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Restoration

2510 St. Laurent Boulevard Ottawa, Ontario

Planning Rationale and Design Brief



Prepared for: Claridge Homes

**2510 ST. LAURENT BOULEVARD
OTTAWA, ONTARIO**

**PLANNING RATIONALE
IN SUPPORT OF APPLICATIONS FOR
SITE PLAN CONTROL,
ZONING BY-LAW AMENDMENT,
AND PLAN OF CONDOMINIUM**

Prepared for:

Claridge Homes



Prepared By:

NOVATECH

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November 2022

Novatech File: 122040

Ref: R-2022-163

November 1, 2022

City of Ottawa
Planning, Real Estate & Economic Development Department
110 Laurier Avenue West, 4th Floor
Ottawa, Ontario
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Attention: Kelby Lodoen Unseth, Planner II

Reference: 2510 St. Laurent Boulevard
Planning Rationale and Design Brief in support of applications for Site Plan Control, Zoning
by-law Amendment, and Plan of Condominium
Our File No.: 122040

Novatech is pleased to submit this Planning Rationale and Design Brief on behalf of Claridge Homes in support of Site Plan Control, Zoning By-law Amendment and Plan of Condominium applications for the properties municipally known as 1900 and 1920 Walkley Road, 2990 and 3000 Conroy Road, 2425 Don Reid Drive, and 2502 and 2510 St. Laurent Boulevard in the City of Ottawa, herein referred to as “the Subject Site”.

Claridge Homes is proposing to construct a planned-unit development consisting of 192 back-to-back townhomes and 36 townhomes on private streets. A portion of the Subject Site will be dedicated as a public park. The proposed development will be accessed from Don Reid Drive and St. Laurent Boulevard.

The northern portion of 1900 Walkley Road is excluded from the proposed Site Plan but is intended for the future development of a retirement home and residential care facility and will be subject to a separate Site Plan Control application. This area is included in the proposed Zoning By-law Amendment application. The Subject Site is currently zoned “General Mixed Use, Urban Exception 1327” (GM [1327]). A Zoning By-law Amendment application is required to facilitate the proposed development.

This Planning Rationale demonstrates how the proposed development is consistent with the *Provincial Policy Statement (PPS)*, conforms to the *City of Ottawa Official Plan*, and complies with the general provisions of *Zoning By-law 2008-250*.

Should you have any questions or comments, please do not hesitate to contact me.

Sincerely,

NOVATECH

Ellen Potts
Planner

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1.0 INTRODUCTION

Novatech was retained by Claridge Homes (“Claridge”) to prepare this Planning Rationale in support of Site Plan Control, Zoning By-law Amendment and Plan of Condominium applications for the Subject Site, which consists of 1900 and 1920 Walkley Road, 2425 Don Reid Drive, 2990 and 3000 Conroy Road, and 2502 and 2510 St. Laurent Boulevard in the City of Ottawa. This report uses the municipal address “2510 St. Laurent Boulevard” to refer to the whole Subject Site.

Claridge proposes to construct a planned-unit development consisting of 192 back-to-back townhomes and 36 townhomes on private streets. A portion of the Subject Site will be dedicated as a public park. The proposed development will be accessed from Don Reid Drive and St. Laurent Boulevard.

This Planning Rationale describes the Subject Site and surrounding context, details the proposed development and zoning by-law amendment, and demonstrates that it is consistent with the Provincial Policy Statement (PPS), conforms to the City of Ottawa Official Plan, and complies with the City of Ottawa Zoning By-law 2008-250.

1.1 Description of Subject Site

The Subject Site is in Ward 10 (Gloucester-Southgate) of the City of Ottawa and is bound by Walkley Road to the north, Conroy Road to the east, St. Laurent Boulevard to the south, and Don Reid Drive to the west, as shown in **Figure 1**. The Subject Site is comprised of a number of existing legal plans, but is generally legally described as Part of Lots A and I, Concession 4 (Rideau Front), Geographic Township of Gloucester.

The Subject Site is irregularly shaped comprising of seven parcels under Claridge’s ownership with a total area of 5.27 hectares and frontages of approximately 62.22 metres on Walkley Road, 115.45 metres on Conroy Road, 105.15 metres on St. Laurent Boulevard, and 129.73 metres on Don Reid Drive.



Figure 1: Subject Site (image base: GeoOttawa, 2021)

The Subject Site is currently undeveloped and largely vegetated. It was previously developed with commercial and office buildings that were demolished circa 2006/2007. Images of the Subject Site are provided in **Figures 2 to 5** below.

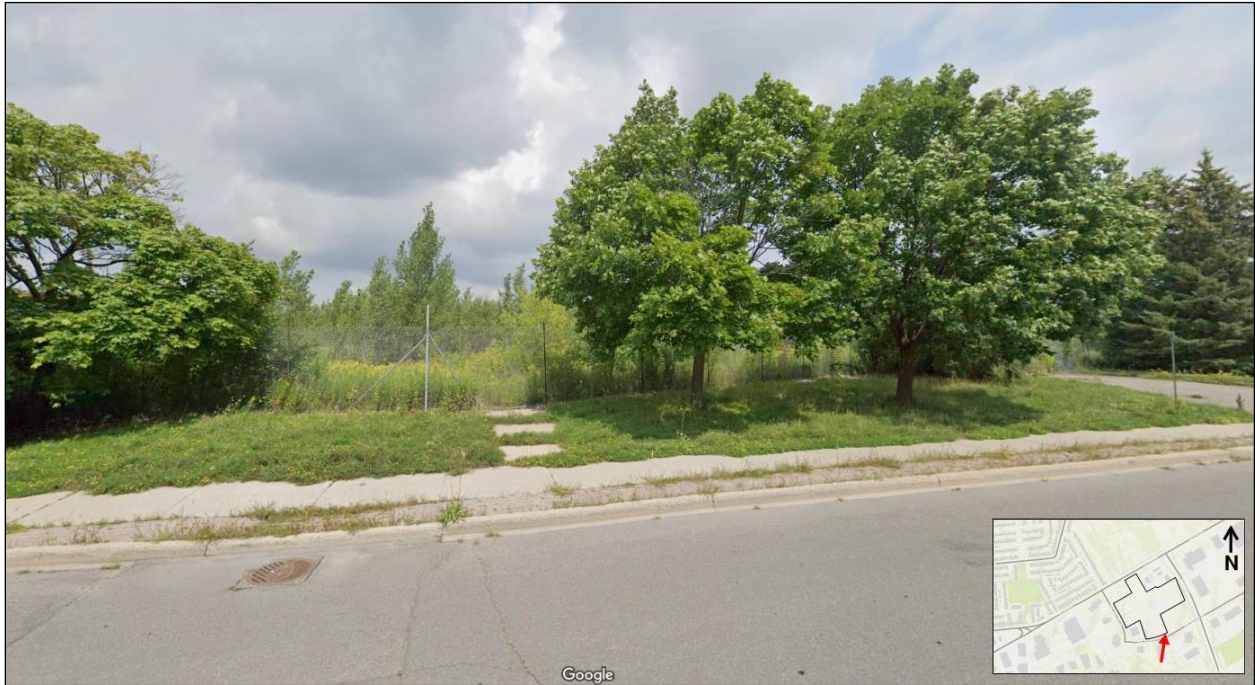


Figure 2: Subject Site looking north from St. Laurent Boulevard



Figure 3: Subject Site looking west from Conroy Road



Figure 4: Subject Site looking northeast from Don Reid Drive



Figure 5: Subject Site looking south from Walkley Road

1.2 Site Location and Community Context

The Subject Site is situated between established residential neighbourhoods on the north side of Walkley Road and a large employment and industrial area on the south side of Walkley Road. Further detail is provided in **Figures 6 to 12** and the description of surrounding land uses below.

North:

A large vacant City-owned parcel is located across from the Subject Site on the north side of Walkley Road with the existing low-density residential neighbourhoods of Guildwood Estates and Elmvale-Canterbury abutting it to the west and east, respectively. A National Capital Commission (NCC) multi-use pathway (MUP) runs north through this City parcel from Walkley Road to the Kilborn Community Gardens and further north to Lynda Lane Park and Smyth Road.

