

# Rosaline J. Hill Architect Inc.

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*designing urban alternatives*

May 19<sup>th</sup>, 2022

Development Review, Central  
Planning, Real Estate and Economic Development Department  
City of Ottawa  
110 Laurier Avenue West,  
Ottawa, Ontario, K1P 1J1

**Re: 436 Athlone Design Brief for Site Plan Control Application**

This brief will provide an overview of the proposed development and highlight the rationale for design choices and the appropriateness within the context of applicable planning policies. The proposed development aligns with the city's Official Plan and is appropriate and compatible with the surrounding area.

The proposed development is a 3 storey (plus basement) apartment building with a total of 16 units. 25% of the dwelling units have 2 bedrooms and the remaining are a mix of 1 bedroom units and bachelors. No on-site parking is proposed. The lot is 15.24m wide and 30.35m deep with a total area of 462sqm. It is legally described as Lot 100, Registered Plan 272. The lot is zoned R4UC[2685] and is in the General Urban Area subject to the Mature Neighbourhoods Overlay.



*Image 1 – Proposed development*



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Our client would like to build a small apartment building that architecturally compliments the existing homes on the street, and is sensitive to existing massing and built-form. The proposed building would provide much needed housing for households who can not presently find this size or type of accommodation in this immediate neighbourhood of predominantly large and expensive dwelling units. Westboro will be enriched through added diversity. Increased density will help to support local businesses and public transit.

### SITE CONTEXT

The site is located in Westboro village, the second lot south of Byron Avenue and a little more than one block south of Richmond Road which is designated a Traditional Main Street.

