



Submitted to:

Development Review (Suburban Services, East)
Planning, Real estate and Economic Development Department
City of Ottawa

In Consideration for:

2458 Cleroux Crescent, Gloucester
3-storey 17 unit apartment dwelling

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Section 1

The Application Submission

- Application type: Site Plan Control and Minor Variance
- Legal description: Part of lot 10, Concession 3 - City of Ottawa
- Municipal address: 2458 Cleroux Crescent, Gloucester, ON, K1W 1A3

For the purposes of this report, Cleroux Crescent is considered a north-south street.

- Purpose of the Application: To facilitate the redevelopment of a property to become a 3-storey 17 unit stacked dwelling with vehicular parking on a lower level. 4 visitor parking spaces are provided in a mid-level and 20 additional spaces are available for residents in the lower level accessed from Cleroux Crescent.
- Vision Statement and Goals for the project: The redevelopment of this property will facilitate realigning the property with the surrounding built fabric by matching height and dwelling type. This smaller-scale residential infill project improves on built density and gives more use to existing infrastructure.

Response to City Comments

Design objectives for building liveable communities in Ottawa from Section 2.5 of the New Official Plan:

1. To enhance the sense of community by creating and maintaining places with their own distinct identity. *The proposal enhances the streetscape through soft and hard landscaping and providing a unique structure to the neighbourhood. By consequence, it enhances the pedestrian experience in the community.*
2. To define quality public and private spaces through development. *The development provides quality shared amenity space for residents and improves upon the existing outdated building frontage.*
3. To ensure that new development respects the character of existing areas. *The building respects setbacks of neighbouring lots and the overall style of the street while introducing modern building forms.*
4. To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterised by variety and choice. *Residents have the ability to personalise their unit entrances found at the side of the building, and there is adaptability in the shared landscaped areas to the front and rear of the building.*

5. To maximise energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment. *The compact nature of the building in combination with a high density of dwelling units is consistent with the City of Ottawa's infill development goals. Also, incorporating various sizes and unit options supports the City's goals of social sustainability.*

The New Official Plan categorises this area as an Outer Urban Transect under the 'Neighbourhood' designation which pertains to classic suburban neighbourhoods built within the greenbelt between approximately 1966 and 2000.

In Section 3.2, the New Official Plan outlines support for intensification. Intensification is permitted as a variety of built forms and height categories including low-rise developments. It is also stated that residential intensification will focus on the 15-minute model and this intensification is permitted in all designations.

This project allows for 17 residential units on an infill site located in close proximity to similar building types. The site is located approximately a 15 minute walk (1.2km) from Innes Road's variety of amenities such as retail, leisure, schools (Norman Johnston Alternative Secondary School - OCDSB, Good Shepherd School - OCSB, Emily Carr Middle School - OCDSB), food services, and transit.

In Section 5.3.1, the New Official Plan outlines goals to evolve these neighbourhoods into an urban, 15-minute model. This is supported by low-rise developments within the outer urban transect neighbourhoods as well as a range of dwelling unit sizes in multi-unit dwellings, ground-oriented forms, and low-rise multi-unit dwellings.

This low-rise multi-unit dwelling proposal meshes with the existing built fabric of the street while adhering to the goals of the New Official Plan. There are multiple unit sizes included in the building with 5 ground-oriented units.

In Section 6.3.1, the City outlines specific goals for the neighbourhood designation such as, building heights shall be limited to low-rise unless otherwise permitted by zoning or secondary plans, or in areas with pre-existing taller buildings. Additionally, it states that zoning by-law will allow lower densities and ground-oriented dwellings in outer urban transect neighbourhoods.

The proposal does not exceed 3 storeys, before the change in grade in the lot rear, keeping with the existing character of the street. The proposal also includes 5 ground-oriented units.

The Official Transportation Master Plan and Community Design Plan include a plan to expand the Brian Coburn rapid bus route directly south of the subject lands as well as a major pathway connecting Cleroux Cres to the bus route.

This reinforces the development's scale and building type as it relates to the New Official Plan's prioritisation of diverse development along rapid transit routes.

The proposed project is in general conformity with the surrounding existing residential development both in scale and building type. Due to its proximity to the Brian Coburn rapid bus transit extension, it is an appropriate land use and density in the community.

Areas of non-compliance

2458 Cleroux is a compact design that complements the existing built context and offers a variety of unit types and sizes. It provides the additional housing options mentioned in the R4 zoning goals while maintaining the residential character of the streetscape. The proposed multi-unit dwelling is zoning compliant for front, side and rear yard setbacks, building height, bicycle parking, and amenity areas.

This proposal is requesting relief from minimum lot width and minimum interior yard setback. The discrepancy between the minimum lot width zoning requirement and the proposal is 0.28m (1.5%), and the minimum interior yard setback discrepancy is 1m (20%). These adjustments do not affect the building's accessibility, design excellence, or resident experience and represent minor relief from the Zoning By-Law.

