com/stormwater-treatment-solutions/stormceptor-ef>

STANDARD STORMCEPTOR EF/EFO SPECIFICATION

For specifications, please visit <http://www.imbriumsystems.com/stormwater-treatment-solutions/stormceptor-ef>



Stormceptor[®] **EF** Sizing Report



**STANDARD PERFORMANCE SPECIFICATION FOR
“OIL GRIT SEPARATOR” (OGS) STORMWATER QUALITY TREATMENT DEVICE**

PART 1 – GENERAL

1.1 WORK INCLUDED

This section specifies requirements for selecting, sizing, and designing an underground Oil Grit Separator (OGS) device for stormwater quality treatment, with third-party testing results and a Statement of Verification in accordance with ISO 14034 Environmental Management – Environmental Technology Verification (ETV).

1.2 REFERENCE STANDARDS & PROCEDURES

ISO 14034:2016 Environmental management – Environmental technology verification (ETV)

Canadian Environmental Technology Verification (ETV) Program’s **Procedure for Laboratory Testing of Oil-Grit Separators**

1.3 SUBMITTALS

1.3.1 All submittals, including sizing reports & shop drawings, shall be submitted upon request with each order to the contractor then forwarded to the Engineer of Record for review and acceptance. Shop drawings shall detail all OGS components, elevations, and sequence of construction.

1.3.2 Alternative devices shall have features identical to or greater than the specified device, including: treatment chamber diameter, treatment chamber wet volume, sediment storage volume, and oil storage volume.

1.3.3 Unless directed otherwise by the Engineer of Record, OGS stormwater quality treatment product substitutions or alternatives submitted within ten days prior to project bid shall not be accepted. All alternatives or substitutions submitted shall be signed and sealed by a local registered Professional Engineer, based on the exact same criteria detailed in Section 3, in entirety, subject to review and approval by the Engineer of Record.

PART 2 – PRODUCTS

2.1 OGS POLLUTANT STORAGE

The OGS device shall include a sump for sediment storage, and a protected volume for the capture and storage of petroleum hydrocarbons and buoyant gross pollutants. The minimum sediment & petroleum hydrocarbon storage capacity shall be as follows:

2.1.1	4 ft (1219 mm) Diameter OGS Units:	1.19 m ³ sediment / 265 L oil
	6 ft (1829 mm) Diameter OGS Units:	3.48 m ³ sediment / 609 L oil
	8 ft (2438 mm) Diameter OGS Units:	8.78 m ³ sediment / 1,071 L oil
	10 ft (3048 mm) Diameter OGS Units:	17.78 m ³ sediment / 1,673 L oil
	12 ft (3657 mm) Diameter OGS Units:	31.23 m ³ sediment / 2,476 L oil



Stormceptor® EF Sizing Report**PART 3 – PERFORMANCE & DESIGN****3.1 GENERAL**

The OGS stormwater quality treatment device shall be verified in accordance with ISO 14034:2016 Environmental management – Environmental technology verification (ETV). The OGS stormwater quality treatment device shall remove oil, sediment and gross pollutants from stormwater runoff during frequent wet weather events, and retain these pollutants during less frequent high flow wet weather events below the insert within the OGS for later removal during maintenance. The Manufacturer shall have at least ten (10) years of local experience, history and success in engineering design, manufacturing and production and supply of OGS stormwater quality treatment device systems, acceptable to the Engineer of Record.

3.2 SIZING METHODOLOGY

The OGS device shall be engineered, designed and sized to provide stormwater quality treatment based on treating a minimum of 90 percent of the average annual runoff volume and a minimum removal of an annual average 60% of the sediment (TSS) load based on the Particle Size Distribution (PSD) specified in the sizing report for the specified device. Sizing of the OGS shall be determined by use of a minimum ten (10) years of local historical rainfall data provided by Environment Canada. Sizing shall also be determined by use of the sediment removal performance data derived from the ISO 14034 ETV third-party verified laboratory testing data from testing conducted in accordance with the Canadian ETV protocol Procedure for Laboratory Testing of Oil-Grit Separators, as follows:

3.2.1 Sediment removal efficiency for a given surface loading rate and its associated flow rate shall be based on sediment removal efficiency demonstrated at the seven (7) tested surface loading rates specified in the protocol, ranging 40 L/min/m² to 1400 L/min/m², and as stated in the ISO 14034 ETV Verification Statement for the OGS device.

3.2.2 Sediment removal efficiency for surface loading rates between 40 L/min/m² and 1400 L/min/m² shall be based on linear interpolation of data between consecutive tested surface loading rates.

3.2.3 Sediment removal efficiency for surface loading rates less than the lowest tested surface loading rate of 40 L/min/m² shall be assumed to be identical to the sediment removal efficiency at 40 L/min/m². No extrapolation shall be allowed that results in a sediment removal efficiency that is greater than that demonstrated at 40 L/min/m².

3.2.4 Sediment removal efficiency for surface loading rates greater than the highest tested surface loading rate of 1400 L/min/m² shall assume zero sediment removal for the portion of flow that exceeds 1400 L/min/m², and shall be calculated using a simple proportioning formula, with 1400 L/min/m² in the numerator and the higher surface loading rate in the denominator, and multiplying the resulting fraction times the sediment removal efficiency at 1400 L/min/m².

The OGS device shall also have sufficient annual sediment storage capacity as specified and calculated in Section 2.1.

3.3 CANADIAN ETV or ISO 14034 ETV VERIFICATION OF SCOUR TESTING

The OGS device shall have Canadian ETV or ISO 14034 ETV Verification of third-party scour testing conducted in

Stormceptor[®] EF Sizing Report

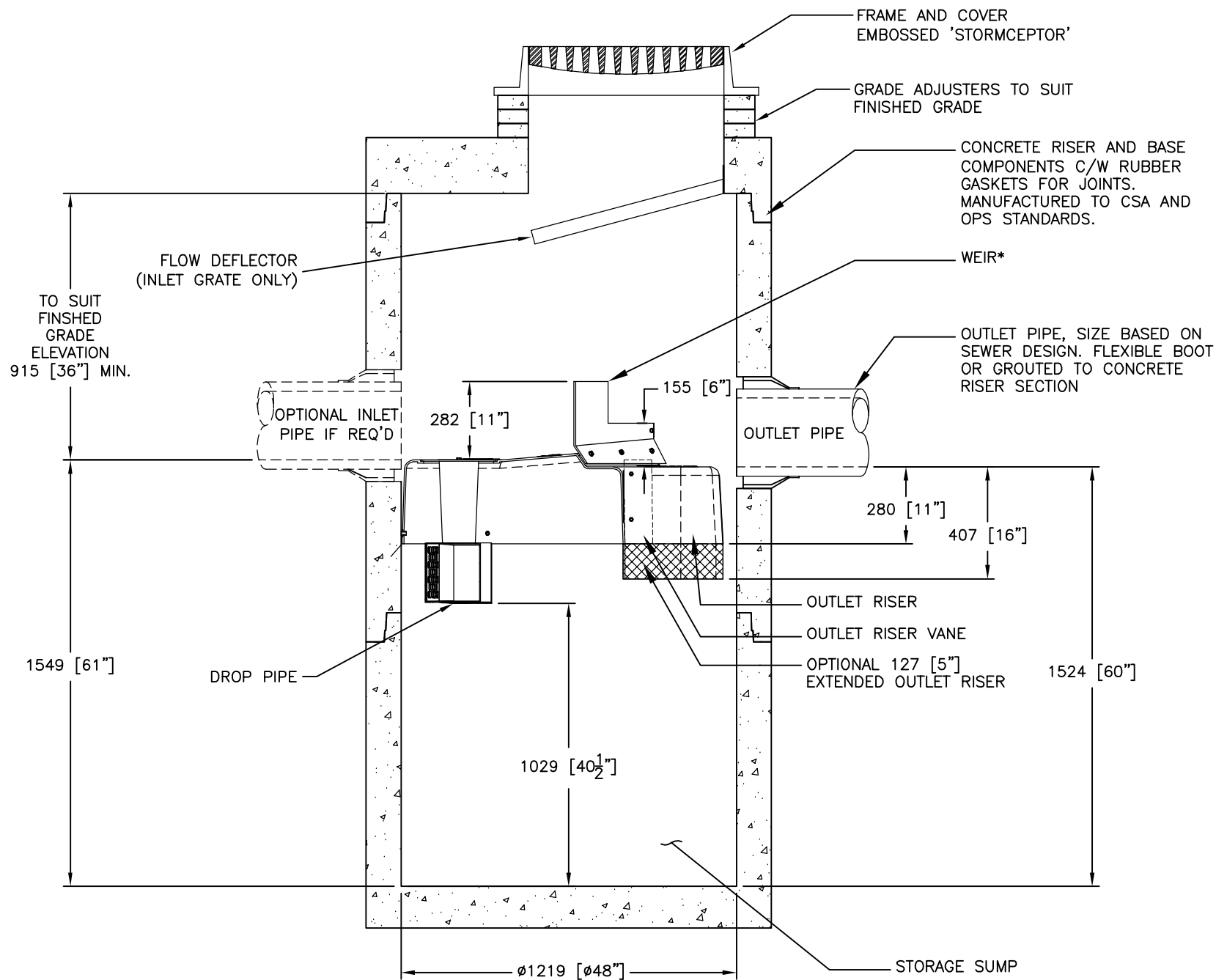
accordance with the Canadian ETV Program's **Procedure for Laboratory Testing of Oil-Grit Separators**.

3.3.1 To be acceptable for on-line installation, the OGS device must demonstrate an average scour test effluent concentration less than 10 mg/L at each surface loading rate tested, up to and including 2600 L/min/m².

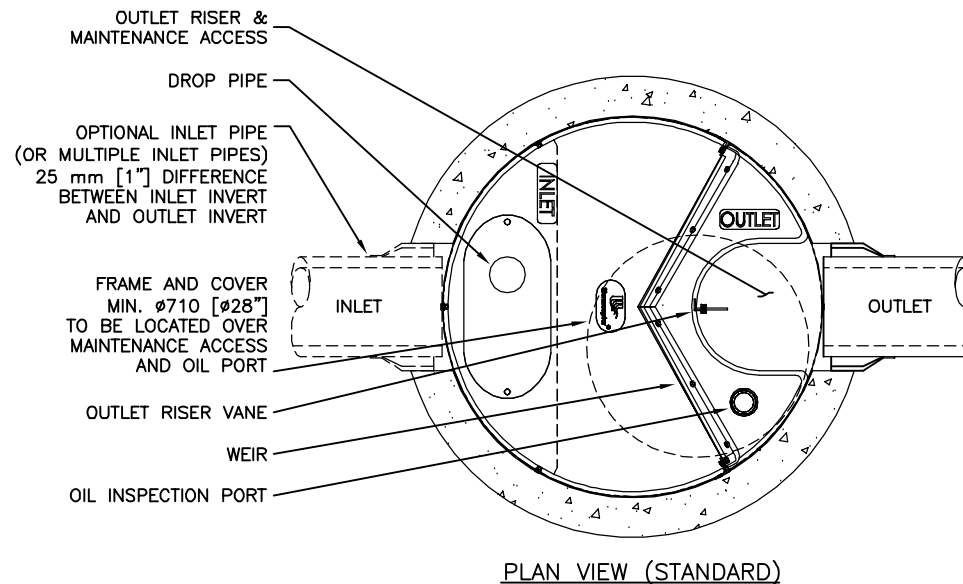
3.4 LIGHT LIQUID RE-ENTRAINMENT SIMULATION TESTING

The OGS device shall have Canadian ETV or ISO 14034 ETV Verification of completed third-party Light Liquid Re-entrainment Simulation Testing in accordance with the Canadian ETV **Program's Procedure for Laboratory Testing of Oil-Grit Separators**, with results reported within the Canadian ETV or ISO 14034 ETV verification. This re-entrainment testing is conducted with the device pre-loaded with low density polyethylene (LDPE) plastic beads as a surrogate for light liquids such as oil and fuel. Testing is conducted on the same OGS unit tested for sediment removal to assess whether light liquids captured after a spill are effectively retained at high flow rates.

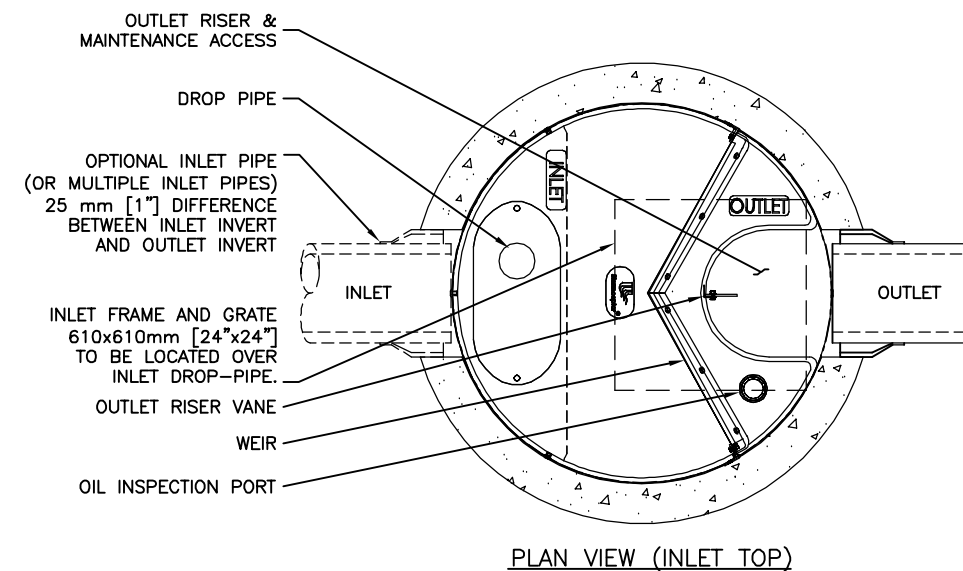
3.4.1 For an OGS device to be an acceptable stormwater treatment device on a site where vehicular traffic occurs and the potential for an oil or fuel spill exists, the OGS device must have reported verified performance results of greater than 99% cumulative retention of LDPE plastic beads for the five specified surface loading rates (ranging 200 L/min/m² to 2600 L/min/m²) in accordance with the Light Liquid Re-entrainment Simulation Testing within the Canadian ETV Program's **Procedure for Laboratory Testing of Oil-Grit Separators**. However, an OGS device shall not be allowed if the Light Liquid Re-entrainment Simulation Testing was performed with screening components within the OGS device that are effective at retaining the LDPE plastic beads, but would not be expected to retain light liquids such as oil and fuel.



SECTION VIEW



PLAN VIEW (STANDARD)



PLAN VIEW (INLET TOP)

FOR SITE SPECIFIC DRAWINGS PLEASE CONTACT YOUR LOCAL STORMCEPTOR REPRESENTATIVE. SITE SPECIFIC DRAWINGS ARE BASED ON THE BEST AVAILABLE INFORMATION AT THE TIME. SOME FIELD REVISIONS TO THE SYSTEM LOCATION OR CONNECTION PIPING MAY BE NECESSARY BASED ON AVAILABLE SPACE OR SITE CONFIGURATION REVISIONS. ELEVATIONS SHOULD BE MAINTAINED EXCEPT WHERE NOTED ON BYPASS STRUCTURE (IF REQUIRED).

GENERAL NOTES:

- * MAXIMUM SURFACE LOADING RATE (SLR) INTO LOWER CHAMBER THROUGH DROP PIPE IS 1135 L/min/m² (27.9 gpm/ft²) FOR STORMCEPTOR EF4 AND 535 L/min/m² (13.1 gpm/ft²) FOR STORMCEPTOR EFO4 (OIL CAPTURE CONFIGURATION). WEIR HEIGHT IS 150 mm (6 INCH) FOR EF04.
- 1. ALL DIMENSIONS INDICATED ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SPECIFIED.
- 2. STORMCEPTOR STRUCTURE INLET AND OUTLET PIPE SIZE AND ORIENTATION SHOWN FOR INFORMATIONAL PURPOSES ONLY.
- 3. UNLESS OTHERWISE NOTED, BYPASS INFRASTRUCTURE, SUCH AS ALL UPSTREAM DIVERSION STRUCTURES, CONNECTING STRUCTURES, OR PIPE CONDUITS CONNECTING TO COMPLETE THE STORMCEPTOR SYSTEM SHALL BE PROVIDED AND ADDRESSED SEPARATELY.
- 4. DRAWING FOR INFORMATIONAL PURPOSES ONLY. REFER TO ENGINEER'S SITE/UTILITY PLAN FOR STRUCTURE ORIENTATION.
- 5. NO PRODUCT SUBSTITUTIONS SHALL BE ACCEPTED UNLESS SUBMITTED 10 DAYS PRIOR TO PROJECT BID DATE, OR AS DIRECTED BY THE ENGINEER OF RECORD.

INSTALLATION NOTES

- A. ANY SUB-BASE, BACKFILL DEPTH, AND/OR ANTI-FLOTATION PROVISIONS ARE SITE-SPECIFIC DESIGN CONSIDERATIONS AND SHALL BE SPECIFIED BY ENGINEER OF RECORD.
- B. CONTRACTOR TO PROVIDE EQUIPMENT WITH SUFFICIENT LIFTING AND REACH CAPACITY TO LIFT AND SET THE STRUCTURE (LIFTING CLUTCHES PROVIDED)
- C. CONTRACTOR WILL INSTALL AND LEVEL THE STRUCTURE, SEALING THE JOINTS, LINE ENTRY AND EXIT POINTS (NON-SHRINK GROUT WITH APPROVED WATERSTOP OR FLEXIBLE BOOT)
- D. CONTRACTOR TO TAKE APPROPRIATE MEASURES TO PROTECT THE DEVICE FROM CONSTRUCTION-RELATED EROSION RUNOFF.
- E. DEVICE ACTIVATION, BY CONTRACTOR, SHALL OCCUR ONLY AFTER SITE HAS BEEN STABILIZED AND THE STORMCEPTOR UNIT IS CLEAN AND FREE OF DEBRIS.

STANDARD DETAIL
NOT FOR CONSTRUCTION

SITE SPECIFIC DATA REQUIREMENTS

STORMCEPTOR MODEL	EFO4
STRUCTURE ID	*
HYDROCARBON STORAGE REQ'D (L)	*
WATER QUALITY FLOW RATE (L/s)	*
PEAK FLOW RATE (L/s)	*
RETURN PERIOD OF PEAK FLOW (yrs)	*
DRAINAGE AREA (HA)	*
DRAINAGE AREA IMPERVIOUSNESS (%)	*
PIPE DATA:	I.E. MAT'L DIA SLOPE % HGL
INLET #1	* * * * *
INLET #2	* * * * *
OUTLET	* * * * *

* PER ENGINEER OF RECORD

This design and information shall be the property of Imbrium Systems ("Imbrium") and shall remain the property of Imbrium. Imbrium Systems ("Imbrium") and its contractors, agents, and subcontractors, shall not be held liable for any errors or omissions in this drawing, for any part thereof, may be used for any purpose other than that intended by Imbrium Systems. Imbrium Systems ("Imbrium") shall not be held liable for any damage or injury resulting from the use of this drawing. Imbrium Systems ("Imbrium") shall not be held liable for any damage or injury resulting from the use of this drawing. Imbrium Systems ("Imbrium") shall not be held liable for any damage or injury resulting from the use of this drawing.

MARK	DATE	REVISION DESCRIPTION	BY
###	###/###/###	INITIAL RELEASE	JSK
###	6/8/18	UPDATES	JSK
###	5/26/17	INITIAL RELEASE	JSK

Stormceptor® EF

SCALE = NTS

imbrium

407 FAIRVIEW DRIVE, WHITBY, ON L1N 3J9
TEL: 905-585-4801 CA: 416-960-9600 INTL: +1-416-960-9600
www.imbrium.com

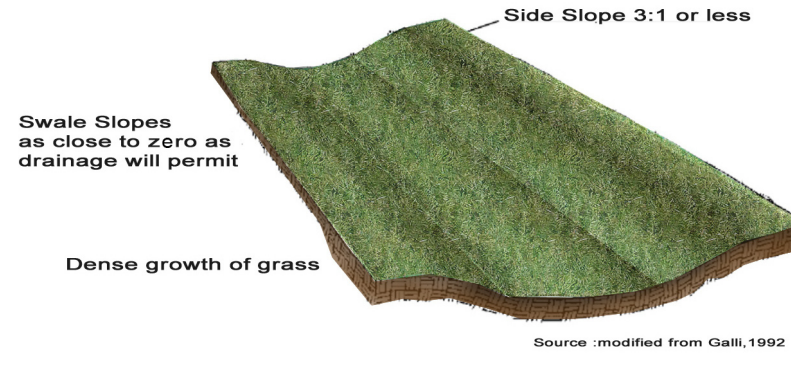
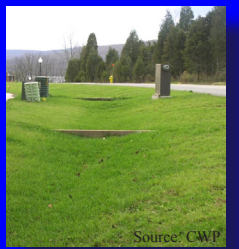
DATE: 10/13/2017

DESIGNED: JSK	DRAWN: JSK
CHECKED: BSF	APPROVED: SP
PROJECT No.: EFO4	SEQUENCE No.: *
SHEET: 1	OF 1

GENERAL DESCRIPTION

Enhanced grass swales are vegetated open channels designed to convey, treat and attenuate stormwater runoff (also referred to as enhanced vegetated swales). Check dams and vegetation in the swale slows the water to allow sedimentation, filtration through the root zone and soil matrix, evapotranspiration, and infiltration into the underlying native soil. Simple grass channels or ditches have long been used for stormwater conveyance, particularly for roadway drainage. Enhanced grass swales incorporate design features such as modified geometry and check dams that improve the contaminant removal and runoff reduction functions of simple grass channel and roadside ditch designs.

Where development density, topography and depth to water table permit, enhanced grass swales are a preferred alternative to both curb and gutter and storm drains as a stormwater conveyance system. When incorporated into a site design, they can reduce impervious cover, accent the natural landscape, and provide aesthetic benefits.



DESIGN GUIDANCE

GEOMETRY AND SITE LAYOUT

- **Shape:** Should be designed with a trapezoidal or parabolic cross section. Trapezoidal swales will generally evolve into parabolic swales over time, so the initial trapezoidal cross-section design should be checked for capacity and conveyance assuming it is a parabolic cross-section. Swale length between culverts should be 5 metres or greater.
- **Bottom Width:** Should be designed with a bottom width between 0.75 and 3.0 metres. Should allow for shallow flows and adequate water quality treatment, while preventing flows from concentrating and creating gullies.
- **Longitudinal Slope:** Slopes should be between 0.5% and 4%. Check dams should be incorporated on slopes greater than 3%.
- **Length:** When used to convey and treat road runoff, the length simply parallels the road, and therefore should be equal to, or greater than the contributing roadway length.
- **Flow Depth:** A maximum flow depth of 100 mm is recommended during a 4 hour, 25 mm Chicago storm event.
- **Side Slopes:** Should be as flat as possible to aid in providing pretreatment for lateral incoming flows and to maximize the swale filtering surface. Steeper side slopes are likely to have erosion gullying from incoming lateral flows. A maximum slope of 2.5:1 (H:V) is recommended and a 4:1 slope is preferred where space permits.

PRE-TREATMENT

A pea gravel diaphragm located along the top of each bank can be used to provide pretreatment of any runoff entering the swale laterally along its length. Vegetated filter strips or mild side slopes (3:1) also provide pretreatment for any lateral sheet flow entering the swale. Sedimentation forebays at inlets to the swale are also a pretreatment option.

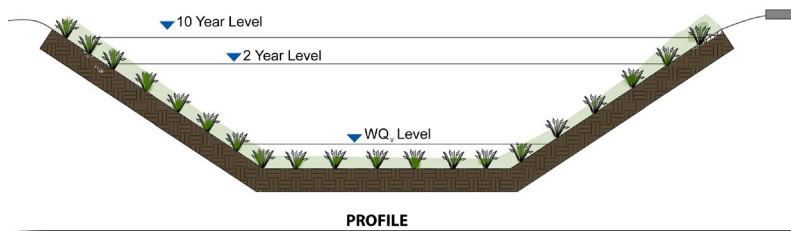
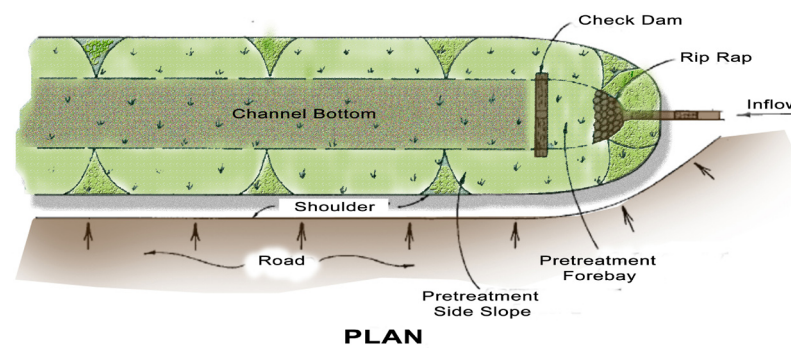
CONVEYANCE AND OVERFLOW

Grass swales must be designed for a maximum velocity of 0.5 m/s or less for the 4 hour 25 mm Chicago storm event. The swale should also convey the locally required design storm (usually the 10 year storm) at non-erosive velocities.

SOIL AMENDMENTS

If soils along the location of the swale are highly compacted, or of such low fertility that vegetation cannot become established, they should be tilled to a depth of 300 mm and amended with compost to achieve an organic content of 8 to 15% by weight or 30 to 40% by volume.

PLAN VIEW OF A GRASS SWALE



PLAN AND PROFILE VIEWS

OPERATION AND MAINTENANCE

Generally, routine maintenance will be the same as for any other landscaped area; weeding, pruning, and litter removal. Grassed swales should be mown at least twice yearly to maintain grass height between 75 and 150 mm. The lightest possible mowing equipment should be used to prevent soil compaction. Routine roadside ditch maintenance practices such as scraping and re-grading should be avoided. Regular watering may be required during the first two years until vegetation is established. Routine inspection is very important to ensure that dense vegetation cover is maintained and inlets and pretreatment devices are free of debris.