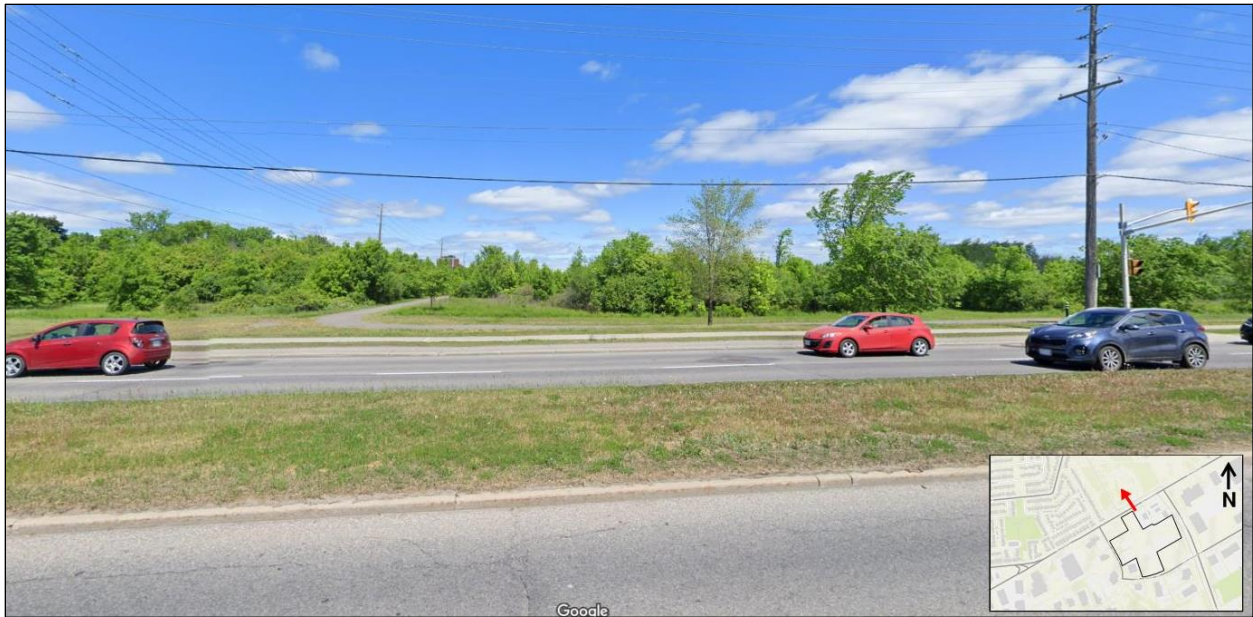


Figure 6: Looking north-west across Walkley Road at the multi-use pathway

East:

A commercial plaza located at the southwest corner of Conroy Road and Walkley Road abuts the northeast portion of the Subject Site. The plaza contains a Tim Horton's/Wendy's building, an Esso gas station and car wash with a Circle K convenience store, and a Boston Pizza restaurant.

A two-storey office building development, known as "The Conroy", is developing at the northwest corner of Conroy Road and St. Laurent Boulevard and abuts the southeast portion of the Subject Site.

The property immediately east of Conroy Road (2935 Conroy Road and 2020 Walkley Road) is currently subject to a *Zoning By-law Amendment* application (D02-02-21-0021) and a *Site Plan Control* application (D07-12-21-0036) for the phased development of three new warehouse buildings. The existing buildings, which contain a range of uses including offices, a research and development centre, a community health and resource centre, and a recreational and athletic centre, are proposed to be demolished.



Figure 7: Looking north at the adjacent commercial plaza

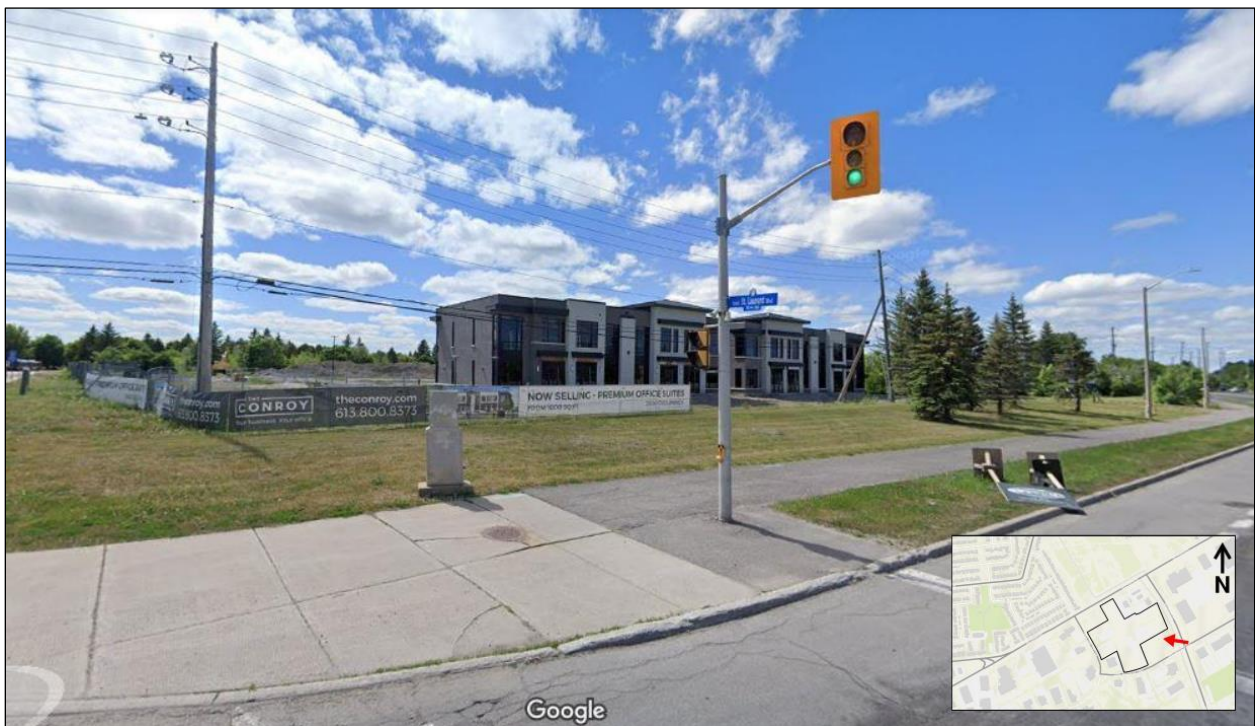


Figure 8: Looking northwest at the developing office plaza ("The Conroy")



Figure 9: Looking east across Conroy Road at the proposed location of three new warehouse buildings

South:

A two-storey office building at 2520 St. Laurent Boulevard abuts the southwest portion of the Subject Site. One- and two-storey office buildings are located south of St. Laurent Boulevard as part of the Ottawa Business Park. The Ottawa Business Park includes a greenspace corridor and soccer fields located along the hydro transmission corridor within 400 metres of the Subject Site.

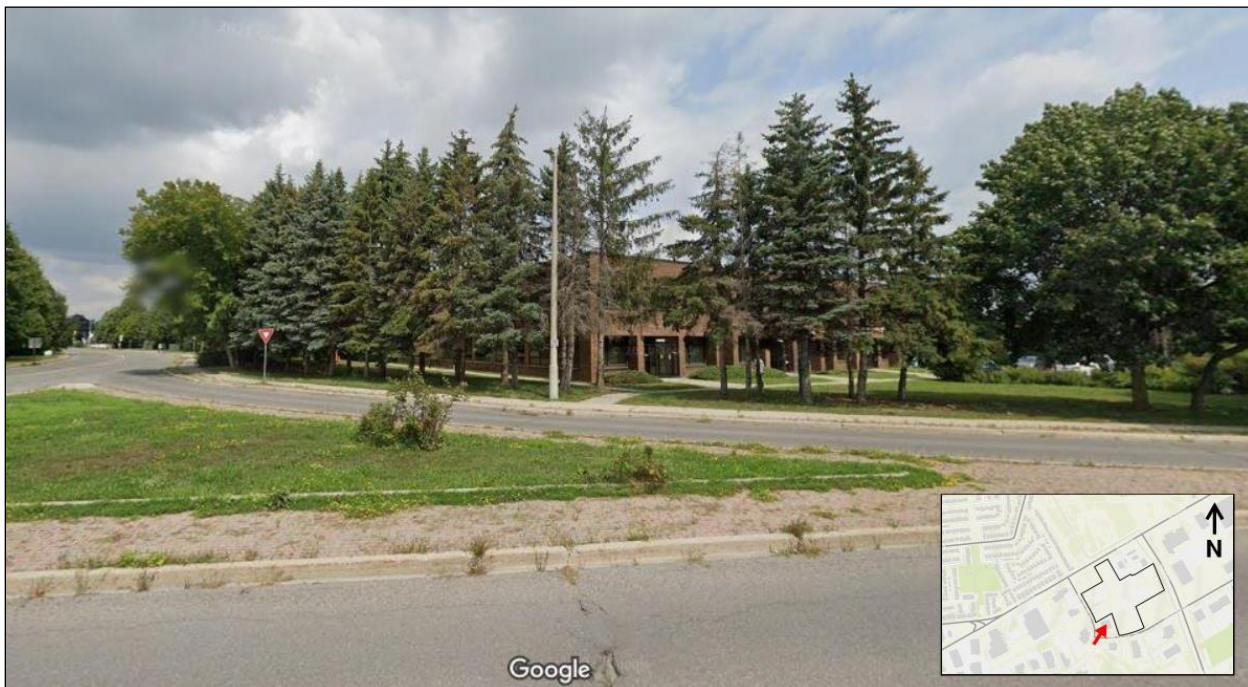


Figure 10: Looking northeast at the existing office building at 2520 St. Laurent Boulevard



Figure 11: Looking south at the existing office building at 2515 St. Laurent Boulevard

West:

The remnant portion of 1900 Walkley Road, abutting the northwest portion of the proposed development, is included in the proposed Zoning By-law Amendment application but will be subject to a separate future Site Plan Control application for a retirement home and residential care facility. Immediately west of Don Reid Drive from the Subject Site is the Cornerstone EarlyON Child and Family Centre and the Heatherington Nursery School, as well as a Swish Clean-it Centre building. Office and warehouse uses continue to the west and southwest of the Subject Site.