Context Plan

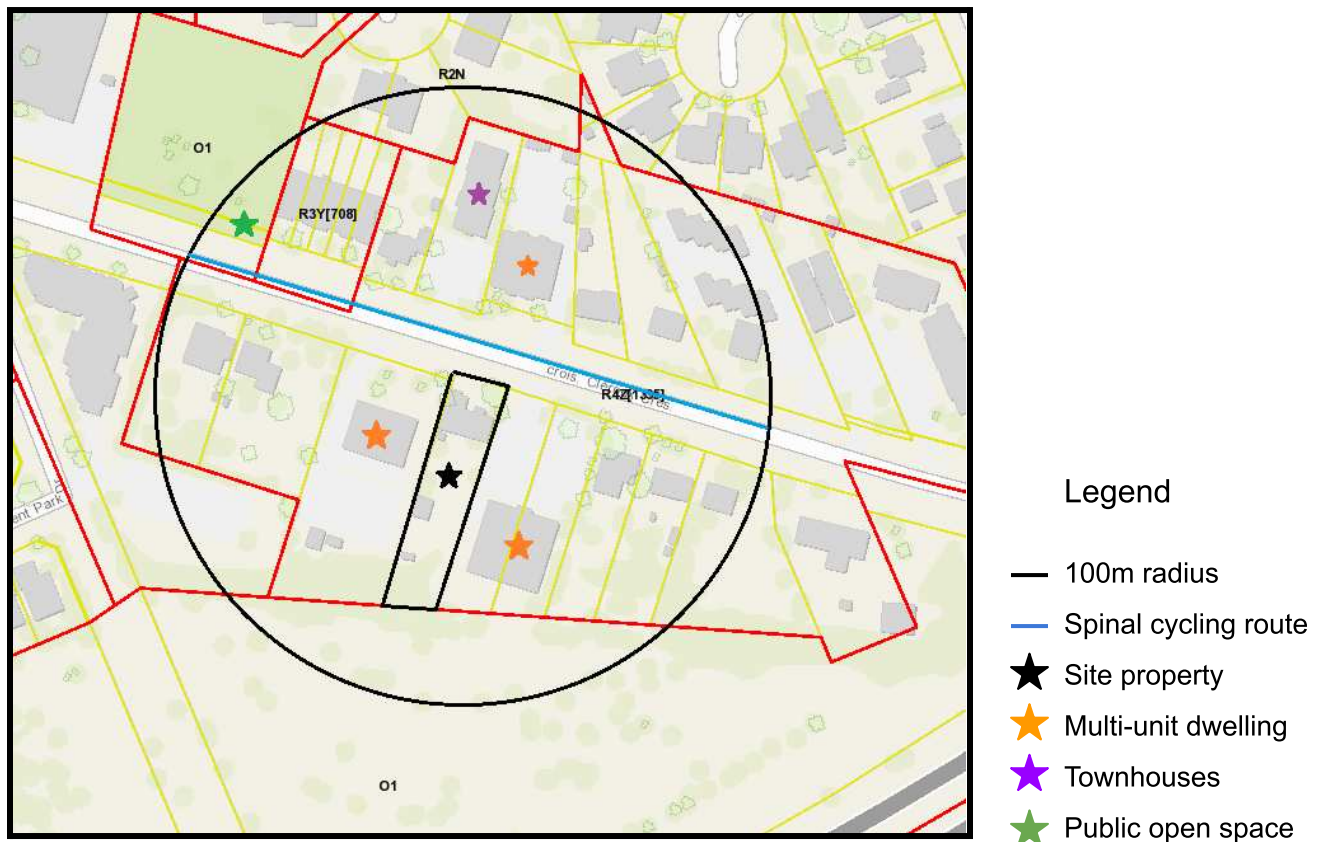


Figure 1. Context plan with 100m radius around 2458 Cleroux Cres.

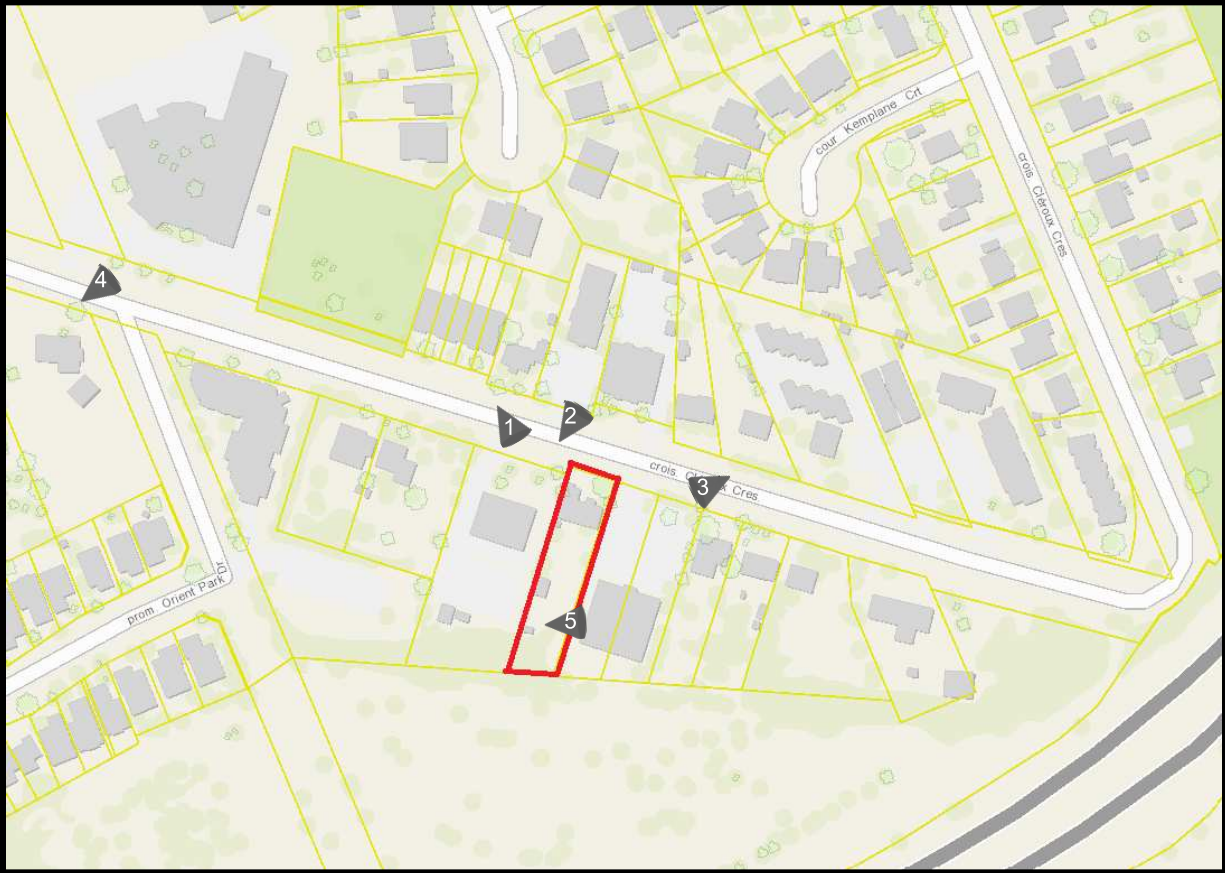


Figure 2. Location plan of 2458 Cleroux Cres.



Figure 3. Context image 1.