ABILITY TO MEET SWM OBJECTIVES

BMP	Water Balance Benefit	Water Quality Improvement	Stream Channel Erosion Control Benefit
Enhanced Grass Swale	Partial - depends on soil infiltration rate	Yes, if design velocity is 0.5 m/s or less for a 4 hour, 25 mm Chicago storm	Partial - depends on soil infiltration rate

GENERAL SPECIFICATIONS

Component	Specification	Quantity
Check Dams	Constructed of a non-erosive material such as suitably sized aggregate, wood, gabions, riprap, or concrete. All check dams should be underlain with geotextile filter fabric. Wood used for check dams should consist of pressure treated logs or timbers, or water-resistant tree species such as cedar, hemlock, swamp oak or locust.	Spacing should be based on the longitudinal slope and desired ponding volume.
Gravel Diaphragm	Washed stone between 3 and 10 mm in diameter.	Minimum of 300 mm wide and 600 mm deep.

CONSTRUCTION CONSIDERATIONS

Grass swales should be clearly marked before site work begins to avoid disturbance during construction. No vehicular traffic, except that specifically used to construct the facility, should be allowed within the swale site. Any accumulation of sediment that does occur within the swale must be removed during the final stages of grading to achieve the design cross-section. Final grading and planting should not occur until the adjoining areas draining into the swale are stabilized. Flow should not be diverted into the swale until the banks are stabilized.

Preferably, the swale should be planted in the spring so that the vegetation can become established with minimal irrigation. Installation of erosion control matting or blanketing to stabilize soil during establishment of vegetation is highly recommended. If sod is used, it should be placed with staggered ends and secured by rolling the sod. This helps to prevent gullies.

For the first two years following construction the swale should be inspected at least quarterly and after every major storm event (> 25 mm). Subsequently, inspections should be conducted in the spring and fall of each year and after major storm events. Inspect for vegetation density (at least 80% coverage), damage by foot or vehicular traffic, accumulation of debris, trash and sediment, and structural damage to pretreatment devices.

Trash and debris should be removed from pretreatment devices and the surface of the swale at least twice annually. Other maintenance activities include weeding, replacing dead vegetation, repairing eroded areas, dethatching and aerating as needed. Remove accumulated sediment on the swale surface when dry and exceeding 25 mm depth.

SITE CONSIDERATIONS

- **Available Space**
Grass swales usually consume about 5 to 15% of their contributing drainage area. A width of at least 2 metres is needed.
- **Site Topography**
Site topography constrains the application of grass swales. Longitudinal slopes between 0.5 and 6% are allowable. This prevents ponding while providing residence time and preventing erosion. On slopes steeper than 3%, check dams should be used.
- **Drainage Area & Runoff Volume**
The conveyance capacity should match the drainage area. Sheet flow to the grass swale is preferable. If drainage areas are greater than 2 hectares, high discharge through the swale may not allow for filtering and infiltration, and may create erosive conditions. Typical ratios of impervious drainage area to treatment facility area range from 5:1 to 10:1.
- **Soil**
Grass swales can be applied on sites with any type of soils.
- **Pollution Hot Spot Runoff**
To protect groundwater from possible contamination, source areas where land uses or human activities have the potential to generate highly contaminated runoff (e.g., vehicle fueling, servicing and demolition areas, outdoor storage and handling areas for hazardous materials and some heavy industry sites) should not be treated by grass swales.
- **Proximity to Underground Utilities**
Utilities running parallel to the grass swale should be offset from the centerline of the swale. Underground utilities below the bottom of the swale are not a problem.
- **Water Table**
The bottom of the swale should be separated from the seasonally high water table or top of bedrock elevation by at least one (1) metre.
- **Setback from Buildings**
Should be located a minimum of four (4) metres from building foundations to prevent water damage.

CVC/TRCA LOW IMPACT DEVELOPMENT PLANNING AND DESIGN GUIDE - FACT SHEET

ENHANCED GRASS SWALES

Appendix C

100 and 5-Year SWM Runoff and Storage Requirements

LRL Associates Ltd.

Storm Watershed Summary



LRL File No. 210341

Project: Proposed Dog Kennel

Location: 5969 Ottawa St., Ottawa, ON

Date: June 25, 2024

Designed: S.Vora

Drawing Ref.: C701, C 702

Pre-Development Catchments

Watershed	C = 0.20	C = 0.80	C = 0.90	Total Area (ha)	Combined C
EWS-01	0.273	0.052	0.000	0.325	0.30
Total	0.273	0.052	0.000	0.325	0.30

Post-Development Catchments

Watershed	C = 0.20	C = 0.8	C = 0.90	Total Area (ha)	Combined C
WS-01 (un-controlled)	0.089	0.000	0.000	0.089	0.20
WS-02 (un-controlled)	0.082	0.000	0.023	0.105	0.35
WS-03 (controlled)	0.052	0.002	0.077	0.131	0.62
Total	0.224	0.002	0.100	0.325	0.42



LRL File No. 210341
 Project: Proposed Dog Kennel
 Location: 5969 Ottawa St., Ottawa, ON
 Date: June 25, 2024
 Designed: S. Vora
 Drawing Ref.: C701, C702

Stormwater Management
Design Sheet

STORM - 100 YEAR

Runoff Equation

$Q = 2.78CIA$ (L/s)
 C = Runoff coefficient
 $I = \text{Rainfall intensity (mm/hr)} = A / (Td + C)^B$
 A = Area (ha)
 T_c = Time of concentration (min)

Pre-Development Catchments within Development Area

	Total Site Area =	0.325	ha	ΣR =	0.30
Uncontrolled	EWS-01	0.325	ha	R =	0.30
	Total Uncontrolled =	0.325	ha	ΣR =	0.30

100 Year Pre-Development Release Rate (Full Site)

$I_{100} = 1735.688 / (Td + 6.014)^{0.820}$ **A = 1735.688** **B = 0.820** **C = 6.014**
 C = 0.30 Max. of 0.5 per City of Ottawa
 I = 178.56 mm/hr
 T_c = 10 min
 A = 0.325 ha

100 Year Pre-Development Release Rate = 48.40 L/s

Allowable Release Rate (L/s)= 14.48
Controlled Release Rate (L/s)= 12.40

Post-development Stormwater Management

	Total Site Area =	0.325	ha	ΣR =	ΣR₅	ΣR₁₀₀
Controlled	WS-03	0.131	ha	R =	0.62	0.77
	Total Controlled =	0.131	ha	ΣR =	0.62	0.77
Uncontrolled	WS-01	0.089	ha	R =	0.20	0.25
Uncontrolled	WS-02	0.105	ha	R =	0.35	0.44
	Total Un-Controlled =	0.194	ha	ΣR =	0.28	0.35

100 Year Post-development Stormwater Management (Controlled catchment WS-03)

$I_{100} = 1735.688 / (Td + 6.014)^{0.820}$ **A = 1735.688** **B = 0.820** **C = 6.014**

100 Year Controlled Release Rate (for storage purpose)

Time (min)	Intensity (mm/hr)	Controlled Runoff (L/s)	Storage Volume (m ³)	*Controlled Release Rate (L/s)	Uncontrolled Release Rate (L/s)	Total Release Rate (L/s)
10	178.56	50.12	26.35	6.20	33.92	40.12
15	142.89	40.11	30.52	6.20	27.15	33.35
20	119.95	33.67	32.97	6.20	22.79	28.99
25	103.85	29.15	34.43	6.20	19.73	25.93
30	91.87	25.79	35.26	6.20	17.45	23.65
35	82.58	23.18	35.66	6.20	15.69	21.89
40	75.15	21.09	35.75	6.20	14.28	20.48
45	69.05	19.38	35.59	6.20	13.12	19.32
50	63.95	17.95	35.26	6.20	12.15	18.35
55	59.62	16.74	34.77	6.20	11.33	17.53
60	55.89	15.69	34.16	6.20	10.62	16.82
70	49.79	13.98	32.66	6.20	9.46	15.66



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LRL File No. 210341
Project: Proposed Dog Kennel
Location: 5969 Ottawa St., Ottawa, ON
Date: June 25, 2024
Designed: S. Vora
Drawing Ref.: C701, C702

**Stormwater Management
Design Sheet**

80	44.99	12.63	30.86	6.20	8.55	14.75
90	41.11	11.54	28.84	6.20	7.81	14.01
100	37.90	10.64	26.64	6.20	7.20	13.40
110	35.20	9.88	24.30	6.20	6.69	12.89
120	32.89	9.23	21.84	6.20	6.25	12.45

* 50% of maximum Allowable Release Rate for storage calculations

Onsite Stormwater Retention

Total Storage Required = 35.75 m³
Overland Storage (Detention Area) = 36.50 m³
Total Overland Storage Provided = 36.50 m³

Storage vol. between the 100yr
HWL and Outlet pipe Inv.



LRL File No. 210341
 Project: Proposed Dog Kennel
 Location: 5969 Ottawa St., Ottawa, ON
 Date: June 25, 2024
 Designed: S. Vora
 Drawing Ref.: C701, C702

Stormwater Management
Design Sheet

STORM - 5 YEAR

Runoff Equation

Q = 2.78CIA (L/s)
 C = Runoff coefficient
 I = Rainfall intensity (mm/hr) = A / (Td + C)^B
 A = Area (ha)
 T_c = Time of concentration (min)

Pre-Development Catchments within Development Area

	Total Site Area =	0.325	ha	ΣR =	0.30
Uncontrolled	EWS-01	0.325	ha	R =	0.30
	Total Uncontrolled =	0.325	ha	ΣR =	0.30

5 Year Pre-Development Release Rate (Full Site)

$I_5 = 998.071 / (Td + 6.053)^{0.814}$ A = 998.071 B = 0.814 C = 6.053

C = 0.30
 I = 104.2 mm/hr
 T_c = 10 min
 A = 0.325 ha

5 Year Pre-Development Release Rate = 28.24 L/s

Allowable Release Rate (L/s) = 12.40
 Controlled Release Rate (L/s) = 12.40

Post-development Stormwater Management

	Total Site Area =	0.325	ha	ΣR =	0.42	ΣR₁₀₀	0.52
Controlled	WS-03	0.131	ha	R =	0.62		0.77
	Total Controlled =	0.131	ha	ΣR =	0.62		0.77
Uncontrolled	WS-01	0.089	ha	R =	0.20		0.25
Uncontrolled	WS-02	0.105	ha	R =	0.35		0.44
	Total Un-Controlled =	0.194	ha	ΣR =	0.28		0.35

5 Year Post-development Stormwater Management (Controlled catchment WS-03, WS-04 & WS-05)

$I_5 = 998.071 / (Td + 6.053)^{0.814}$ A = 998.071 B = 0.814 C = 6.053

5 Year Controlled Release Rate

Time (min)	Intensity (mm/hr)	Controlled Runoff (L/s)	Storage Volume (m ³)	*Controlled Release Rate (L/s)	Uncontrolled Release Rate (L/s)	Total Release Rate (L/s)
10	104.19	23.40	10.32	6.20	15.84	22.04
15	83.56	18.76	11.31	6.20	12.70	18.90
20	70.25	15.78	11.49	6.20	10.68	16.88
25	60.90	13.68	11.21	6.20	9.26	15.46
30	53.93	12.11	10.64	6.20	8.20	14.40
35	48.52	10.90	9.86	6.20	7.37	13.57
40	44.18	9.92	8.93	6.20	6.72	12.92
45	40.63	9.12	7.89	6.20	6.17	12.37
50	37.65	8.46	6.77	6.20	5.72	11.92
55	35.12	7.89	5.57	6.20	5.34	11.54
60	32.94	7.40	4.31	6.20	5.01	11.21
70	29.37	6.60	1.66	6.20	4.46	10.66
80	26.56	5.97	0.00	6.20	4.04	10.24
90	24.29	5.45	0.00	6.20	3.69	9.89



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LRL File No. 210341

Project: Proposed Dog Kennel

Location: 5969 Ottawa St., Ottawa, ON

Date: June 25, 2024

Designed: S. Vora

Drawing Ref.: C701, C702

**Stormwater Management
Design Sheet**

100	22.41	5.03	0.00	6.20	3.41	9.61
110	20.82	4.68	0.00	6.20	3.16	9.36
120	19.47	4.37	0.00	6.20	2.96	9.16

* 50% of maximum Allowable Release Rate for storage calculations

Onsite Stormwater Retention

Total Storage Required = 11.49 m³



LRL File No. 210341
Project: Proposed Dog Kennel
Location: 5969 Ottawa St., Ottawa, ON
Date: May 13, 2024
Designed: S.Vora
Checked: M. Basnet
Dwg. Ref.: C601,C702

Rational Method
 Q = 2.78CIA
 Q = Peak flow (L/s)
 A = Drainage area (ha)
 C = Runoff coefficient
 I = Rainfall intensity (mm/hr)
Runoff coefficient (C)
 Grass = 0.2
 Gravel = 0.8
 Asphalt / rooftop = 0.9

IDF curve
 Ottawa Macdonald-Cartier International Airport
 Storm event: 5 Years
Intensity equation:
 $I_s = 998.071 / (Td + 6.053)^{0.814}$ (mm/hr)
Pipe Design Parameters
 Minimum velocity = 0.80 m/s
 Manning's "n" = 0.013

LOCATION			AREA (ha)			FLOW						STORM SEWER							
WATERSHED / STREET	From MH	To MH	C = 0.20	C = 0.80	C = 0.90	Indiv. 2.78AC	Accum. 2.78AC	Time of Conc.	Rainfall Intensity	Peak Flow (Q)	Controlled Flow (Q)	Pipe Dia.	Type	Slope	Length	Capacity Full (Q _{FULL})	Velocity Full	Time of Flow	Ratio Q / Q _{FULL}
									(mm/hr)	(L/s)	(L/s)								
SITE	ICD	OGS	0.032	0.002	0.077	0.21	0.21	10.00	104.19	22.39	12.40	250	PVC	0.70%	2.0	49.75	1.01	0.03	0.25
SITE	OGS	MUN. DRAIN					0.21	10.03	104.02	22.35	12.40	250	PVC	0.80%	9.0	53.19	1.08	0.14	0.23



LRL File No. 210341
Project: Proposed Dog Kennel
Location: 5969 Ottawa St., Ottawa, ON
Date: June 25, 2024
Designed: S.Vora
Checked: M. Basnet
Drawing Ref.: C601

**Stormwater Management Orifice Plate
 Design Sheet**

Orifice Equation

$$Q = 0.61 * A * \text{sqrt} (2 * g * H)$$

Where; Q = release rate (in m3/s)
 0.61 = coefficient
 A = area of the orifice (m2)
 g = gravitational constant (9.81 m/s2)
 H = head above CL of orifice (m)

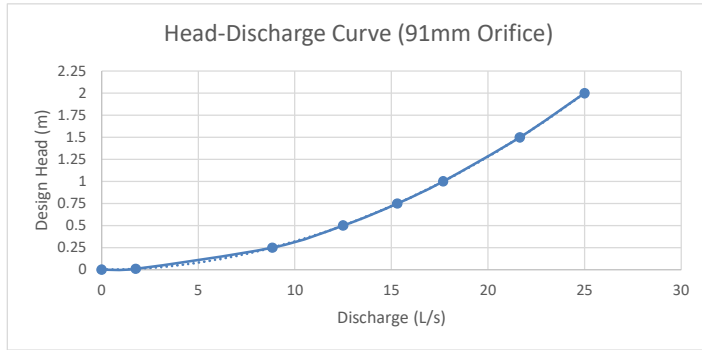
Orifice Plate Diameter Design

Storm - 100 year

Allowable Release Rate = 14.48L/s
 Controlled Release Rate = 12.40L/s
 Q (m3/s) = 0.0124
 g (9.81m/s2) = 9.81
 A (m2) = 0.00654

H (m) =	0.49
---------	------

100yr STM Design Head = 0.49 m



SWM Design Parameters

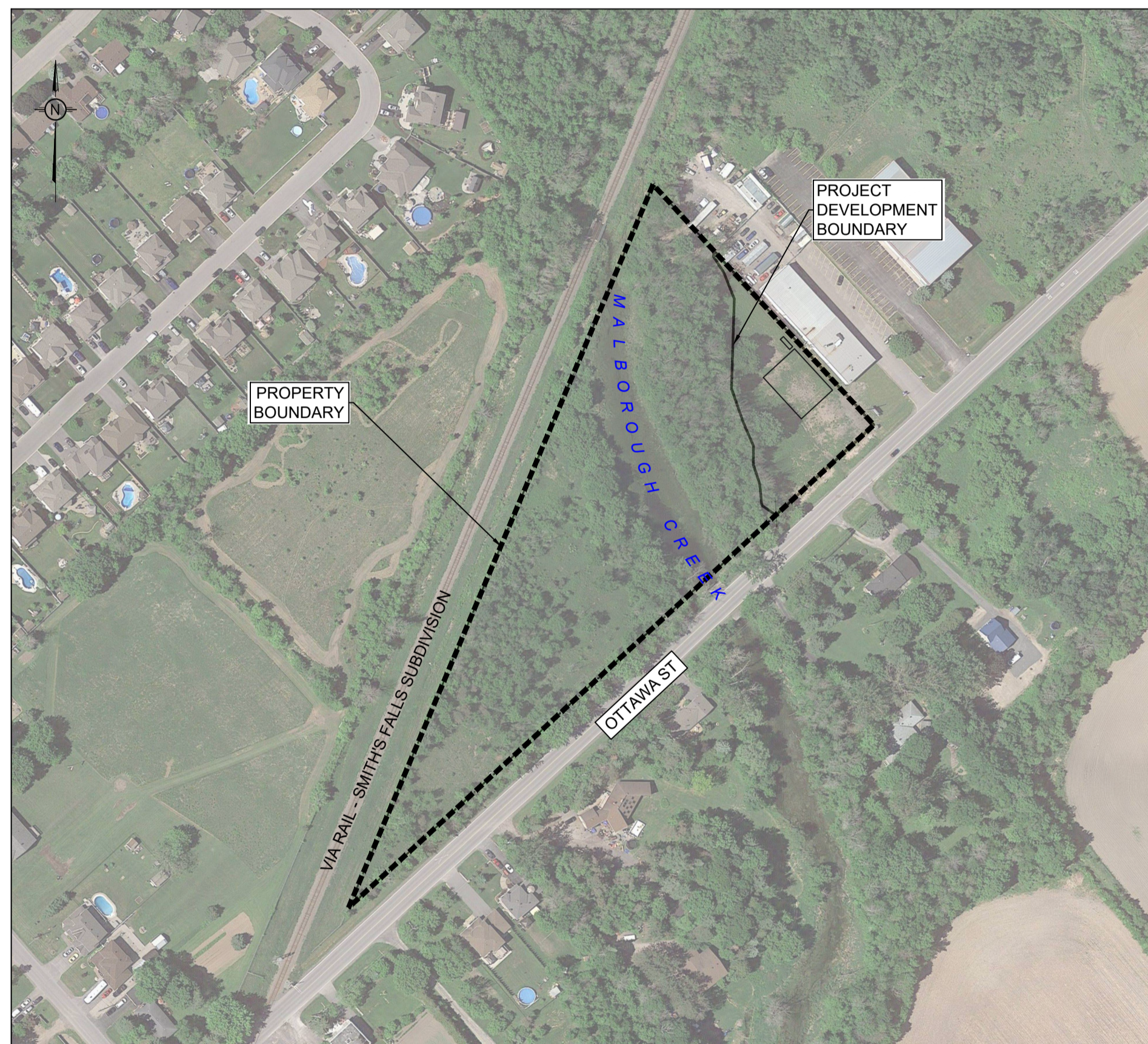
Orifice Plate ϕ (m)	0.091
Host Pipe Invert	93.41
Head above C/L Orifice	93.46
100y High Water Level	93.95
100y Design Head	0.49

Appendix D

Civil Engineering Drawings

PROPOSED DOG KENNEL 5969 OTTAWA ST., OTTAWA, ON

REVISION 05



KEY PLAN (NTS)

DRAWING INDEX	
TITLE PAGE	
SEDIMENT AND EROSION CONTROL PLAN	C101
DEMOLITION PLAN	C102
SITE DEVELOPMENT PLAN	C201
GRADING AND DRAINAGE PLAN	C301
STORMWATER MANAGEMENT & SERVICING PLAN	C601
PRE-DEVELOPMENT WATERSHED PLAN	C701
POST-DEVELOPMENT WATERSHED PLAN	C702
CONSTRUCTION DETAIL PLAN	C901



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www.lrl.ca | (613) 842-3434

PROPOSED DOG KENNEL
5969 OTTAWA ST., OTTAWA, ON
REV.04 - RE-ISSUED FOR APPROVAL - JUNE, 2024
LRL PROJECT no: 210341



GENERAL NOTES

1. ALL WORKS MATERIALS SHALL CONFIRM TO THE LAST REVISION OF THE STANDARDS AND SPECIFICATIONS FOR THE CITY OF OTTAWA, ONTARIO PROVINCIAL STANDARD DRAWINGS (OPSD) AND SPECIFICATIONS (OPSS), WHERE APPLICABLE. LOCAL UTILITY STANDARDS AND MINISTRY OF TRANSPORTATION STANDARDS WILL APPLY WHERE REQUIRED.
2. THE CONTRACTORS SHALL CONFIRM THE LOCATION OF ALL EXISTING UTILITIES WITHIN THE SITE AND ADJACENT WORK AREAS. THE CONTRACTORS SHALL BE RESPONSIBLE FOR PROTECTING ALL EXISTING UTILITIES TO THE SATISFACTION OF THE AUTHORITY HAVING JURISDICTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPAIR OR REPLACEMENT OF ANY SERVICES OR UTILITIES DISTURBED DURING CONSTRUCTION, TO THE SATISFACTION OF THE AUTHORITY HAVING JURISDICTION.
3. ALL DIMENSIONS SHALL BE CHECKED AND VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO THE START OF CONSTRUCTION, ANY DISCREPANCIES SHALL BE REPORTED IMMEDIATELY TO THE ENGINEER. LOST TIME DUE TO FAILURE OF THE CONTRACTORS TO CONFIRM UTILITY LOCATIONS AND NOTIFY ENGINEER OF POSSIBLE CONFLICTS PRIOR TO CONSTRUCTION WILL BE AT CONTRACTORS EXPENSE.
4. ANY AREA BEYOND THE LIMIT OF THE SITE DISTURBED DURING CONSTRUCTION SHALL BE RESTORED TO ORIGINAL CONDITION OR BETTER TO THE SATISFACTION OF THE AUTHORITY HAVING JURISDICTION AT THE CONTRACTORS EXPENSE. RELOCATING OF EXISTING SERVICES AND/OR UTILITIES SHALL BE AS SHOWN ON THE DRAWINGS OR DETECTED BY THE ENGINEER AT THE EXPENSE OF DEVELOPERS.
5. ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH THE OCCUPATIONAL HEALTH AND SAFETY ACT AND REGULATIONS FOR CONSTRUCTION PROJECTS. THE GENERAL CONTRACTORS SHALL BE DEEMED TO BE THE CONTRACTOR AS DEFINED IN THE ACT.
6. ALL THE CONSTRUCTION SIGNAGE MUST CONFIRM TO THE MINISTRY OF TRANSPORTATION OF ONTARIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES PER LATEST AMENDMENT.
7. THE CONTRACTOR IS ADVISED THAT WORKS BY OTHERS MAY BE ONGOING DURING THE PERIOD OF THE CONTRACT. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO PREVENT CONFLICTS.
8. ALL DIMENSIONS ARE IN METRES UNLESS SPECIFIED OTHERWISE.
9. THERE WILL BE NO SUBSTITUTION OF MATERIALS UNLESS PRIOR WRITTEN APPROVAL IS RECEIVED FROM THE ENGINEER.
10. ALL CONSTRUCTION SHALL BE CARRIED OUT IN ACCORDANCE WITH THE RECOMMENDATIONS MADE IN THE GEOTECHNICAL REPORT.
11. FOR DETAILS RELATING TO STORMWATER MANAGEMENT AND ROOF DRAINAGE REFER TO THE SITE SERVICES AND STORMWATER MANAGEMENT REPORT.
12. ALL SEWERS CONSTRUCTED WITH GRADES LESS THAN 1.0% SHALL BE INSTALLED USING LASER ALIGNMENT AND CHECKED WITH LEVEL INSTRUMENT PRIOR TO BACKFILLING.
13. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED AND TO BEAR THE COST OF THE SAME.
14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADDITIONAL BEDDING, OR ADDITIONAL STRENGTH PIPE IF THE MAXIMUM TRENCH WIDTH AS SPECIFIED BY OPSD IS EXCEEDED.
15. ALL PIPE/CULVERT SECTION SIZES REFER TO INSIDE DIMENSIONS.
16. SHOULD DEEPLY BURIED ARCHAEOLOGICAL REMAINS BE FOUND ON THE PROPERTY DURING CONSTRUCTION ACTIVITIES, THE HERITAGE OPERATIONS UNIT OF THE ONTARIO MINISTRY OF CULTURE MUST BE NOTIFIED IMMEDIATELY.
17. ALL NECESSARY CLEARING AND GRUBBING SHALL BE COMPLETED BY THE CONTRACTOR. REVIEW WITH CONTRACT ADMINISTRATOR AND THE CITY OF OTTAWA PRIOR TO ANY TREE CUTTING/REMOVAL.
18. DRAWINGS SHALL BE READ IN CONJUNCTION WITH ARCHITECTURAL SITE PLAN.
19. THE CONTRACTOR SHALL PROVIDE THE PROJECT ENGINEER ON SET OF AS CONSTRUCTED SITE SERVICING AND GRADING DRAWINGS.
20. BENCHMARKS: IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THAT THE SITE BENCHMARK(S) HAS NOT BEEN ALTERED OR DISTURBED AND THAT ITS RELATIVE ELEVATION AND DESCRIPTION AGREES WITH THE INFORMATION DEPICTED ON THIS PLAN.

EROSION AND SEDIMENT CONTROL NOTES

GENERAL

THE CONTRACTOR SHALL IMPLEMENT BEST MANAGEMENT PRACTICES, TO PROVIDE FOR PROTECTION OF THE AREA DRAINAGE SYSTEM AND THE RECEIVING WATERCOURSE, DURING CONSTRUCTION ACTIVITIES. THE CONTRACTOR ACKNOWLEDGES THAT FAILURE TO IMPLEMENT APPROPRIATE EROSION AND SEDIMENT CONTROL MEASURES MAY BE SUBJECT TO PENALTIES IMPOSED BY ANY APPLICABLE REGULATORY AGENCY.