Figure 12: Looking west at the entrances to the Swish Clean-it Centre building and the EarlyON Child and Family Centre

1.2.1 Transportation Framework and Linkages

Walkley Road and Conroy Road are designated as arterial roads and Don Reid Drive and St. Laurent Boulevard are designated as collector roads per “Schedule C4 – Urban Road Network” of the new Official Plan, as shown in **Figure 13**.



Figure 13: Excerpt of new Official Plan Schedule C4

Within proximity of the Subject Site, concrete sidewalks are located along both sides of Walkley Road, along the east side of Don Reid Drive, along the north side of St. Laurent Boulevard, and along the east side of Conroy Road. An asphalt multi-use pathway (MUP) is also located on the west side of Conroy Road. This MUP is designated as a Major Pathway on “Schedule C3 – Active Transportation Network” in the new Official Plan (see **Figure 14**) and continues along the north side of Walkley Road and through the City-owned open space as described above. Bike lanes are also provided in both directions on Conroy Road.

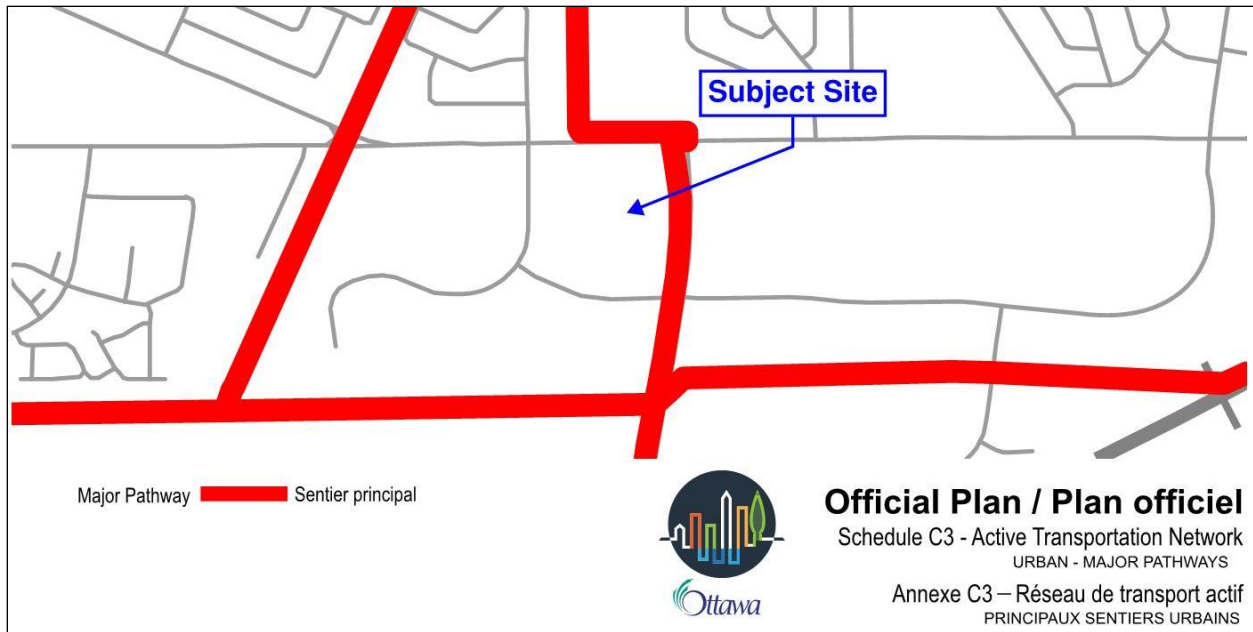


Figure 14: Excerpt of new Official Plan Schedule C3

The Subject Site is well served by transit routes. Conroy Road and Walkley Road are both part of the Rapid Transit Network with Conroy Road designated as a “Transit Priority Route” and Walkley Road designated as Transitway – At-Grade Crossings per “Schedule C2 – Transit Network (Ultimate)” in the new Official Plan (see Figure 15). Please refer to the *Transportation Impact Assessment*, prepared by Novatech, dated October 31, 2022 for detailed information about OC Transpo transit stops and routes in proximity to Subject Site.

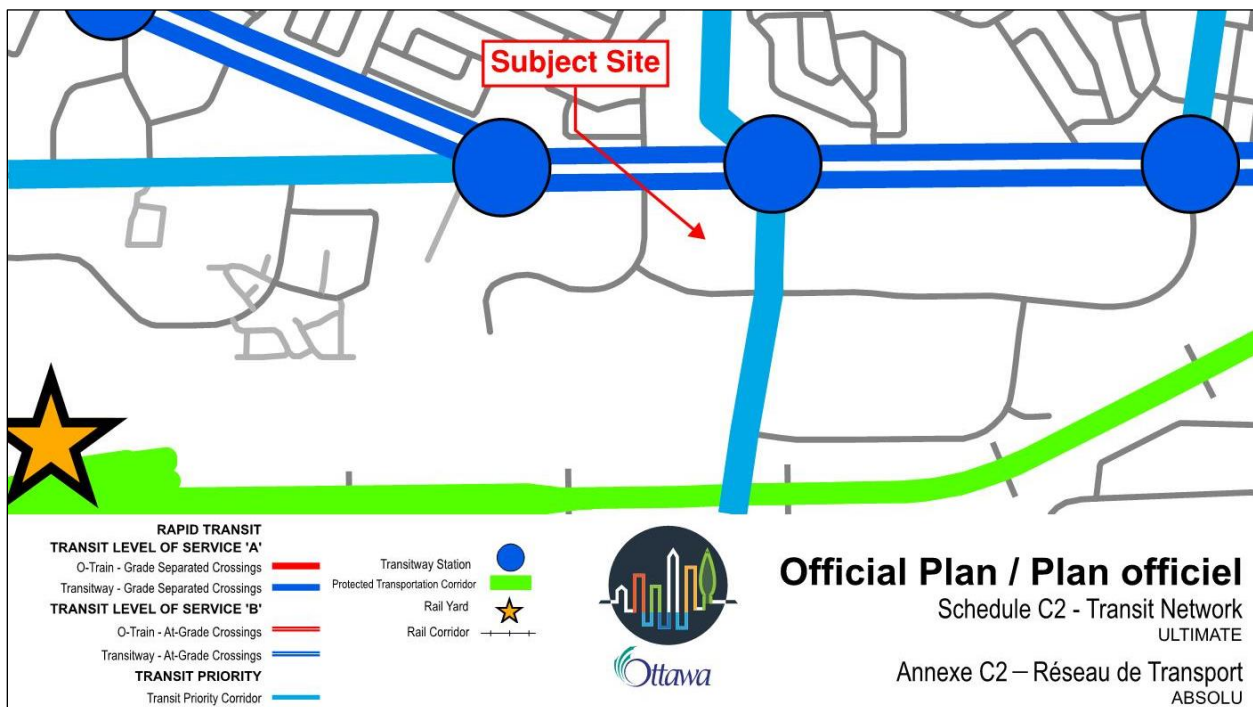


Figure 15: Excerpt of new Official Plan Schedule C2

2.0 PROPOSED DEVELOPMENT

The proposed planned-unit development consists of 192 back-to-back townhomes and 36 standard townhomes, as well as a park block that will be dedicated to the City of Ottawa, as shown in **Figure 16**. Please see **Appendix A** for a reduced copy of the proposed Site Plan. Access to the dwellings will be provided by a network of private streets with two connections to Don Reid Drive and two connections to St. Laurent Boulevard. The proposed development will be municipally serviced, as detailed in the Servicing and Stormwater Management Report prepared by Novatech, dated November 2022.