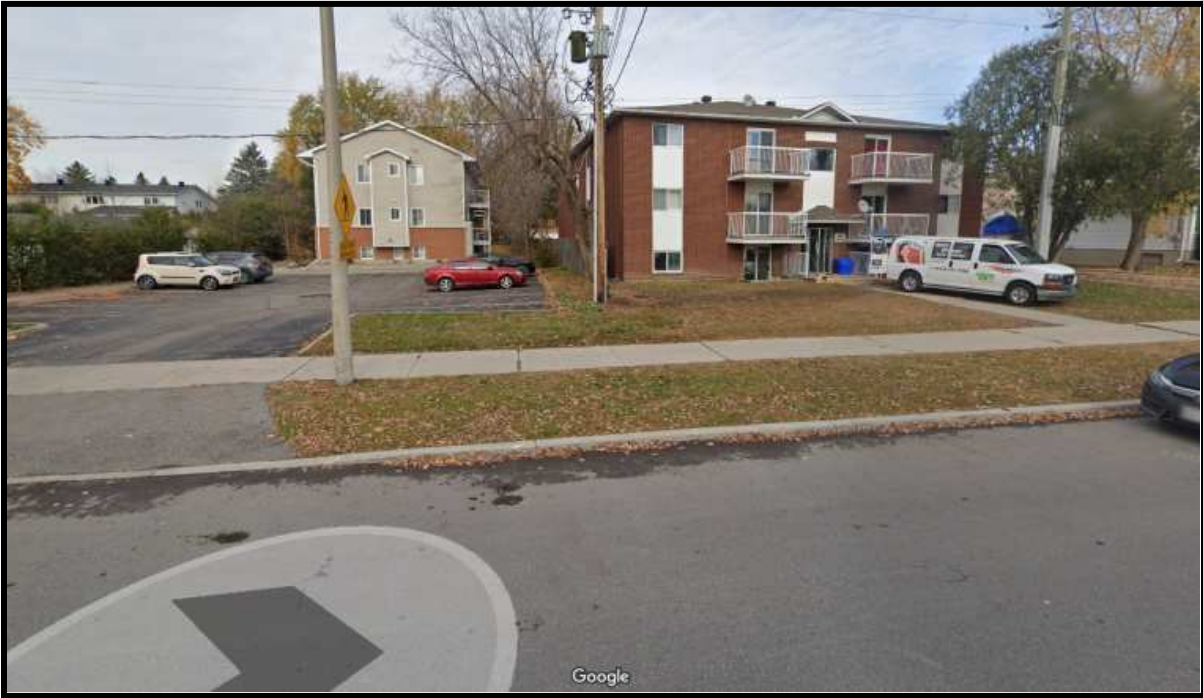


Figure 4. Context image 2.



Figure 5. Context image 3.



Figure 6. Context image 4.



Figure 7. Google Earth image of 2458 Cleroux Cres. (left) and context image 5 of existing rear yard conditions of Kings Hill Development from 2458 Cleroux.

Cleroux Crescent is a low density residential street featuring single storey detached and 2 and 3 storey multi-unit dwellings. The properties immediately adjacent to the site are 2 and a half and 3 storey apartment buildings (2444 and 2468 Cleroux respectively). Directly across the street from the site are a 3 storey apartment building and a series of 3 storey stacked townhouses (2459 and 2449 Cleroux respectively). Architectural style varies between the existing buildings. Some primary materials include dark red brick, dark sloped roofs, and the use of white vinyl siding. Other elements of note are walkways, balconies, and entry into ground units from the front or sides of the buildings.

Key destinations located in close proximity to the site are a secondary school, fire station, and Catholic Church. The site is also within a 5-minute walking distance (approx. 350m) from Innes road containing multiple bus stops and entrances to various pedestrian pathways within the Greenbelt. There are more retail and personal service shops west of the site along Innes road.

Section 2 - Design Proposal



Figure 8. Massing image of 2458 Cleroux Cres looking southwest.



Figure 9. Massing image of 2458 Cleroux Cres looking northwest.

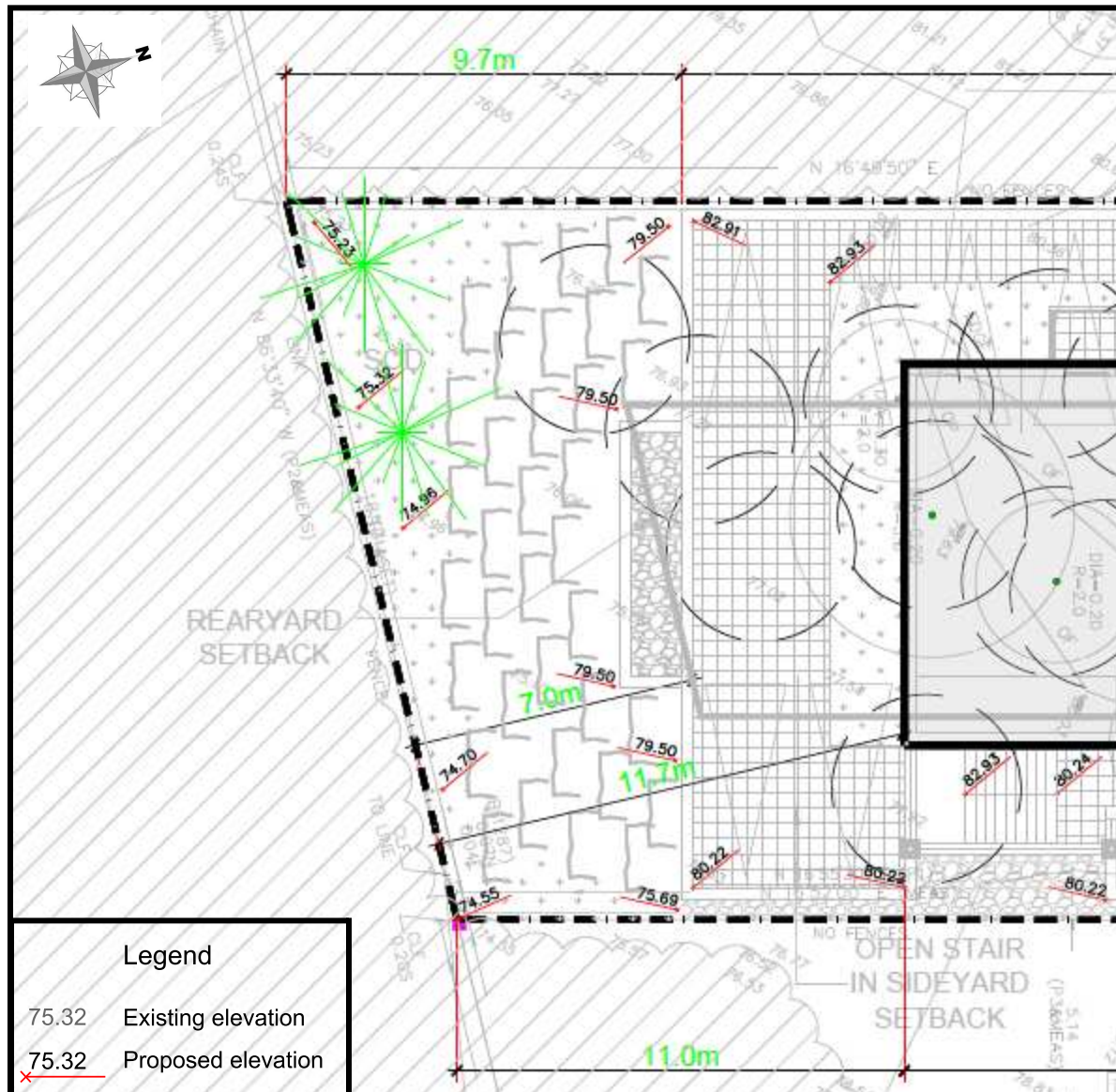


Fig. 10. Proposed Grading Plan for 2458 Cleroux Cres.