THE CONTRACTOR ACKNOWLEDGES THAT SURFACE EROSION AND SEDIMENT RUNOFF RESULTING FROM THEIR CONSTRUCTION OPERATIONS HAS POTENTIAL TO CAUSE A DETRIMENTAL IMPACT TO ANY DOWNSTREAM WATERCOURSE OR SEWER, AND THAT ALL CONSTRUCTION OPERATIONS THAT MAY IMPACT UPON WATER QUALITY SHALL BE CARRIED OUT IN MANNER THAT STRICTLY MEETS THE REQUIREMENT OF ALL APPLICABLE LEGISLATION AND REGULATIONS.

AS SUCH, THE CONTRACTOR SHALL BE RESPONSIBLE FOR CARRYING OUT THEIR OPERATIONS, AND SUPPLYING AND INSTALLING ANY APPROPRIATE CONTROL MEASURES, SO AS TO PREVENT SEDIMENT LADEN RUNOFF ENTERING ANY SEWER OR WATERCOURSE WITHIN OR DOWNSTREAM OF THE WORKING AREA.

THE CONTRACTOR ACKNOWLEDGES THAT NO ONE MEASURE IS LIKELY TO BE 100% EFFECTIVELY FOR EROSION PROTECTION AND CONTROLLING SEDIMENT RUNOFF AND DISCHARGES FROM THE SITE. THEREFORE, WHERE NECESSARY THE CONTRACTOR SHALL IMPLEMENT ADDITIONAL MEASURES ARRANGED IN SUCH MANNER AS TO MITIGATE SEDIMENT RELEASE FROM THE CONSTRUCTION OPERATIONS AND ACHIEVE SPECIFIC MAXIMUM PERMITTED CRITERIA WHERE APPLICABLE. SUGGESTED ON-SITE MEASURES MAY INCLUDE, BUT SHALL NOT BE LIMITED TO, THE FOLLOWING METHODS: SEDIMENT PONDS, FILTER BAGS, PUMP FILTERS, SETTLING TANKS, SILT FENCE, STRAW BALES, FILTER CLOTHS, CATCH BASIN FILTERS, CHECK DAMS AND/OR OTHER RECOGNIZED TECHNOLOGIES AND METHOD AVAILABLE AT THE TIME OF CONSTRUCTION. SPECIFIC MEASURES SHALL BE INSTALLED IN ACCORDANCE WITH REQUIREMENTS OF OPSS 577 WHERE APPROPRIATE, OR IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.

WHERE, IN THE OPINION OF THE CONTRACT ADMINISTRATOR OR REGULATORY AGENCY, THE INSTALLED CONTROL MEASURES FAIL TO PERFORM ADEQUATELY, THE CONTRACTOR SHALL SUPPLY AND INSTALL ADDITIONAL OR ALTERNATIVE MEASURES AS DIRECTED BY THE CONTRACT ADMINISTRATOR OR REGULATORY AGENCY, AS SUCH, THE CONTRACTOR SHALL HAVE ADDITIONAL CONTROL MATERIALS ON SITE AT ALL TIME WHICH ARE EASILY ACCESSIBLE AND MAY BE IMPLEMENTED BY HIM AT THE MOMENT'S NOTICE.

PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL SUBMIT TO THE CONTRACT ADMINISTRATOR SIX COPIES OF A DETAILED EROSION AND SEDIMENT CONTROL PLAN (ESCP). THE ESCP WILL CONSIST OF WRITTEN DESCRIPTION AND DETAILED DRAWINGS INDICATING THE ON-SITE ACTIVITIES AND MEASURES TO BE USED TO CONTROL EROSION AND SEDIMENT MOVEMENT FOR EACH STEP OF THE WORK.

CONTRACTOR'S RESPONSIBILITIES

THE CONTRACTOR SHALL ENSURE THAT ALL WORKERS, INCLUDING SUB-CONTRACTOR, IN THE WORKING AREA ARE AWARE OF THE IMPORTANCE OF THE EROSION AND SEDIMENT CONTROL MEASURES AND INFORMED OF THE CONSEQUENCES OF THE FAILURE TO COMPLY WITH THE REQUIREMENTS OF ALL REGULATORY AGENCIES.

THE CONTRACTOR SHALL PERIODICALLY, AND WHEN REQUESTED BY THE CONTRACT ADMINISTRATOR, CLEAN OUT ACCUMULATED SEDIMENT DEPOSITS AS REQUIRED AT THE SEDIMENT CONTROL DEVICES, INCLUDING THOSE DEPOSITS THAT MAY ORIGINATE FROM OUTSIDE THE CONSTRUCTION AREA. ACCUMULATED SEDIMENT SHALL BE REMOVED IN SUCH A MANNER THAT PREVENTS THE DEPOSITION OF THIS MATERIAL INTO THE SEWER WATERCOURSE AND AVOIDS DAMAGE TO CONTROL MEASURES. THE SEDIMENT SHALL BE REMOVED FROM THE SITE AT THE CONTRACTOR'S EXPENSE AND MANAGED IN COMPLIANCE WITH REQUIREMENTS FRO EXCESS EARTH MATERIAL, AS SPECIFIED ELSEWHERE IN THE CONTRACT.

THE CONTRACTOR SHALL IMMEDIATELY REPORT TO THE CONTRACT ADMINISTRATOR ANY ACCIDENTAL DISCHARGES OF SEDIMENT MATERIAL INTO EITHER THE WATERCOURSE OR THE STORM SEWER SYSTEM. FAILURE TO REPORT WILL BE CONSTITUTE A BRACH OF THIS SPECIFICATION AND THE CONTRACTOR MAY ALSO BE SUBJECT TO THE PENALTIES IMPOSED BY THE APPLICABLE REGULATORY AGENCY. APPROPRIATE RESPONSE MEASURES, INCLUDING ANY REPAIRS TO EXISTING CONTROL MEASURES OR THE IMPLEMENTATION OF ADDITIONAL CONTROL MEASURES, SHALL BE CARRIED OUT BY THE CONTRACTOR WITHOUT DELAY.

THE SEDIMENT CONTROL MEASURES SHALL ONLY BE REMOVED WHEN, IN THE OPINION OF THE CONTRACT ADMINISTRATOR, THE MEASURE OR MEASURES, IS NO LONGER REQUIRED. NO CONTROL MEASURE MAY BE PERMANENTLY REMOVED WITHOUT PRIOR AUTHORIZATION FROM THE CONTRACT ADMINISTRATOR. ALL SEDIMENT AND EROSION CONTROL MEASURES SHALL BE REMOVED IN A MANNER THAT AVOIDS THE ENTRY OF ANY EQUIPMENT, OTHER THAN HAND-HELD EQUIPMENT, INTO ANY WATERCOURSE, AND PREVENTS THE RELEASE OF ANY SEDIMENT OR DEBRIS INTO ANY SEWER OR WATERCOURSE WITHIN OR DOWNSTREAM OF THE WORKING AREA. ALL ACCUMULATED SEDIMENT SHALL BE REMOVED FROM THE WORKING AREA AT THE CONTRACTOR'S EXPENSE AND MANAGED IN COMPLIANCE WITH THE REQUIREMENTS FOR EXCESS EARTH MATERIAL.

WHERE, IN THE OPINION OF EITHER THE CONTRACT ADMINISTRATOR OR A REGULATORY AGENCY, ANY OF THE TERMS SPECIFIED HEREIN HAVE NOT BEEN COMPLIED WITH OR PERFORMED IN A SUITABLE MANNER, OR THAT ALL, THE CONTRACTOR OR ADMINISTRATOR OR A REGULATORY AGENCY HAS THE RIGHT TO IMMEDIATELY WITHDRAW ITS PERMISSION TO CONTINUE THE WORK BUT MAY REVEW ITS PERMISSION UPON BEING SATISFIED THAT THE DEFAULTS OR DEFICIENCIES IN THE PERFORMANCE OF THIS SPECIFICATION BY THE CONTRACTOR HAVE BEEN REMEDIED.

SPILL CONTROL NOTES

1. ALL CONSTRUCTION EQUIPMENT SHALL BE RE-FUELED, MAINTAINED, AND STORED NO LESS THAN 30 METRES FROM WATERCOURSE, STREAMS, CREEKS, WOODLOTS, AND ANY ENVIRONMENTALLY SENSITIVE AREAS, OR AS OTHERWISE SPECIFIED.
2. THE CONTRACTOR MUST IMPLEMENT ALL NECESSARY MEASURES IN ORDER TO PREVENT LEAKS, DISCHARGES OR SPILLS OF POLLUTANTS, DELETERIOUS MATERIALS, OR OTHER SUCH MATERIALS OR SUBSTANCES WHICH WOULD OR COULD CAUSE AN ADVERSE IMPACT TO THE NATURAL ENVIRONMENT.
3. IN THE EVENT OF A LEAK, DISCHARGE OR SPILL OF POLLUTANT, DELETERIOUS MATERIAL OR OTHER SUCH MATERIAL OR SUBSTANCE WHICH WOULD OR COULD CAUSE AN ADVERSE IMPACT TO THE NATURAL ENVIRONMENT, THE CONTRACTOR SHALL:
 - 3.1. IMMEDIATELY NOTIFY APPROPRIATE FEDERAL, PROVINCIAL, AND LOCAL GOVERNMENT MINISTRIES, DEPARTMENTS, AGENCIES, AND AUTHORITIES OF THE INCIDENT IN ACCORDANCE WITH ALL CURRENT LAWS, LEGISLATION, ACTS, BY-LAWS, PERMITS, APPROVALS, ETC.
 - 3.2. TAKE IMMEDIATE MEASURES TO CONTAIN THE MATERIAL OR SUBSTANCE, AND TO TAKE SUCH MEASURES TO MITIGATE AGAINST ADVERSE IMPACTS TO THE NATURAL ENVIRONMENT.
 - 3.3. RESTORE THE AFFECTED AREA TO THE ORIGINAL CONDITION OR BETTER TO THE SATISFACTION OF THE AUTHORITIES HAVING JURISDICTION.

MUD MAT NOTES

1. THE GRANULAR MATERIAL WILL REQUIRE PERIODIC REPLACEMENT AS IT BECOMES CONTAMINATED BY VEHICLE TRAFFIC.
2. SEDIMENT SHALL BE CLEANED FROM PUBLIC ROADS AT THE END OF EACH DAY.
3. SEDIMENT SHALL BE REMOVED FROM PUBLIC ROADS BY SHOVELING OR SWEEPING AND DISPOSED OR PROPERLY IN A CONTROLLED SEDIMENT DISPOSAL AREA.

SITE GRADING NOTES

1. PRIOR TO THE COMMENCEMENT OF THE SITE GRADING WORKS, ALL SILTATION CONTROL DEVICES SHALL BE INSTALLED AND OPERATIONAL PER EROSION CONTROL PLAN.
2. ALL GRANULAR AND PAVEMENT FOR ROADS/PARKING AREAS SHALL BE CONSTRUCTED IN ACCORDANCE WITH GEOTECHNICAL ENGINEER'S RECOMMENDATIONS.
3. ALL TOPSOIL AND ORGANIC MATERIAL SHALL BE STRIPPED WITHIN THE ROAD AND PARKING AREAS ALLOWANCE PRIOR TO THE COMMENCEMENT OF CONSTRUCTION.
4. GRANULAR 'A' SHALL BE PLACED TO A MINIMUM THICKNESS OF 30MM AROUND ALL STRUCTURES WITHIN THE PAVEMENT AREA.
5. SUB-EXCAVATE SOFT AREAS AND FILL WITH GRANULAR 'B' COMPACTED IN MAXIMUM 30MM LIFTS.
6. ALL WORK ON THE MUNICIPAL RIGHT OF WAY AND EASEMENTS TO BE INSPECTED BY THE MUNICIPALITY PRIOR BACKFILLING.
7. CONTRACTOR TO OBTAIN A ROAD OCCUPANCY PERMIT 48 HOURS PRIOR TO COMMENCING ANY WORK WITHIN THE MUNICIPAL ROAD ALLOWANCE, IF REQUIRED BY THE MUNICIPALITY.
8. ALL PAVEMENT MARKING FEATURES AND SITE SIGNAGE SHALL BE PLACED PER ARCHITECTURAL SITE PLAN. LINE PAINTING AND DIRECTIONAL SYMBOLS SHALL BE APPLIED WITH A MINIMUM OF TWO COATS OF ORGANIC SOLVENT PAINT.
9. REFER TO ARCHITECTURAL SITE PLAN FOR DIMENSIONS AND SITE DETAILS.
10. STEP JOINTS ARE TO BE USED WHERE PROPOSED ASPHALT MEETS EXISTING ASPHALT. ALL JOINTS MUST BE SEALED.
11. WHERE APPLICABLE THE CONTRACTOR IS TO SUBMIT SHOP DRAWINGS TO THE ENGINEER FOR APPROVAL PRIOR TO CONSTRUCTION. SHOP DRAWINGS MUST BE SITE SPECIFIC, SIGNED AND SEALED BY A LICENSED STRUCTURAL ENGINEER. THE CONTRACTOR WILL ALSO BE REQUIRED TO SUPPLY AND GEOTECHNICAL CERTIFICATION OF THE AS-CONSTRUCTED RETAINING WALL TO THE ENGINEER PRIOR TO FINAL ACCEPTANCE.

STORM SEWER NOTES

STORM

12. RIP-RAP TREATMENT SEWER AND CULVERT OUTLETS PER OPSD 810.010.
13. ALL STORM SEWER/ CULVERTS TO BE INSTALLED WITH FROST TREATMENT PER OPSD 803.031 WHERE APPLICABLE.

USE AND INTERPRETATION OF DRAWINGS

GENERAL CONDITIONS OF THE CONTRACT FOR CONSTRUCTION ARE PART OF THE CONTRACT DOCUMENTS AND DESCRIBE USE AND INTENT OF THE DRAWING. THE CONTRACT DOCUMENTS INCLUDE NOT ONLY THE DRAWINGS, BUT ALSO THE OWNER-CONTRACTOR AGREEMENTS, CONDITIONS OF THE CONTRACT, THE SPECIFICATIONS, ADDENDA, AND MODIFICATIONS ISSUED AFTER EXECUTION OF THE CONTRACT. THESE CONTRACT DOCUMENTS ARE COMPLEMENTARY, AND WHAT IS REQUIRED BY ANY ONE SHALL BE BINDING AS IF REQUIRED BY ALL. WORK NOT COMPLETELY DELINEATED HEREON SHALL BE CONSTRUCTED OF THE SAME MATERIALS AND DETAIL AS SHOWN MORE COMPLETELY ELSEWHERE IN THE CONTRACT DOCUMENTS.

BY USE OF THE DRAWINGS FOR CONSTRUCTION OF THE PROJECT, THE OWNER CONFIRMS THAT HE HAS REVIEWED AND APPROVED THE DRAWINGS. THE CONTRACTOR CONFIRMS THAT HE HAS VISITED THE SITE, FAMILIARIZED HIMSELF WITH THE LOCAL CONDITIONS, VERIFIED FIELD DIMENSIONS AND CORRELATED HIS OBSERVATIONS WITH THE REQUIREMENTS OF THE CONTRACT DOCUMENTS.

AS INSTRUMENTS OF SERVICE, ALL DRAWINGS, SPECIFICATIONS, CAD FILES OR OTHER ELECTRONIC MEDIA AND COPIED THERE OF FURNISHED BY THE ENGINEER ARE HIS PROPERTY. THEY ARE TO BE USED ONLY FOR THIS PROJECT AND ARE NOT TO BE USED ON ANY OTHER PROJECT, INCLUDING REPEATS OF THE PROJECT. CHANGES TO THE DRAWINGS MAY ONLY BE MADE BY THE ENGINEER.

UNLESS THE REVISION TITLE IS ISSUED FOR CONSTRUCTION, THESE DRAWINGS SHALL BE CONSIDERED PRELIMINARY AND SHALL NOT BE USED AS A CONSTRUCTION DOCUMENT.

THESE DRAWINGS ILLUSTRATES THE WORK TO BE DONE. THE ENGINEER IS NOT RESPONSIBLE FOR THE MEANS, METHODS, TECHNIQUES, SEQUENCES, AND PROCEDURES USED TO DO THE WORK, OR THE SAFETY ASPECTS OF CONSTRUCTION, AND NOTHING ON THESE DRAWINGS EXPRESSED OR IMPLIED CHANGES THIS CONDITION. CONTRACTOR SHALL DETERMINE ALL CONDITIONS AT THE SITE AND SHALL BE RESPONSIBLE FOR KNOWING HOW THEY AFFECT THE WORK. SUBMITTAL OF A BID TO PERFORM THIS WORK IS ACKNOWLEDGEMENT OF THE RESPONSIBILITIES, AND THAT THEY HAVE BEEN FULLY CONSIDERED IN PLANNING OF THE WORK, AND THE BID PRICE. NO CLAIMS FOR EXTRA CHARGES DUE TO THESE CONDITIONS WILL BE FORTHCOMING.

UNAUTHORIZED CHANGES

IN THE EVENT THE CLIENT, THE CLIENT'S CONTRACTORS OR SUBCONTRACTORS, OR ANYONE FOR WHOM THE CLIENT IS LEGALLY LIABLE MAKES OR PERMITS TO BE MADE ANY CHANGES TO ANY REPORTS, PLANS, SPECIFICATIONS OR OTHER CONSTRUCTION DOCUMENTS PREPARED BY LRL ASSOCIATES LTD. (LRL) WITHOUT OBTAINING LRL'S PRIOR WRITTEN CONSENT, THE CLIENT SHALL ASSUME FULL RESPONSIBILITY FOR THE RESULTS OF SUCH CHANGES. THEREFORE THE CLIENT AGREES TO WAIVE ANY CLAIM AGAINST LRL AND TO RELEASE LRL FROM ANY LIABILITY ARISING DIRECTLY OR INDIRECTLY FROM SUCH UNAUTHORIZED CHANGES.

IN ADDITION, THE CLIENT AGREES, TO THE FULLEST EXTENT PERMITTED BY LAW, TO INDEMNIFY AND HOLD HARMLESS LRL FROM ANY DAMAGES, LIABILITIES OR COST, INCLUDING REASONABLE ATTORNEY'S FEES AND COST OF DEFENSE, ARISING FROM SUCH CHANGES.

IN ADDITION, THE CLIENT AGREES TO INCLUDE IN ANY CONTRACTS FOR CONSTRUCTION APPROPRIATE LANGUAGE THAT PROHIBITS THE CONTRACTOR OR ANY SUBCONTRACTORS OF ANY TIER FROM MAKING ANY CHANGES OR MODIFICATIONS TO LRL'S CONSTRUCTION DOCUMENTS WITHOUT THE PRIOR WRITTEN APPROVAL OF LRL AND THAT FURTHER REQUIRES THE CONTRACTOR TO INDEMNIFY BOTH LRL AND THE CLIENT FROM ANY LIABILITY OR COST ARISING FROM SUCH CHANGES MADE WITHOUT SUCH PROPER AUTHORIZATION.

GENERAL NOTES

EXISTING SERVICES AND UTILITIES SHOWN ON THESE DRAWINGS ARE TAKEN FROM THE BEST AVAILABLE RECORDS, BUT MAY NOT BE COMPLETE OR TO DATE. CONTRACTOR SHALL VERIFY IN FIELD FOR LOCATION AND ELEVATION OF PIPES AND CHECK WITH THE UTILITY COMPANIES BEFORE DIGGING OR PERFORMING WORK.

CONTRACTOR IS ADVISED TO COLLECT INFORMATION ON SOIL CONDITIONS BEFORE START OF CONSTRUCTION.

THE ENGINEER WAIVES ANY AND ALL RESPONSIBILITY AND LIABILITY FOR PROBLEMS WHICH ARISE FROM FAILURE TO FOLLOW THESE PLANS, SPECIFICATIONS AND THE CONTRACT DOCUMENTS, OR FOR PROBLEMS WHICH ARISE FROM OTHERS' FAILURE TO OBTAIN AND/OR FOLLOW THE ENGINEER'S GUIDANCE WITH RESPECT TO ANY ERRORS, OMISSIONS, INCONSISTENCIES AMBIGUITIES OR CONFLICTS WHICH ARE ALLEGED.

CONTRACTOR TO VERIFY ALL DIMENSIONS AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES BEFORE WORK COMMENCES. DO NOT SCALE DRAWINGS.

05	RE-ISSUED FOR APPROVAL	S.V.	25 JUN 2024
04	RE-ISSUED FOR APPROVAL	S.V.	16 MAY 2024
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02	ISSUED FOR APPROVAL	M.L.	02 JUN 2022
01	ISSUED FOR APPROVAL	M.A.	25 NOV 2021

No.	REVISIONS	BY	DATE
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NOT AUTHENTIC UNLESS SIGNED AND DATED



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ENGINEERING | INGÉNIERIE

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CLIENT

AL ROBERTS

DESIGNED BY:	DRAWN BY:	APPROVED BY:
M.A.	M.A.	M.B.

PROJECT

PROPOSED DOG KENNEL
5969 OTTAWA STREET,
OTTAWA, ON

DRAWING TITLE

GENERAL NOTES

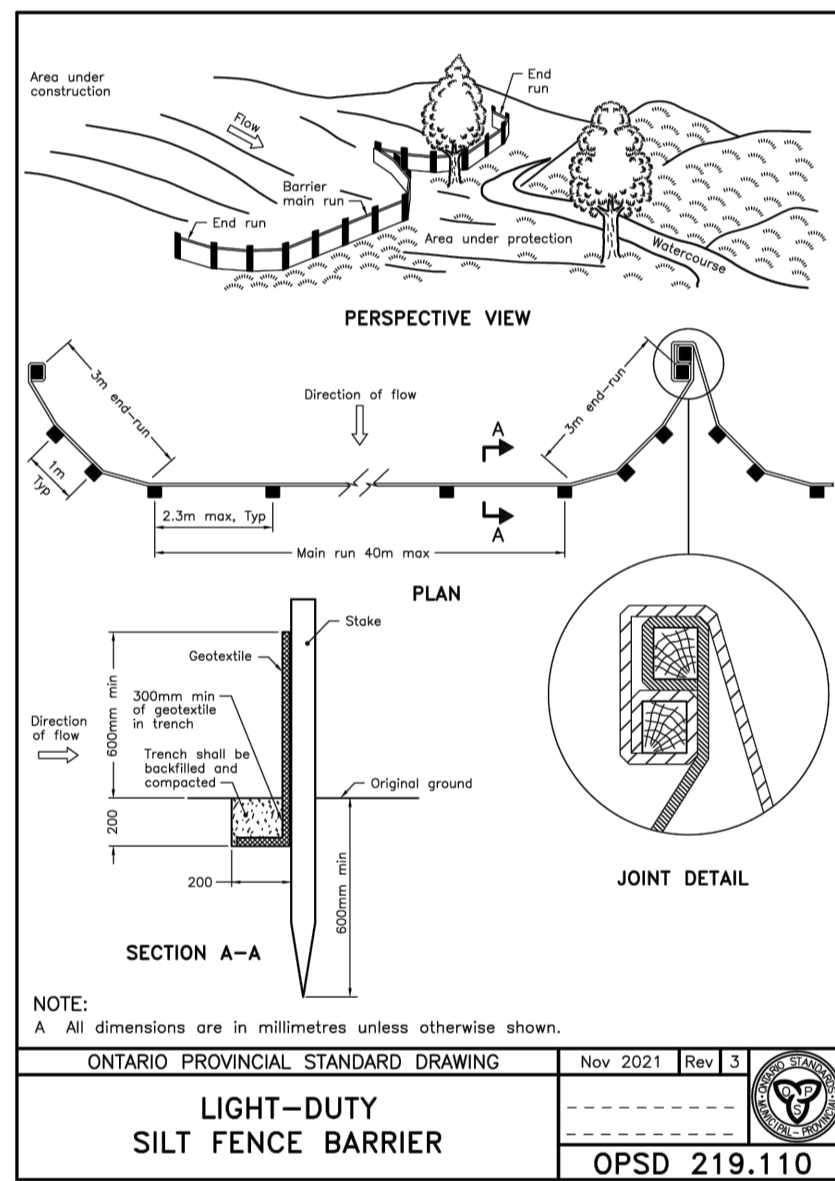
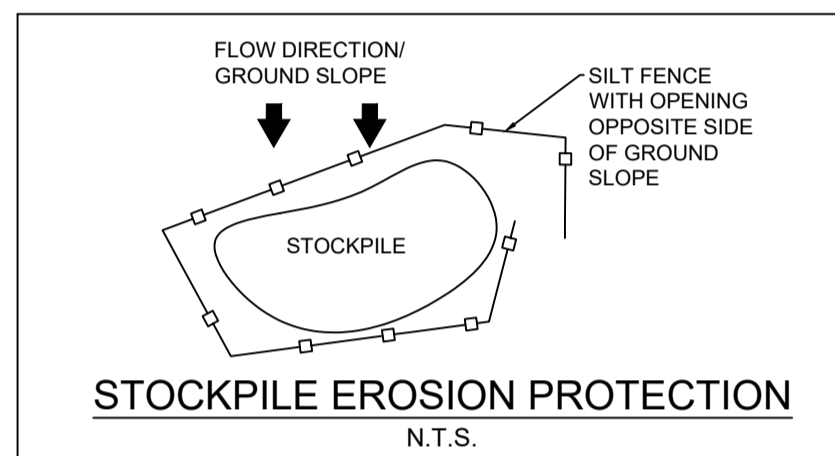
PROJECT NO.