Each dwelling unit has its own garage and driveway. Additional parking is provided along the east side of Street 5 as parallel parking, along the east side of Street 6 as perpendicular parking, and along the south side of Street 3 adjacent to the proposed park and as parallel parking. The narrow private streets combined with the proposed townhouse dwellings allow for the intensification of the Subject Site in accordance with the policies of the City’s Official Plan but are not conducive to on-street parking everywhere. These proposed designated areas for on-street parking allow for orderly and convenient locations for visitors to park without causing overflow parking outside of the development.

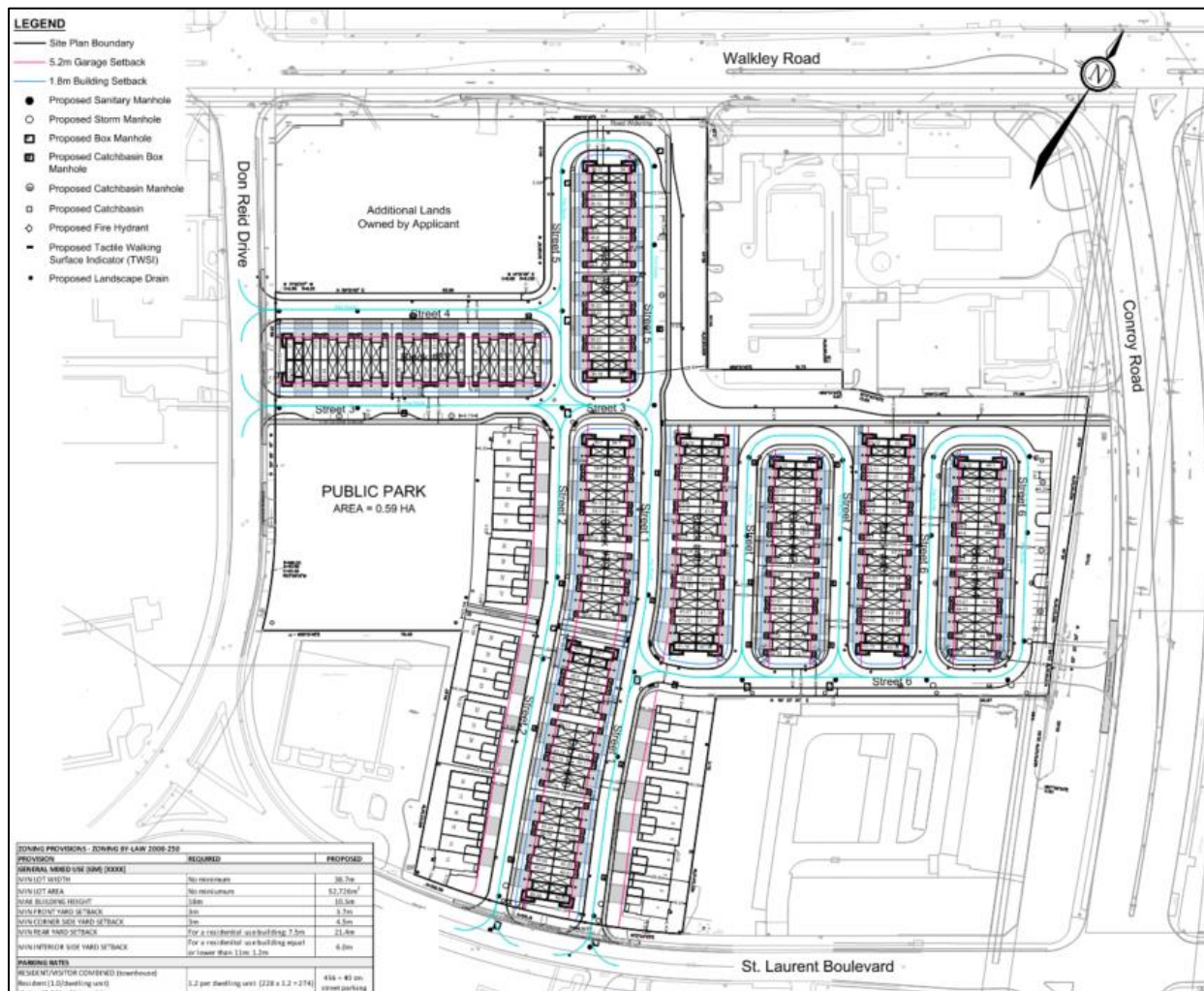


Figure 16: Excerpt of proposed Site Plan

Sidewalks are proposed along the east side of Street 1 and Street 5 to provide a north-south connection through the development between Walkley Road and St. Laurent Boulevard, and along the south side of Street 3 and north side of Streets 6 and 7 to provide an east-west connection through the development between Conroy Road and Don Reid Drive, as shown in **Figure 17**. Mid-block pathway connections are provided between back-to-back townhouse Blocks 39 and 40 and townhouse lots 26 and 27 to provide a direct connection to the proposed park and improve pedestrian circulation through the development. Please see the *Pedestrian Circulation Plan* in **Appendix B** of this report for an illustration of the proposed and existing pedestrian network.

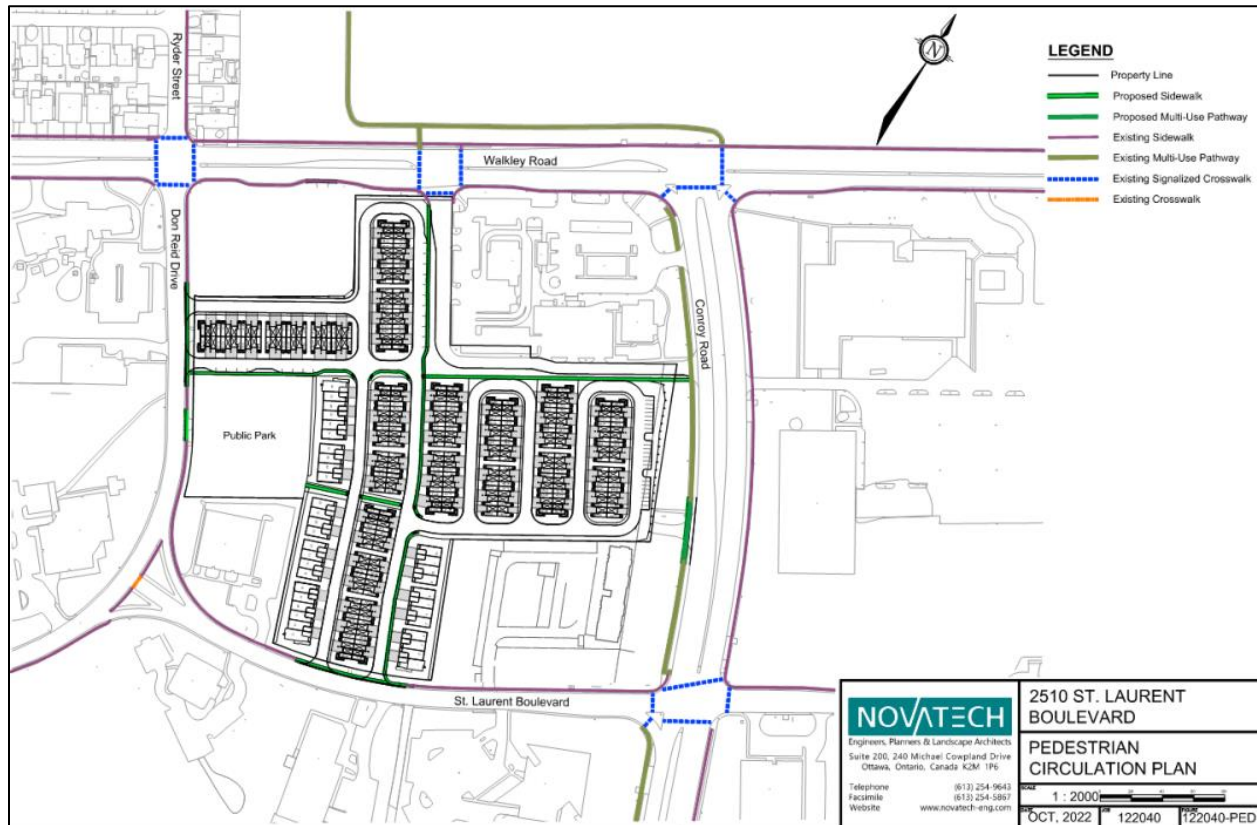


Figure 17: Excerpt of Pedestrian Circulation Plan

The proposed public park is 0.59 hectares in area and has approximately 79 metres of frontage along Don Reid Drive and 77 metres of frontage along Street 3. The location of the park avoids frontage on busy arterial roads while providing two street frontages along Don Reid Drive (a public street) and proposed Street 3 as shown on the Site Plan.