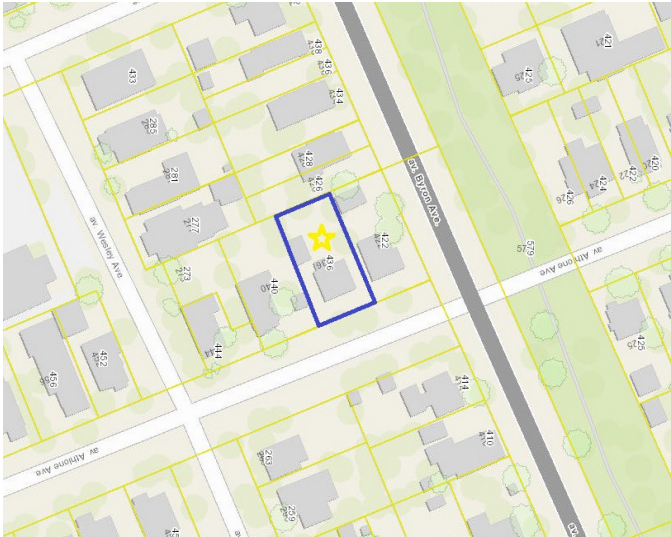


Image 2 – 436 Athlone parcel as shown on Geo Ottawa



Image 3 – 3D aerial view of 436 Athlone in immediate surrounding context

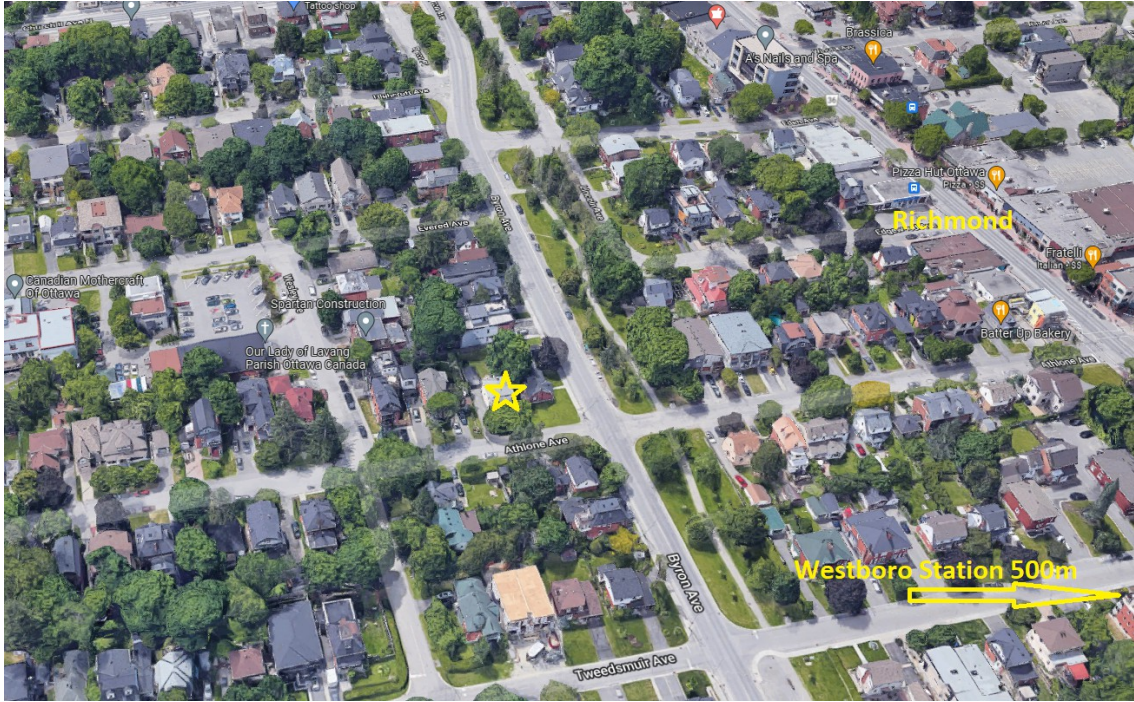


Image 4 – wider 3D aerial view of 436 Athlone in surrounding context

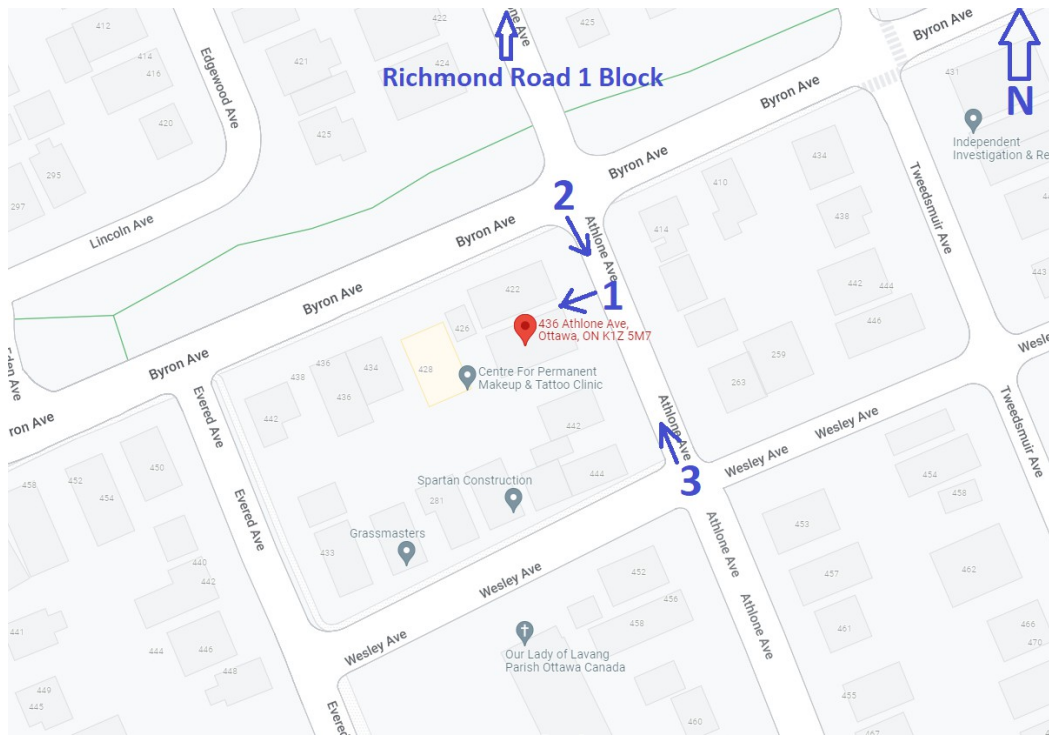


Image 5 – 100m radius of 436 Athlone (views 1-3 correspond with images 5.1, 5.2, & 5.3 that follow)



*Image 5.1 – view from the street looking at the existing home at 436 Athlone*



*Image 5.2 – view looking south down Athlone from the corner of Byron Avenue*



*Image 6 – neighbour to the rear, with 1.2m setback abutting our rear property line*

The site of our proposed development is equally distanced from Churchill Avenue North as from Richmond Road, both well served with public transit options. The site is less than 600m from Westboro Station, a major rapid transit hub and future site of an upgraded Phase II light rail station.

The site is in view of the linear park along Byron Avenue, a main corridor for vehicular, cyclist, and pedestrian traffic traveling east-west through Westboro. Within short walking distance is a large grocery store in addition to restaurants, shops, and other services. Clare Gardens Park at the south end of Athlone Avenue is just over 300m away.

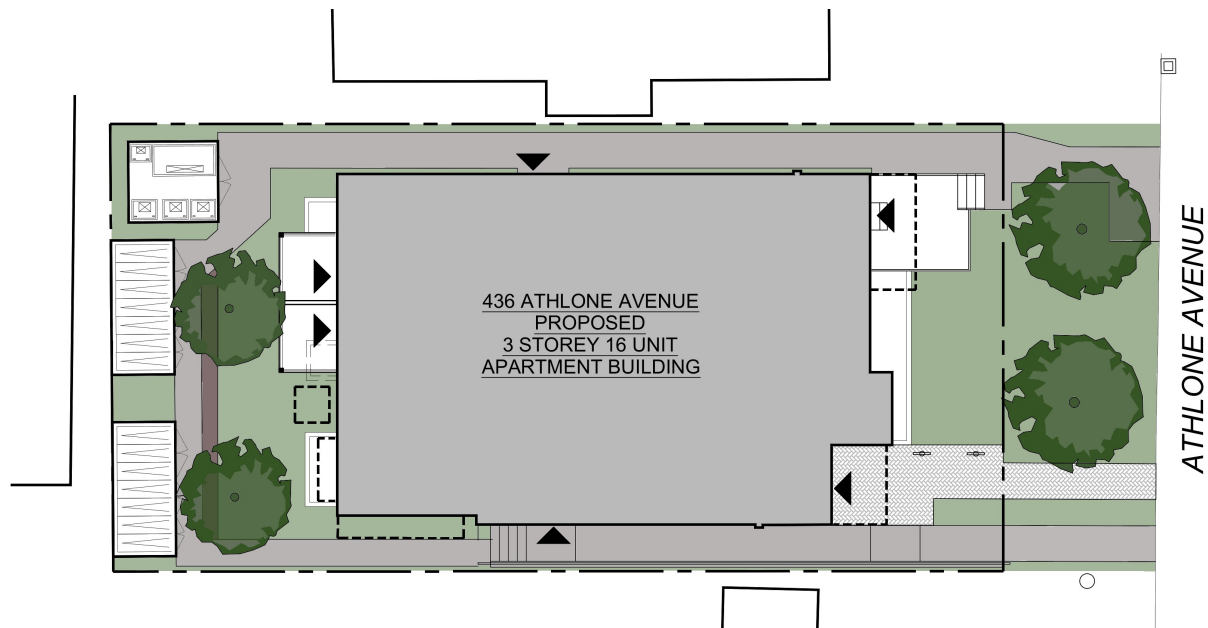
The site is located one in from the corner, with its rear lot line abutting a side lotline of a recently redeveloped semi. This results in a unique condition where the rear yard feels a bit like a courtyard, boxed in from behind. See Image 6.



*Image 5.3 – view looking north down Athlone from the corner of Wesley Avenue*

The neighbouring homes, though diverse in architectural expression, almost all have sloped roofs. To the south the homes are red brick with 3<sup>rd</sup> floor spaces within the slope of the roof and behind gables. Although it would be inaccurate to describe these elegant red brick homes as the dominant architectural style on the street (the street is very diverse) these buildings are particularly eye catching and memorable.