210341

DATE

27 JULY 2021

C001



EROSION AND SEDIMENT CONTROL MEASURES:

**** CONTRACTOR IS RESPONSIBLE FOR ALL INSTALLATION, MONITORING, REPAIR AND REMOVAL OF ALL EROSION AND SEDIMENT CONTROL FEATURES ****

1. PRIOR TO START OF CONSTRUCTION:

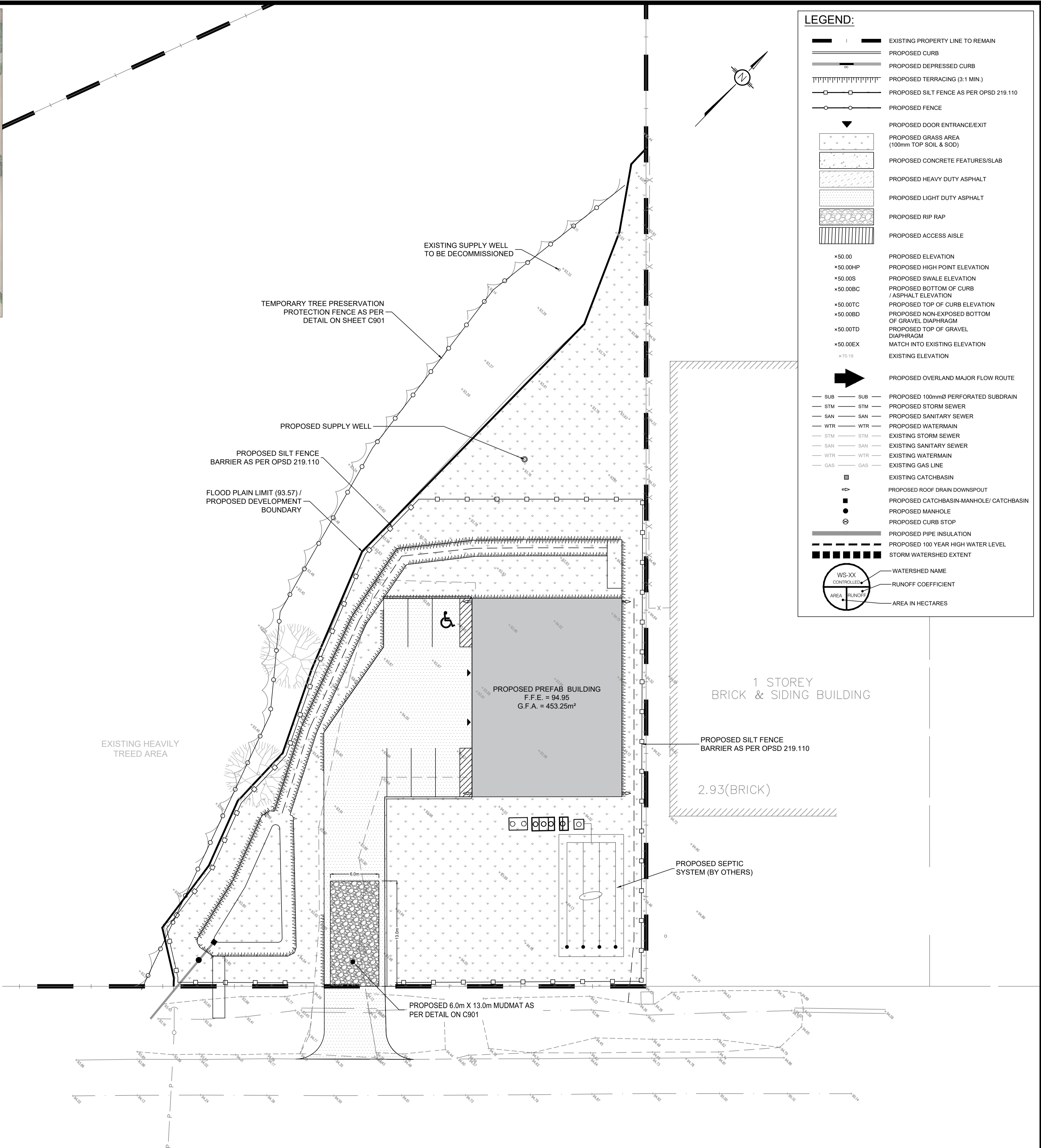
- PRIOR TO THE REMOVAL OF ANY VEGETATIVE COVER, MOVING OF SOIL AND CONSTRUCTION:
- INSTALL SILT FENCE IMMEDIATELY DOWNSTREAM FROM AREAS TO BE DISTURBED (SEE PLAN FOR LOCATION).
- INSPECT MEASURES IMMEDIATELY AFTER INSTALLATION.

2. DURING CONSTRUCTION:

- WORK TO BE DONE IN THE VICINITY OF MAJOR WATERWAYS TO BE CARRIED OUT FROM JULY TO SEPTEMBER ONLY.
- MINIMIZE THE EXTENT OF DISTURBED AREAS AND THE DURATION OF EXPOSURE.
- PROTECT DISTURBED AREAS FROM RUNOFF.
- PROVIDE TEMPORARY COVER SUCH AS SEEDING OR MULCHING IF DISTURBED AREA WILL NOT BE REHABILITATED WITHIN 30 DAYS.
- INSPECT SILT FENCES, FILTER CLOTHS AND CATCH BASIN SUMPS WEEKLY AND AFTER EVERY MAJOR STORM EVENT. CLEAN AND REPAIR WHEN NECESSARY.
- CONSTRUCT SWALES AS PER DETAIL.
- PLAN TO BE REVIEWED AND REVISED AS REQUIRED DURING CONSTRUCTION.
- EROSION CONTROL FENCING TO BE ALSO INSTALLED AROUND THE BASE OF ALL STOCKPILES.
- DO NOT LOCATE TOPSOIL PILES AND EXCAVATION MATERIAL CLOSER THAN 2.5m FROM ANY PAVED SURFACE, OR ONE WHICH IS TO BE PAVED BEFORE THE PILE IS REMOVED. ALL TOPSOIL PILES ARE TO BE SEEDDED IF THEY ARE TO REMAIN ON SITE LONG ENOUGH FOR SEEDS TO GROW LONGER THAN 30 DAYS.
- CONTROL WIND-BLOWN DUST OFF SITE TO ACCEPTABLE LEVELS BY SEEDING TOPSOIL PILES AND OTHER AREAS TEMPORARILY (PROVIDE WATERING AS REQUIRED).
- ALL EROSION CONTROL STRUCTURE TO REMAIN IN PLACE UNTIL ALL DISTURBED GROUND SURFACES HAVE BEEN STABILIZED EITHER BY PAVING OR RESTORATION OF VEGETATIVE GROUND COVER.
- NO ALTERNATE METHODS OF EROSION PROTECTION SHALL BE PERMITTED UNLESS APPROVED BY THIS CONSULTING ENGINEER AND THE CITY DEPARTMENT OF PUBLIC WORKS.
- CONTRACTOR RESPONSIBLE FOR CITY ROADWAY AND SIDEWALK TO BE CLEANED OF ALL SEDIMENT FROM VEHICULAR TRACKING ETC. AT THE END OF EACH WORK DAY.
- PROVIDE GRAVEL ENTRANCE WHEREVER EQUIPMENT LEAVES THE SITE TO PREVENT MUD TRACKING ONTO PAVED SURFACES. GRAVEL BED SHALL BE A MINIMUM OF 15m LONG, 4M WIDE AND 0.3m DEEP AND SHALL CONSIST OF COARSE (60mm CRUSHERT-RUN LIMESTONE) MATERIAL. MAINTAIN GRAVEL ENTRANCE IN CLEAN CONDITION.
- DURING WET CONDITIONS, TIRES OF ALL VEHICLES/EQUIPMENT LEAVING THE SITE ARE TO BE SCRAPPED.
- ANY MUD/MATERIAL TRACKED ONTO THE ROAD SHALL BE REMOVED IMMEDIATELY BY HAND OR RUBBER TIRE LOADER.
- TAKE ALL NECESSARY STEPS TO PREVENT BUILDING MATERIAL, CONSTRUCTION DEBRIS OR WASTE BEING SPILLED OR TRACKED ONTO ADJUTING PROPERTIES OR PUBLIC STREETS DURING CONSTRUCTION AND PROCEED IMMEDIATELY TO CLEAN UP ANY AREAS SO AFFECTED.

3. AFTER CONSTRUCTION:

- PROVIDE PERMANENT COVER CONSISTING OF TOPSOIL AND SEED TO DISTURBED AREAS.
- REMOVE STRAW BALE FLOW CHECK DAMS, SILT FENCES AND FILTER CLOTHS ON CATCH BASINS AND MANHOLE COVERS AFTER DISTURBED AREAS HAVE BEEN REHABILITATED AND STABILIZED.
- INSPECT AND CLEAN CATCH BASIN SUMPS AND STORM SEWERS.



LEGEND:

	EXISTING PROPERTY LINE TO REMAIN
	PROPOSED CURB
	PROPOSED DEPRESSED CURB
	PROPOSED TERRACING (3:1 MIN.)
	PROPOSED SILT FENCE AS PER OPSD 219.110
	PROPOSED FENCE
	PROPOSED DOOR ENTRANCE/EXIST
	PROPOSED GRASS AREA (100mm TOP SOIL & SOD)
	PROPOSED CONCRETE FEATURES/SLAB
	PROPOSED HEAVY DUTY ASPHALT
	PROPOSED LIGHT DUTY ASPHALT
	PROPOSED RIP RAP
	PROPOSED ACCESS AISLE
	PROPOSED ELEVATION
	PROPOSED HIGH POINT ELEVATION
	PROPOSED SWALE ELEVATION
	PROPOSED BOTTOM OF CURB / ASPHALT ELEVATION
	PROPOSED TOP OF CURB ELEVATION
	PROPOSED NON-EXPOSED BOTTOM OF GRAVEL DIAPHRAGM
	PROPOSED TOP OF GRAVEL DIAPHRAGM
	MATCH INTO EXISTING ELEVATION
	EXISTING ELEVATION
	PROPOSED OVERLAND MAJOR FLOW ROUTE
	PROPOSED 100mm PERFORATED SUBDRAIN
	PROPOSED STORM SEWER
	PROPOSED SANITARY SEWER
	PROPOSED WATERMAIN
	EXISTING STORM SEWER
	EXISTING SANITARY SEWER
	EXISTING WATERMAIN
	EXISTING GAS LINE
	EXISTING CATCHBASIN
	PROPOSED ROOF DRAIN DOWNSPOUT
	PROPOSED CATCHBASIN-MANHOLE/ CATCHBASIN
	PROPOSED MANHOLE
	PROPOSED CURB STOP
	PROPOSED PIPE INSULATION
	PROPOSED 100 YEAR HIGH WATER LEVEL
	STORM WATERSHED EXTENT
	WATERSHED NAME
	RUNOFF COEFFICIENT
	AREA IN HECTARES

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CLIENT: **AL ROBERTS**

DESIGNED BY: M.A. DRAWN BY: M.A. APPROVED BY: M.B.

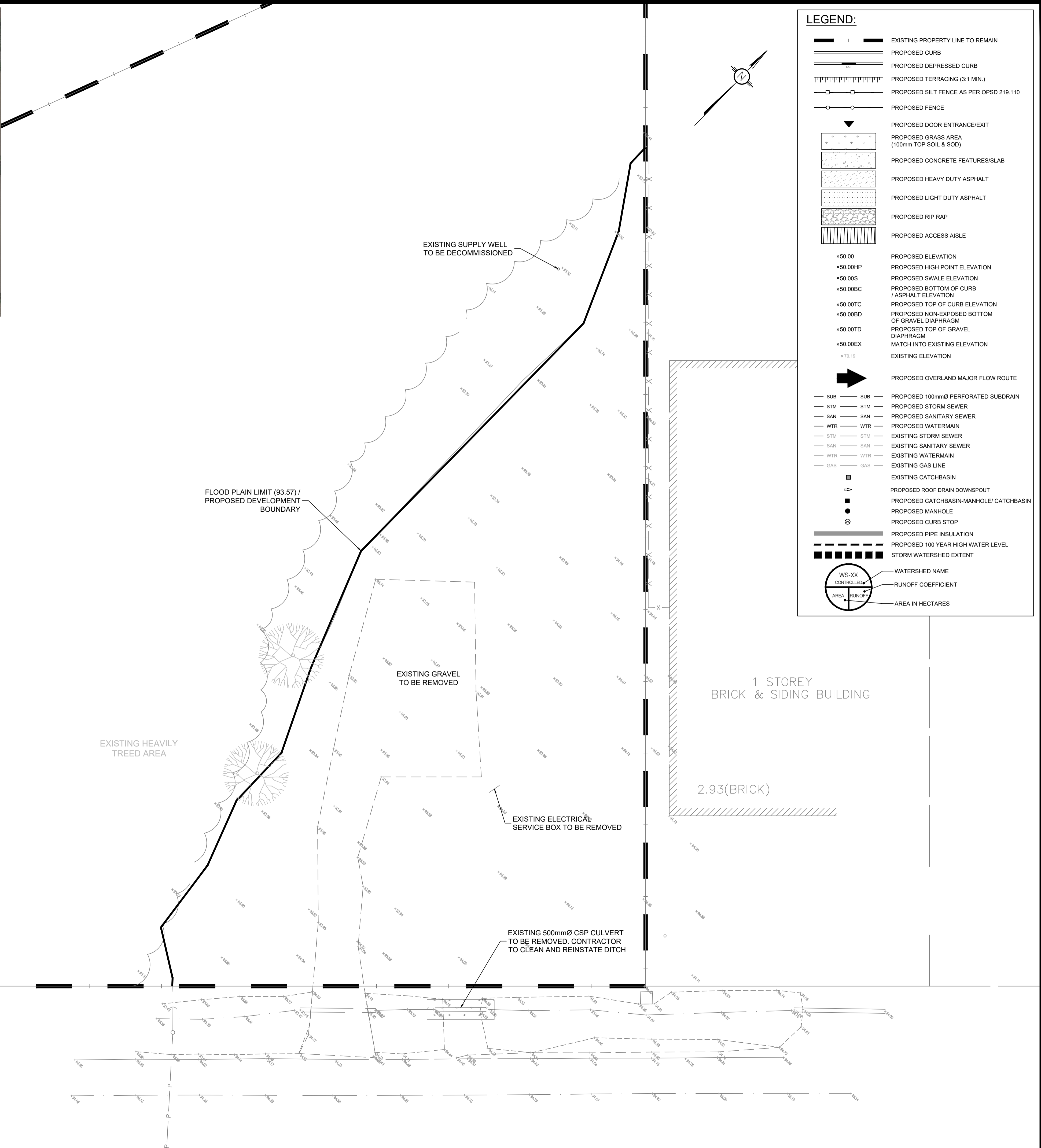
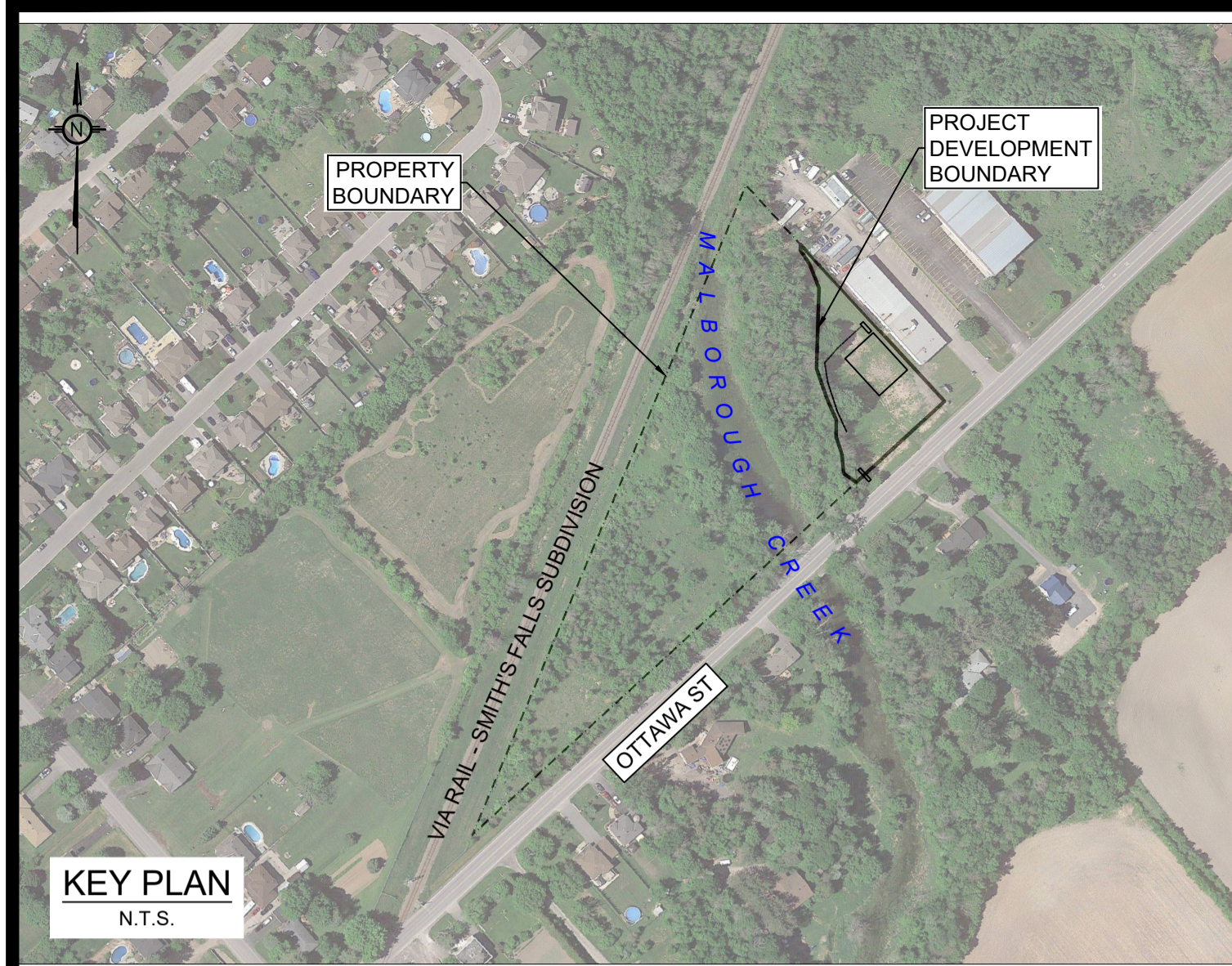
PROJECT: **PROPOSED DOG KENNEL
5969 OTTAWA STREET,
OTTAWA, ON**

DRAWING TITLE: **EROSION AND SEDIMENT
CONTROL PLAN**

PROJECT NO: **210341**

DATE: **27 JULY 2021**

C101



LEGEND:

- EXISTING PROPERTY LINE TO REMAIN
- PROPOSED CURB
- PROPOSED DEPRESSED CURB
- PROPOSED TERRACING (3:1 MIN.)
- PROPOSED SILT FENCE AS PER OPSD 219.110
- PROPOSED FENCE
- PROPOSED DOOR ENTRANCE/EXIST
- PROPOSED GRASS AREA (100mm TOP SOIL & SOD)
- PROPOSED CONCRETE FEATURES/SLAB
- PROPOSED HEAVY DUTY ASPHALT
- PROPOSED LIGHT DUTY ASPHALT
- PROPOSED RIP RAP
- PROPOSED ACCESS AISLE
- PROPOSED ELEVATION
- PROPOSED HIGH POINT ELEVATION
- PROPOSED SWALE ELEVATION
- PROPOSED BOTTOM OF CURB / ASPHALT ELEVATION
- PROPOSED TOP OF CURB ELEVATION
- PROPOSED NON-EXPOSED BOTTOM OF GRAVEL DIAPHRAGM
- PROPOSED TOP OF GRAVEL DIAPHRAGM
- MATCH INTO EXISTING ELEVATION
- EXISTING ELEVATION
- PROPOSED OVERLAND MAJOR FLOW ROUTE
- PROPOSED 100mmØ PERFORATED SUBDRAIN
- PROPOSED STORM SEWER
- PROPOSED SANITARY SEWER
- PROPOSED WATERMAIN
- EXISTING STORM SEWER
- EXISTING SANITARY SEWER
- EXISTING WATERMAIN
- EXISTING GAS LINE
- EXISTING CATCHBASIN
- PROPOSED ROOF DRAIN DOWNSPOUT
- PROPOSED CATCHBASIN-MANHOLE/ CATCHBASIN
- PROPOSED MANHOLE
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- PROPOSED PIPE INSULATION
- PROPOSED 100 YEAR HIGH WATER LEVEL
- STORM WATERSHED EXTENT
- WATERSHED NAME
- RUNOFF COEFFICIENT
- AREA IN HECTARES

USE AND INTERPRETATION OF DRAWINGS

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SCALE: 1:250

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NOT AUTHENTIC UNLESS SIGNED AND DATED

LRJ
ENGINEERING | INGÉNIÉRIE
5430 Canotek Road | Ottawa, ON, K1J 9G2
www.lrl.ca | (613) 842-3434

CLIENT: **AL ROBERTS**

DESIGNED BY: M.A. DRAWN BY: M.A. APPROVED BY: M.B.

PROJECT: **PROPOSED DOG KENNEL
5969 OTTAWA STREET,
OTTAWA, ON**

DRAWING TITLE: **DEMOLITION PLAN**

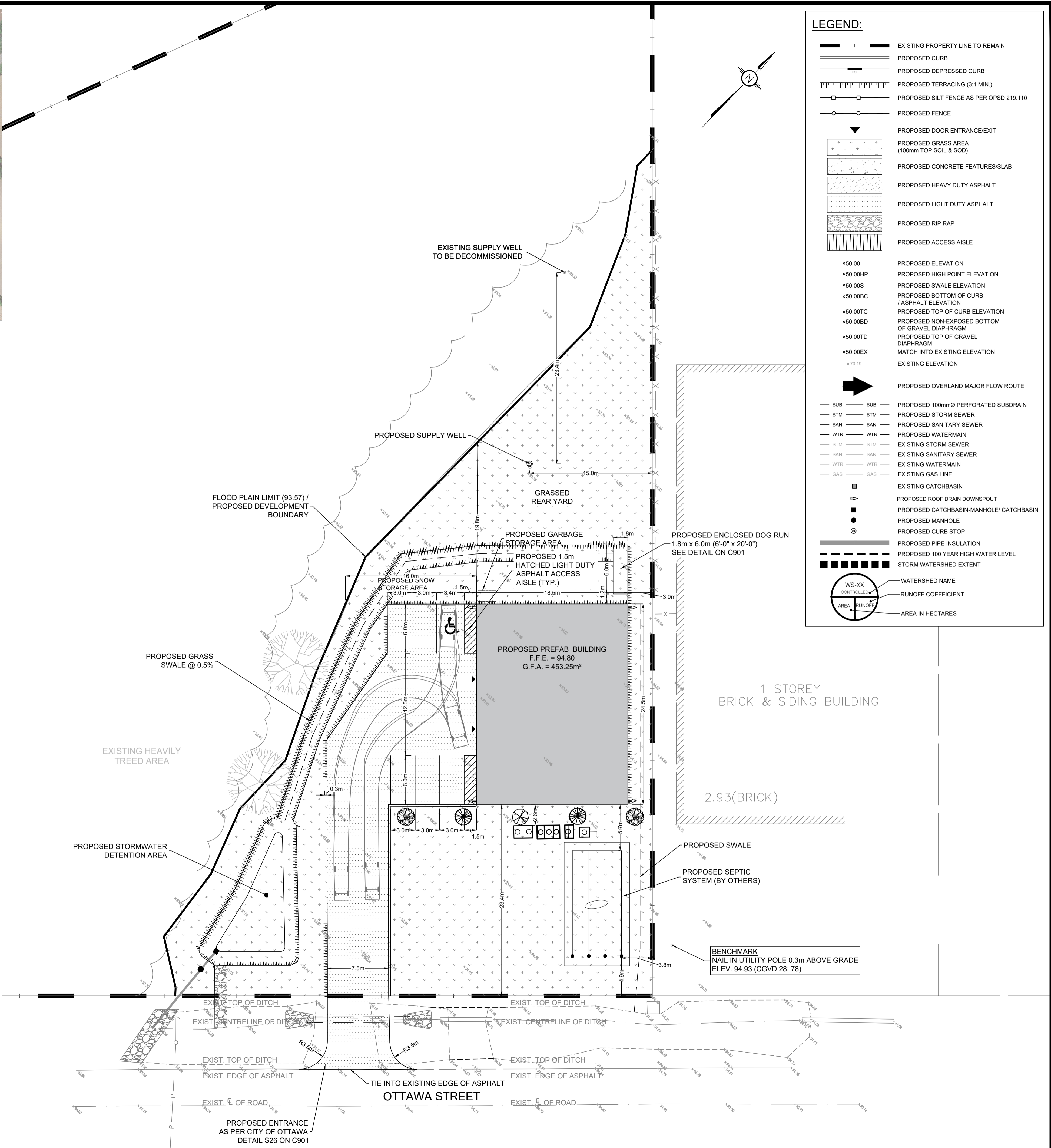
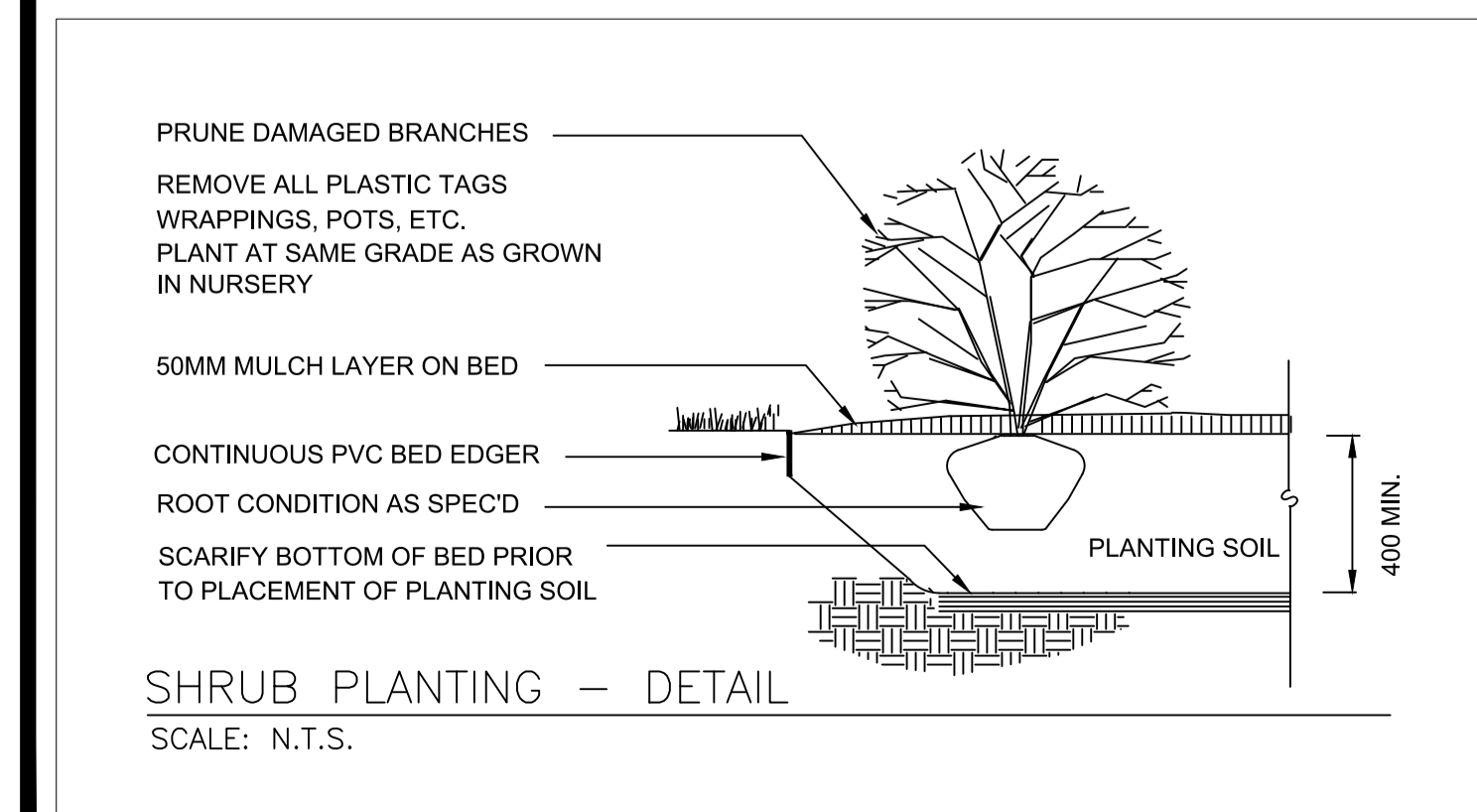
PROJECT NO.: 210341 DATE: 27 JULY 2021

C102



DETAILS OF DEVELOPMENT			
DATA	REQUIRED	PROVIDED	
ZONING	RG3[385r] (RURAL GENERAL)		
SETBACKS		FY	15.0m
		RY	15.0m
		INT.SY	3.0m
		EXT.SY	3.0m
TOTAL LOT AREA (sqm)	22160 sqm		
BUILDING COVERAGE	50 % (MAX)	14 %	
BUILDING HEIGHT	15 m (MAX)	7.62 m (25')	
GROSS FLOOR AREA	453.25 sqm		
No. of UNITS	1		
LOADING SPACES	N/A	N/A	
PARKING:	4	5 + 1 HC	
No. OF STOREYS		2	
OTHER:			

LANDSCAPING LEGEND				
SYMBOL	COMMON NAME	QUANTITY	SIZE	IMAGE
	LILACS	2	TALL	
	BRIDAL WREATH	2	TALL	
	BURNING BUSH	1	MEDIUM	



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CLIENT

AL ROBERTS

DESIGNED BY: M.A. DRAWN BY: M.A. APPROVED BY: M.B.