The property is a full 3 storeys on the street-facing facade of the building, and as the grade slopes towards the back of the site the building becomes 4 storeys. The front facade will mesh with the adjacent building heights as well as the buildings across the street.

The building is characterised by its sloped driveway down a half level to accommodate visitor parking, and a paved walkway connecting the sidewalk to the building unit entrances. The extruded roof over the stairway entry and side facing balconies provide visual similarities to the surrounding context.

Building design

The proposed building offers a modern style of a desired building type in this area, improving upon the existing low-rise context without overshadowing neighbouring properties. This is done through simple linear details and the mixing of natural materials, glazed balconies, and dark accents along the roof and outlining building openings. Thoughtful landscaping with various trees was included with the pedestrian experience in mind increasing connectivity from the units to shared spaces and adding extra privacy to the dwellings.

The colour pallet of this project is simplistic with warm, natural tones to avoid visual conflict with the surrounding buildings. The light-coloured natural stone veneer mixed with the dark casing and natural wood accents provide an updated look to the similar building style found in existing context.

The general lot shape and size limit the amount of possible street-accessible units, but the gallery-access access style allows for entry at grade with increased privacy for residents.

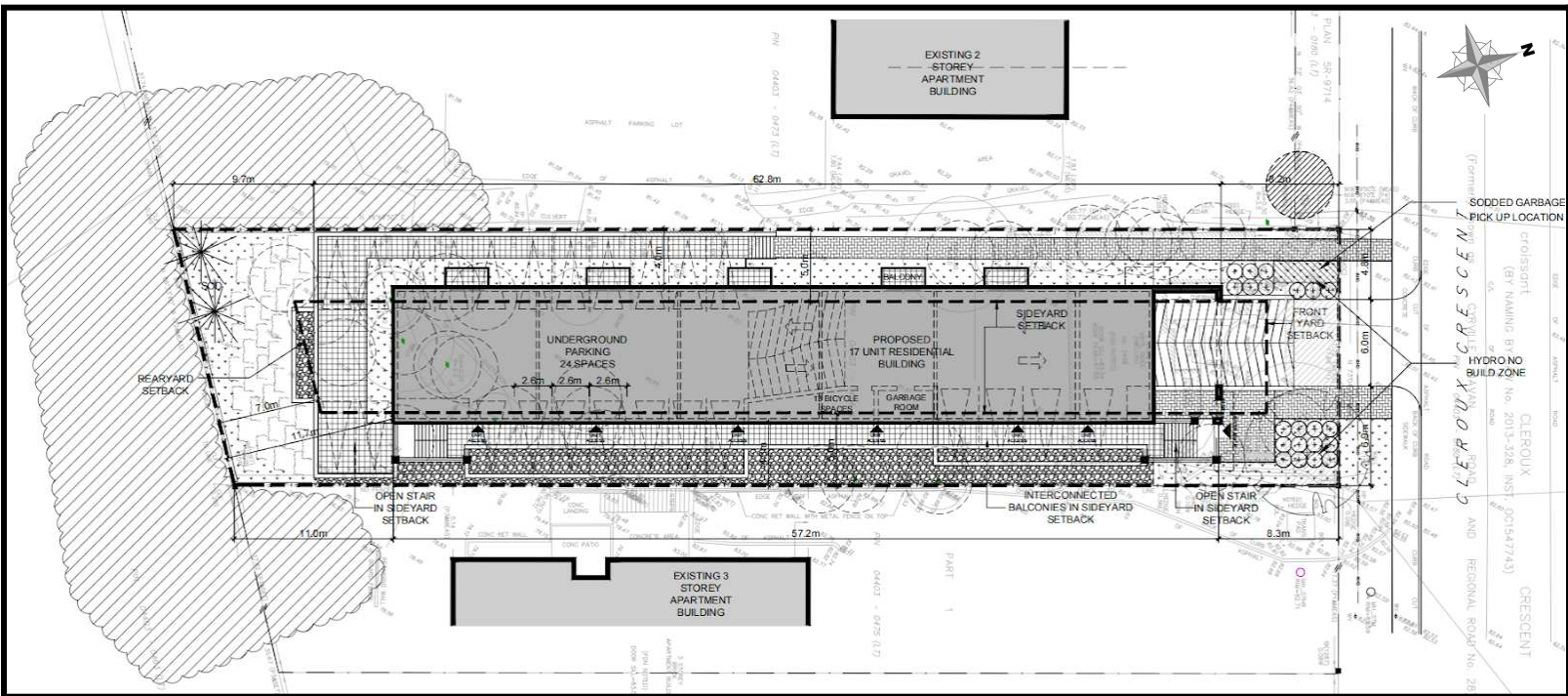


Figure 11. 2458 Cleroux Cres site plan.