Athlone Avenue south of Byron has no sidewalks, as do some other streets in the area. The neighbourhood has tree lined streets and a mixture of singles, semis, and other types of low density multi-unit buildings and secondary dwelling units. The most significant aspect of context here is that this is a genuinely walkable location where tenants could enjoy walking year round to small shops, parks, services, and a nearby rapid transit station.



*Image 7 – Concept Plan of proposed development*

## DESIGN OVERVIEW

This apartment has been designed to provide 16 small high quality rental units in a simple wood frame walk-up apartment building. Unit sizes vary from bachelors (with sleeping nooks) to 2 bedroom units. All units are designed to be bright and to feel spacious with the use of; combined spaces, layered views that pass through different spaces, and interior frosted glass walls with windows beyond. Eight of the twelve above grade units have balconies, the ground floor front facing 2 bedroom unit has a walk out terrace, and the rear facing ground floor units have back doors and steps into the yard.

Exterior cladding is a combination of brick and horizontal lapped siding. Large contemporary windows contrast the more traditional use of materials. Although most of the roof is flat, a front portion is sloped and has a gable. This roof area will be clad in asphalt shingles in keeping with neighbours.

Accessibility is provided along a ramp starting at the front entrance walkway, leading to a side door. Two accessible units are located on the ground floor; a 2 bedroom unit and a bachelor.

The main entrance is at the front left corner of the building and contains the mailboxes. Mechanical and electrical equipment and meters are located on or next to the back and side walls, out of sight, as well as on the roof. Roof top equipment is toward the center of the roof, and surrounded by a sound barrier.

Garbage will be stored in a shed in the back yard, accessed by tenants through the north side exit door, then along a short path. Garbage will be rolled from the shed to the garbage pad (next to the road) on collection days.

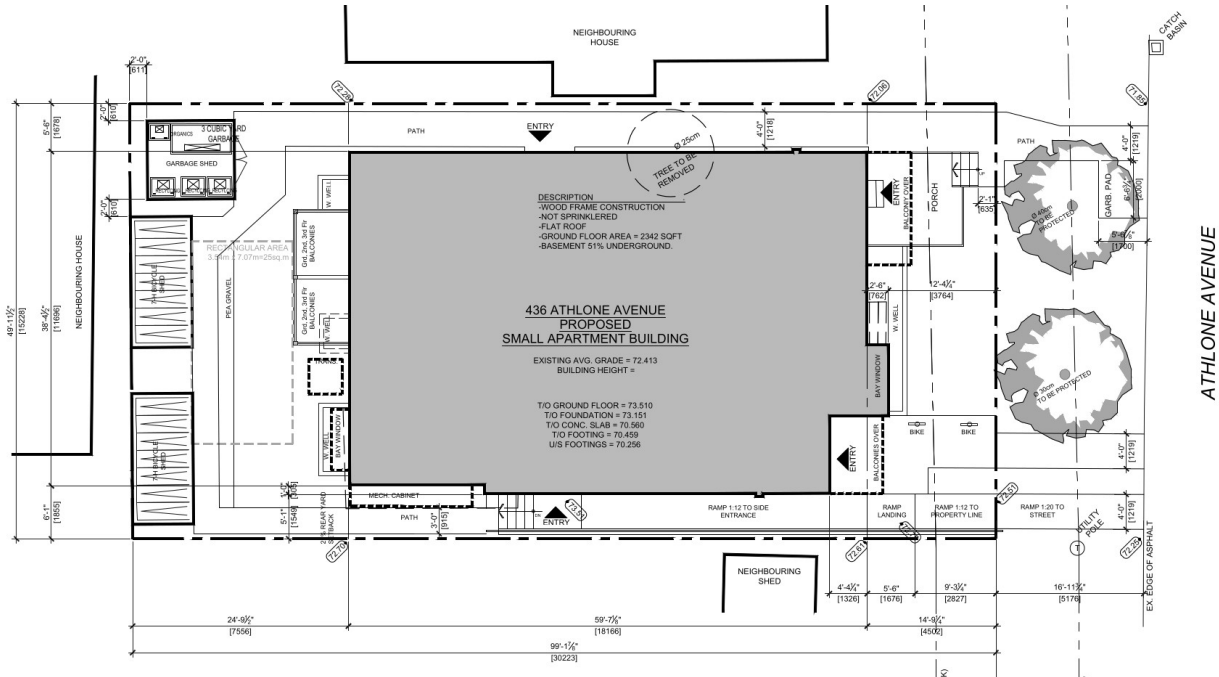


Image 8 – Site Plan of proposed development

## Front Facade and Street Context

The building was designed with its slope-roofed neighbours in mind. Design cues were taken largely from the older brick homes to the south side which are red brick with sloped roofs and eaves that are level with their third floor structures. To compliment this we propose a partial sloped roof with eaves at level of third floor structure, and front facing living spaces behind a front gable.

The front facade has been designed to break up the visual mass into elements in scale with neighbours. A front bay window extends the full height of the building, dividing breaking the massing vertically. The lower eave line breaks the front facade with a strong horizontal feature. And the grouping of large windows (with door) at the ground and second floor on the right side of the facade sets this part of the building apart at this smaller scale.



*Image 9 – 3D rendering of proposed building in context*

The design makes use of traditional brick colour and rooflines, together with more contemporary window design, balconies and box bay windows. A simple 3 colour pallet is used as per other simple colour pallets in the immediate neighbourhood; red brick, dark grey trim, light grey lapped siding.

There are two entrances on the front facade. The primary entrance is the visual focus of the facade. It is glassy, protected by a balcony above, and lit with pot lights so that it is a safe and inviting highlight feature after dark. The secondary front entrance is dedicated to the largest ground floor unit, leading out onto it's patio. Front balconies provide needed private amenity and also add interest and animation to the facade, providing a semi private interface between the public street and private interior spaces.

Front city owned trees will be retained and trimmed to allow site access during construction.



## Rear Yard

The rear yard is designed for regular use by tenants, for playing, gardening, parking bikes, and eating outdoors. Private balconies overlook the rear yard, but are toward the centre of the rear facade such that they do not significantly overlook neighbour's yards. Two trees will be planted, with space for large canopies, to contribute to our City's 40% tree canopy goal. They will provide a shaded amenity space for outdoor summer eating. Bike sheds are designed for convenient and regular bike use. Bay windows add articulation and interest to the rear facade on the exposed southern side. In combination with proposed balconies, the amenity space exceeds zoning requirements, allowing for private and shared outdoor spaces.