PROJECT

PROPOSED DOG KENNEL
5969 OTTAWA STREET,
OTTAWA, ON

DRAWING TITLE

SITE DEVELOPMENT PLAN

PROJECT NO.
210341

DATE
27 JULY 2021

C201



TOPOGRAPHIC INFORMATION
 TOPOGRAPHIC INFORMATION PROVIDED BY H.A.KEN SHIPMAN SURVEYING LTD.
 FILE NO.: 20-12471 B
 DATE: JULY 19, 2021 (SURVEY DATE: JULY 05, 2021)

ELEVATION NOTES:

- ELEVATIONS ARE IN METERS AND ARE GEODETIC, DERIVED FROM GSC BENCHMARK 68-U-124, HAVING A PUBLISHED ELEVATION OF 93.186M. (CGVD 28.78)
- IT IS THE RESPONSIBILITY OF THE USER OF THIS INFORMATION TO VERIFY THAT THE JOB BENCHMARK HAS NOT BEEN ALTERED OR DISTURBED AND THAT ITS RELATIVE ELEVATION AND DESCRIPTION AGREE WITH THE INFORMATION SHOWN ON THIS DRAWING.

UTILITIES NOTES:

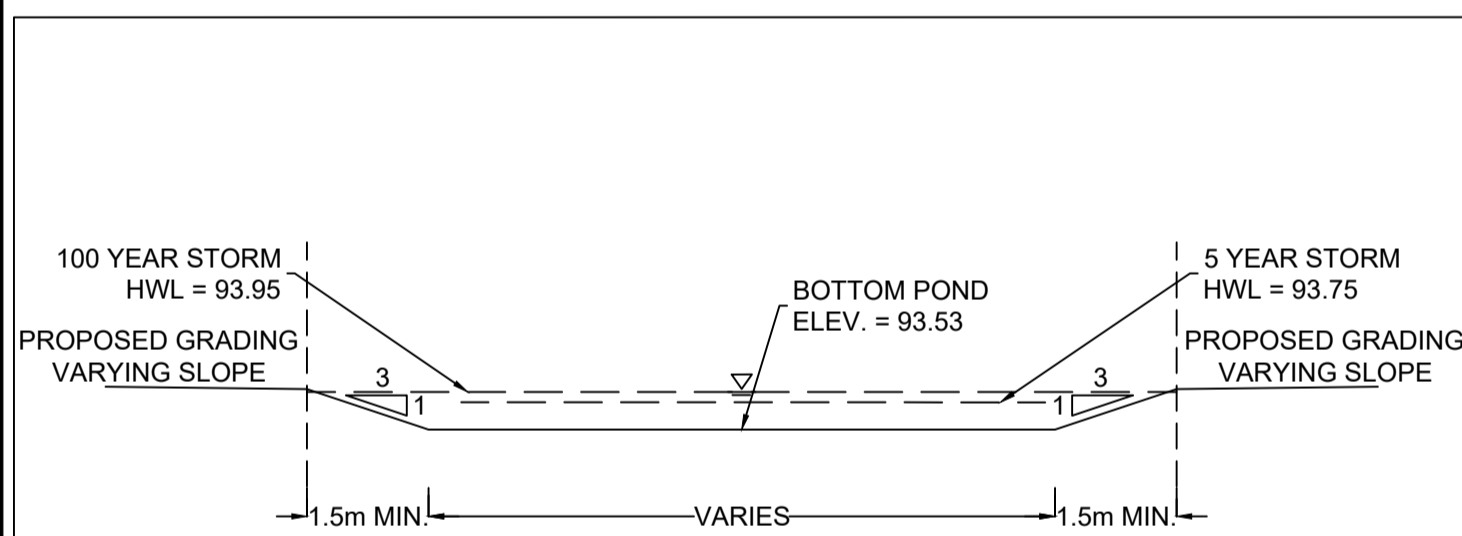
- THIS DRAWING CANNOT BE ACCEPTED AS ACKNOWLEDGING ALL OF THE UTILITIES AND IT WILL BE THE RESPONSIBILITY OF THE USER TO CONTACT THE RESPECTIVE UTILITY AUTHORITIES FOR CONFIRMATION.
- A FIELD LOCATION OF UNDERGROUND PLANT BY THE PERTINENT UTILITY AUTHORITY IS MANDATORY BEFORE ANY WORK INVOLVING BREAKING GROUND, PROBING, EXCAVATING ETC.

PAVEMENT STRUCTURE

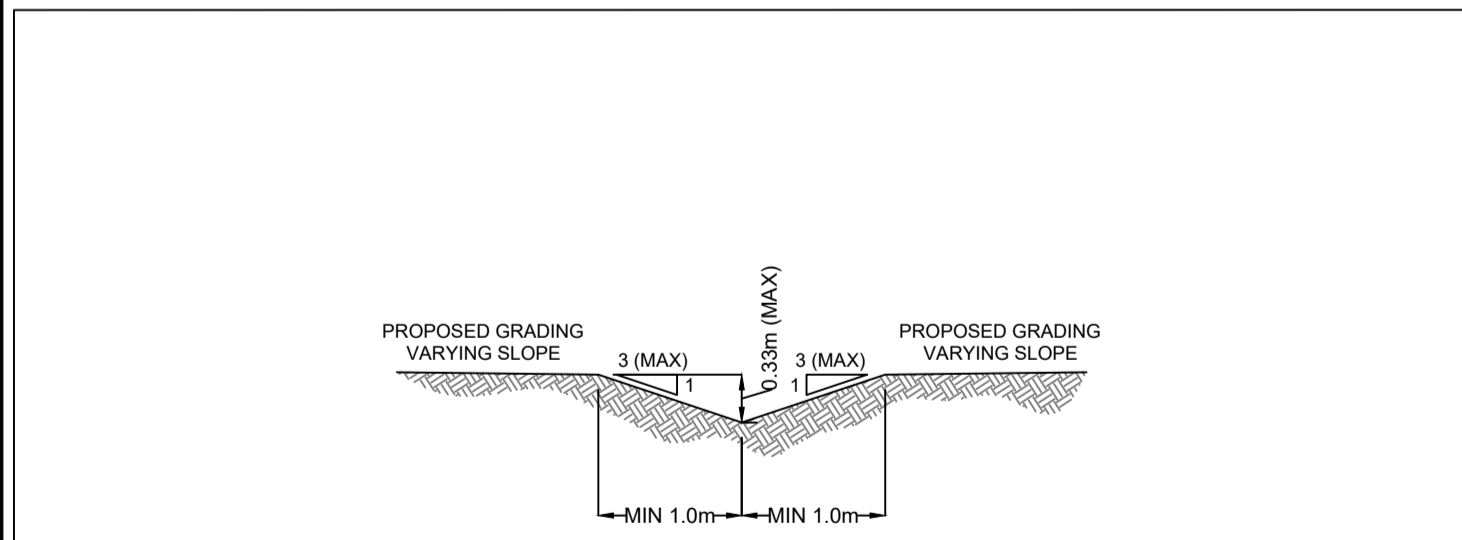
COURSE	MATERIAL	THICKNESS (mm)	
			AUTOMOBILE PARKING
SURFACE	HL.3 A/C (PG 58-28)	50	
BINDER	HL.8 A/C (PG 58-28)	—	
BASECOURSE	OPSS GRANULAR "A"	150	
SUBBASE	OPSS GRANULAR "B" TYPE II	350	

NOTE:
 IN PREPARATION FOR PAVEMENT CONSTRUCTION AT THIS SITE ANY SURFICIAL OR NEAR SURFACE/SUBGRADE LEVEL TOPSOIL AND ANY SOFT, WET OR DELETERIOUS MATERIALS SHOULD BE REMOVED FROM THE PROPOSED PAVED AREAS. THE EXPOSED SUBGRADE SHOULD BE INSPECTED AND APPROVED BY GEOTECHNICAL PERSONNEL AND ANY SOFT AREAS EVIDENT SHOULD BE SUBEXCAVATED AND REPLACED WITH SUITABLE EARTH BORROW APPROVED BY THE GEOTECHNICAL ENGINEER. THE SUBGRADE SHOULD BE SHAPED AND CROWNED TO PROMOTE DRAINAGE OF THE SITE DRAINAGE STRUCTURES. FOLLOWING APPROVAL OF THE PREPARATION OF THE SUBGRADE, THE PAVEMENT GRANULARS MAY BE PLACED.

PAVEMENT STRUCTURE AS PER GEOTECHNICAL REPORT PREPARED BY LRL, DATED OCTOBER 2021.

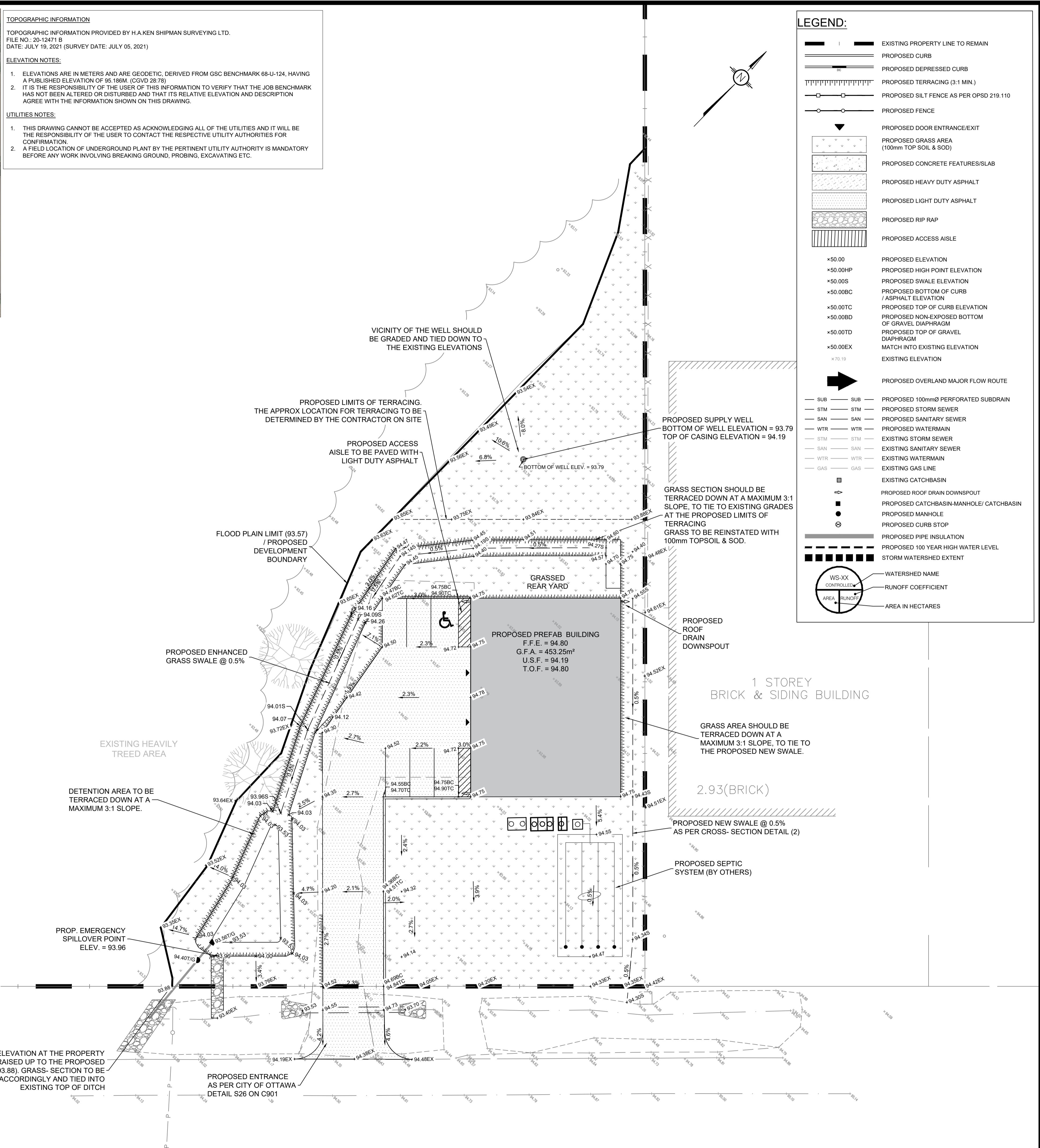


TYPICAL STORMWATER RETENTION AREA CROSS SECTION (1)
(N.T.S.)



TYPICAL STORMWATER SWALE CROSS SECTION (2)
(N.T.S.)

NOTE:
 AS PER GEOTECHNICAL REPORT PREPARED BY LRL, DATED OCTOBER 2021, THE ALLOWABLE GRADE RAISE FOR THE BUILDING FOUNDATION SHOULD BE RESTRICTED TO 2.5 M.



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1 STOREY BRICK & SIDING BUILDING

GRASS AREA SHOULD BE TERRACED DOWN AT A MAXIMUM 3:1 SLOPE, TO TIE TO THE PROPOSED NEW SWALE.

2.93(BRICK)

PROPOSED NEW SWALE @ 0.5% AS PER CROSS-SECTION DETAIL (2)

PROPOSED SEPTIC SYSTEM (BY OTHERS)

PROPOSED ENTRANCE AS PER CITY OF OTTAWA DETAIL S26 ON C901

EXISTING ELEVATION AT THE PROPERTY LINE TO BE RAISED UP TO THE PROPOSED ELEVATION (93.88). GRASS-SECTION TO BE GRADED ACCORDINGLY AND TIED INTO EXISTING TOP OF DITCH

PROPOSED SUPPLY WELL. BOTTOM OF WELL ELEVATION = 93.79. TOP OF CASING ELEVATION = 94.19

VICINITY OF THE WELL SHOULD BE GRADED AND TIED DOWN TO THE EXISTING ELEVATIONS

PROPOSED LIMITS OF TERRACING. THE APPROX LOCATION FOR TERRACING TO BE DETERMINED BY THE CONTRACTOR ON SITE

PROPOSED ACCESS AISLE TO BE PAVED WITH LIGHT DUTY ASPHALT

PROPOSED ENHANCED GRASS SWALE @ 0.5%

EXISTING HEAVILY TREED AREA

DETENTION AREA TO BE TERRACED DOWN AT A MAXIMUM 3:1 SLOPE.

PROP. EMERGENCY SPILLOVER POINT ELEV. = 93.96

PROPOSED ROOF DRAIN DOWNSPOUT

PROPOSED 100 YEAR HIGH WATER LEVEL

PROPOSED PIPE INSULATION

PROPOSED CURB STOP

PROPOSED CATCHBASIN-MANHOLE/ CATCHBASIN

PROPOSED MANHOLE

PROPOSED CURB STOP

PROPOSED PIPE INSULATION

PROPOSED 100 YEAR HIGH WATER LEVEL

STORM WATERSHED EXTENT

Watershed Name

Runoff Coefficient

Area Runoff

Area in Hectares

NOT AUTHENTIC UNLESS SIGNED AND DATED

LRJ
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 5430 Canotek Road | Ottawa, ON, K1J 9G2
 www.lrl.ca | (613) 842-3434

CLIENT: AL ROBERTS

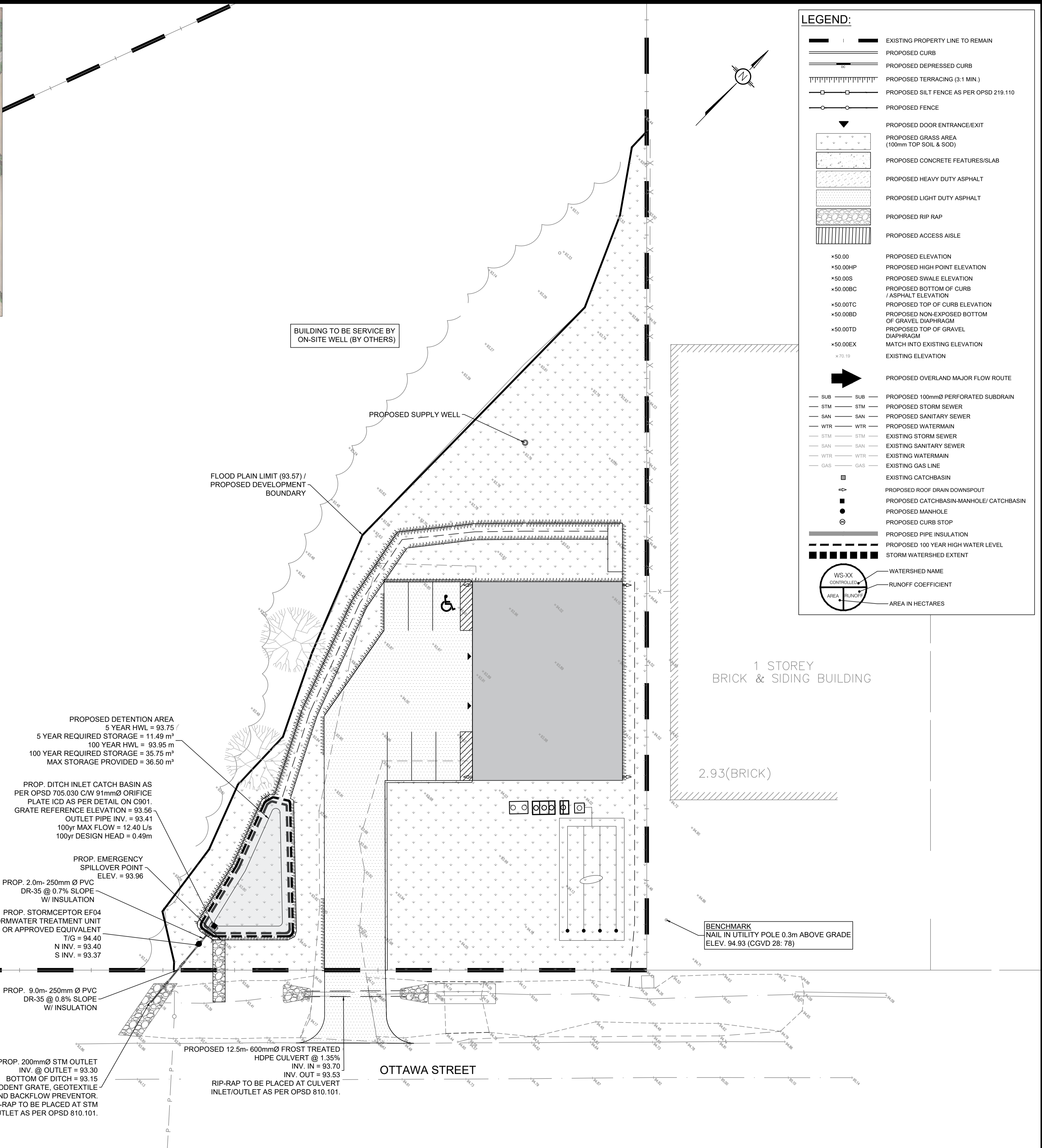
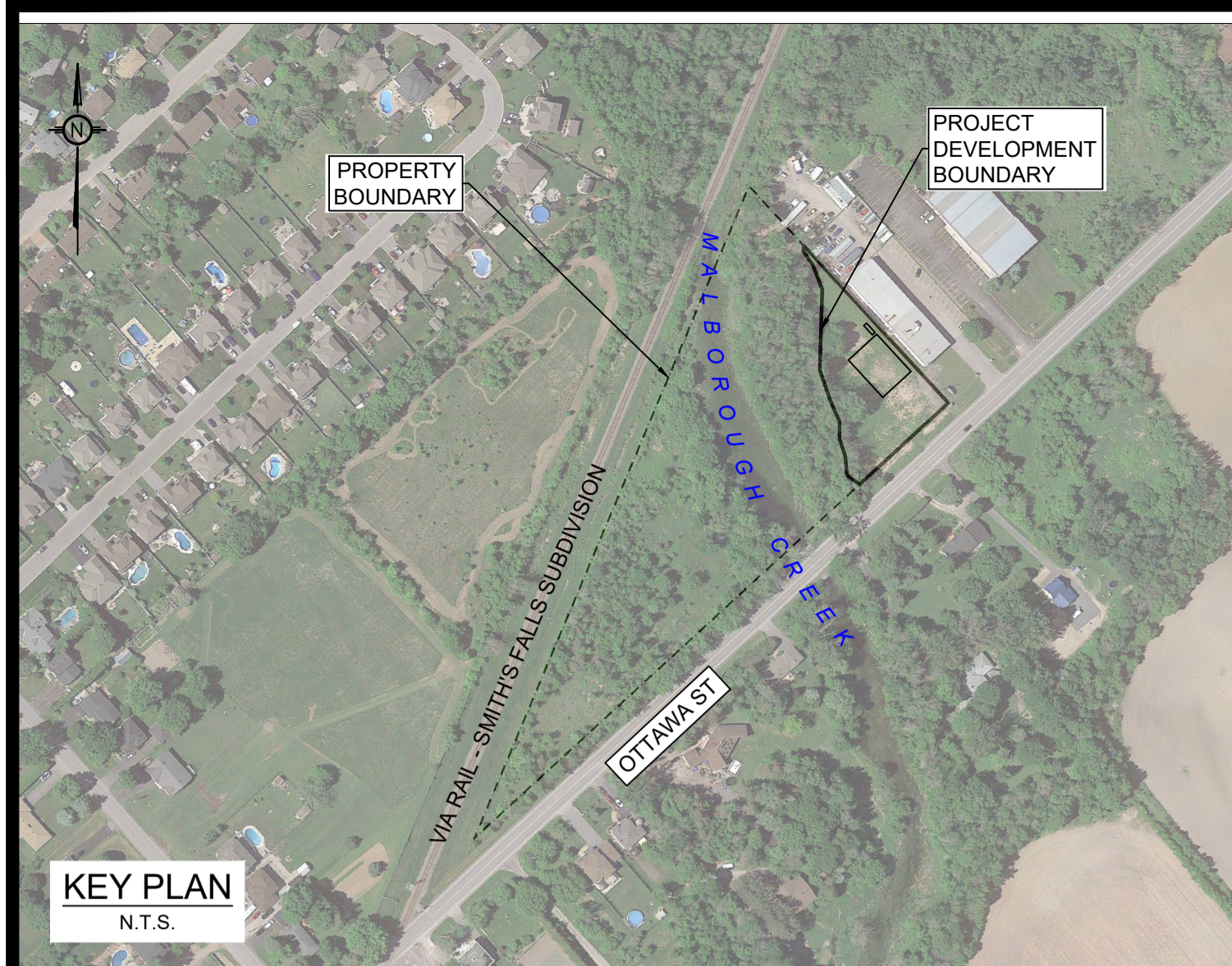
DESIGNED BY: M.A. DRAWN BY: M.A. APPROVED BY: M.B.

