

# **CSV** ARCHITECTS

**78 ROSEMOUNT AVE INC**

Design Brief

78 Rosemount Avenue, Ottawa, ON, K1Y 1P6

*PC2021-0394*

*CSV project # 2021-1130*

**May 13, 2022**



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# 1.0 SECTION 1

## 1.1 Application Submission

78 Rosemount Ave Inc. is proposing a three-storey, 12-unit housing development at 78 Rosemount Avenue (the site). The vision is for a high-quality, sustainable rental apartment building targeting long-term tenants who enjoy a neighbourhood with easy access to amenities on foot, bicycle, and public transit. A ground floor rear yard amenity space and accessory building with refuse and bicycle storage is proposed for residents. Additional bicycle parking for residents is offered in the front yard. This Design Brief is intended to demonstrate the objectives and vision for the site.

## 1.2 Response to City Documents:

### 1.2.1 City of Ottawa Official Plan Policies under Section 2.5.1

The property is designated General Urban Area on Schedule B of the Official Plan. “The General Urban Area designation permits the development of a full range and choice of housing types to meet the needs of all ages, incomes and life circumstances, in combination with conveniently located employment, retail, service, cultural, leisure, entertainment and institutional uses.”

Policy 3.6.1.5 indicates “The City supports intensification in the General Urban Area where it will complement the existing pattern and scale of development and planned function of the area.” This new building is sympathetic to the existing character, in line with the planned “development and intensification,” of multi-unit housing and builds on the range of housing types in the neighbourhood. These are the merits on which we believe this expansion should be evaluated.

The proposed development supports five (5) of the urban design objectives and principles set out in Section 2.5.1, as demonstrated in Table 1 below.

**Table 1: Official Plan Section 2.5.1 Urban Design Objectives and Principles**

Urban Design Objectives and Principles	
Policy	Response
1. To enhance the sense of community by creating and maintaining places with their own distinct identity.	The proposed multi-unit residential development will maintain and strengthen the existing identity and character of the neighbourhood. The proposed development re-enforces a continuous streetscape along Rosemount Ave while still distinguishing itself as a multi-unit home through the expression of the main entrance and awning and balconies to define the front-facing facade.
2. To define quality public and private spaces through development.	Public space: The proposed development includes softscaping at the front yard including a small tree to strengthen the curb appeal of the building and therefore the streetscape.  Private space: The proposed development will feature a shared softscaped outdoor amenity space at the rear yard for residents, eight private balconies for above-grade dwelling units, and one accessory building at the north-west side of the site for protected bicycle storage as well as waste collection.

<b>Policy</b>	<b>Response</b>
3. To create places that are safe, accessible and are easy to get to, and move through.	<p>The site will be universally accessible featuring an at-grade main entrance and accessory building. One of the residential suites will be barrier free and will be provided on the first floor, accessible by lift from the lobby level.</p> <p>The proposed development is located within proximity to public transit lines and is located within 550 m of Wellington St. West, a prominent mainstreet providing a variety of services</p>
4. To ensure that new development respects the character of existing areas.	<p>The character of the existing area can be defined by a mix of housing types. Many buildings along Rosemount Avenue and environs are constructed of brick. Development along Rosemount Ave. is generally built close to the front lot line.</p> <p>The proposed built form respects the character of the area by providing a 3 m front setback consistent with other buildings on the street and utilizing brick on all facades. The proposed building height follows existing buildings on Rosemount Ave. which are generally 2-3 storeys. The massing of the development corresponds with the existing low-rise apartment buildings along the street.</p>
5. To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.	<p>The proposed residential development is a multi-unit home supporting the intensification of the neighbourhood and providing flexibility in the use of each dwelling unit. While the development proposes a single-use, a majority 2-bedroom unit ratio and 3-unit layout per floor ensures large suites that can adapt their function and evolve alongside their streetscape.</p> <p>The development's roof plan is efficiently laid out to provide the necessary space for the potential installation of solar panels in the future.</p>

### 1.2.2 City of Ottawa Official Plan Policies under Section 3.6.1

Official Plan Policy 3.6.1.5 states that the City supports intensification in the General Urban Area where it will complement the existing pattern and scale of development and planned function of the area, and such proposals will be assessed based on the considerations in Table 2 below.