*Image 10 – View from south end of rear yard*

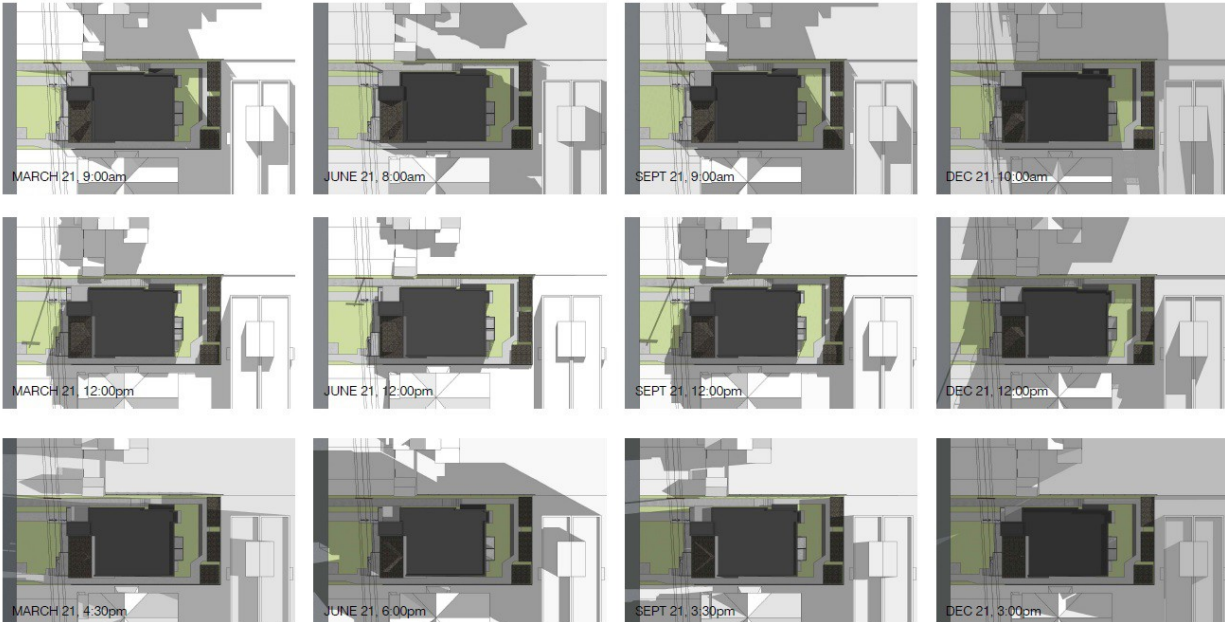
## Sustainability

The proposed 16 unit apartment building would replace a single family home with on-site parking. This existing house supports a single household in their car-dependent lifestyle. The proposed apartment would support 16 households in a walking / biking / transit lifestyle. This density of dwelling units will move the neighbourhood toward a density that is more able to support rapid transit. More residents without more cars is a very significant step toward sustainable living.

Unlike the existing home, this new development is designed to manage storm water and hold water on-site during extreme storm events.

## Sun Shadowing

Neighbours to the south expressed concern about sun shadowing. As per the study below, sun shadow effects are limited, no more significant than those cast by neighbouring buildings, and do not fall in the southern open space at the 'green centre of the block'. Below and included with our application are the full results of the study.



*Image 11 – Shadow Analysis for proposed building*

## Bike Storage

Two front bike parking spaces are provided near the front entrance. Two rear yard bike storage sheds are proposed, each housing 7 bikes. Rear yard sheds are proposed to be in line with the fence (a continuation of the fence line), and at the same height as the wood board privacy fence. These sheds have been designed with their back wall at the same height as the privacy fence, and to shed water only onto the subject property. Their eaves do not overhang onto neighbouring properties. Where a small retaining wall is needed at the property line, to meet the needs for site grading and rain water management, the retaining wall will be poured with the floor slab of the shed. From the neighbour's view, the sheds would look like a continuation of the fence line. Zoning requires a 2' setback, however by eliminating this setback we have eliminated a 'weed-zone' and increased the usable space of the rear yard for amenity and the enjoyment of tenants.

Below is a detail of the proposed bike shed designed to retain rain water on site. The rear wall height and style are designed to match the privacy fence as seen by the neighbour.