PROJECT: PROPOSED DOG KENNEL 5969 OTTAWA STREET, OTTAWA, ON

DRAWING TITLE: GRADING AND DRAINAGE PLAN

PROJECT NO.: 210341 DATE: 27 JULY 2021

C301



LEGEND:

- | — EXISTING PROPERTY LINE TO REMAIN
- | — PROPOSED CURB
- | — PROPOSED DEPRESSED CURB
- | — PROPOSED TERRACING (3:1 MIN.)
- | — PROPOSED SILT FENCE AS PER OPSD 219.110
- | — PROPOSED FENCE
- ▼ PROPOSED DOOR ENTRANCE/EXIT
- PROPOSED GRASS AREA (100mm TOP SOIL & SOD)
- PROPOSED CONCRETE FEATURES/SLAB
- PROPOSED HEAVY DUTY ASPHALT
- PROPOSED LIGHT DUTY ASPHALT
- PROPOSED RIP RAP
- PROPOSED ACCESS AISLE
- ×50.00 PROPOSED ELEVATION
- ×50.00HP PROPOSED HIGH POINT ELEVATION
- ×50.00S PROPOSED SWALE ELEVATION
- ×50.00BC PROPOSED BOTTOM OF CURB / ASPHALT ELEVATION
- ×50.00TC PROPOSED TOP OF CURB ELEVATION
- ×50.00BD PROPOSED NON-EXPOSED BOTTOM OF GRAVEL DIAPHRAGM
- ×50.00TD PROPOSED TOP OF GRAVEL DIAPHRAGM
- ×50.00EX MATCH INTO EXISTING ELEVATION
- ×70.19 EXISTING ELEVATION
- ➔ PROPOSED OVERLAND MAJOR FLOW ROUTE
- SUB — SUB — PROPOSED 100mmØ PERFORATED SUBDRAIN
- STM — STM — PROPOSED STORM SEWER
- SAN — SAN — PROPOSED SANITARY SEWER
- WTR — WTR — PROPOSED WATERMAIN
- STM — STM — EXISTING STORM SEWER
- SAN — SAN — EXISTING SANITARY SEWER
- WTR — WTR — EXISTING WATERMAIN
- GAS — GAS — EXISTING GAS LINE
- EXISTING CATCHBASIN
- ➔ PROPOSED ROOF DRAIN DOWNSPOUT
- PROPOSED CATCHBASIN-MANHOLE/ CATCHBASIN
- PROPOSED MANHOLE
- ⊗ PROPOSED CURB STOP
- PROPOSED PIPE INSULATION
- PROPOSED 100 YEAR HIGH WATER LEVEL
- STORM WATERSHED EXTENT
- WS-XX WATERSHED NAME
- AREA RUNOFF RUNOFF COEFFICIENT
- AREA IN HECTARES

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No.	REVISIONS	BY	DATE
05	RE-ISSUED FOR APPROVAL	S.V.	25 JUN 2024
04	RE-ISSUED FOR APPROVAL	S.V.	16 MAY 2024
03	RE-ISSUED FOR APPROVAL	S.V.	25 AUG 2023
02	ISSUED FOR APPROVAL	M.L.	02 JUN 2022
01	ISSUED FOR APPROVAL	M.A.	25 NOV 2021

PROPOSED DETENTION AREA
 5 YEAR HWL = 93.75 /
 5 YEAR REQUIRED STORAGE = 11.49 m³
 100 YEAR HWL = 93.95 m³
 100 YEAR REQUIRED STORAGE = 35.75 m³
 MAX STORAGE PROVIDED = 36.50 m³

PROP. DITCH INLET CATCH BASIN AS PER OPSD 705.030 C/W 91mmØ ORIFICE PLATE ICD AS PER DETAIL ON C901. GRATE REFERENCE ELEVATION = 93.56
 OUTLET PIPE INV. = 93.41
 100yr MAX FLOW = 12.40 L/s
 100yr DESIGN HEAD = 0.49m

PROP. EMERGENCY SPILLOVER POINT ELEV. = 93.96

PROP. 2.0m- 250mm Ø PVC DR-35 @ 0.7% SLOPE W/ INSULATION

PROP. STORMCEPTOR EF04 STORMWATER TREATMENT UNIT OR APPROVED EQUIVALENT
 T/G = 94.40
 N INV. = 93.40
 S INV. = 93.37

PROP. 9.0m- 250mm Ø PVC DR-35 @ 0.8% SLOPE W/ INSULATION

PROP. 200mmØ STM OUTLET INV. @ OUTLET = 93.30
 BOTTOM OF DITCH = 93.15
 C/W RODENT GRATE, GEOTEXTILE LINING, AND BACKFLOW PREVENTOR. RIP-RAP TO BE PLACED AT STM INLET/OUTLET AS PER OPSD 810.101.

PROPOSED 12.5m- 600mmØ FROST TREATED HDPE CULVERT @ 1.35%
 INV. IN = 93.70
 INV. OUT = 93.53
 RIP-RAP TO BE PLACED AT CULVERT INLET/OUTLET AS PER OPSD 810.101.

1 STOREY BRICK & SIDING BUILDING

2.93(BRICK)

BENCHMARK NAIL IN UTILITY POLE 0.3m ABOVE GRADE ELEV. 94.93 (CGVD 28: 78)

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CLIENT: AL ROBERTS

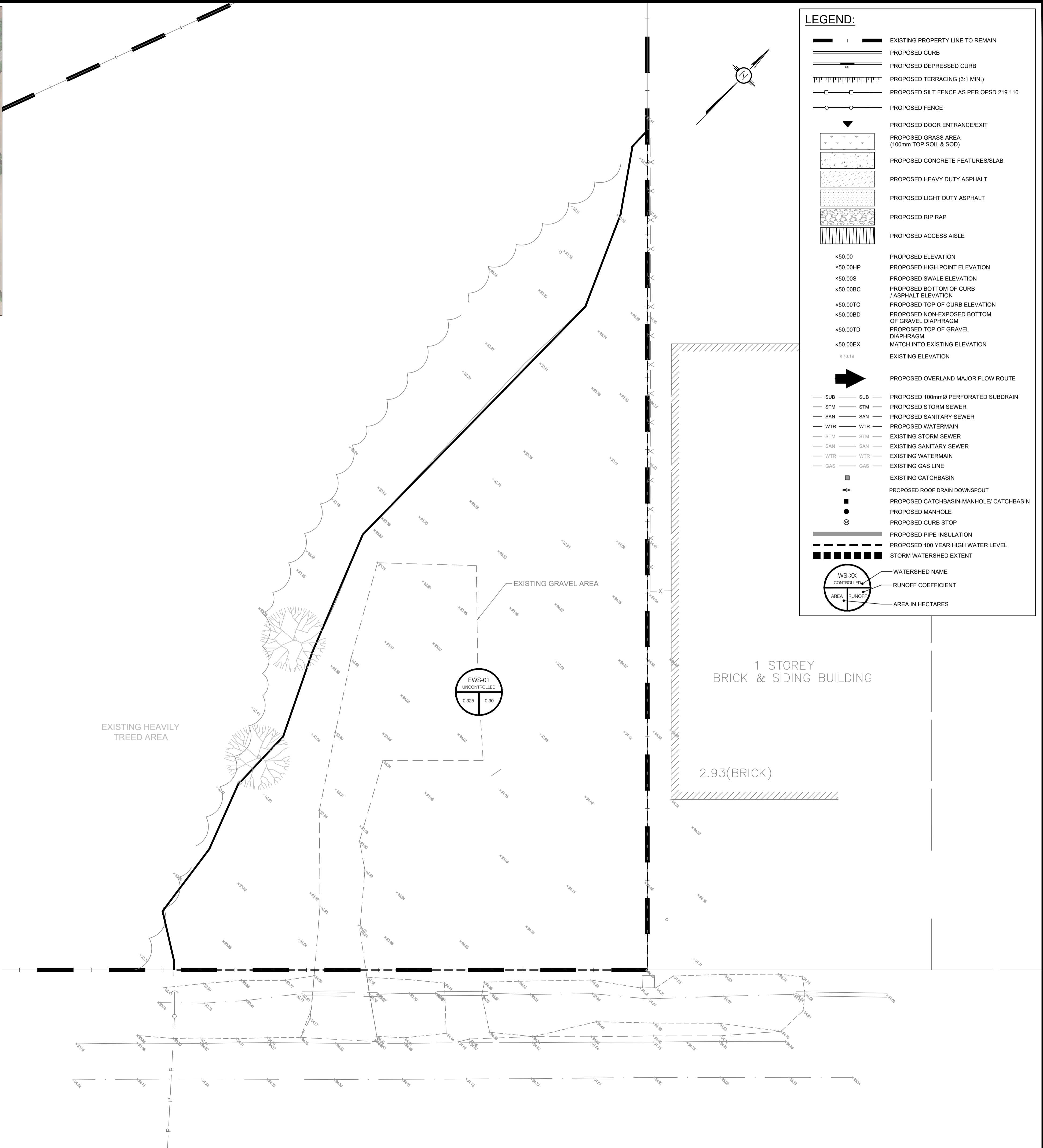
DESIGNED BY: M.A. DRAWN BY: M.A. APPROVED BY: M.B.

PROJECT: PROPOSED DOG KENNEL 5969 OTTAWA STREET, OTTAWA, ON

DRAWING TITLE: STORMWATER MANAGEMENT & SERVICING PLAN

PROJECT NO. 210341 DATE 27 JULY 2021

C601



LEGEND:

- EXISTING PROPERTY LINE TO REMAIN
- PROPOSED CURB
- PROPOSED DEPRESSED CURB
- PROPOSED TERRACING (3:1 MIN.)
- PROPOSED SILT FENCE AS PER OPSD 219.110
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- PROPOSED GRASS AREA (100mm TOP SOIL & SOD)
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- PROPOSED LIGHT DUTY ASPHALT
- PROPOSED RIP RAP
- PROPOSED ACCESS AISLE
- PROPOSED ELEVATION
- PROPOSED HIGH POINT ELEVATION
- PROPOSED SWALE ELEVATION
- PROPOSED BOTTOM OF CURB / ASPHALT ELEVATION
- PROPOSED TOP OF CURB ELEVATION
- PROPOSED NON-EXPOSED BOTTOM OF GRAVEL DIAPHRAGM
- PROPOSED TOP OF GRAVEL DIAPHRAGM
- MATCH INTO EXISTING ELEVATION
- EXISTING ELEVATION
- PROPOSED OVERLAND MAJOR FLOW ROUTE
- PROPOSED 100mmØ PERFORATED SUBDRAIN
- PROPOSED STORM SEWER
- PROPOSED SANITARY SEWER
- PROPOSED WATERMAIN
- EXISTING STORM SEWER
- EXISTING SANITARY SEWER
- EXISTING WATERMAIN
- EXISTING GAS LINE
- EXISTING CATCHBASIN
- PROPOSED ROOF DRAIN DOWNSPOUT
- PROPOSED CATCHBASIN-MANHOLE / CATCHBASIN
- PROPOSED MANHOLE
- PROPOSED CURB STOP
- PROPOSED PIPE INSULATION
- PROPOSED 100 YEAR HIGH WATER LEVEL
- STORM WATERSHED EXTENT
- WATERSHED NAME
- RUNOFF COEFFICIENT
- AREA IN HECTARES

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02	ISSUED FOR APPROVAL	M.L.	02 JUN 2022
01	ISSUED FOR APPROVAL	M.A.	25 NOV 2021

SCALE: 1:250

NOT AUTHENTIC UNLESS SIGNED AND DATED

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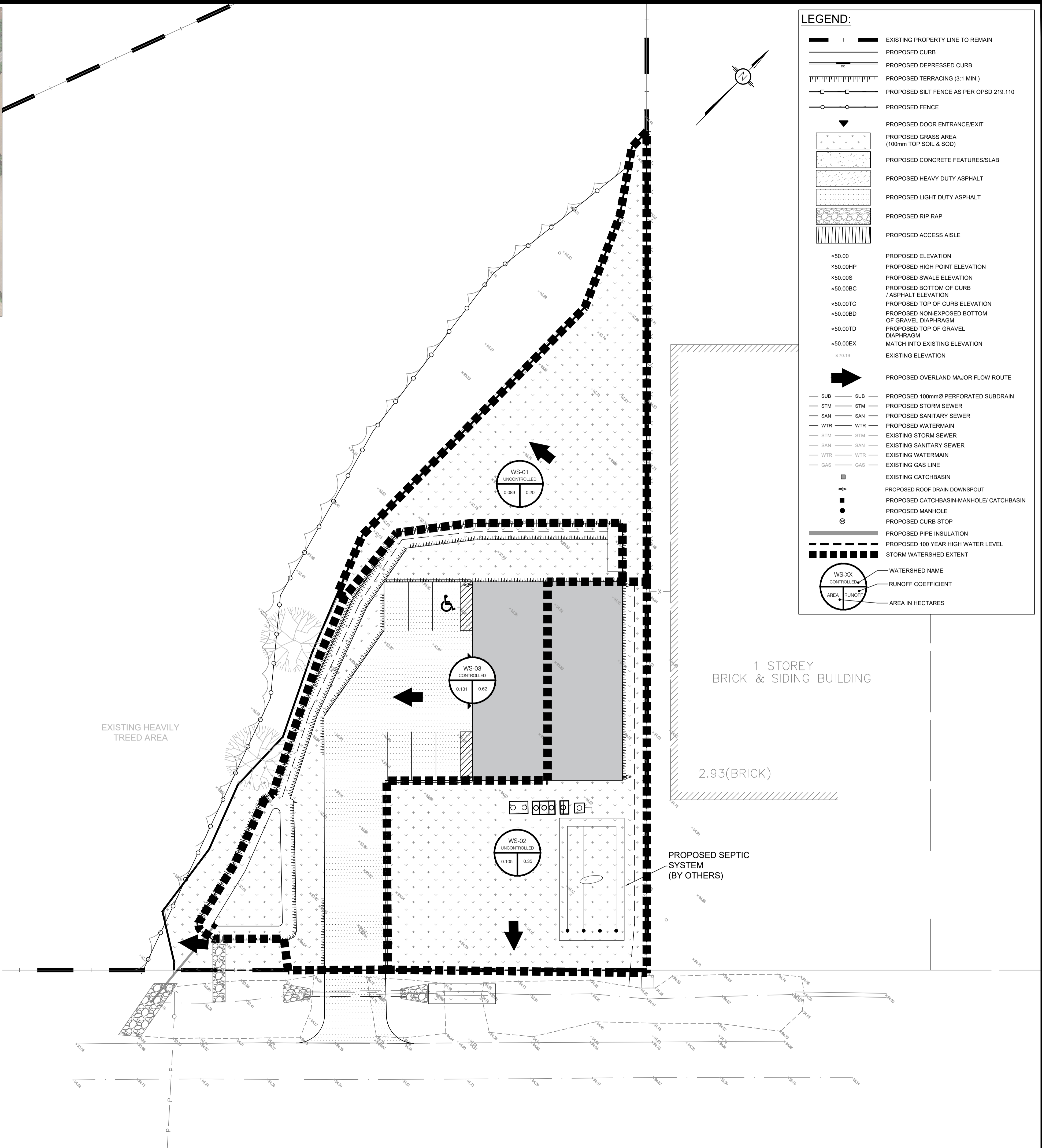
CLIENT: **AL ROBERTS**

DESIGNED BY: M.A. DRAWN BY: M.A. APPROVED BY: M.B.

PROJECT: **PROPOSED DOG KENNEL
5969 OTTAWA STREET,
OTTAWA, ON**

DRAWING TITLE: **PRE-DEVELOPMENT
WATERSHED PLAN**

PROJECT NO. 210341 DATE 27 JULY 2021 **C701**



LEGEND:

	EXISTING PROPERTY LINE TO REMAIN
	PROPOSED CURB
	PROPOSED DEPRESSED CURB
	PROPOSED TERRACING (3:1 MIN.)
	PROPOSED SILT FENCE AS PER OPSD 219 1.10
	PROPOSED FENCE
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	PROPOSED GRASS AREA (100mm TOP SOIL & SOD)
	PROPOSED CONCRETE FEATURES/SLAB
	PROPOSED HEAVY DUTY ASPHALT
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	PROPOSED ACCESS AISLE
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	PROPOSED TOP OF CURB ELEVATION
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	MATCH INTO EXISTING ELEVATION
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	PROPOSED OVERLAND MAJOR FLOW ROUTE
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	PROPOSED WATERMAIN
	EXISTING STORM SEWER
	EXISTING SANITARY SEWER
	EXISTING WATERMAIN
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SCALE: 1:250

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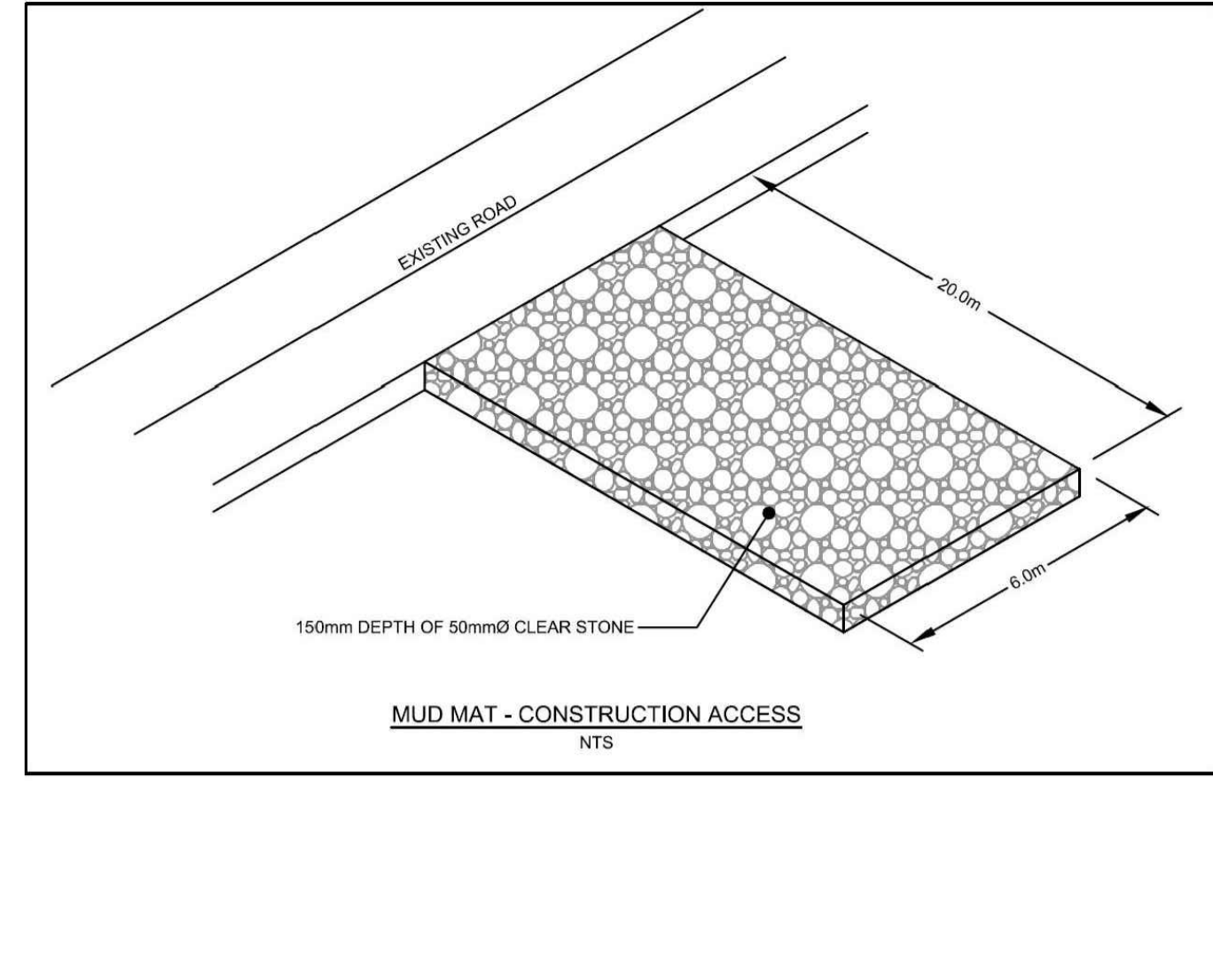
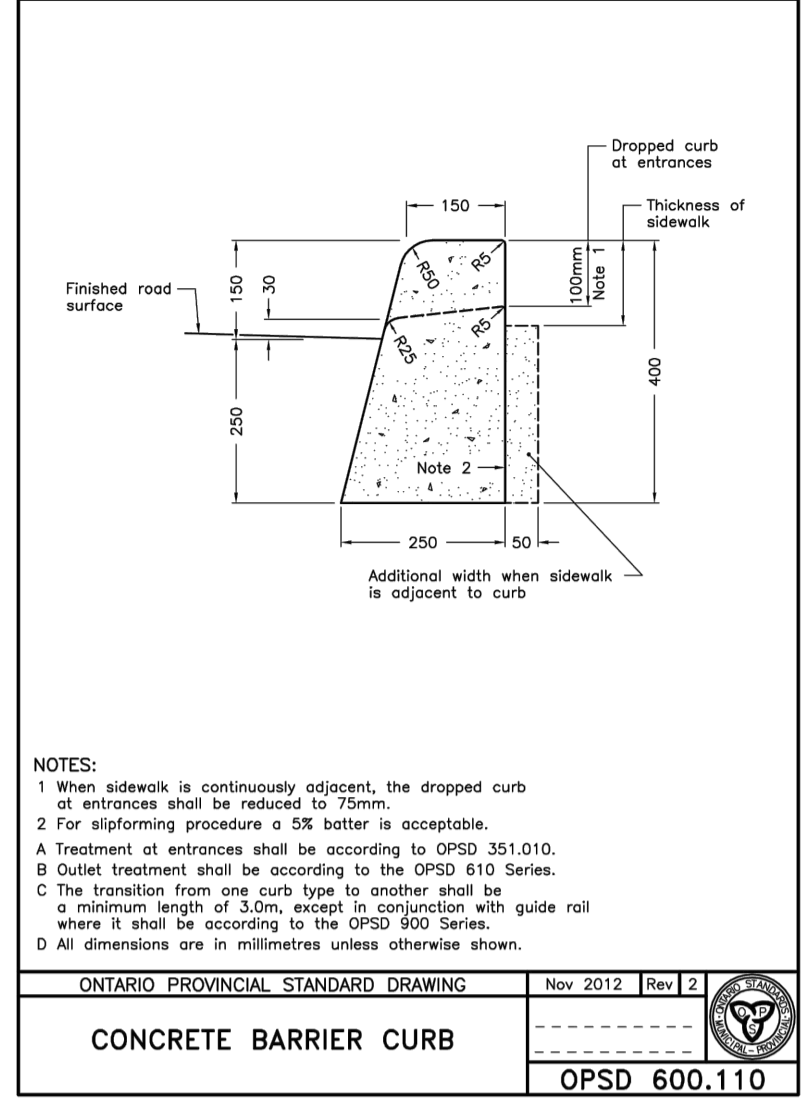
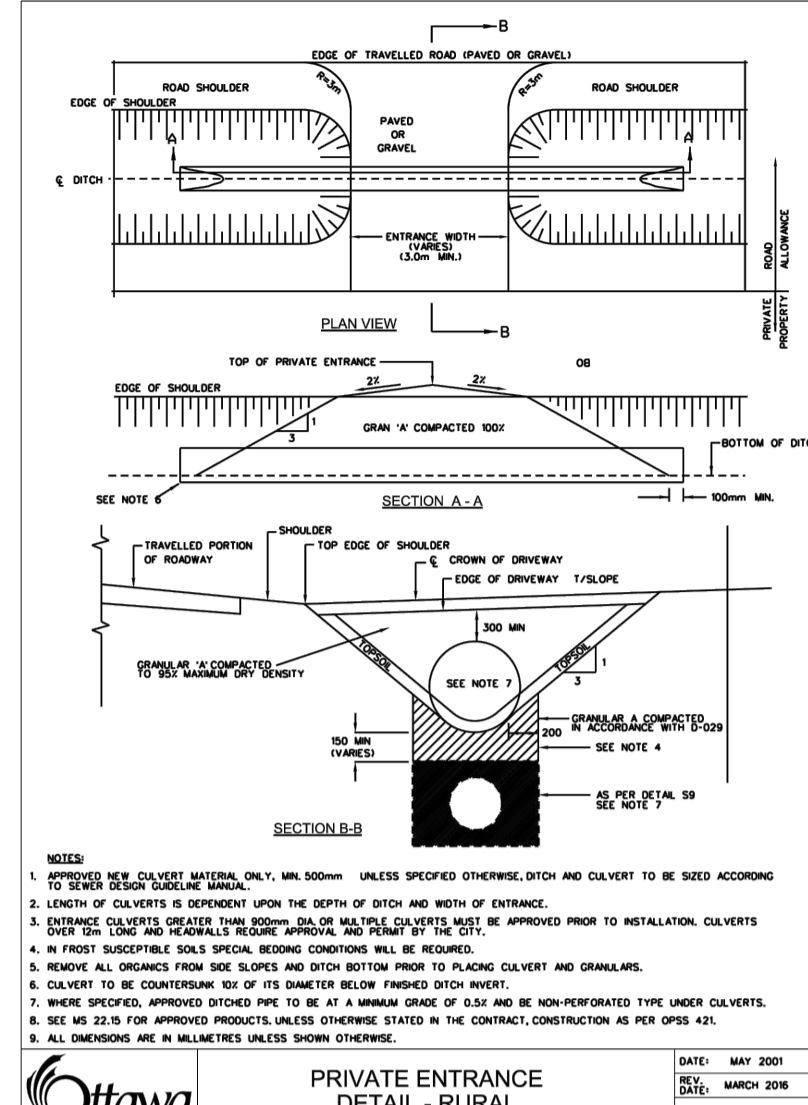
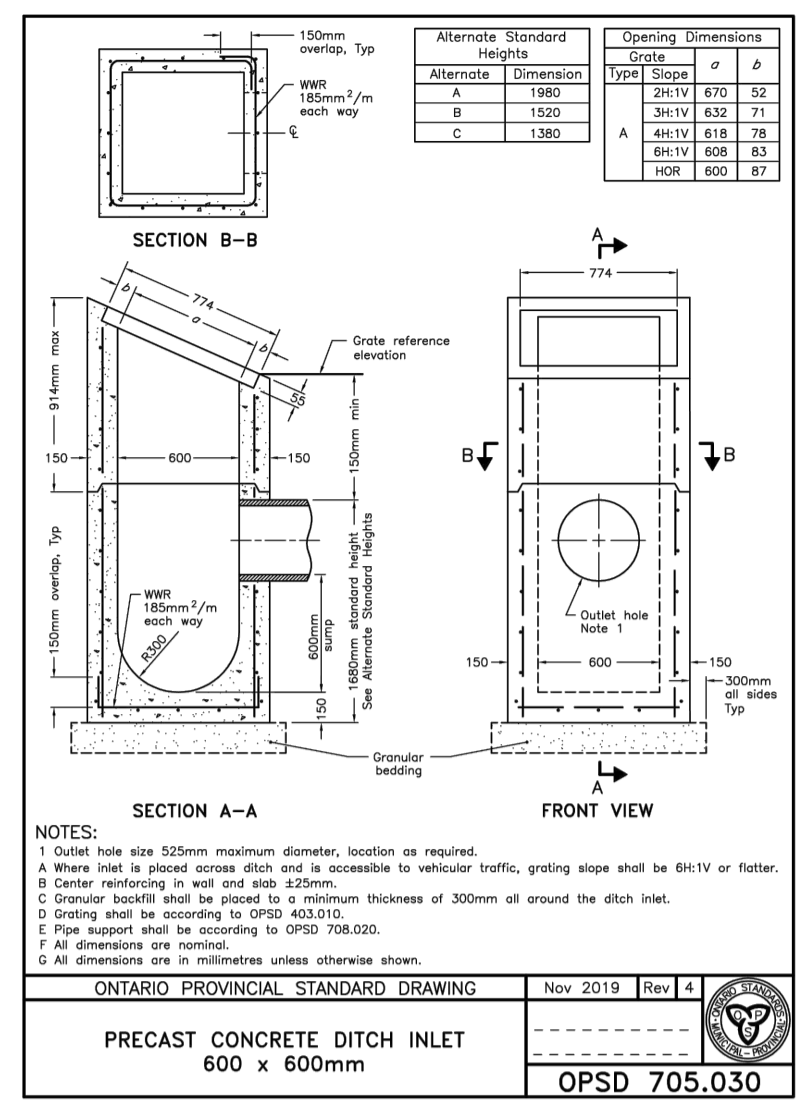
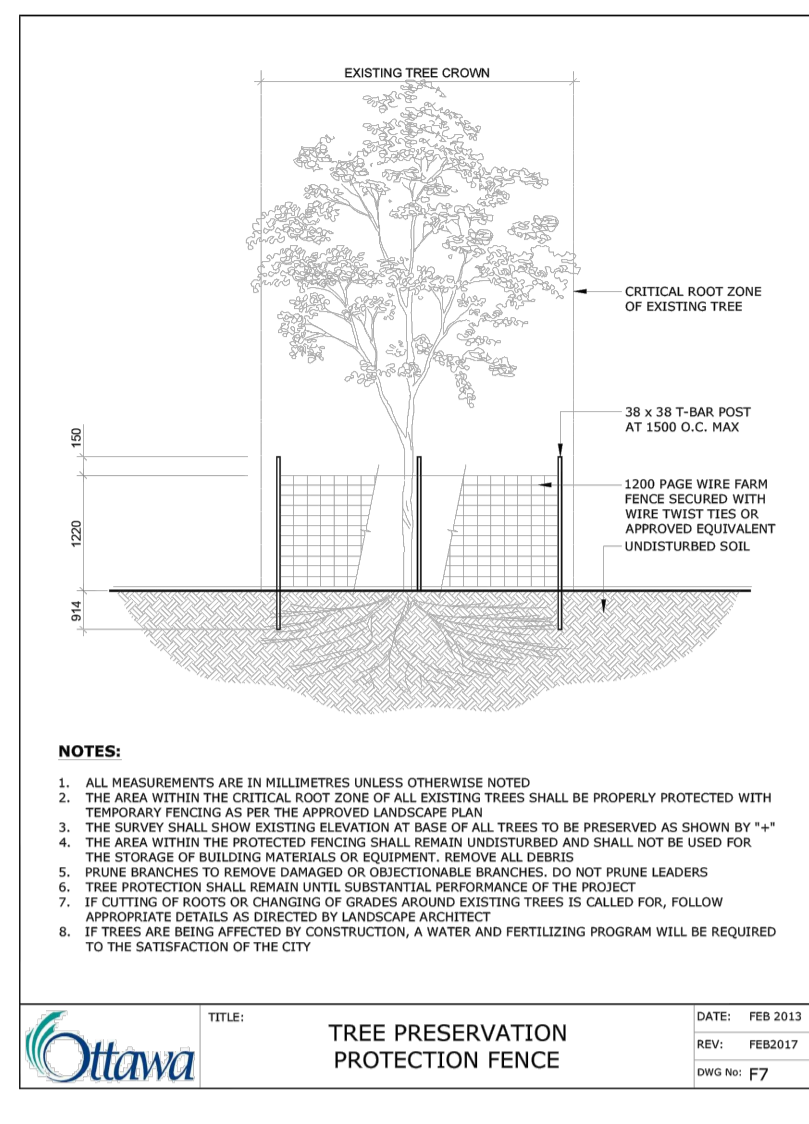
DESIGNED BY: M.A. DRAWN BY: M.A. APPROVED BY: M.B.