Table 2: Official Plan Section 3.6.1.5

<b>Enhancing the Appearance and Liveability of the Central Area</b>	
<b>Policy</b>	<b>Response</b>
1. Assess the compatibility of new development as it relates to existing community character so that it enhances and builds upon desirable established patterns of built form and open spaces;	<p>The proposed design respects the established character of the surrounding low-rise buildings through massing and façade definition. Balconies on the front of the development are sympathetic with the pattern set by neighbouring low-rises and provide appealing private outdoor space for residents.</p> <p>The development maintains uniformity with the other homes in the area, including the existing building to be demolished at 78 Rosemount Ave. regarding general setbacks, finishes, and height.</p>

Policy	Response
2. Consider its contribution to the maintenance and achievement of a balance of housing types and tenures to provide a full range of housing for a variety of demographic profiles throughout the General Urban Area	<p>The proposed building is in an area generally surrounded by detached homes with some existing low-rise apartments and semi-detached homes.</p> <p>The development will contribute to balancing and widening the range of housing types along Rosemount Avenue by increasing the number of dwelling units on the site by ten (10).</p>

### 1.2.3 Transit-Oriented Development Guidelines (2007)

The site falls within a 1500 m cycling distance of both Bayview and Tunney’s Pasture Rapid Transit Stations on the Confederation Line. It will also be less than 1000 m cycling distance to the future Trillium Line Corso Italia Station. As such, enhanced cycling facilities and cycling infrastructure should be considered when applying the City’s Transit-Oriented Development Guidelines (2007). The applicable guidelines are noted below along with the development’s response.

Guideline 1: The site will be developed into a low-rise apartment, increasing the residential density of the streetscape.

Guideline 2: No automobile-oriented land uses will be developed on the site. No automobile parking is proposed (or required) for the development.

Guideline 3: The proposed development will complement the existing local services, such as those provided along the mainstreet: Wellington Street West, which is close the site. The proposed low-rise housing will expand the range and mix of housing options in the area.

Guideline 14: All three storeys consist of windows on the front façade. The proposed materials of the building are red brick and glass, providing a connection to neighbouring buildings. Juliet balconies along the first floor and a recessed front main entrance with an awning, clad with cement board strips is proposed to add character and variety.

Guideline 15: The pedestrian level façade facing Rosemount Avenue will be made up of tall clear glazing to provide interest for residents in their dwelling units. The main entrance will also have a glazed door with a large sidelight and transom to draw interest from passersby and provide a sense of security through visibility for residents going in and out of the building.

Guideline 28: Dwelling units with Juliet balconies on the front façade at the pedestrian level are provided for residents and to animate the streetscape of Rosemount Ave.

Guideline 29: The twelve (12) bicycle parking provided for residents and visitors easily exceeds the zoning requirement of six (6) spots. Half are located conveniently at the front of the building, protected by an awning, and visible from inside the lobby. The other half are located securely inside the accessory building at the rear of the development, which connects to Rosemount Ave. via a concrete pathway.

Guideline 32: There are no vehicle parking spaces required by the zoning by-law and none are proposed.

### 1.2.4 Site-specific Urban Design Objectives

Safety of community members and residents and compact development are driving factors in the design. Connection between the interior of the building and exterior is intentional to reinforce the principles of CPTED (Crime Prevention Through Environmental Design). The building has been designed with

balconies and large glazing facing Rosemount Ave. to increase animation and natural surveillance at pedestrian level.

The accessory building for bicycle storage and refuse is proposed to the rear of the site to limit public access. Its primary connection to Rosemount Avenue is with a pathway that wraps around the North façade of the building and comes out beside the lobby.

The proposed low-rise development increases the housing density of the existing streetscape, helping to discourage urban sprawl. It prioritizes cycling by providing extensive bicycle parking and no motor vehicle parking. Located close to a public school, a main street with a variety of services and several transit stations, it encourages walking, transit use, and diversifies the demographic of residents.

### **1.2.5 Pre-application Meeting with the City of Ottawa**

A Pre-application Consultation Meeting with the City of Ottawa was held on December 1, 2021. The key urban design comments received from City staff are addressed here:

City Staff advised maximizing the soft scaping wherever possible and expressed concern over the window wells projecting into the front setback.

Response: The window wells are minimized to still provide basement units with natural light but maximize soft scaping and space for planting. Hard scaped pathways are kept to a minimum while maintaining accessibility across the site and connecting the main entrance at the front and accessory building at the rear to the sidewalk.

City Staff recommended the use of the existing built content in the neighbourhood to help inform the materiality of the new development.

Response: The proposed materials consist of red brick with large windows/glazed doors on all facades to provide interest while remaining consistent with the established character of the streetscape. Soldier course bricks for windowsills and lintels with creative brick patterns on the exterior walls will be used in moderation to create an appealing look.