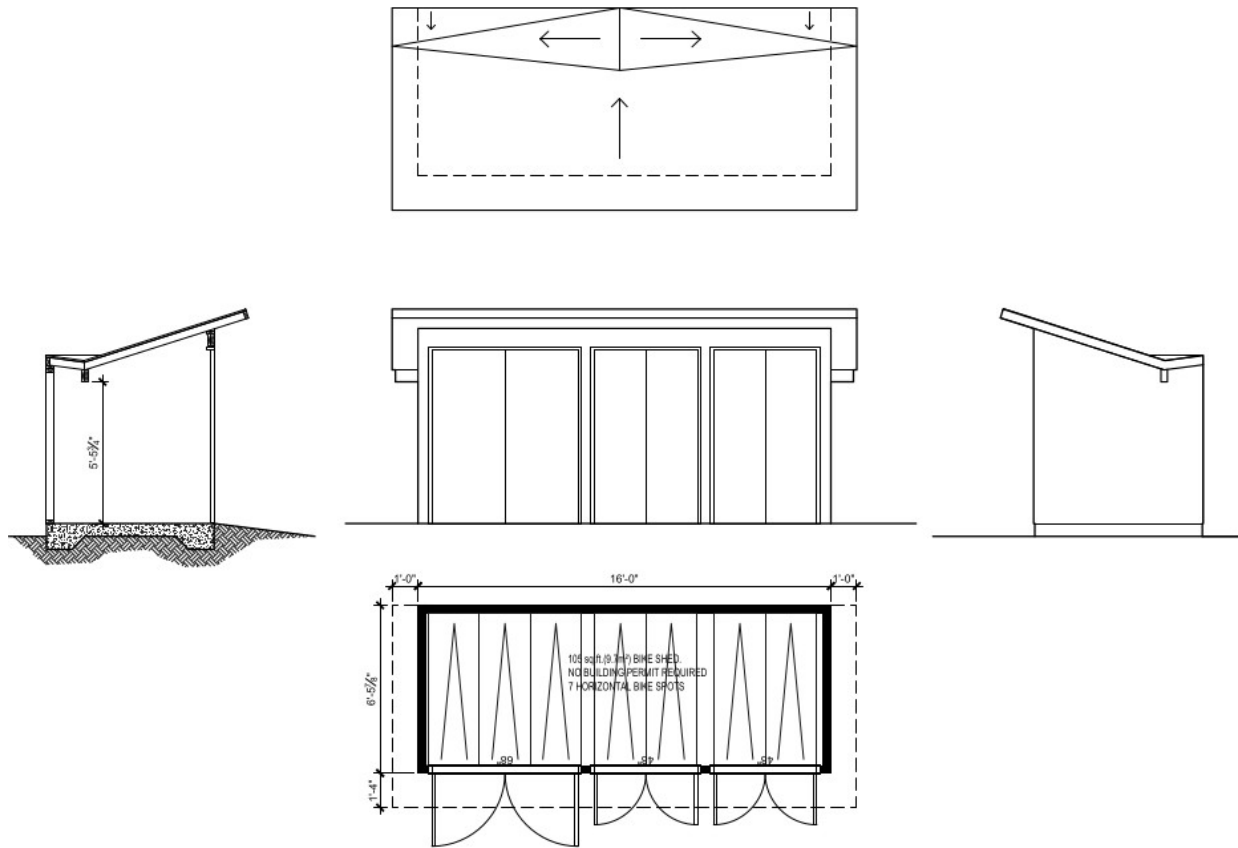


Image 12 – Detail of proposed bike shed

## PLANNING FRAMEWORK

The site is located in the Westboro Development Overlay (Schedule 430 to Zoning By-Law No. 2008-250) passed as By-law Number 2021-75 in February, 2021.

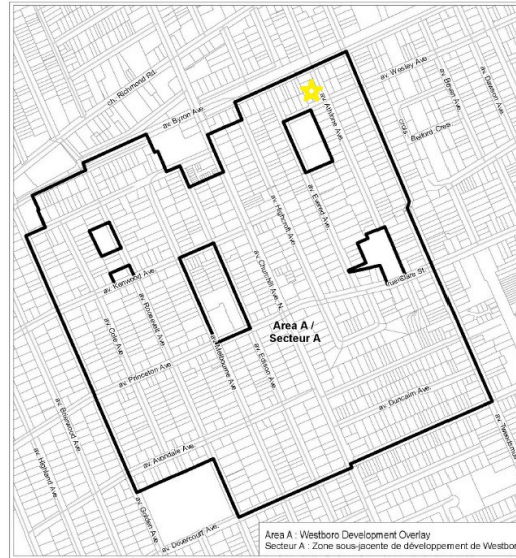


Image 13 – Map of Westboro Development Overlay

The purpose of this overlay was to test new zoning provisions that would support greater levels of intensification while preserving valued characteristics of the neighbourhood. In particular, this overlay permits higher density developments on lots close to busier streets (edge conditions). The subject property is the second lot from Byron Avenue and is zoned for an unlimited number of dwelling units.

### R4 Zoning

The R4 zoning for this site permits an unlimited number of dwelling units, but requires parking with all units in excess of 12 units. It is intended to allow multi unit infill apartments without parking in areas that are well serviced and transit served, but at the time that the R4 zoning was drafted, it was thought that a lot of this size could fit only 12 dwelling units, so the threshold for a zero parking requirement was set at 12 units. Since that time, real estate costs and rents have sky rocketted and need for smaller units is dire. It is therefore appropriate that our proposed building would house 16 small households rather than 12, and that these 16 units should be without parking. Our real estate advisors tell us that there are many tenants looking for a small unit in this location without parking. We are confident in finding 12 tenant households without cars, and we are equally confident in finding an additional 4 households without cars.

Front articulation is prescribed by R4 zoning, to encourage compatibility with neighbours. The proposed design exceeds these requirements, adding further articulation to the roofline to compliment southern neighbours.

R4 zoning is more prescriptive than any other zone with respect to landscaping. This proposal exceeds all landscaping and amenity requirements. The Westboro Development Overlay however applied an additional landscaping requirement; a 3m rear yard landscaped buffer. It is unclear whether sheds are permitted in this area. We propose to locate bike sheds at the property line and a garbage shed set 0.6m from lot lines. Proposed sheds are low, and predominantly out of sight from neighbours. A wood board fence will be installed around the rear yard, such that neighbours will see the proposed trees, not the proposed sheds.

Rear yard setback requirements for this property have increased over the past few years, from 25% to 28% of lot depth. The purpose of rear yard setbacks is multi-faceted; to preserve the 'green center of the block', to provide amenity space to residents, to preserve the enjoyment of outdoor spaces in adjacent yards, to facilitate orderly development.

The neighbour's house to the north is new, completed in the last few years, and has a rear yard setback of 25% of lot depth. The neighbour's house to the west has a setback of only 1.2m abutting the rear lot line of the subject property – this neighbour's side yard abuts the rear lot line. Our proposed rear yard setback is appropriate in this context.



*Image 14 – Green centre of the block*

As a result of the orientation of lots toward the corner of this block, the rear yard of the subject property has only one corner that is part of what one might call the 'green centre of the block', see diagram.

3D modeling demonstrates that the reduced setback (1.5m difference) has little or no impact of views, shadows, privacy, or neighbours' sense of space in the rear yard. See images 15 & 16 below noting the almost indistinguishable difference.



*Image 15 - Rear yard with requested variance, viewed from the south*



*Image 16 - Rear yard **without** requested variance, viewed from the south*

## Committee of Adjustment

Minor variances are needed in order to proceed with this proposed development, as follows:

1. Reduce rear yard setback from 28% to 25% (0.9m)
2. Eliminate the 3m landscape buffer at the rear property line, allowing sheds in this area
3. Eliminate the 0.6m setback required for a bike shed in the rear yard

## PRE-APPLICATION CONSULTATION

On February 15<sup>th</sup>, we met with city planning staff along with our client and our client's planner. The following were the comments from city planning staff included in the minutes from the meeting, as well as our response to these comments.