PROJECT: PROPOSED DOG KENNEL
5969 OTTAWA STREET,
OTTAWA, ON

DRAWING TITLE: POST-DEVELOPMENT
WATERSHED PLAN

PROJECT NO: 210341 DATE: 27 JULY 2021

C702



USE AND INTERPRETATION OF DRAWINGS

GENERAL CONDITIONS OF THE CONTRACT FOR CONSTRUCTION ARE PART OF THE CONTRACT DOCUMENTS AND DESCRIBE USE AND INTENT OF THE DRAWING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING AND REVIEWING THE CONTRACT DOCUMENTS AND DESCRIBING USE AND INTENT OF THE DRAWING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING AND REVIEWING THE CONTRACT DOCUMENTS AND DESCRIBING USE AND INTENT OF THE DRAWING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING AND REVIEWING THE CONTRACT DOCUMENTS AND DESCRIBING USE AND INTENT OF THE DRAWING.

BY USE OF THE DRAWINGS FOR CONSTRUCTION OF THE PROJECT, THE OWNER CONFIRMS THAT HE HAS REVIEWED AND APPROVED THE DRAWINGS. THE CONTRACTOR CONFIRMS THAT HE HAS NOTICED THE SITE, FAMILIARIZED HIMSELF WITH THE LOCAL CONDITIONS, VERIFIED FIELD DIMENSIONS AND CORRELATED HIS OBSERVATIONS WITH THE REQUIREMENTS OF THE CONTRACT DOCUMENTS.

AS INSTRUMENTS OF SERVICE, ALL DRAWINGS, SPECIFICATIONS, CAD FILES OR OTHER ELECTRONIC MEDIA AND COPIES THEREOF FURNISHED BY THE ENGINEER ARE HIS PROPERTY. THEY ARE TO BE USED ONLY FOR THIS PROJECT AND ARE NOT TO BE USED ON ANY OTHER PROJECT, INCLUDING REPEATS OF THE PROJECT. CHANGES TO THE DRAWINGS MAY ONLY BE MADE BY THE ENGINEER.

UNLESS THE REVISION TITLE IS ISSUED FOR CONSTRUCTION, THESE DRAWINGS SHALL BE CONSIDERED PRELIMINARY AND SHALL NOT BE USED AS A CONSTRUCTION DOCUMENT.

THESE DRAWINGS ILLUSTRATE THE WORK TO BE DONE. THE ENGINEER IS NOT RESPONSIBLE FOR THE MEANS, METHODS, TECHNIQUES, SEQUENCES, AND PROCEDURES USED TO DO THE WORK, OR THE SAFETY ASPECTS OF CONSTRUCTION AND NOTHING ON THESE DRAWINGS EXPRESSED OR IMPLIED CHANGES THIS CONDITION. CONTRACTOR SHALL DETERMINE ALL CONDITIONS AT THE SITE AND SHALL BE RESPONSIBLE FOR KNOWING HOW THEY AFFECT THE WORK. SUBMITTAL OF A BID TO PERFORM THIS WORK IS ACKNOWLEDGEMENT OF THE RESPONSIBILITIES, AND THAT THEY HAVE BEEN FULLY CONSIDERED IN PLANNING OF THE WORK, AND THE BID PRICE. NO CLAIMS FOR EXTRA CHARGES DUE TO THESE CONDITIONS WILL BE FORTHCOMING.

UNAUTHORIZED CHANGES:

IN THE EVENT THE CLIENT, THE CLIENT'S CONTRACTORS OR SUBCONTRACTORS, OR ANYONE FOR WHOM THE CLIENT IS LEGALLY LIABLE MAKES OR PERMITS TO BE MADE ANY CHANGES TO ANY REPORTS, PLANS, SPECIFICATIONS OR OTHER CONSTRUCTION DOCUMENTS PREPARED BY LRL ASSOCIATES LTD. (LRL) WITHOUT OBTAINING LRL'S PRIOR WRITTEN CONSENT, THE CLIENT SHALL ASSUME FULL RESPONSIBILITY FOR THE RESULTS OF SUCH CHANGES. THEREFORE THE CLIENT AGREES TO WAIVE ANY CLAIM AGAINST LRL AND TO RELEASE LRL FROM ANY LIABILITY ARISING DIRECTLY OR INDIRECTLY FROM SUCH UNAUTHORIZED CHANGES.

IN ADDITION, THE CLIENT AGREES TO THE FULLEST EXTENT PERMITTED BY LAW, TO INDEMNIFY AND HOLD HARMLESS LRL FROM ANY DAMAGES, LIABILITIES OR COSTS, INCLUDING REASONABLE ATTORNEY'S FEES AND COST OF DEFENSE, ARISING FROM SUCH CHANGES.

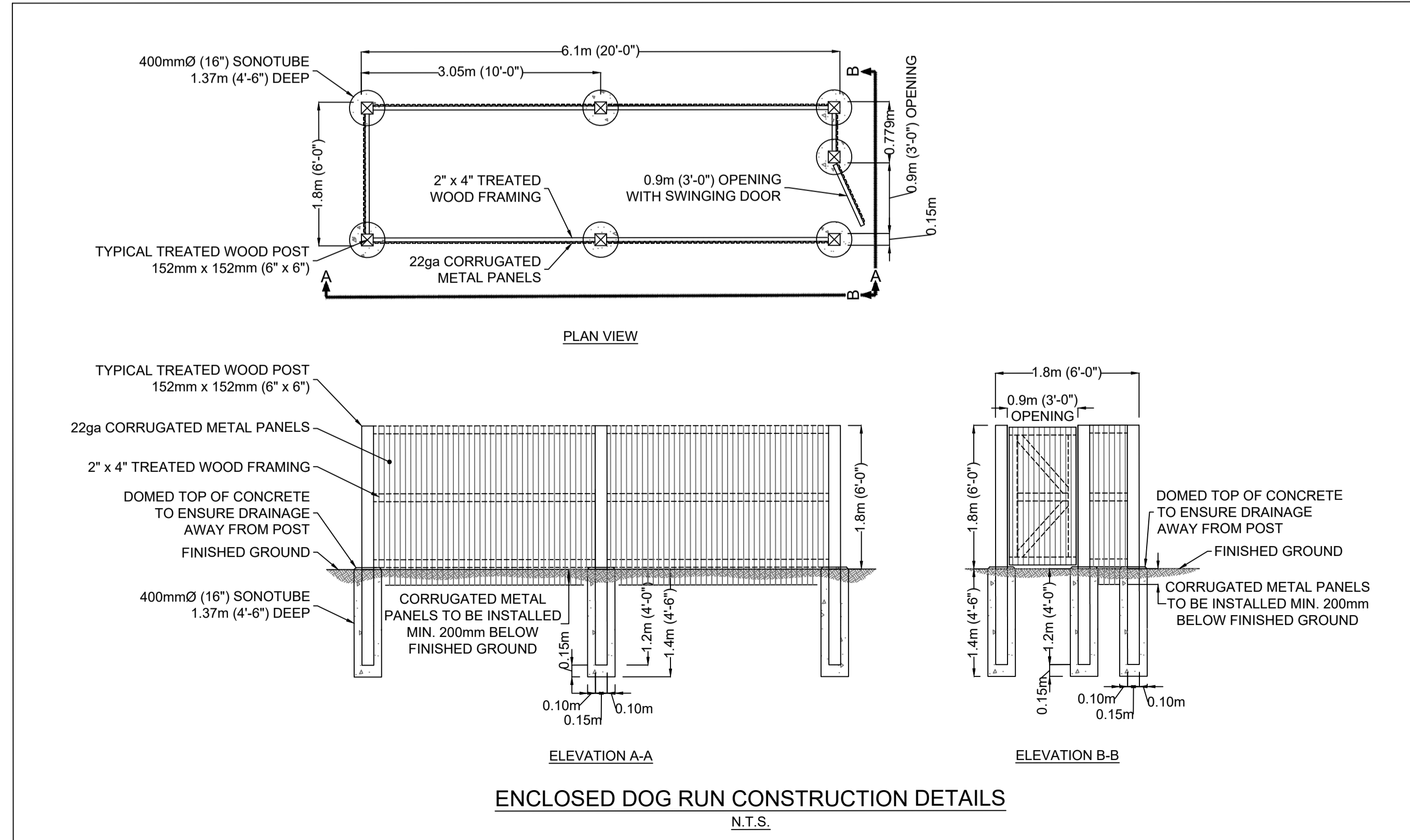
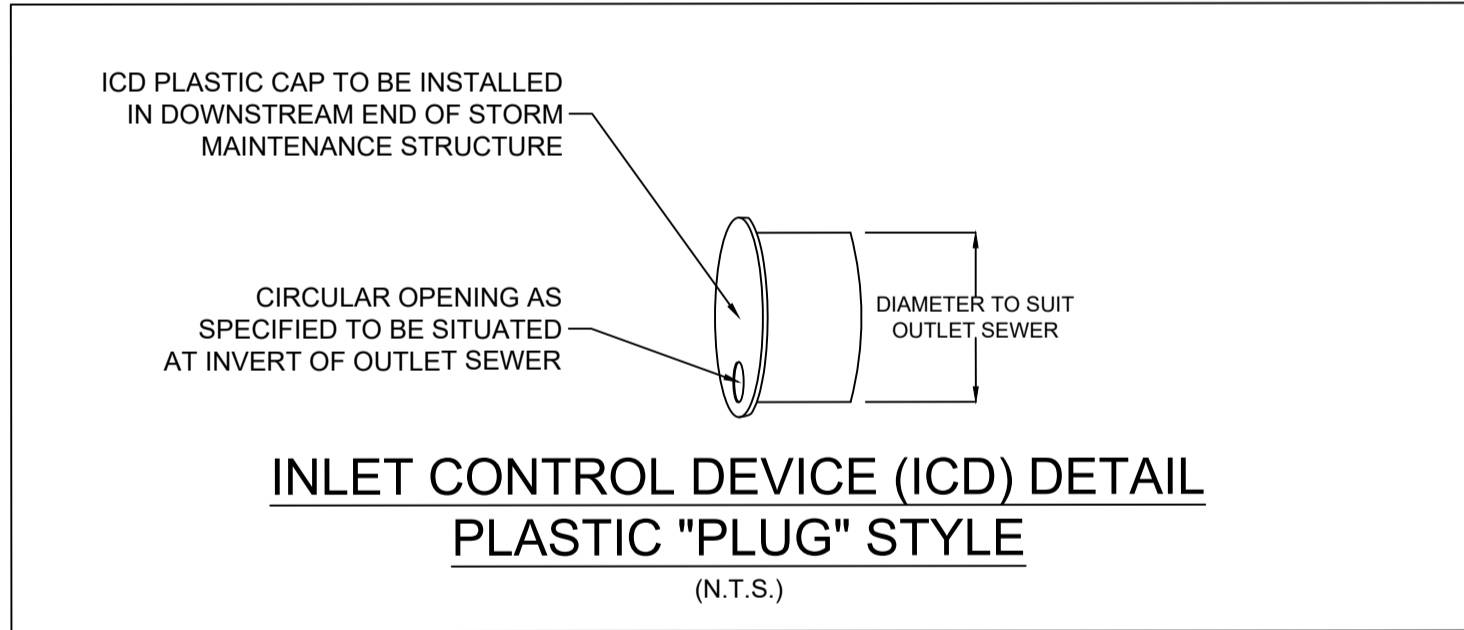
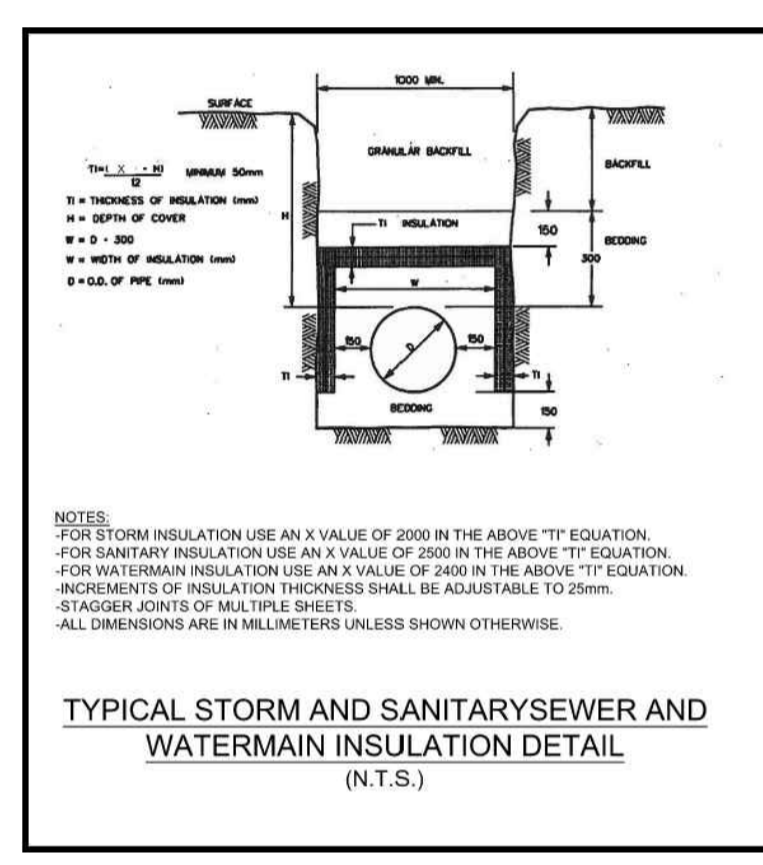
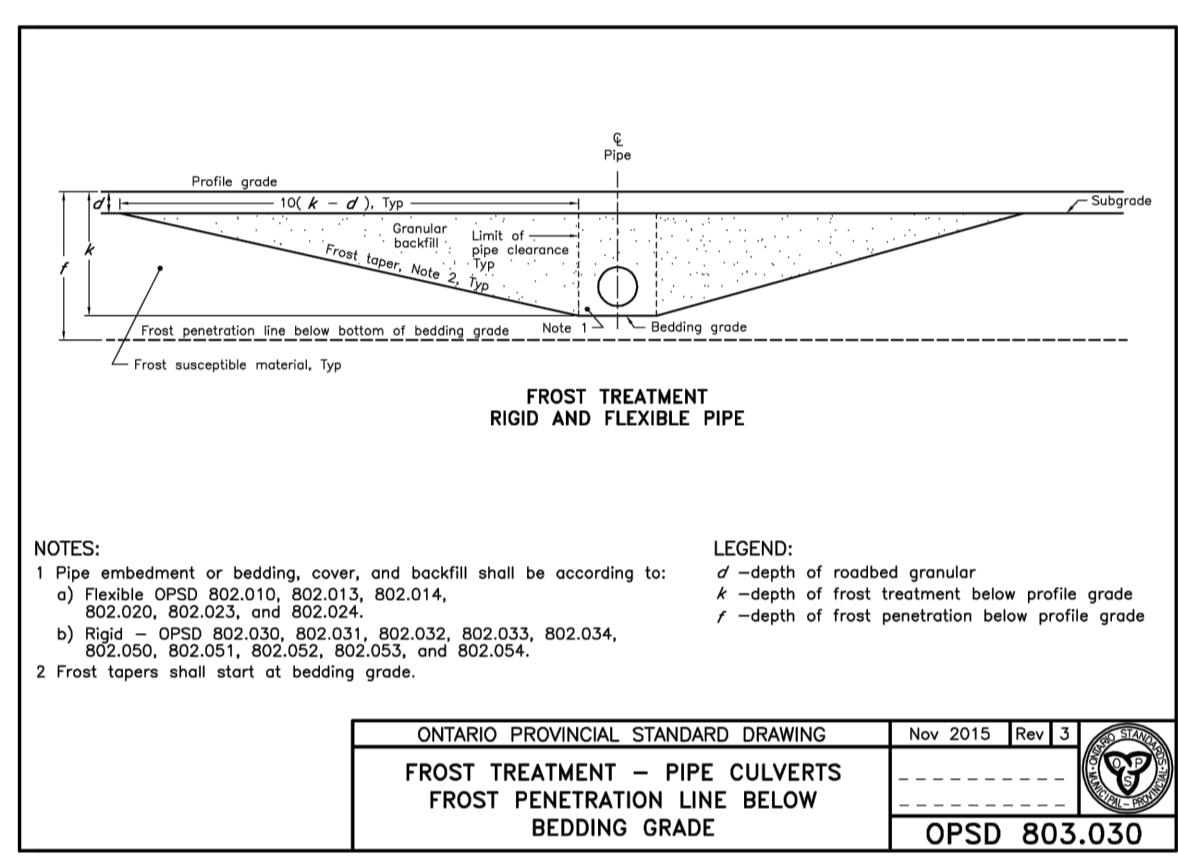
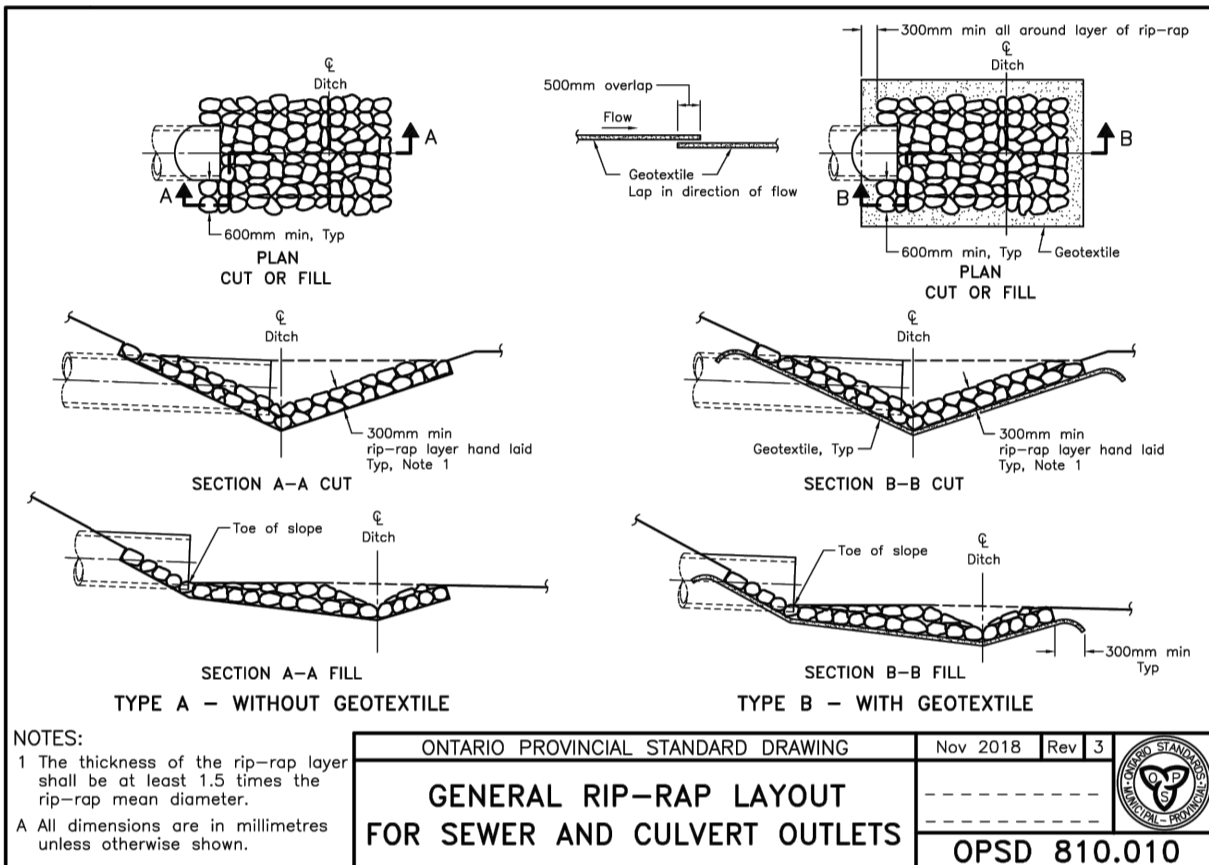
GENERAL NOTES:

EXISTING SERVICES AND UTILITIES SHOWN ON THESE DRAWINGS ARE TAKEN FROM THE BEST AVAILABLE RECORDS, BUT MAY NOT BE COMPLETE OR TO DATE. CONTRACTOR SHALL VERIFY IN FIELD FOR LOCATION AND ELEVATION OF PIPES AND CHECK WITH THE UTILITY COMPANIES BEFORE DIGGING OR PERFORMING WORK.

CONTRACTOR IS ADVISED TO COLLECT INFORMATION ON SOIL CONDITIONS BEFORE START OF CONSTRUCTION.

THE ENGINEER WAIVES ANY AND ALL RESPONSIBILITY AND LIABILITY FOR PROBLEMS WHICH ARISE FROM FAILURE TO FOLLOW THESE PLANS, SPECIFICATIONS AND THE DESIGN CONTRACT TO CONVEY, OR FOR PROBLEMS WHICH ARISE FROM OTHERS' FAILURE TO OBTAIN AND/OR FOLLOW THE ENGINEER'S GUIDANCE IN RESPECT TO ANY ERRORS, OMISSIONS, INCONSISTENCIES AMBIGUITIES OR CONFLICTS WHICH ARE ALLEGED.

CONTRACTOR TO VERIFY ALL DIMENSIONS AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES BEFORE WORK COMMENCES. DO NOT SCALE DRAWINGS.



No.	REVISIONS	BY	DATE
05	RE-ISSUED FOR APPROVAL	S.V.	25 JUN 2024
04	RE-ISSUED FOR APPROVAL	S.V.	16 MAY 2024
03	RE-ISSUED FOR APPROVAL	S.V.	25 AUG 2023
02	ISSUED FOR APPROVAL	M.L.	02 JUN 2022
01	ISSUED FOR APPROVAL	M.A.	25 NOV 2021



NOT AUTHENTIC UNLESS SIGNED AND DATED



CLIENT

AL ROBERTS

DESIGNED BY: M.A. DRAWN BY: M.A. APPROVED BY: M.B.