The proposed development will be subject to a condominium to share in the cost and maintenance of common elements. Details of the condominium will be determined through the development review process and a Draft Plan of Condominium will be submitted separately.

3.0 PROPOSED ZONING BY-LAW AMENDMENT

Although not part of this Site Plan Control or Plan of Condominium applications, the abutting lands owned by Claridge at the southeast corner of Walkley Road and Don Reid Drive (labelled as “Additional lands owned by Applicant” on the Site Plan) are included in this Zoning By-law Amendment application and are intended for the future development of a retirement home and residential care facility.

The Subject Site is currently zoned “General Mixed Use, Urban Exception 1327” (GM[1327]). Exception 1327 applies additional provisions to the GM zone specific to the large format retail use that was formerly planned for the Subject Site. These provisions are no longer applicable to the proposed residential development.

The parent GM zone permits the proposed uses for Claridge’s lands being “Planned Unit Development”, “Townhouse Dwelling”, “Retirement Home”, and “Residential Care Facility”.

The proposed development for the Subject Site has largely been designed to comply with the provisions of the GM zone. A new Urban Exception “GM[XXXX]” is proposed to remove GM provisions:

- Remove provision 187(3)(g), which requires a Maximum Floor Space Index (FSI) of 2, *unless otherwise shown*
- Remove provision 187(h)(i), (ii), and (iii), which requires a *minimum width of landscape area (i) abutting a street: 3m; (ii) abutting a residential or institutional zone: 3m; (iii) all other cases: no minimum.*

It is also proposed to rezone the lands intended for the public park from GM[1327] to Parks and Open Space (O1).

4.0 PLANNING POLICY AND REGULATORY FRAMEWORK

4.1 Provincial Policy Statement (2020)

The Provincial Policy Statement, 2020 (PPS) was issued under Section 3 of the Planning Act and came into effect on May 1, 2020. The PPS provides policy direction on matters of provincial interest and sets the foundation for regulating the development and use of all land. All decisions affecting planning matters must “be consistent with” policies of the PPS. The following demonstrates that the proposed development is consistent with applicable policies of the PPS.

Building Strong Healthy Communities

Policy 1.1.1 states that:

“Healthy, liveable and safe communities are sustained by:

- a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*
- b) accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*

- c) *avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
- d) *avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;*
- e) *promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;*
- f) *improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;*
- g) *ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs;*
- h) *promoting development and land use patterns that conserve biodiversity; and*
- i) *preparing for the regional and local impacts of a changing climate.”*

The proposed development represents an efficient use of a vacant urban site that supports the provision of a range and mix of residential dwelling types within the community. The proposed development is in proximity to a range of recreational, commercial, employment, and institutional uses which supports the use of the surrounding active and public transportation systems by future residents of the development and reduces the need to rely on personal vehicles.

Policy 1.5.1(b) states that:

“Healthy, active communities should be promoted by:

- b) *planning and providing for a full range and equitable distribution of publicly accessible built and natural settings for recreation, including facilities, parklands, public spaces, open space areas, trails and linkages, and, where practical, water-based resources”*

The proposed development contributes to the equitable distribution of publicly accessible recreational spaces in the community by providing a public park, as shown on the Site Plan.

Wise Use and Management of Resources

Section 2.0 of the PPS provides policies for the protection and management of natural heritage, water, agriculture, mineral aggregate, petroleum, and cultural heritage and archaeological resources for their economic, environmental, and social benefits. These resource interests have been reviewed in the context of the proposed development, as summarized below:

- Section 2.1 – Natural Heritage: There are no natural heritage features within or adjacent to the Subject Site.
- Section 2.2 – Water: There are no water features within or adjacent to the Subject Site.
- Section 2.3 – Agriculture: The Subject Site is located within the urban boundary and will not impact prime agricultural areas.
- Section 2.4 – Minerals and Petroleum: The proposed development has no impact on the supply of mineral and petroleum resources.

- Section 2.5 – Mineral Aggregate Resources: The proposed development has no impact on the supply of mineral aggregate resources.
- Section 2.6 – Cultural Heritage and Archaeology: The Subject Site is not identified as having cultural heritage or archaeological potential.

Protecting Public Health and Safety

Section 3.0 of the PPS states that development shall be directed away from areas of natural or human-made hazards where there is an unacceptable risk to public health or safety, or of property damage, and not create new or aggravate existing hazards.

Regarding Section 3.1 – Natural Hazards, a Geotechnical Investigation report was prepared by Paterson Group (dated April 7, 2022, Report No. PG6149-1) to determine the subsurface conditions of the Subject Site and provide geotechnical recommendations for the design and construction of the proposed development. The report finds that the Subject Site is suitable for the proposed residential development.

Regarding Section 3.2 – Human-Made Hazards, Phase I and II Environmental Site Assessments (ESA) were prepared by Paterson Group to identify any environmental concerns with the potential to impact the Subject Site:

- The Phase I ESA (dated April 4, 2022, Report No. PE5607-1) identified nine potentially contaminating activities (PCA) within the Subject Site resulting in seven areas of potential environmental concern (APEC). Based on these findings, the Phase I ESA recommended that a Phase II ESA be completed.
- The Phase II ESA (dated May 3, 2022, Report No. PE5607-2) assessed the seven APECs by obtaining various soil and groundwater samples and determined that all the samples were in compliance with the Ministry of Environmental Conservation and Parks (MECP) Table 3 Standards. As such, the Phase II ESA concluded that no further environmental investigation for the Subject Site was required.

The proposed development is consistent with the policies of the Provincial Policy Statement (2020).

4.2 City of Ottawa Official Plan

The City of Ottawa’s new Official Plan was adopted by City Council on November 24, 2021. During the writing of this report, the new Official Plan had not yet received approval from Ontario’s Ministry of Municipal Affairs and Housing. As such, this report will address the existing Official Plan (2003, as amended) and new Official Plan (2021, Council Adopted).

4.2.1 City of Ottawa Official Plan (2003, as amended)

Per *Schedule B – Urban Policy Plan*, the Subject Site is designated as “General Urban Area”, as shown in **Figure 18**.

Section 2.1 of the Official Plan speaks to the continued growth of the City and how it will be managed, emphasizing that growth will be distributed throughout the urban areas where services are available and

where it can be accommodated as compact and mixed-use development. Section 2.2 of the Official Plan states that the General Urban Area will continue to mature and evolve through intensification and infill at a scale that considers proximity to major roads and transit, the planned function of an area, and compatibility with the surrounding community.

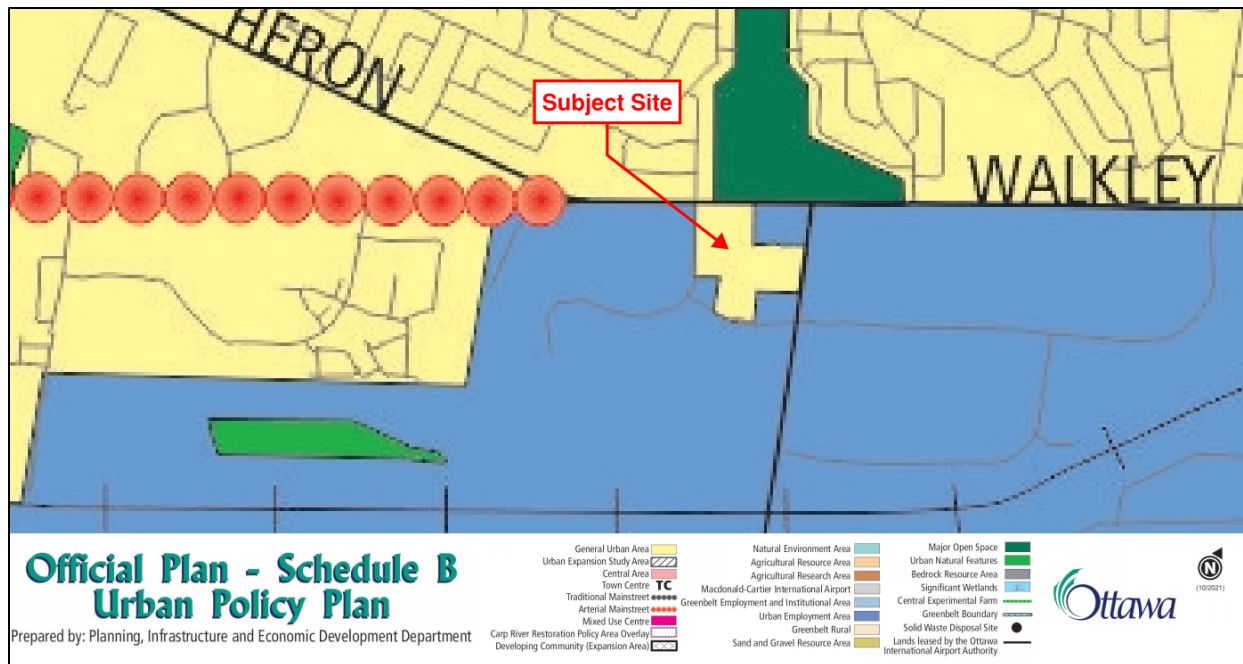


Figure 18: Excerpt of current Official Plan - Schedule B Urban Policy Plan

Policies for the General Urban Area are provided in Section 3.6.1 of the Official Plan. The General Urban Area permits a full range and choice of housing types in combination with a variety of uses including employment, retail, service, cultural, leisure, entertainment, and institutional uses to facilitate the development of complete and sustainable communities.