Comments from City Planning Staff	RJH Architect Responses
<b>PLANNING</b>	
Prior to making a complete submission, I also encourage you to discuss the proposal with the area Councillor, Jeff Leiper, immediate neighbours and local community associations.	We met with the city councilor on March 13. Following that we had a meeting with neighbours on May 6. We also had follow-up phone calls with southern neighbour.
<b>WASTE MANAGEMENT</b>	
Waste Management requires a minimum pathway width of 2 metres. If this distance cannot be met then a staging area for pickup day will need to be included at the front of the property.	We have added a garbage pad of 3.2m x 1.7m dimensions next to the curb.
The following containers are required: Garbage: 1 x 2 yard bin Fiber: 2 x 360L carts Glass metal plastic: 1 x 360L cart Organics: 1 x 240L cart	Provide as required.
<b>TECHNICAL REQUIREMENTS</b>	
The R4 provisions include parking prohibitors to ensure the front yard does not become a parking lot. The city would like to see an active front yard to act as a prohibitor, this can be combined with a landscape element.	We have designed the site plan so as to limit the potential of vehicular parking in the front yard in the future. Landscaping includes a boulder and a couple of trees in the front yard.
If required, ensure that you include railings to the ramp and stairs, as its proximity to the side yard might be impactful to the neighboring property.	Railings are shown on the site plan. The railing is largely behind the neighbour's shed.

<p>There are three separate walkways, consider combining the two to the north may help maximize soft landscaping at the front.</p>	<p>The two walkways on the northeast corner have been combined into one.</p>
<p>When justifying the reduction in the rear yard setback, ensure that you are not relying entirely on lining up with the neighboring property to the north as it was built under the old Zoning By-Law, and it also benefits from a corner yard context. Consider investigating more ways to increase the rear-yard and provide more green space. Alternatively, look into design articulation to alleviate massing impacts that you will have on rear-yard neighbours who are also offering their side yard.</p> <p>Garbage/bike storage: We appreciate that these storage facilities are protected, however, a large portion of the yard is being taken up by these services, we recommend efficiencies be investigated to reduce this. Internalizing the garbage storage would be another way to free space in the rear yard for amenity and landscaping/trees;</p>	<p>We have increased the southern side yard setback such that, close to the rear yard, the side yard is 1.85m rather than the 1.5m that is required. We have rearranged the bike sheds, reduced them in size, and located them abutting the property line. In this way, we have increased the amenity space by more than 2 feet and eliminated wasted “weed” space. See bike shed detail. There is space for two large trees.</p> <p>The rear yard of the subject property is not part of the 'green center of the block' – it is located at the edge of this area, out toward one corner. By reducing the rear yard setback (by only 0.9m), there is no reduction to the 'green center of the block'. 3D modeling demonstrates that the reduced setback has little or no impact of views, shadows, privacy, or neighbours' sense of space in the rear yard. The reduced setback matches the northern neighbour so it is of no impact to the north.</p>
<p><b>URBAN DESIGN COMMENTS</b></p>	
<p>We appreciate the strong approach this proposal makes to relate and fit within its neighbourhood context for its street facing facade, however, we have the following comments/questions that will need to be addressed when a full submission is made:</p>	<p>Thank you.</p>
<p>Rear yard amenity: Is suitable amenity space provided? Will new trees be provided?</p>	<p>Yes</p>
<p>Entrances: What do each of the entrances service? Will they have canopies? Are they all necessary?</p>	<p>There are four significant entrances to the building. One is the front entrance that is the shared formal entry to the building that has a welcome presence on the street. Another is the front facing entrance to a dwelling unit on the ground floor that animates the facade and allows the tenants of that unit to use their front facing outdoor patio. This entrance is prescribed by the R4 zoning and meets zoning intent to animate the streetscape. On the north side, there's an exit door for fire safety, and on the south side there is an entry point that is fully accessible.</p>



<p>Ramp: If there is an entrance at grade can the external ramp be removed and replaced with a small internal lift? We have concerns about placing the ramp in the front yard as it will require a guard rail and retaining wall along the side lot line;</p>	<p>We drafted a layout with a small internal lift and found that it consumed space equivalent to a small bedroom on the ground and basement floors. On balance, given housing need, the ramp seems like a better choice.</p>
<p>Rear yard massing: Can the rear yard facing elevation provide similar articulation to soften a potential box massing?</p>	<p>A bay window has been added to the rear facade to add articulation and architectural interest to the back.</p>
<p>Materiality: As a variety of materials are envisioned, we recommend any metal/vinyl siding be limited in quantity as they do not improve the neighbourhood character. Perhaps a cementitious panel/siding would suit a pairing with masonry better;</p>	<p>Neighbours tell us that they like our choice of brick at the front, and light coloured lapped siding on side walls.</p>
<p>Snow Storage: Any portion of the subject property which is intended to be used for permanent or temporary snow storage shall be as shown on the approved site plan and grading plan. Storage shall not interfere with approved grading and drainage patterns or servicing. Snow storage areas shall be setback from the property lines, foundations, fencing or landscaping a minimum of 1.5m. Snow storage areas shall not occupy driveways, aisles, required parking spaces or any portion of a road allowance. If snow is to be removed from the site, please indicate this on the plan(s).</p>	<p>No pavement and no parking and no designated areas are for snow storage, but there is room to clear pathways of snow.</p>
<b>TRANSPORTATION</b>	
<p>The purchaser, tenant or sub-lessee acknowledges the unit being rented/sold is not provided with any on-site parking and should a tenant/purchaser have a vehicle for which they wish to have parking that alternative and lawful arrangements will need to be made to accommodate their parking need at an alternative location. The Purchaser/Tenant also acknowledges that the availability and regulations governing on-street parking vary; that access to on-street parking, including through residential on-street parking permits issued by the City cannot be guaranteed now or in the future; and that a purchaser, tenant, or sub-lessee intending to rely on on-street parking for their vehicle or vehicles does so at their own risk.</p>	<p>Owner has been informed and will add on title</p>