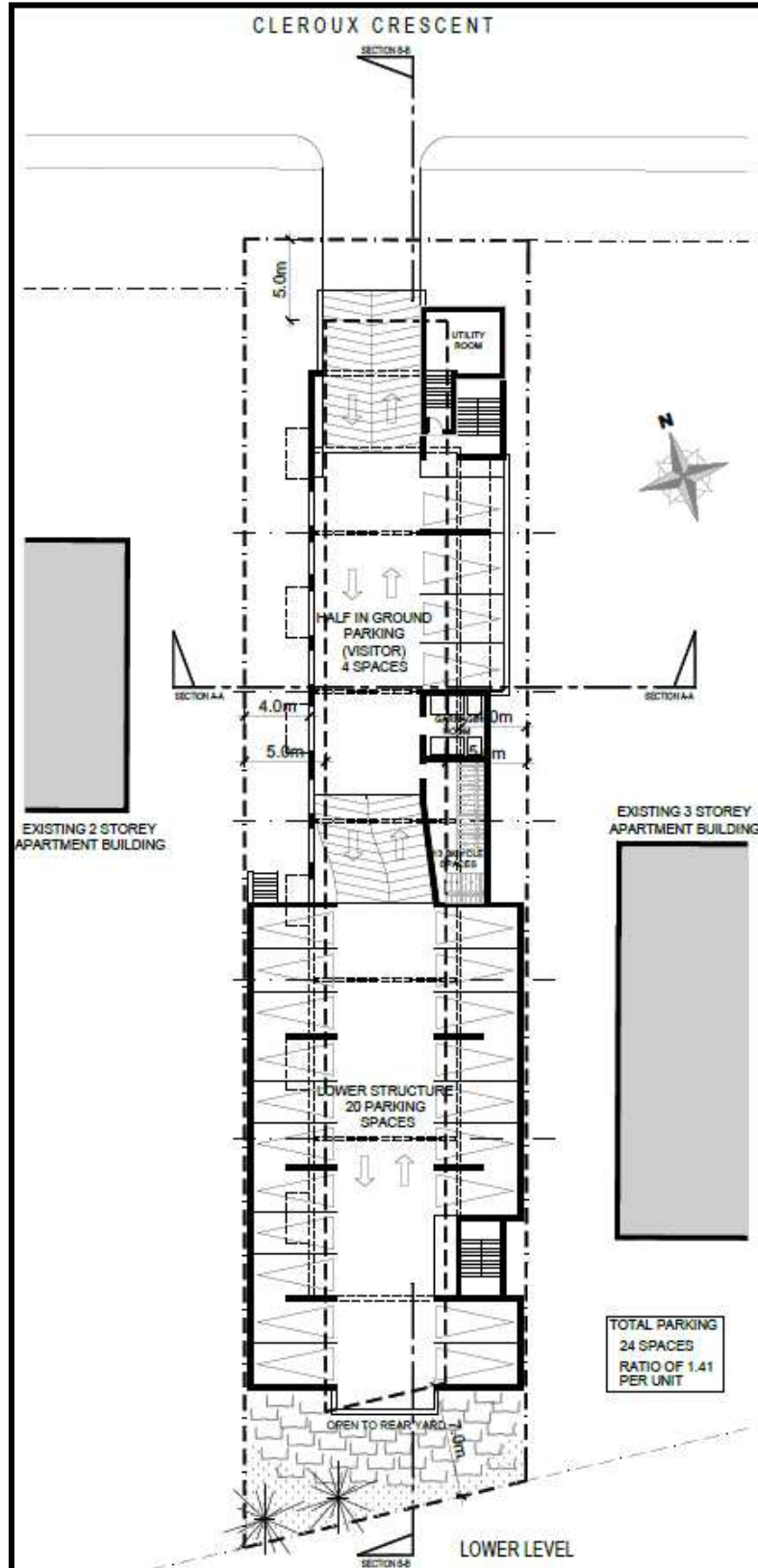


Figure 12. 2458 Cleroux Cres lower level.

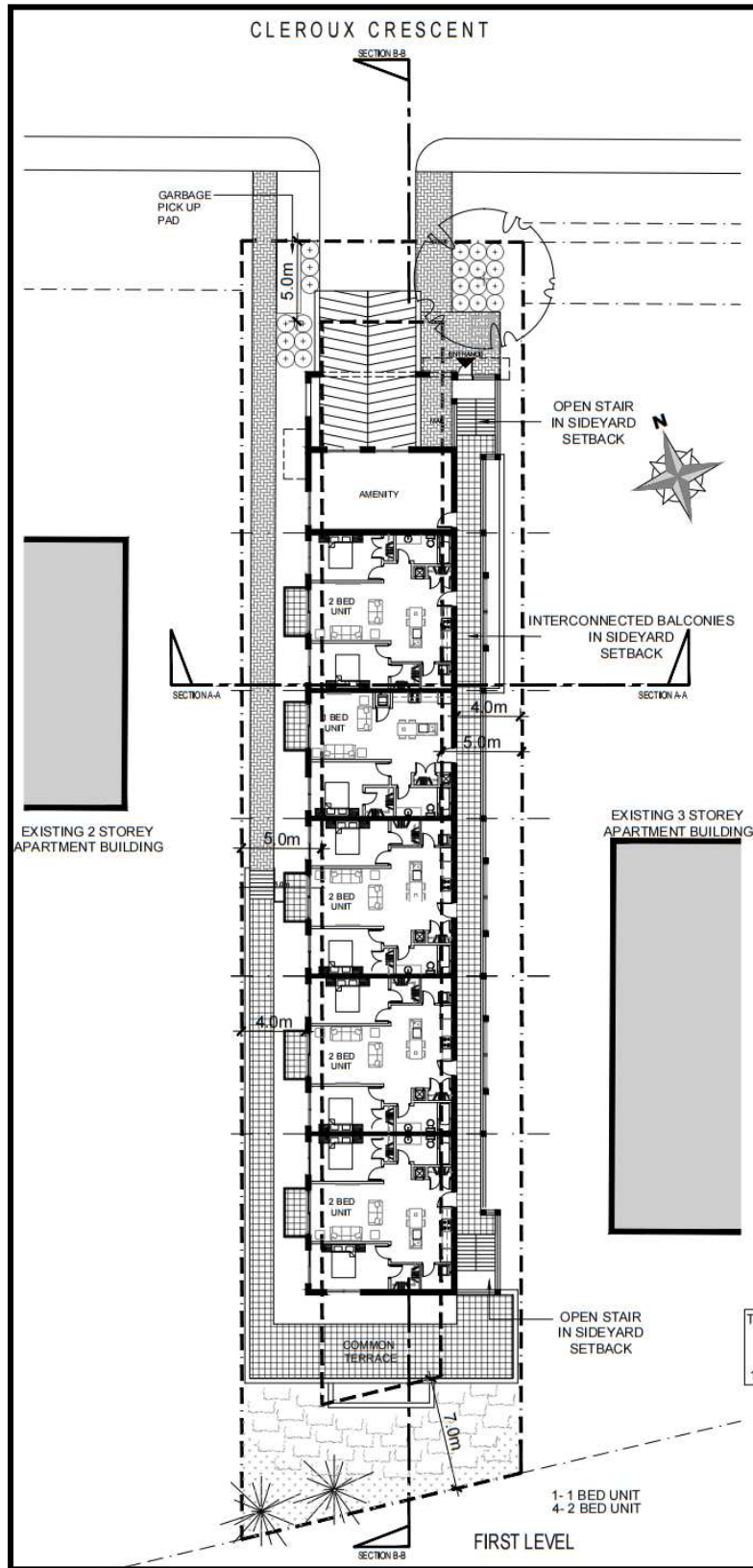


Figure 13. 2458 Cleroux Cres first level.