City Staff reminded the requirement of secure and protected bike parking and encouraged a 1:1 ratio when no parking is provided.

Response: Bicycle parking is provided at a 1:1 ratio of dwelling units to parking spots. A bicycle rack securely mounted to a concrete pad is offered at the front of the lot, recessed from the sidewalk, limiting direct access to it by passersby. Sitting below the awning at the front entrance, it is protected from the weather, lit, and can be naturally surveyed from the lobby. Additional interior bicycle parking is provided in the accessory building at the rear of the site.

### **1.2.6 Pre-application Meetings with the Councilor and HCA**

The owner, Jake Levinson, met with Councilor Jeff Leiper on April 26, 2022 and the Hintonburg Community Association (HCA) on May 10, 2022. The latter meeting also included interested neighbours of the property. Beyond confirming that all by-law and zoning requirements were met, the following comments were gathered from these meetings:



1. Tree Canopy - there was a request that trees are included on the property in both the front and rear yards. Per the submitted landscape plan, this request is being met.
2. Bike Parking - the Councillor and residents both suggested that bike parking should be increased from the minimum requirement. Per the site plan, 12 bike parking spots are included (double the required 6 spots).
3. Garbage Storage - the HCA requested that steps were taken to contain odour from the garbage bins. All garbage will be stored in an enclosed space on the site and odour permeation should be minimal.
4. Noise Control - the HCA asked that neighbouring properties were considered as it relates to noise created by the HVAC systems of the building. Based on the placement of the individual condensers on the roof of the property, we feel that noise will be reduced as much as possible.

### 1.3 Context Plan

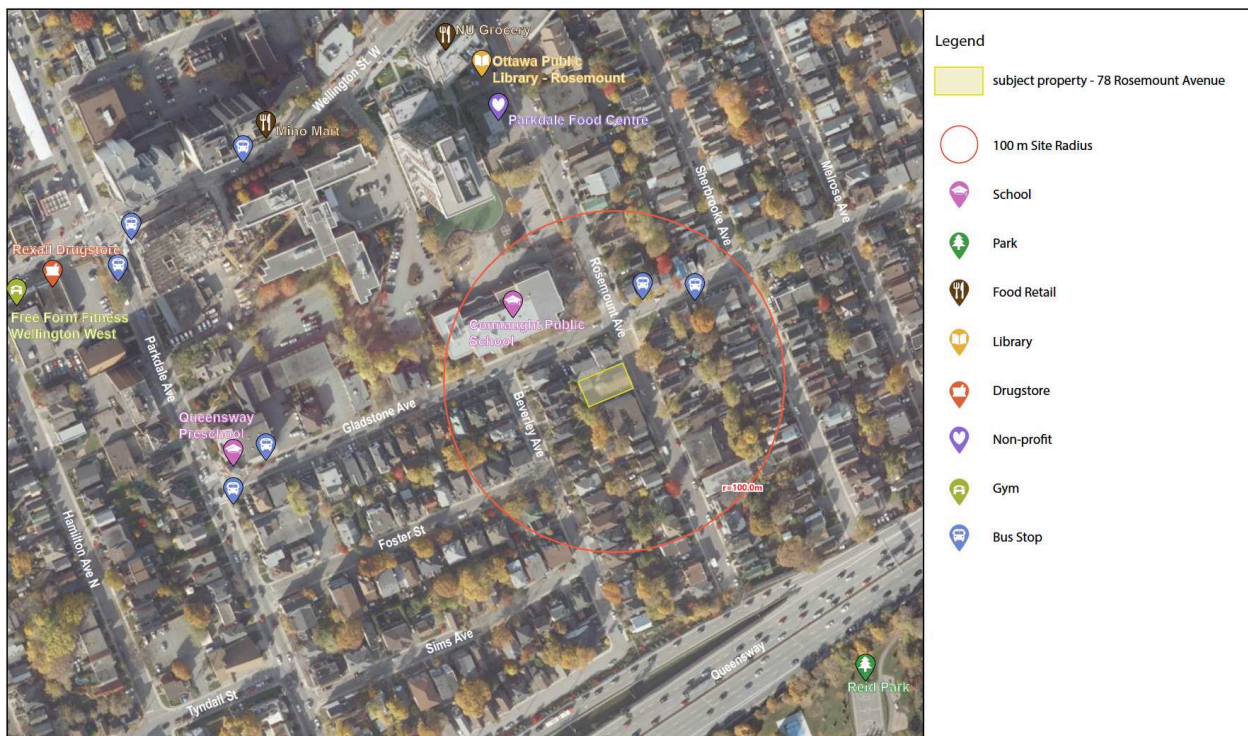


Figure 1: Context Plan showing relationship to local amenities

## 2.0 SECTION 2

### 2.1 Design Proposal

#### 2.1.1 Massing and Scale



*Figure 2: Bird's Eye View of Proposed Building*



*Figure 3: Perspective View of Proposed Building*

### 2.1.2 Public Realm

This development is exempt from the requirement for a Streetscape Character Analysis (SCA) due to by-law Section 140, clause 4(a).