PROJECT

PROPOSED DOG KENNEL 5969 OTTAWA STREET, OTTAWA, ON

DRAWING TITLE

CONSTRUCTION DETAIL PLAN

PROJECT NO. 210341 DATE 27 JULY 2021

C901

Appendix E

Water Service Calculations



Water Demand Calculations

LRL File No. 210341
 Project Proposed Dog Kennel
 Location 5969 Ottawa St, Ottawa, ON
 Date August 18, 2023
 Prepared by Sarthak V

Industrial- Light Demand

	Gross Area (m2)	Gross Area (ha)
Industrial Area	3250	0.325

Average Water Consumption Rate	35000	L/ha/d		
Maximum Daily Peak Factor	1.5			
Maximum Hourly Peak Factor	1.8			
Average Domestic Water Demand	11,375	L/d	0.132	L/s
Maximum Daily Domestic Water Demand	17,063	L/d	0.197	L/s
Maximum Hourly Water Demand	30,713	L/d	0.355	L/s

Total Water Demand

Average Total Water Demand	11,375	L/d	0.132	L/s
Maximum Daily Total Water Demand	17,063	L/d	0.197	L/s
Maximum Hourly Total Water Demand	30,713	L/d	0.355	L/s

Water Service Pipe Sizing

$Q = VA$

Where: V = velocity
 A = area of watermain pipe
 Q = water supply flow rate

By deriving the above formula, and assuming a maximum velocity of 1.8m/s we can obtain the diameter of the pipe:

Minimum pipe diameter:

$$d = (4Q/\pi V)^{1/2}$$

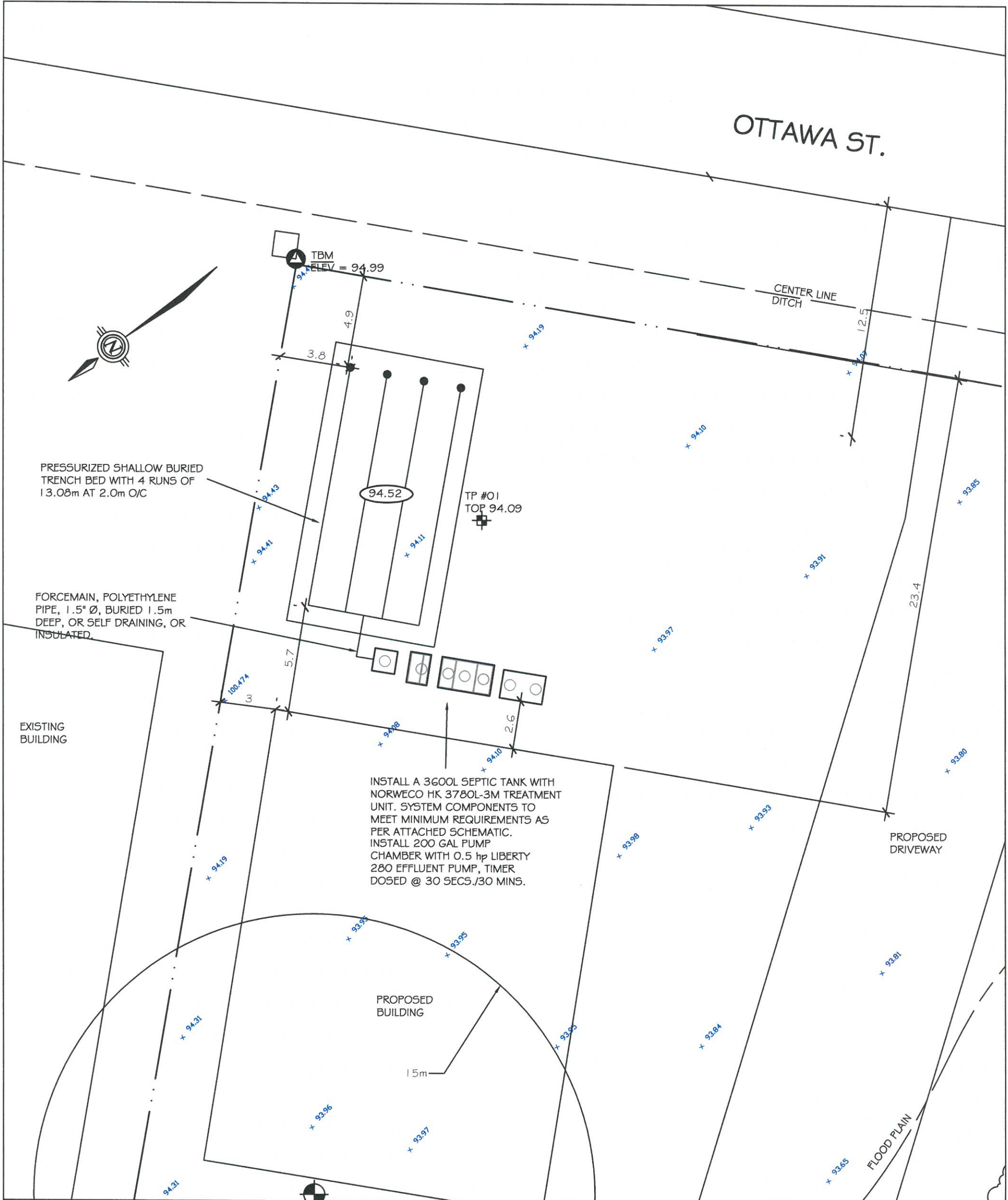
d =	0.016	m
d =	16	mm

Proposed pipe diameter: **25** mm

Appendix F

Septic System Assessment

OTTAWA ST.



NOTES:

1. ALL TREATMENT UNITS AND LEACHING BED ARE TO BE INSTALLED IN ACCORDANCE WITH MINIMUM OBC CLEARANCE DISTANCES. ANY OMISSIONS OR INACCURACIES SHALL BE BROUGHT TO THE ATTENTION OF GVE AND OSSO.
2. CARE IS TO BE EXERCISED DURING CONSTRUCTION ACTIVITIES NEAR OVERHEAD HYDRO WIRES.
3. EXISTING ELEVATIONS ARE APPROXIMATE. CONTRACTOR MUST VERIFY ALL ELEVATIONS AND DIMENSIONS PRIOR TO CONSTRUCTION.
4. SOIL CONDITIONS ARE ACCURATE FOR THE LOCATIONS SHOWN. CONTRACTOR MUST CONTACT THE DESIGN ENGINEER OR REGULATORY AUTHORITY SHOULD SOIL CONDITIONS DIFFER.
5. ALL DIMENSIONS AND CONDITIONS TO BE VERIFIED ON SITE, FIGURED DIMENSIONS TAKE PRECEDENCE OVER SCALE.
6. UTILITY LOCATES SHALL BE COMPLETED PRIOR TO ANY EXCAVATION.
7. THIS IS NOT A PLAN OF SURVEY AND SHALL NOT BE USED EXCEPT FOR THE PURPOSE INDICATED IN THE TITLE BLOCK.

8. THIS DOCUMENT IS COPYRIGHT PROTECTED AND IS THE SOLE PROPERTY OF GVE GROUP. THIS DRAWING SHALL NOT BE ALTERED IN ANY MANNER.

9. EXISTING LOT SERVICED WITH A DRILLED WELL.

METRIC:
DISTANCES AND ELEVATIONS SHOWN ON THIS PLAN ARE IN METERS AND MAY BE CONVERTED TO FEET BY DIVIDING BY 0.3048.

LEGEND:

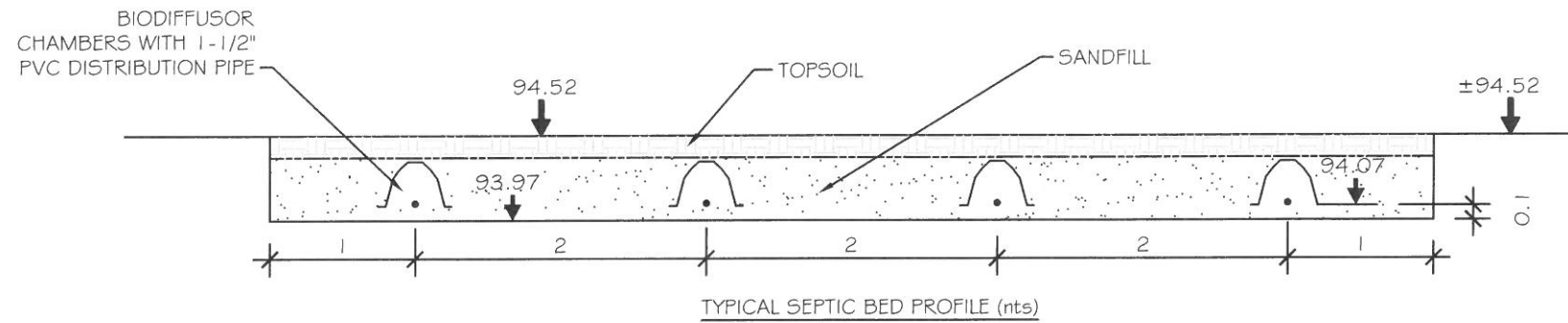
- PROPOSED ELEVATION
- EXISTING ELEVATION
- EXISTING WORKS
- PROPOSED SEWAGE WORKS
- FENCE LINE
- PROPERTY LINE
- TBM TEMPORARY BENCH MARK (DESCRIPTION: TOP OF CONCRETE PAD)
- TEST PIT LOCATION

SEPARATION DISTANCES:

1. MINIMUM CLEARANCE FROM SEPTIC PIPE TO:
LOT LINE = 3.75m
HOUSE = 5.75m
DRILLED WELL = 15.75m
2. MINIMUM CLEARANCE FROM TREATMENT UNITS TO:
LOT LINE = 3.0m
HOUSE = 1.5m
DRILLED WELL = 15.0m

Drawn by: DP		Drawn by: DP		Checked by: WS	
Rev.	Description				Date
Township	Plan#	Lot	Sublot	Con	
County:	Civic Address: 5969 OTTAWA ST.			Date: 11/08/23	Scale: 1:200
GREEN VALLEY ENVIRONMENTAL					
On-Site Sewage Treatment Plan for the Residence of: AL ROBERTS					

1. THIS CROSS SECTION IS NOT TO SCALE, ALL FIGURED DIMENSIONS TAKE PRECEDENCE OVER SCALE
2. THIS DOCUMENT IS COPYRIGHT PROTECTED AND IS THE SOLE PROPERTY OF GREEN VALLEY ENVIRONMENTAL INC. THIS DRAWING SHALL NOT BE ALTERED IN ANY MANNER.



PRETREATMENT TANK

- INSTALL MIN. 3600L PRETREATMENT TANK.
- A MAXIMUM OF 300mm OF SOIL SHALL COVER THE PRETREATMENT TANK.
- RISERS AND LIDS SHALL BE INSTALLED FOR EASE OF ACCESS

NORWECO TREATMENT UNIT

- THE TREATMENT UNIT SHALL CONSIST OF A NORWECO HYDRO-KENETIC 3780L-3M TREATMENT UNIT.
- THE TREATMENT UNIT SHALL BE INSTALLED IN SERIES AND DOWN STREAM FROM THE PRETREATMENT TANK.
- THE TREATMENT UNIT SHALL PRODUCE A TERTIARY TREATMENT EFFLUENT QUALITY IN ACCORDANCE WITH COLUMN 2 AND 3 OPPOSITE A LEVEL IV TREATMENT UNIT OF TABLE 8.6.2.2. OF THE ONTARIO BUILDING CODE.
- THE TREATMENT UNIT SHALL BE INSTALLED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS BY A CERTIFIED INSTALLER.
- THE OWNER OF THE TREATMENT UNIT MUST ENTER INTO A MAINTENANCE AGREEMENT WITH THE MANUFACTURER'S REPRESENTATIVE.
- THE TREATMENT UNIT SHALL BE BACKFILLED AND COMPACTED, IN LIFTS, WITH SELECT GRANULAR FILL, SUCH AS SAND OR CLEAR STONE
- THE TOP OF THE TREATMENT UNIT SHALL BE ACCESSIBLE TO THE SURFACE. INSTALL RISERS AND LIDS TO SUIT.

NORWECO FILTER VAULT(S)

- FILTER VAULT(S) SHALL BE INSTALLED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS
- FILTER VAULT(S) SHALL BE INSTALLED IN SERIES AND DOWN STREAM FROM THE TREATMENT UNIT
- FILTER VAULT(S) SHALL BE ACCESSIBLE TO THE SURFACE. INSTALL RISERS AND LIDS TO SUIT.

SHALLOW BURIED TRENCH BED

- THE DISPERSAL BED SHALL CONSIST OF A TOTAL LENGTH EQUAL TO $Q/75 = 3450/75 = 46m$
- TOTAL LENGTH USED = 52.32m
- SAND FILL SHALL EXTEND 1.0m ON ALL SIDES.
- REMOVE LAYER OF TOP SOIL TO APPROXIMATE FOOT PRINT OF SEPTIC BED AND SIDE SLOPES
- THE PRESSURIZED DISTRIBUTION SYSTEM SHALL HAVE A PRESSURE HEAD OF NOT LESS THAN 600mm WHEN MEASURED AT THE MOST DISTANT POINT FROM THE PUMP.
- DISPERSAL BED SHALL BE BACKFILLED SO AS TO ENSURE THAT THE SURFACE WILL NOT FORM ANY DEPRESSIONS
- ALL SIDE SLOPES SHALL BE AT 1:4
- AT NO POINT DURING OR AFTER CONSTRUCTION SHALL A WHEELED VEHICLE DRIVE OVER THE SEPTIC BED AREA.
- EACH RUN SHALL CONSIST OF ONLY FULL CHAMBERS.
- SEPTIC DESIGN BASED ON ADS BIO3 CHAMBERS.
- EACH RUN SHALL CONSIST OF 6 FULL ADS BIO3 CHAMBERS WITH A TOTAL OF 24 FULL BIO3 CHAMBERS FOR THE ENTIRE SEPTIC BED.

MINIMUM CLEARANCE DISTANCE FROM LEACHING BED

- 3.75m FROM ANY PROPERTY LINE
- 5.75m FROM ANY STRUCTURE
- 15.75m FROM ANY DRILLED WELL

MINIMUM CLEARANCE DISTANCE FROM TANKS

- 3.0m FROM ANY PROPERTY LINE
- 1.5m FROM ANY STRUCTURE
- 15.0m FROM ANY DRILLED WELL

GENERAL

- THE BACKWASH WATERS FROM ANY HOUSEHOLD TREATMENT SUCH AS WATER SOFTENER SHALL NOT DISCHARGE INTO THE SEWAGE SYSTEM
- CONTRACTOR SHALL BE QUALIFIED AND REGISTERED UNDER PART 8 OF THE ONTARIO BUILDING CODE.
- CONTRACTOR SHALL VISIT THE SITE AND REVIEW ALL DOCUMENTATION TO DETERMINE SUITABLE METHODS OF CONSTRUCTION.
- INSPECTION BY THE REGULATING AUTHORITIES IS A REQUIREMENT BY SOME REGULATING AUTHORITIES AND IS STRONGLY RECOMMENDED BY GREEN VALLEY ENVIRONMENTAL INC.
- IT IS RECOMMENDED THAT ALL TREES WITHIN 5m OF THE BED AREA BE REMOVED TO PREVENT ROOTS FROM INFILTRATING THE SYSTEM.
- THE CONTRACTOR SHALL BE RESPONSIBLE TO LOCATE AND PROTECT ALL EXISTING UNDERGROUND SERVICES.
- SHOULD THE CONTRACTOR AT ANY TIME DURING CONSTRUCTION ENCOUNTER CONDITIONS THAT DIFFER FROM THE DESIGN CRITERIA IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY THE DESIGNER AND THE REGULATING AUTHORITY.
- GREEN VALLEY ENVIRONMENTAL INC. HAS PROVIDED DESIGNS BASED ON OUR INTERPRETATION OF THE ONTARIO BUILDING CODE AND THE TEST HOLES DUG ON THE PROPERTY.

Drawn by: DP	Designed by: DP	Checked by: WS	
Rev.	Description	Date	Approved
Township	Plan#	Lot	Sublot
			Con
			Draw. No.: 157261-22
County:	5969 OTTAWA ST	Date: 11/08/23	Scale: NTS
GREEN VALLEY ENVIRONMENTAL			
On-Site Sewage Treatment Plan for the Residence of: AL ROBERTS			

Appendix G

Topographical Site Survey

METRIC
DISTANCES SHOWN ON THIS PLAN ARE IN METRES AND CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048

NOTES
BEARINGS HEREON ARE GRID, DERIVED FROM THE BEARING OF N47°26'E AS SHOWN FOR THE NORTHWESTERLY LIMIT OF OTTAWA STREET ON PLAN 4R-7050.

- S.I.B. DENOTES 0.025 SQ., 1.2 LONG, STANDARD IRON BAR
- S.S.I.B. DENOTES 0.025 SQ., 0.6 LONG, SHORT STANDARD IRON BAR
- I.B. DENOTES 0.016 SQ., 0.6 LONG, IRON BAR
- R.I.B. DENOTES ROUND IRON BAR
- DENOTES SURVEY MONUMENT FOUND
- DENOTES SURVEY MONUMENT PLANTED
- WT. DENOTES WITNESS
- S.U. DENOTES SOURCE UNKNOWN
- 725 DENOTES ARNETT, KENNEDY, RIDDELL & JASON SURVEYING LTD.
- 1050 DENOTES D.R. BARCHAM, O.L.S.
- P1 DENOTES PLAN 4R-7050
- M DENOTES MEASURED
- S DENOTES SET
- E.O.A. DENOTES EDGE OF ASPHALT
- E.O.G. DENOTES EDGE OF GRAVEL
- T.O.B. DENOTES TOP OF BANK
- C.S.P. DENOTES CORRUGATED STEEL PIPE
- N.T.S. DENOTES NOT TO SCALE

ELEVATION NOTES

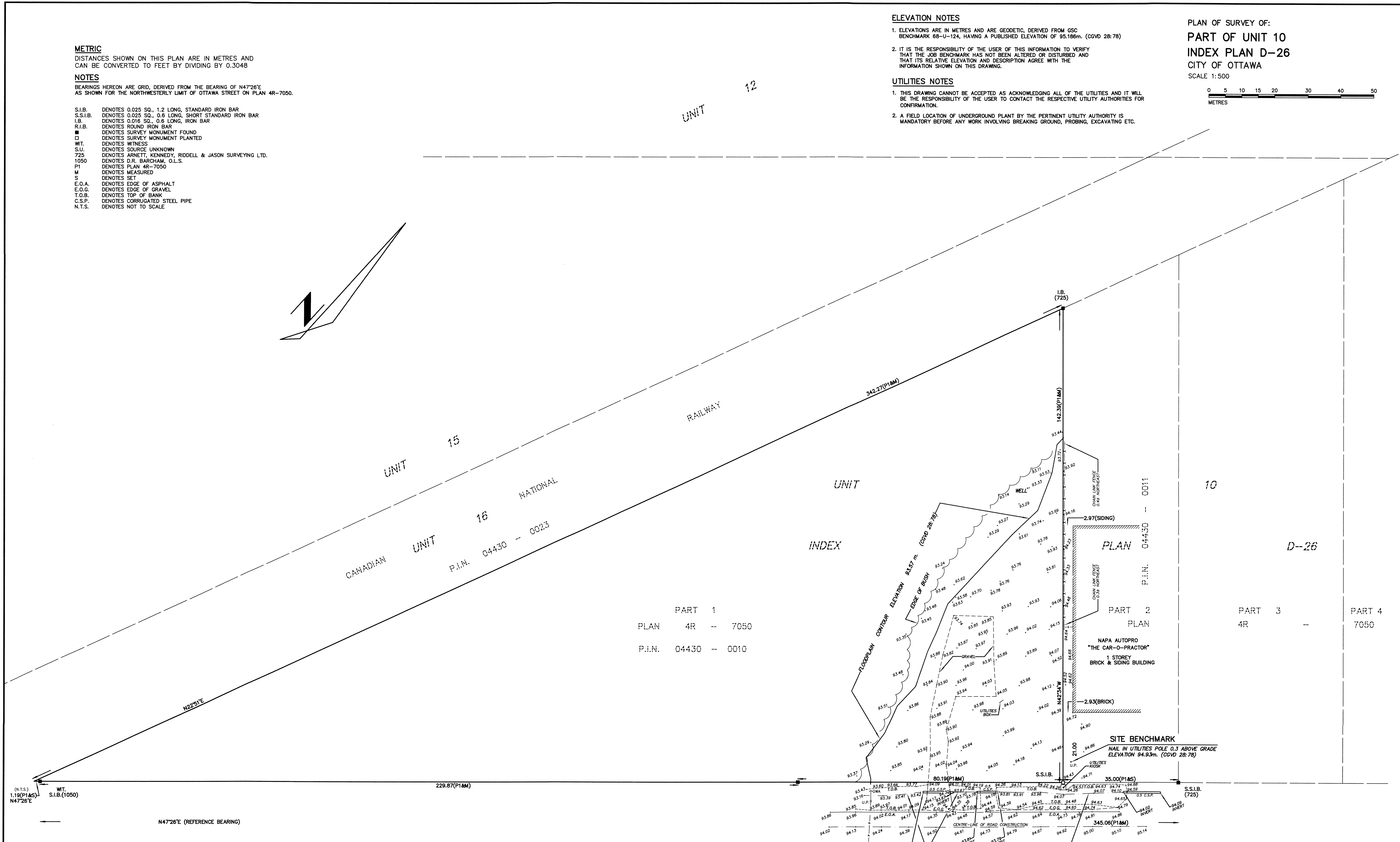
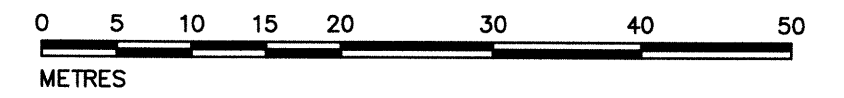
1. ELEVATIONS ARE IN METRES AND ARE GEODETIC, DERIVED FROM GSC BENCHMARK 88-U-124, HAVING A PUBLISHED ELEVATION OF 95.186m. (CGVD 28:78)

2. IT IS THE RESPONSIBILITY OF THE USER OF THIS INFORMATION TO VERIFY THAT THE JOB BENCHMARK HAS NOT BEEN ALTERED OR DISTURBED AND THAT ITS RELATIVE ELEVATION AND DESCRIPTION AGREE WITH THE INFORMATION SHOWN ON THIS DRAWING.

UTILITIES NOTES

- 1. THIS DRAWING CANNOT BE ACCEPTED AS ACKNOWLEDGING ALL OF THE UTILITIES AND IT WILL BE THE RESPONSIBILITY OF THE USER TO CONTACT THE RESPECTIVE UTILITY AUTHORITIES FOR CONFIRMATION.
- 2. A FIELD LOCATION OF UNDERGROUND PLANT BY THE PERTINENT UTILITY AUTHORITY IS MANDATORY BEFORE ANY WORK INVOLVING BREAKING GROUND, PROBING, EXCAVATING ETC.

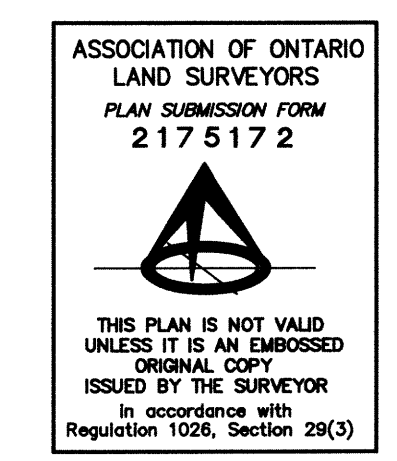
PLAN OF SURVEY OF:
PART OF UNIT 10
INDEX PLAN D-26
CITY OF OTTAWA
SCALE 1:500



PART 1
PLAN 4R -- 7050
P.I.N. 04430 -- 0010

PART 2
PLAN 4R -- 7050
P.I.N. 04430 -- 0011

SURVEYOR'S CERTIFICATE
I CERTIFY THAT:
(1) THIS SURVEY AND PLAN ARE CORRECT AND IN ACCORDANCE WITH THE SURVEYS ACT, THE SURVEYORS ACT AND THE LAND TITLES ACT AND THE REGULATIONS MADE UNDER THEM;
(2) THE SURVEY WAS COMPLETED ON THE 5th DAY OF JULY, 2021.
DATE: JULY 19, 2021
J.P. SHIPMAN
ONTARIO LAND SURVEYOR



H.A.KEN SHIPMAN SURVEYING LTD.
P.O. BOX 53, NORTH GOWER, ONT. K0A 2T0
REF No.: GLB-549
FILE No.: 20-12471 B

(N.T.S.) 1.19(P1&S) N47°26'E
WT. S.I.B.(1050)
N47°26'E (REFERENCE BEARING)

INDEX PLAN D-24

OTTAWA STREET
P.I.N. 04430 -- 0297

345.06(P1&M)
CENTRE-LINE OF ROAD CONSTRUCTION
CENTRE-LINE OF DITCH
UTILITIES BOX
SITE BENCHMARK
NAIL IN UTILITIES POLE 0.3 ABOVE GRADE
ELEVATION 94.93m. (CGVD 28:78)

229.87(P1&M)
N22°51'E

342.27(P1&M)
142.39(P1&M)
I.B. (725)

CANADIAN UNIT 15
UNIT 16
NATIONAL P.I.N. 04430 -- 0023

RAILWAY

INDEX PLAN D-26
PART 3
PLAN 4R -- 7050
P.I.N. 04430 -- 0011

PART 4
PLAN 4R -- 7050
P.I.N. 04430 -- 0011

FLOODPLAIN CONTOUR ELEVATION 93.57 m. (CGVD 28:78)
EDGE OF BUSH
GRAVEL
UTILITIES BOX
S.S.I.B. (725)
35.00(P1&S)
NAPA AUTOPRO "THE CAR-O-TRACTOR"
1 STOREY BRICK & SIDING BUILDING
2.97(SIDING)
2.93(BRICK)
U.P. UTILITIES
E.O.A. EDGE OF ASPHALT
E.O.G. EDGE OF GRAVEL
T.O.B. TOP OF BANK
C.S.P. CORRUGATED STEEL PIPE
N.T.S. NOT TO SCALE