<p>The Owner shall be required to enter into maintenance and liability agreement for all pavers, plant and landscaping material placed in the City right-of-way and the Owner shall assume all maintenance and replacement responsibilities in perpetuity.</p>	<p>This doesn't apply to our proposal. We have nothing except a pathway to the street. It is unnecessary to require such an agreement.</p>
<p>Bicycle parking spaces are required as per Section 111 of the Ottawa Comprehensive Zoning By-law. Bicycle parking spaces should be located in safe, secure places near main entrances and preferably protected from the weather.</p>	<p>Bike parking shed is provided for 14 spaces and they are covered. 2 bike parking spaces are provided at the front entrance. There is a total of 1 bike parking space per dwelling unit, which is twice the minimum required.</p>

## COMMUNITY CONSULTATION

On March 14<sup>th</sup>, we met virtually with the city ward councilor in order to share the development proposal in its early stages and receive some preliminary feedback. We were advised to meet with immediate neighbours. Soon after, we delivered letters to immediate neighbours inviting them to a virtual information session to share with them the proposal in its early stages of design and get their feedback. On April 6<sup>th</sup> we met virtually with about 10 residents who were immediate neighbours of the proposed development site, as well as a representative of the Westboro Community Association, so that we could hear their suggestions and concerns, and include their perspective in our design problem solving. There were a number of suggestions that we implemented.

Neighbours thought that the front design of the building fit well on the street, but were concerned about the number of units proposed and the volume of new people.

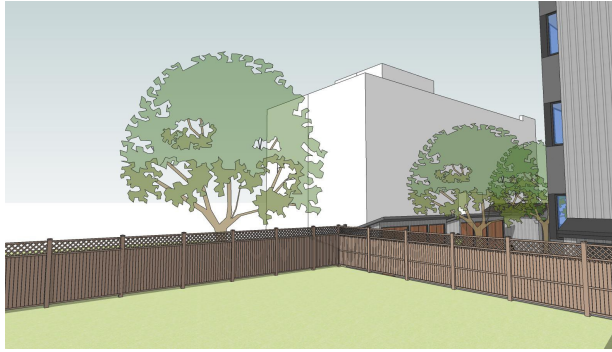
Neighbours also expressed concern about potential impacts on their rear yards. They requested a light coloured siding on side walls, and not corrugated metal. We therefore propose light grey lapped siding. They also requested fewer windows on the south facade overlooking rear yards, so we propose only small windows in the exit stair (not a location one would sit and watch a neighbour's backyard activities). Also in accordance with the wishes of neighbours, rear balconies have been moved toward the centre of the rear yard to minimize overlook.



*Image 17 – Building elevations*

Southern neighbours expressed concern about the proposed 0.9m (3 foot) reduction to rear yard setback, and in particular the ways in which this change would impact their views of the Gatineau Hills to the north. We have, therefore, provided 3D views from the southern neighbours' vantage points, comparing the proposed building to a zoning compliant building, see below. The difference has only a very marginal effect on the views of the Gatineau Hills, that were compromised by the developments at 426 and 422 Byron.

In response to sensitivities about rear yard impacts, we have moved balconies to the centre of the back facade and increased the southern side yard setback such that, close to the rear yard, the side yard setbacks exceeds the required setback by 0.3m (1 foot).

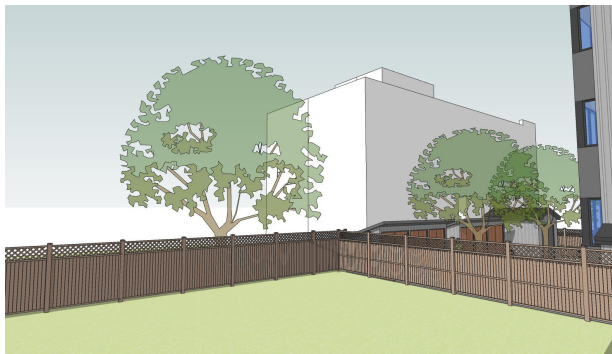


*Image 18.1*



*Image 18.2*

The above images show a proposed rear yard setback (3 foot reduction to bylaw requirement) with zoning compliant landscape area and amenity space.



*Image 19.1*



*Image 19.2*

The above images show a zoning compliant rear yard setback. The one on the left is a view from the back wall of the southern neighbour. The one on the right is a birds eye view from above the rear yard of the southern neighbour.

Neighbours requested that we advocate for neighbourhood parking -- we will continue, relentlessly, to advocate both for neighbourhood parking and for transitional parking solutions.

## THE OFFICIAL PLANS

The new City of Ottawa Official Plan (“the adopted Official Plan”) was adopted by Ottawa City Council on November 24, 2021. It is noted that at the time this document was prepared, the adopted Official Plan has not received approval from the Province of Ontario’s Ministry of Municipal Affairs and Housing.

### In-force Official Plan

The in-force City of Ottawa Official Plan was adopted by City Council on May 14, 2003 and modified by the Minister of Municipal Affairs on November 10, 2003. The Subject Property is designated General Urban Area on Schedule B of the in-force City of Ottawa Official Plan (see Figure 12).

Section 3.6.1 Policy 1 of the in-force Official Plan states that: *“The General Urban Area designation permits many types and densities of housing, as well as employment, retail uses, service, industrial, cultural, leisure, greenspace, entertainment and institutional uses.”* The proposed development of a low-rise apartment building is a permitted land use on the Subject Property.

Section 3.6.1 Policy 3 of the in-force Official Plan states that: *“Building height in the General Urban Area will continue to be predominantly Low-Rise. Within this range, changes in building form, height and density will be evaluated based upon compatibility with the existing context and the planned function of the area.”*

The proposed development of a three storey residential apartment building on the Subject Property is consistent with permitted building heights in the General Urban Area designation. The Subject Property is currently underutilized as a single family development, and is suitable for intensification. Policies of the in-force Official Plan are supportive of increased densities through intensification and redevelopment in the General Urban Area that will be predominantly low-rise.