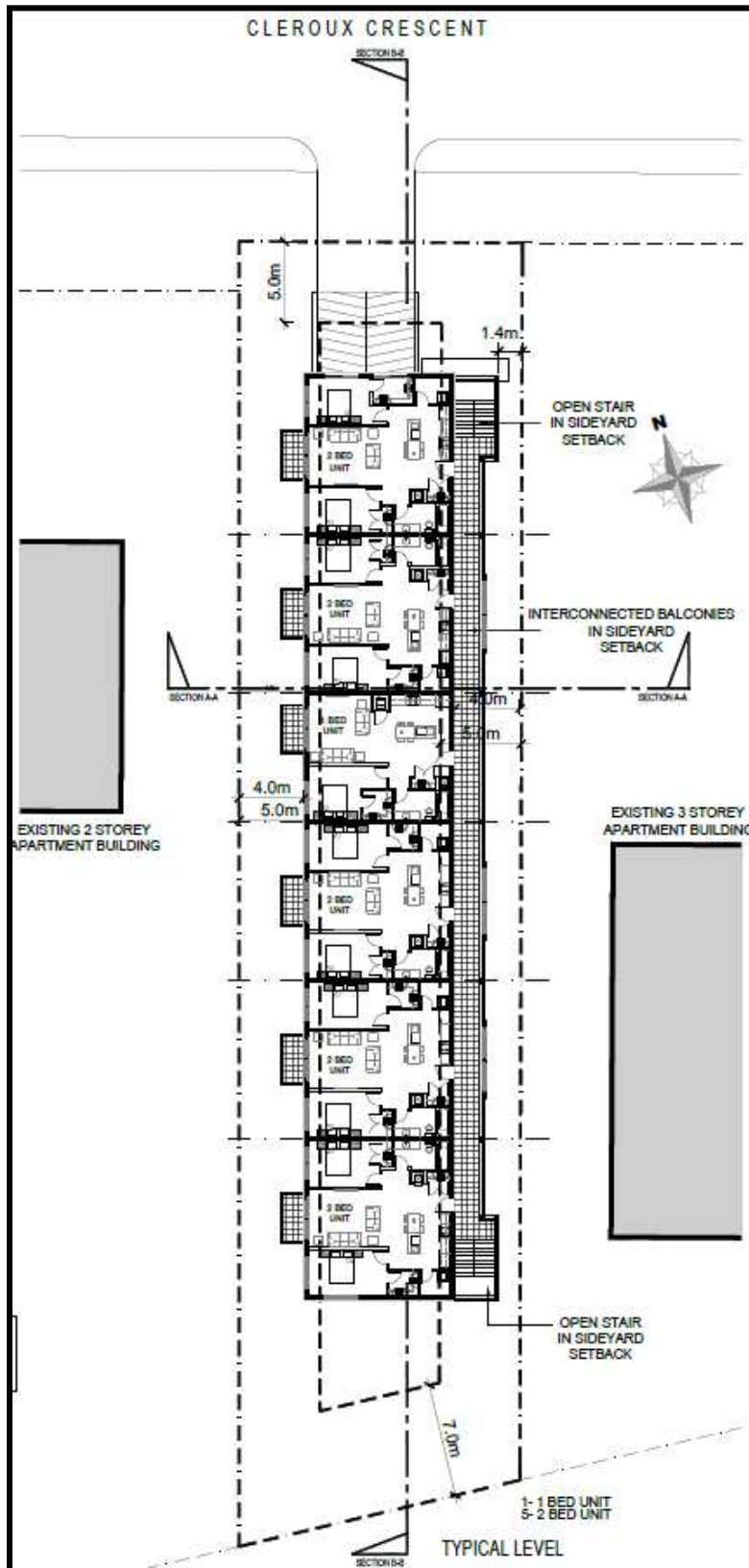


Figure 14. 2458 Cleroux Cres second and third level.

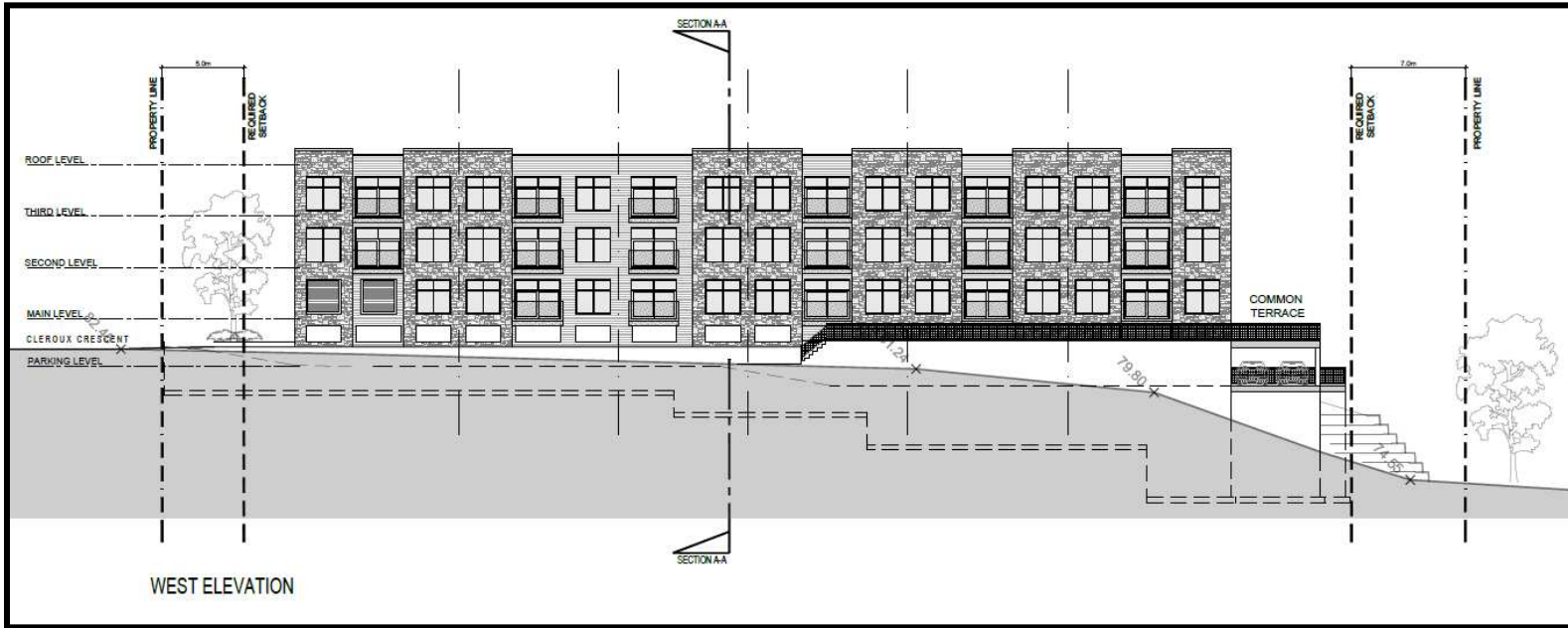


Figure 15. 2458 Cleroux Cres west elevation.