Policy 2 of Section 3.6.1 states that *“The evaluation of development applications, studies, other plans and public works undertaken by the City in the General Urban Area will be in accordance with Section 2.5.1 and Section 4.11”*. Please see Section 4.2.3 of this report for a discussion of how the proposed development is in accordance with the applicable policies of Sections 2.5.1 and 4.11 of the Official Plan.

Policy 3 of Section 3.6.1 states that *“Building height in the General Urban Area will continue to be predominantly Low-Rise. Within this range, changes in building form, height and density will be evaluated based upon compatibility with the existing context and the planned function of the area. Secondary plans or zoning that currently permit building heights greater than four Storeys will remain in effect.”* The proposed dwellings are predominantly low-rise (i.e. under four storeys) and comply with the height limit of 18 metres required by the existing General Mixed Use zone. The details of the future retirement home and residential care facility, as mentioned above, will be determined through the subsequent Site Plan Control application.

Policy 5 of Section 3.6.1 states that:

“The City supports intensification in the General Urban Area where it will complement the existing pattern and scale of development and planned function of the area. The predominant form of development and

intensification will be semi-detached and other ground-oriented multiple unit housing. When considering a proposal for residential intensification through infill or redevelopment in the General Urban Area, the City will:

- a) Assess the compatibility of new development as it relates to existing community character so that it enhances and builds upon desirable established patterns of built form and open spaces;
- b) Consider its contribution to the maintenance and achievement of a balance of housing types and tenures to provide a full range of housing for a variety of demographic profiles throughout the General Urban Area; [Amendment #150, LPAT July 18, 2019]”

The proposed ground-oriented townhouse and back-to-back townhouse dwellings implement the intended form of development for intensification in the General urban Area and contribute to the range of housing types and tenures available in the community, and the proposed park adds to the availability of open space for existing and future residents of the area.

4.2.2 City of Ottawa Official Plan (2021, Council adopted)

The Subject Site is in the “Outer Urban Transect Policy Area” per Schedule A – Transect Policy Areas, as shown in **Figure 19**. Per Schedule B3 – Outer Urban Neighbourhood, as shown in **Figure 20**, Walkley Road abutting the Subject Site is designated as a “Minor Corridor”. The Minor Corridor designation applies to a depth of 120 metres from the centreline of the street identified as a Minor Corridor (Policy 6.2.1(1)(a)(ii)). The remainder of the Subject Site is designated as “Neighbourhood” and is subject to the “Evolving Neighbourhood” overlay.



Figure 19: Excerpt of new Official Plan Schedule A - Transect Policy Area

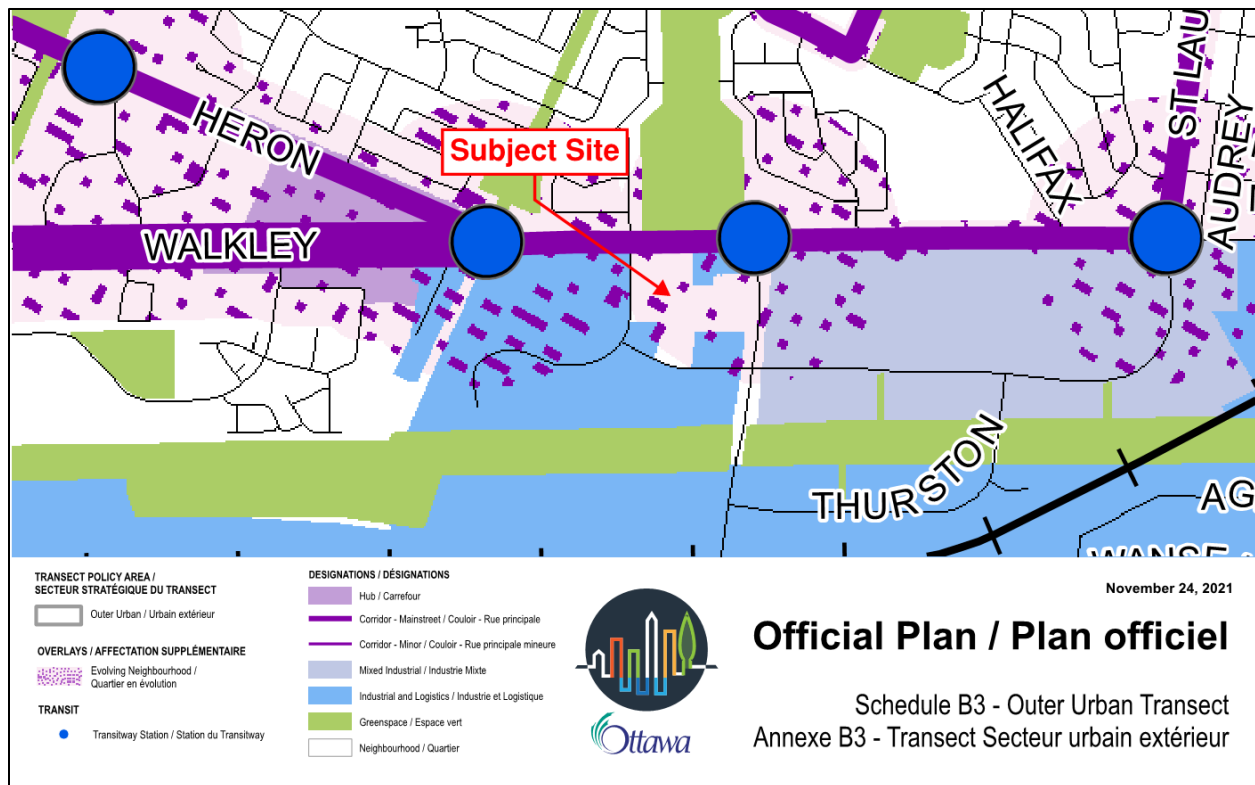


Figure 20: Excerpt of new Official Plan Schedule B3 - Outer Urban Transect

Outer Urban Policy Area

The direction in the new Official Plan for the Outer Urban Transect is that it will undergo a gradual evolution from a classic suburban model towards an urban “15-minute” neighbourhood model. The policy intent for 15-minute neighbourhoods is that they are complete, well-connected places with a diverse mix of land uses, including a range of housing types and affordability, that support active transportation and transit and reduce car dependency (Policy 2.2.4.1). Table 8 in the Official Plan lists the suburban built form and land-use characteristics of the 15-minute neighbourhood. The following table lists these characteristics with a corresponding description of how the proposed development aligns with the 15-minute model.

Table 1: 15 Minute Neighbourhood Characteristics per new Official Plan, Table 8

15-Minute Neighbourhood Characteristics	Proposed Development
<i>Nature, buildings and infrastructure are harmonized with a continued emphasis on park amenity as part of an integrated urban fabric.</i>	A public park is integrated with the existing and proposed street network and the proposed residential dwellings.
<i>Smaller proportion of detached housing. Replaced with higher-density ground-oriented housing, with some mid- and high-rise buildings within transit hubs.</i>	The proposed development consists of ground-oriented multi-unit dwellings including back-to-back and standard townhomes.
<i>Highly integrated commercial, civic and institutional uses with residential areas creating highly connected 15-minute neighbourhoods.</i>	The Subject Site is near a range of employment, commercial, institutional, and recreational uses. The addition of residential dwellings on this site

	supports the creation of a highly connected 15-minute neighbourhood.
<i>Land-use patterns that focus on transit and connectivity, and a built environment that prioritizes the safety and convenience of active transportation.</i>	The Subject Site is bordered by existing sidewalks on Walkley Road, St. Laurent Boulevard, and Don Reid Drive, and a multi-use pathway and bike lanes on Conroy Road. Several existing transit stops and routes are located along these streets. The proposed development includes sidewalks that provide north-south and east-west connectivity to the existing surrounding pedestrian, cycling and transit facilities.