Section 3.6.1 Policy 5 of the in-force Official Plan states that: *“The City supports intensification in the General Urban Area where it will complement the existing pattern and scale of development and planned function of the area. The predominant form of development and intensification will be semi-detached and other ground-oriented multiple unit housing. When considering a proposal for residential intensification through infill or redevelopment in the General Urban Area, the City will:”*

1. *Assess the compatibility of new development as it relates to existing community character so that it enhances and builds upon desirable established patterns of built form and open spaces;*
2. *Consider its contribution to the maintenance and achievement of a balance of housing types and tenures to provide a full range of housing for a variety of demographic profiles throughout the General Urban Area”*

Policies of the in-force Official Plan are generally supportive of intensification in the General Urban Area where it is comprised of low-rise housing forms that are compatible with the existing site context and community character. The Subject Property is surrounded by two storey singles to the north and east, and three storey singles and semi's to the south and west. The proposed development of a three storey apartment building represents appropriate intensification of the Subject Property with a compatible built form that is established within the existing site context. The proposed apartment building will contribute to expanding the range of available housing types and will create opportunities for a broad variety of residents to locate within the community.

Section 2.2 of the in-force Official Plan sets out policies to direct growth to target areas for intensification. The majority of growth is directed within the urban boundary where services are available or can be easily provided for new development to accommodate the creation of jobs, housing and increased transit use.

Section 2.2.2, Policy 1 a) of the in-force Official Plan identifies *“Redevelopment (the creation of new units, uses or lots on previously developed land in existing communities)”* as one of a range of supported methods for intensification within the Urban Area.

The Subject Property is designated General Urban Area on Schedule B of the in-force Official Plan. The Subject Property is located within the Urban Area and is within an appropriate location that can support residential intensification. The proposed development of a low-rise apartment building on the Subject Property is consistent with residential growth objectives of the in-force Official Plan.

Section 2.2.2, Policy 23 of the in-force Official Plan sets out policies for intensification located outside of target areas and states that: *“The interior portions of established low-rise residential neighbourhoods will continue to be characterized by low-rise buildings (as defined in Figure 2.4). The City supports intensification in the General Urban Area where it will enhance and complement its desirable characteristics and long-term renewal. Generally, new development, including redevelopment, proposed within the interior of established neighbourhoods will be designed to complement the area’s desirable character reflected in the pattern of built form and open spaces.”*

The Subject Property is designated General Urban Area and is situated within an established low-rise residential neighbourhood. The in-force Official Plan is supportive of new development opportunities on the Subject Property that promote intensification which is compatible with the surrounding low-rise residential context. The proposed development of a low-rise apartment building on the Subject Property complements the desirable characteristics of abutting properties along Athlone, is appropriate for the existing community context and is consistent with the General Urban Area policies in section 3.6.1 of the in-force Official Plan. It matches existing heights and setbacks, provided much needed smaller units to compliment the

larger units predominant on the street, provides dwellings at a lower price point without parking to compliment the expensive and car-depend dwellings.

## Urban Design and Compatibility

*Section 2.5.1 of the in-force Official Plan states: “compatible development means development that, although it is not necessarily the same as or similar to existing buildings in the vicinity, can enhance an established community through good design and innovation and coexists with existing development without causing undue adverse impact on surrounding properties. It ‘fits well’ within its physical context and ‘works well’ with the existing and planned function. Generally speaking, the more a new development can incorporate the common characteristics of its setting in the design, the more compatible it will be. Nevertheless, a development can be designed to fit and work well in a certain existing context without being ‘the same as’ the existing development.”*



*Image 20 – proposed development in street context*

The proposed development is close to the height of its neighbours and matches setbacks of neighbours to the north and west. It does not impact or invade the 'green centre of the block' but instead will add much needed tree canopy to adjoining rear yard space. The front roofline echos the rooflines to the south, with eaves in line with neighbours. The front facade elements are in scale with neighbours and include features notable and valued in the area; bay window, dormer, front porch, red brick, simple colour and material palette.

## Section 3.3.4 Designing Ottawa

### Design Objective 1

*To enhance the sense of community by creating and maintaining places with their own distinct identity.*

In this context, next door to brick homes that, for many, define the best of Westboro's residential character, it is significant that this design uses materials and features that honour and reflect these existing homes; red brick, sloping roof with dormer, bay window and front volumes scaled to match.

### Design Objective 2

*To define quality public and private spaces through development.*

The front elevation of the building continues the long tradition of high quality architecture along this street. The rear elevation is equally animated with bay window and balconies. The rear yard space will support outdoor social activities, gardening and play.

### Design Objective 3

*To create places that are safe, accessible and are easy to get to, and move through.*

The front entrance is well lit. Front balconies and windows will add lots of 'eye's on the street'. Bike sheds are easily accessed and safe. Accessibility is practical and in a prime location, and it is also affordable, making use of a ramp rather than an elevator which would inflate unit costs in such a small building. Pathways to the rear yard are simple and direct. The front entrance is clear and inviting.

### Design Objective 4

*To ensure that new development respects the character of existing areas.*

The front elevation of the building continues the long tradition of high quality residential architecture along this street. The building is in scale and in character with surrounding homes, despite it's dramatic increase in density.

### Design Objective 5

*Consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.*

### Design Objective 7

*To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.*



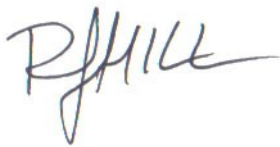
By introducing a low-rise apartment unit to this area, people will have more housing choice. Different size units, bachelor, 1 and 2 bedrooms will appeal to different purchasers at different stages in their housing life cycle.

## CONCLUSION

The proposed development at 436 Athlone Avenue is an opportunity to provide 16 new rental apartment units with no parking in a highly walkable neighbourhood in the City of Ottawa. This is an area that the City wishes to direct infill residential growth to help achieve intensification targets. The proposed development has been designed to meet the planning framework. It is consistent with the Official Plan. The intent of the zoning has been met, therefore the project can be built in accordance with the City's vision. The proposed building is a desirable contribution to this neighbourhood, and will provide much needed diversity of housing choice.

If you require any further information, please call me at 613-853-2822.

Regards,



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