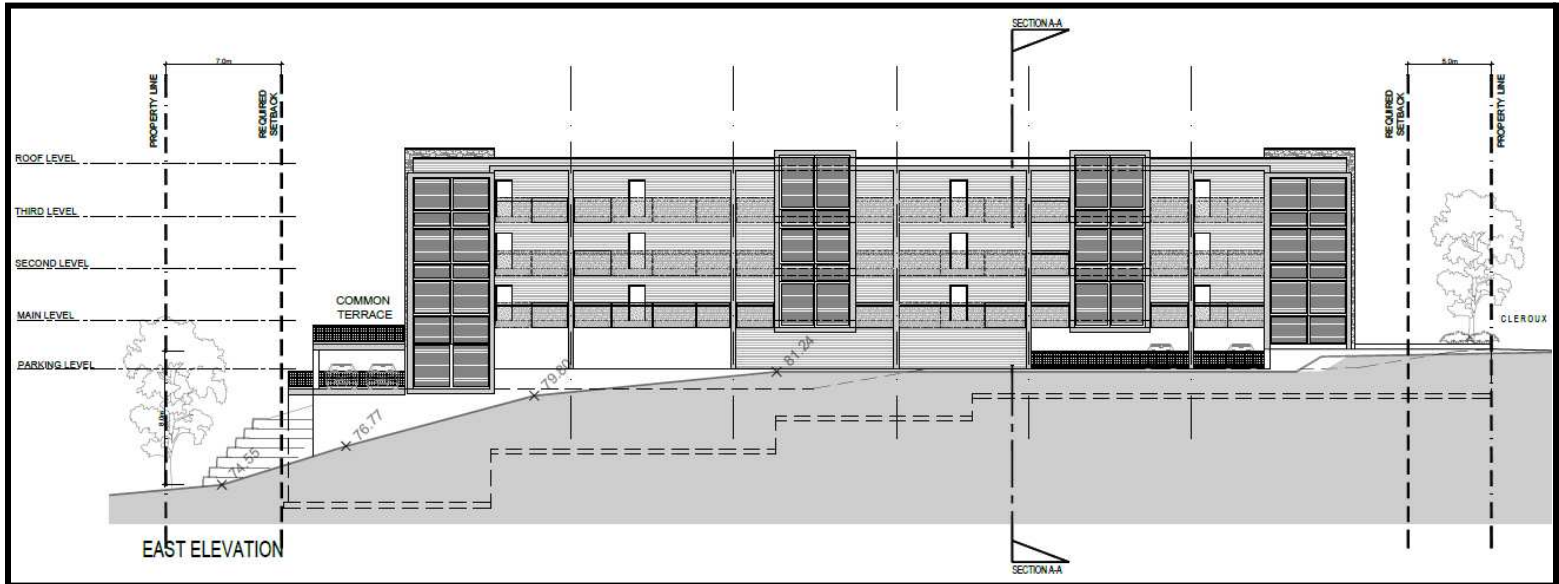


Figure 16. 2458 Cleroux Cres east elevation.

Sustainability

The compact nature of the building in combination with a high density of dwelling units is consistent with the City of Ottawa's infill development goals. Also, incorporating various sizes and unit options supports the City's goals of social sustainability.

Additionally, minimal south facing windows reduces need for cooling the units in the summer months, and using through-units exclusively maximises opportunities for cross-ventilation.

Heritage

According to Canada's Historic Places register, there are no heritage properties on or adjacent to the subject property.

Signatures

Signature of Planning Representative,
P-Squared Concepts Inc.:

Signature of Design Team Representative,
P-Squared Concepts Inc.:

September 29th, 2022

Date



Jasmine Paoloni, B.A.S

October 3rd, 2022

Date



Patrick England, President

Appendix

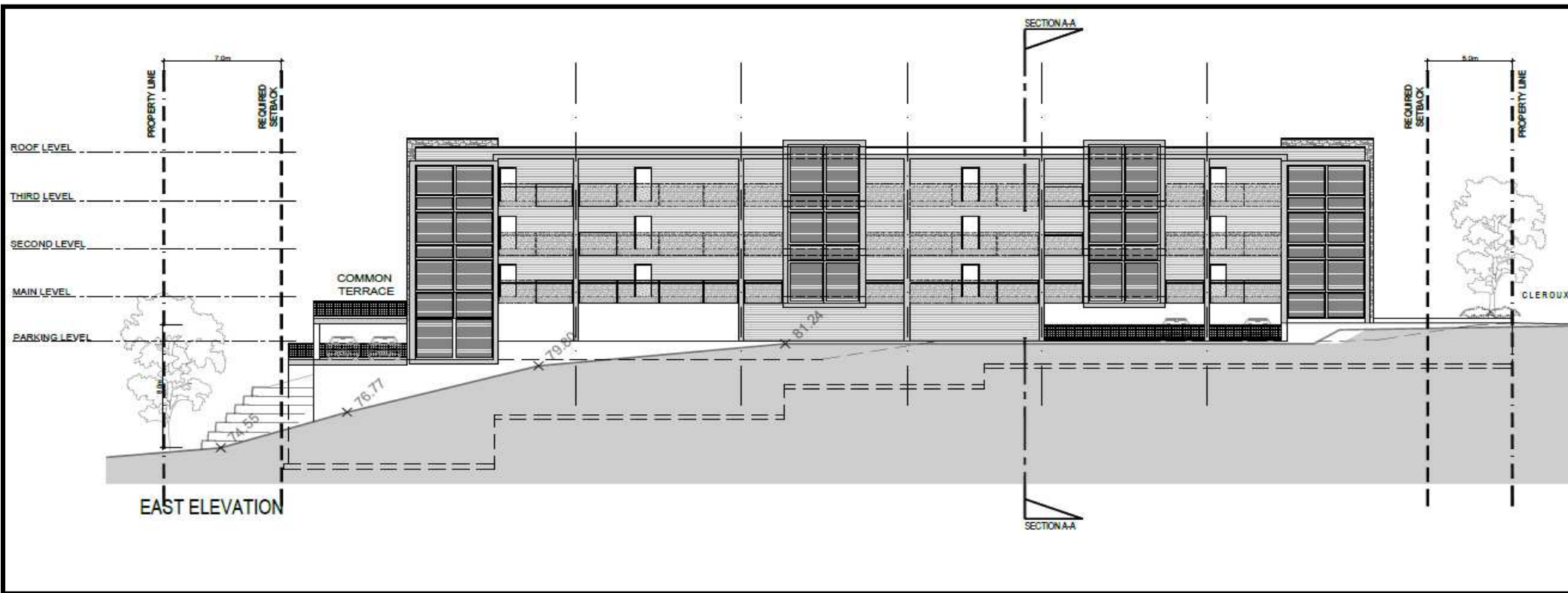
Large format images shown under Section 2 - Design Proposal, and others:

1. West elevation
2. East elevation
3. Site plan
4. Survey plan
5. Landscape plan

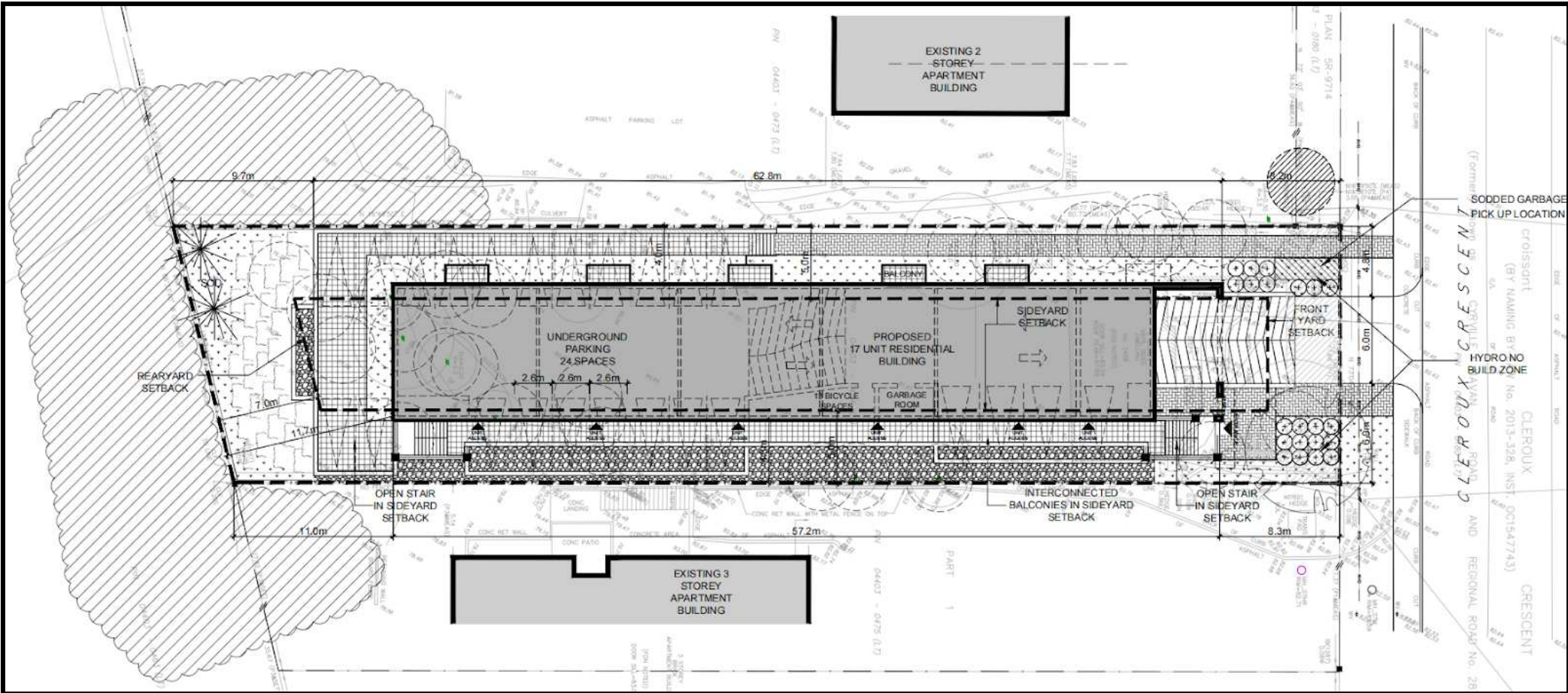
1. West Elevation



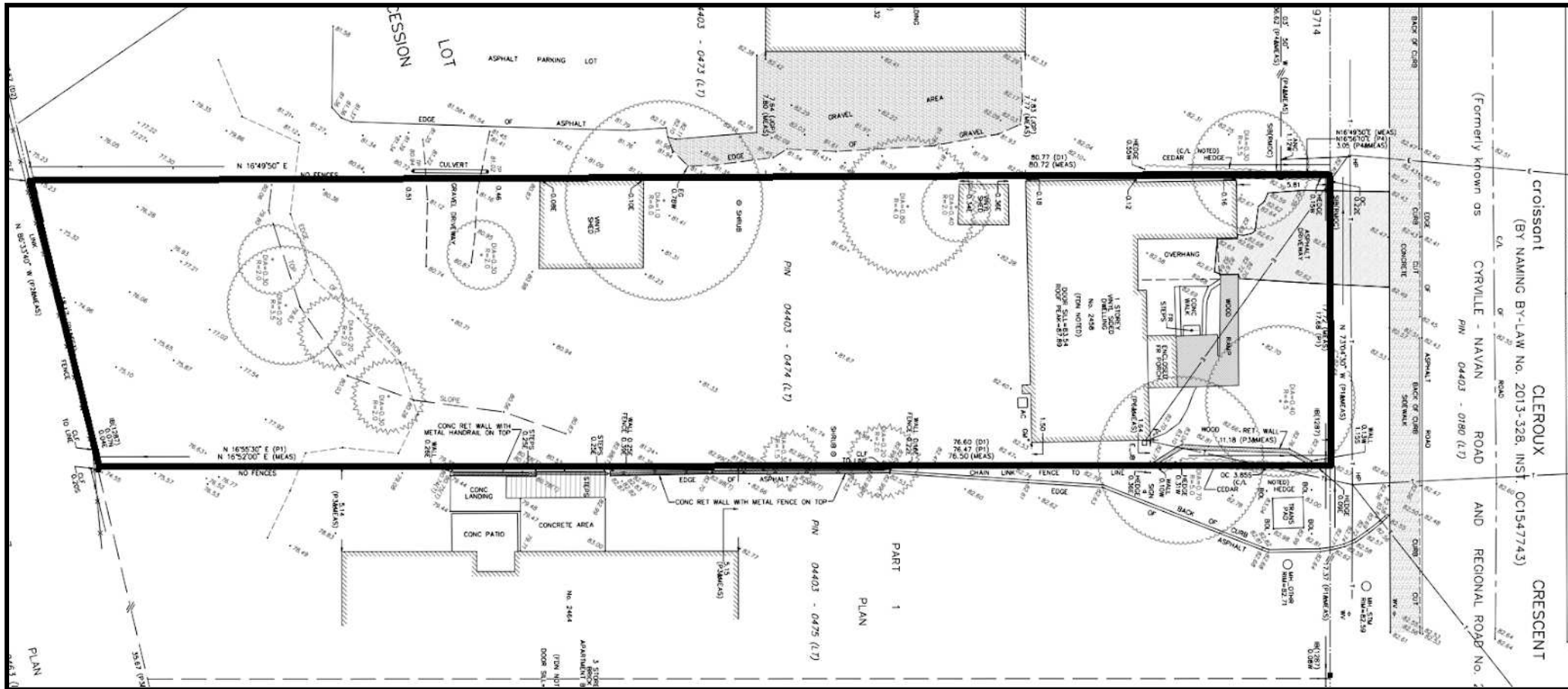
2. East Elevation



3. Site Plan



4. Survey Plan



5. Landscape Plan