Section 5.3 of the new Official Plan contains policy direction for development in the Outer Urban Transect. Development within Neighbourhoods and along Minor Corridors is to be low-rise, meaning up to four storeys (Policy 5.3.1.2). Along Minor Corridors, buildings are to be a minimum of two storeys (Policy 5.3.3.4). The Zoning By-law will permit a range of dwelling unit sizes with multi-unit dwellings along Corridors and predominantly ground-oriented forms in Neighbourhoods with low-rise multi-unit dwellings permitted near rapid transit and frequent transit routes (Policy 5.3.1.4).

Within Neighbourhoods in the Outer Urban Area, the Zoning By-law will implement development standards that transition away from a suburban model of built form towards an urban built form that (Policy 5.3.4.1):

- a) *Allows and supports a wide variety of housing types with a focus on lower density missing-middle housing which generally reflects the existing built form context of the neighbourhood, which may include new housing types that are currently not contemplated in this Plan;*
- b) *The application, as appropriate, of Zoning By-law development standards to be applied as one lot for zoning purposes to support missing middle housing;*
- c) *Generally provides for up to 3 storeys height permission, and where appropriate 4 storeys height permission to allow for ground oriented higher-density Low-rise residential development;*
- d) *Provides an emphasis on regulating the maximum built form envelope that frames the public right of way; and*
- e) *In appropriate locations, to support the production of missing middle housing, prohibit lower-density typologies.*

The new Official Plan defines **Missing Middle Housing** as:

“In Ottawa's context and for the purposes of this Plan, missing middle housing generally refers to low-rise, multiple unit infill residential development of between three and sixteen units, or more in the case of unusually large lots and for the lower-density types is typically ground oriented”.

The proposed development supports the 15-minute neighbourhood policy objectives of the Outer Urban Transect by introducing low-rise multi-unit dwellings to an area that is well-served by transit and active transportation networks and is in proximity to a full range of commercial, institutional, and employment uses.

Minor Corridor Policies

Section 6.2 of the Official Plan contains policies for this designation. Per policy 1(a)(ii) of Section 6.2.1, the Minor Corridor designation applies to a maximum depth of 120 metres from the centreline of the street

identified as a Minor Corridor. Generally, back-to-back townhouse Blocks 37 and 38 on the Site Plan are within the Minor Corridor designation area.

Per Policy 2 of Section 6.2.1:

“Development within the Corridor designation shall establish buildings that locate the maximum permitted building heights and highest densities close to the Corridor, subject to building setbacks where appropriate. Further, development:

- a) Shall ensure appropriate transitions in height, use of land, site design and development character through the site, to where the Corridor designation meets abutting designations;*
- b) May be required to provide public mid-block pedestrian connections to nearby streets or abutting designations;*
- c) For sites generally of greater than one hectare in area or 100 metres in depth:
 - i. Shall be required to establish an enhanced circulation network throughout the site that prioritizes the needs of pedestrians, cyclists and transit users; and*
 - ii. Where development is proposed to occur in phases, may be required to build phases closest to the Corridor before phases located at the back of the site, subject to any overlay that may apply; and**
- d) Shall be prohibited from including functions or uses causing or likely to cause nuisance due to noise, odour, dust, fumes, vibration, radiation, glare or high levels of heavy truck traffic.”*

The proposed development locates the densest built forms, being the reserved land for the future retirement home and residential care facility (not subject to this Site Plan Control application) and the back-to-back towns, within the limits of the Minor Corridor designation and locates the standard townhomes towards the south end of the Subject Site furthest from the Minor Corridor. Further, the proposed residential uses will not cause nuisance due to noise, odour, dust, fumes, vibration, radiation, glare or high levels of heavy truck traffic.

Per Policy 4 of Section 6.2.1:

“Unless otherwise indicated in an approved secondary plan, the following applies to development of lands with frontage on both a Corridor and a parallel street or side street:

- a) Development shall address the Corridor as directed by the general policies governing Mainstreet Corridors Minor Corridors, particularly where large parcels or consolidations of multiple smaller parcels are to be redeveloped; and*
- b) Vehicular access shall generally be provided from the parallel street or side street.”*

The future retirement and residential home, which will be subject to a separate Site Plan Control application, has frontage on Walkley Road and will be designed to address the Corridor in accordance with the applicable policies. Vehicular access to the proposed development is provided from Don Reid Drive and St. Laurent Boulevard.

Per Policy 2 of Section 6.2.2:

“In the Minor Corridor designation, this Plan shall permit a mix of uses which support residential uses and the evolution of a neighbourhood towards 15-minute neighbourhoods. Development may:

- a) Include residential-only and commercial-only buildings;*
- b) Include buildings with an internal mix of uses, but which remain predominantly residential;*
- c) Include limited commercial uses which are meant to mainly serve local markets; or*
- d) Be required, where contextually appropriate, to provide commercial or service uses on the ground floor.”*

The proposed development consists of residential dwellings which provide additional housing choice to the existing low-density residential neighbourhoods on the north side of Walkley Road and integrate with the largely commercial and employment uses on the south side of Walkley Road to support the evolution of the area towards a 15-minute neighbourhood.

Neighbourhood and Evolving Overlay Policies

The remaining portion of the Subject Site beyond the Minor Corridor buffer is designated as “Neighbourhood”. Section 6.3 of the Official Plan contains policies for this designation.

A variety of residential built forms are permitted within the Neighbourhood designation, including a range of low-rise housing options with the predominant new building form being Missing Middle Housing (Policy 6.3.1.4). Higher densities and heights will be permitted near rapid transit stations, Corridors, and major neighbourhood amenities with a gradation to lower densities and predominantly ground-oriented dwelling forms (Policy 6.3.1.5).

The proposed back-to-back townhouse and standard townhouse dwellings are appropriate for the Neighbourhood designation, providing low-rise, multi-unit, ground-oriented built forms that increase the range of available housing options in the area while increasing potential ridership for the nearby transit stations.

Policy 3 of subsection 6.3.2 states that:

“form-based regulation will provide for built form and site development characteristics that are:

- b) In those parts of the Inner Urban, Outer Urban and Suburban Transects covered by the Evolving Overlay where substantial increases of density are planned and where building form and massing is anticipated to change significantly from existing context, urban as described in Table 6”.*

As shown in **Figure 18** above, the Subject Site is covered by the “Evolving Overlay”. Section 5.6 of the new Official Plan contains policies for Overlays. The Evolving Overlay identifies land intended for a gradual change in character from suburban to urban to support intensification and allow new built forms and diverse uses. Per Policy 6 of subsection 5.6.1.1, *“Zoning By-law development standards and development on lands with an Evolving Overlay should generally include built form and site design attributes that meet most of the urban characteristics described in Table 6 of Section 5, and where suburban attributes are retained, that these do not structurally impede the achievement of a full urban site design over time”.*

Table 6 of the new Official Plan provides a comparison of the general characteristics of urban built form and suburban built form, as listed below:

Table 2: General Characteristics of Urban and Suburban Built Form and Site Design (per new Official Plan, Table 6)

Urban Built Form	Suburban Built Form
<i>Shallow front yard setbacks and in some contexts zero front yards with an emphasis on built-form relationship with the public realm</i>	<i>Moderate front yard setbacks focused on soft landscaping and separation from the right-of-way</i>
<i>Principal entrances at grade with direct relationship to public realm</i>	<i>Principal entrances oriented to the public realm but set back from the street</i>

<i>Range of lot sizes that will include smaller lots, and higher lot coverage and floor area ratios</i>	<i>Larger lots, and lower lot coverage and floor area ratios</i>
<i>Minimum of two functional storeys</i>	<i>Variety of building forms including single storey</i>
<i>Buildings attached or with minimal functional side yard setbacks</i>	<i>Generous spacing between buildings</i>
<i>Small areas of formal landscape that should include space for soft landscape, trees and hard surfacing</i>	<i>Informal and natural landscape that often includes grassed areas</i>
<i>No automobile parking, or limited parking that is concealed from the street and not forming an integral part of a building, such as in a front facing garage</i>	<i>Private automobile parking that may be prominent and visible from the street</i>

The proposed development implements the policy direction of the Evolving Overlay and Neighbourhood policies of the Official Plan by increasing residential density within proximity to rapid transit with low-rise multi-unit dwellings. The development contributes to a change in character from suburban to urban built form by providing a range of smaller lot sizes with higher lot coverage and reduced front yard setbacks, while maintaining the setbacks required by *Zoning by-law 2008-250*. Parking is provided to accommodate two vehicles per dwelling unit (one in the garage and one in the driveway) plus visitor spaces as required by the *Zoning By-law 2008-250*, and sidewalks are provided through the development to provide connections to the surrounding pedestrian and transit networks.