### 2.1.3 Building Design



Figure 4: Front Elevation



Figure 5: Rear Elevation

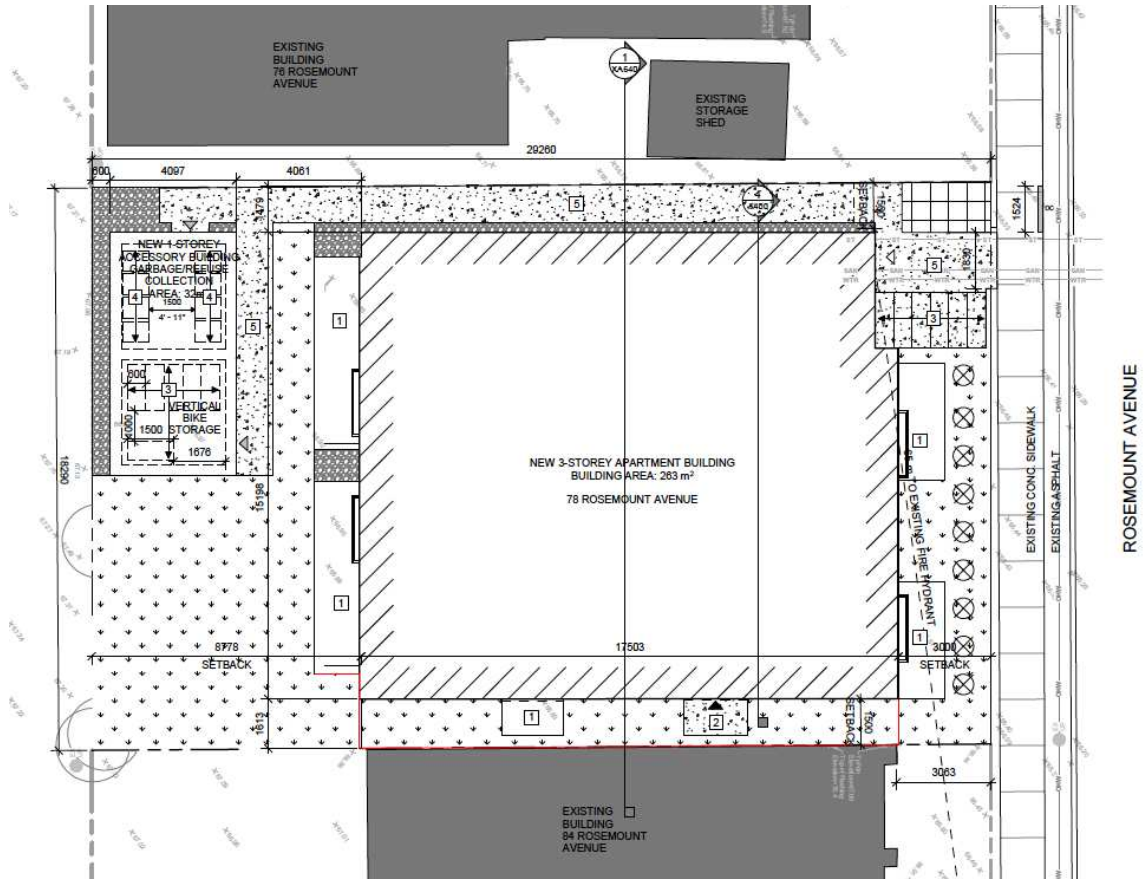


Figure 6: Proposed Site Layout

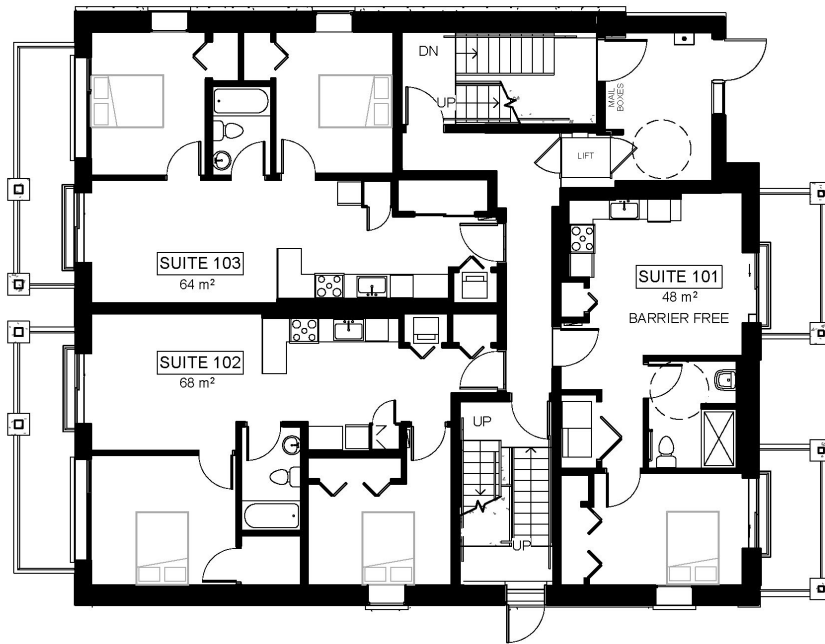


Figure 7: Ground Floor Plan

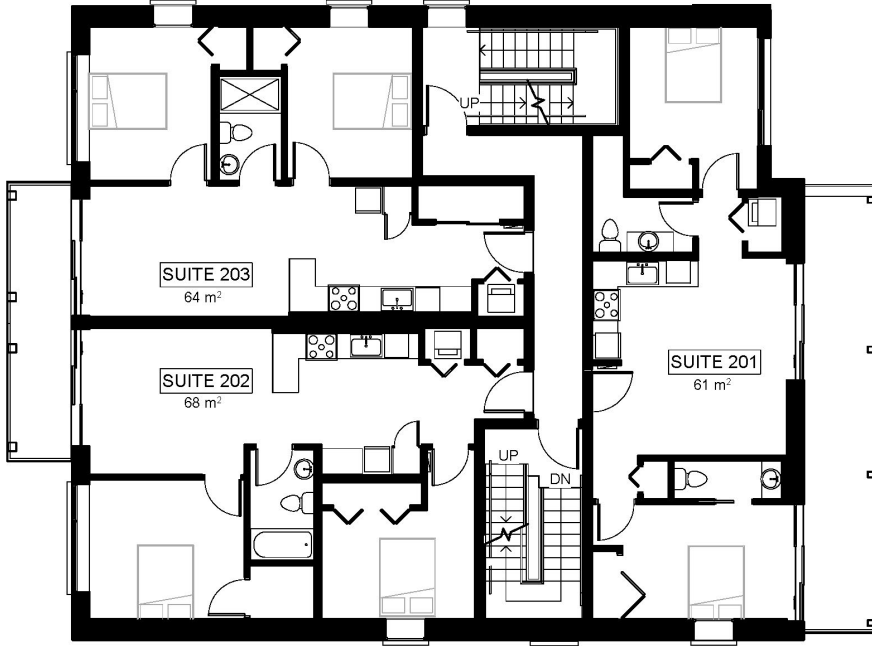


Figure 8: Second Floor Plan (Third Floor sim.)

#### 2.1.4 Sustainability

Below are details of the sustainability of the site, referencing the future City of Ottawa High Performance Development Standard.

##### Building Energy Efficiency

The building is designed to have an increased quality building envelope. Wall assemblies and windows will be of high quality to reduce energy consumption. The building will also be run on electricity only, with no natural gas connection which will significantly reduce carbon consumption.

##### Site Plan Accessibility

The public entrance of the building is at grade and serves all users equally.

##### Tree Planting and Plant Species

A small tree will be planted in the front yard. Plant species will be native, drought-resistant species. There will be no invasive species planted.

##### Exterior Lighting

All exterior lighting will be full cut-off with no up lighting.

##### Sustainable Roofing

The roof will have a high solar reflectance value to reduce heat island effect. An electrical connection will be provided to make the building ready for future solar panel installation.

##### Cool Landscape and Paving

Paving materials selected will have a high solar reflectance value and will be constructed out of permeable pavers where possible.

**Common Area Waste Storage**

The solid waste collection guidelines will be followed. There is sufficient space in the waste collection area for separate bins for compost, blue bin recycling, black bin recycling, and garbage.

**Bicycle Access and Storage**

Bicycle parking infrastructure exceeds the zoning bylaw requirements by 100%.

**2.1.5 Heritage**

A heritage review is not applicable to this development.