4.2.3 Scoped Design Brief

The existing Official Plan (2003, as amended) states that development applications will be evaluated in the context of *Section 2.5.1 – Designing Ottawa* and *Section 4.11 – Urban Design and Compatibility*. Section 2.5.1 sets out broad design objectives that describe how the City wants to influence the built environment over time, while Section 4.11 outlines the criteria for high-quality urban design and compatibility at a City-wide scale.

Design Objectives

The following lists the design objectives of Section 2.5.1 of the current Official Plan with a description of how the proposed development addresses them.

- 1. To enhance the sense of community by creating and maintaining places with their own distinct identity.*

The proposed development contributes to the sense of community by repurposing a vacant site into residential dwellings in proximity to existing residential neighbourhoods, major public transit routes, employment areas, schools, active transportation networks, and other services. The development also adds to the availability of greenspaces and parks in the area with a proposed public park that is easily accessible from within the proposed development as well as from the surrounding public street network with frontage along Don Reid Drive that is served by an existing sidewalk.

- 2. To define quality public and private spaces through development*

The proposed street-oriented townhouses and back-to-back townhouses frame the proposed private streets and surrounding adjacent public streets. The proposed units will be freehold, with back-to-back units having access to private front yard amenity space and townhouses have access to both front yard and back-yard private amenity spaces. A public park is proposed to serve existing and future residents of the area with street frontage along Don Reid Drive and a proposed private street.

3. To create places that are safe, accessible and are easy to get to, and move through.

The Subject Site is bounded by an existing pedestrian network, with abutting sidewalks along Don Reid Drive, St. Laurent Boulevard, Walkley Road, and a multi-use pathway (MUP) along Conroy Road, as shown on the Pedestrian Circulation Plan in **Appendix B** of this report. The proposed development has two vehicular accesses off Don Reid Drive, and two vehicular accesses off St. Laurent Boulevard leading into a modified grid layout of private streets for easy wayfinding and narrow pavement widths (six to seven metres) to discourage speeding. Sidewalks are proposed along the south side of Street 3 and along the north side of Streets 7 and 6 to provide an east-west pedestrian connection through the development that connects to the sidewalk on Don Reid Drive and the MUP on Conroy Road. Sidewalks are also proposed on the east side of Street 1 and Street 5 to provide a north-south pedestrian connection through the development that connects to the sidewalk on St. Laurent Boulevard and the sidewalk on Walkley Road at the signalized crossing to the MUP on the north side of Walkley Road. Mid-block pathways are also proposed to provide a pedestrian connection between Street 1 and Street 2 to access the southeast corner of the proposed park.

4. To ensure that new development respects the character of existing areas.

The proposed development respects the character of the existing area by redeveloping a large vacant site into a residential development that improves the visual interest and character of the area while appropriately integrating with the existing commercial and employment lands on the south side of Walkley Road and the low-rise residential neighbourhoods on the north side of Walkley Road. Window Streets are proposed along the surrounding arterial streets (Walkley Road and Conroy Road), as well as the existing commercial plaza abutting the northeast corner of the Subject Site and the developing commercial/office lands at the southeast corner of the Subject Site to provide additional setback between uses. The proposed public park provides additional recreational space to the area while providing opportunities for landscaping and tree planting.

5. To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.

The proposed townhouses and back-to-back townhouses increase the availability and choice of housing types in the area. The land reserved for the future development of a retirement home and residential care facility at the northwest corner of the Subject Site abutting Walkley Road will further increase housing options and services and enable residents to age in the community.

6. To understand and respect natural processes and features in development design.

The proposed development has been designed with the understanding of existing conditions, such as stormwater and soil conditions. The Subject Site does not contain natural features, and as such an Environmental Impact Statement was not required. A Tree Conservation Report (TCR) has been prepared by IBI Group, dated June 30, 2022, for the proposed development to assess on-site trees, identify

opportunities for tree retention, and establish a mitigative framework for required tree removals to reduce impacts. Please refer to the TCR for further information.

7. *To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.*

The proposed development represents an efficient use of a vacant property that provides new housing that is integrated with the surrounding active and public transportation work and in proximity to employment, commercial and service uses thereby reducing reliance on personal vehicles. Please see the Landscape Plan, prepared by James B. Lennox & Associates Inc., dated October 2022 for proposed landscaping and tree planting locations.

Urban Design and Compatibility

Several policies within Section 4.11 are not relevant to the Subject Site. This section of the Design Brief has been scoped to reflect the relevant policies. A brief explanation of the policies not reviewed as part of this Brief are provided below:

- Views related to high-rise buildings (Policy 2) and protecting the views of the Parliament Buildings (Policies 3 and 4): high-rise buildings are not proposed as part of this development, and the Subject Site is not located within an area applicable to protecting views of the Parliament Buildings.
- Massing and Scale (Policies 10 to 13): the proposed back-to-back townhouses are low-rise, consistent with the surrounding existing built forms. Appropriate setbacks from existing buildings are provided by proposed private window streets, surrounding public streets, and proposed yard setbacks as required by the Zoning By-law.
- High-Rise Buildings (Policies 14 to 18): high-rise buildings are not proposed as part of this development.
- Impacts to adjacent existing private amenity area (Policy 19) and proposed amenity areas (Policy 20): the proposed development is not adjacent to private amenity areas of existing residential units, and private and communal amenity areas per Section 137 of the Zoning By-law (2008-250) are not required for the proposed townhouse and back-to-back townhouses. Amenity area for the future retirement home will be addressed through its separate Site Plan Control application.
- Public Art (Policy 21): requirements for public art do not apply to the proposed development.
- Design Priority Areas (Policies 22 to 24): the Subject Site is not located within a Design Priority Area.
- First Nations Peoples Design Interests (Policy 25): this policy is applied to public lands.

The following lists the applicable policies of Section 4.11 with a description of how the proposed development addresses them.

Building Design

5. *Compatibility of new buildings with their surroundings will be achieved in part through the design of the portions of the structure adjacent to existing buildings and/or facing the public realm. Proponents of new development will demonstrate, at the time of application, how the design of their development fits with the existing desirable character and planned function of the surrounding area in the context of:*

- a) *Setbacks, heights and transition;*
- b) *Façade and roofline articulation;*
- c) *Colours and materials;*
- d) *Architectural elements, including windows, doors and projections;*
- e) *Pre- and post-construction grades on site; and*
- f) *Incorporating elements and details of common characteristics of the area.*

Conceptual renderings of the proposed townhouses and back-to-back townhouses are provided in **Figures 21 and 22** and Elevation Drawings are provided in **Appendix C**. The height of proposed low-rise dwellings is consistent with the existing low-rise buildings in the area and appropriate setbacks and transitions have been provided from the adjacent, commercial and office buildings with the use of window streets, rear yard setbacks, and the proposed park. Architectural elements including porches, and window and door projections provide articulation to the façades and rooflines of the buildings with further visual interest provided through variations in colours and building materials.

6. *The City will require that all applications for new development:*
 - a) *Orient the principal façade and entrance(s) of main building(s) to the street.*
 - b) *Include windows on the building elevations that are adjacent to public spaces;*
 - c) *Use architectural elements, massing, and landscaping to accentuate main building entrances.*

The principal façade and entrances to the proposed dwelling units are oriented towards the proposed streets and emphasized with porches. Corner end units will include wrap around porches where appropriate and windows to address both street frontages.

7. *The intersections of arterial and collector roads can serve as gateways into communities and can support high levels of pedestrian and vehicular traffic, the greatest density of housing, and other land uses and services, and commercial services and other land uses that are focal points for a community. The City will encourage development proposals at such locations to include the following:*
 - a) *Strong architectural design elements that feature the corner or street axis by: locating buildings close to the street edge, and/or orienting the highest and most interesting portion of a building (e.g. the main entrance) to the corner or axis which has a view of the terminus.*
 - b) *Capitalizing on design possibilities for both street façades (by wrapping the materials used on the front façade around the building where any façades are exposed to the public realm); and*
 - c) *Soft landscaping features, special paving materials, and/or curb extensions to shorten the distance across the street and larger sidewalk area to accommodate sidewalk activity.*

The proposed Site Plan reserves a portion of Claridge's land at the intersection of Walkley Road (an arterial road) and Don Reid Drive (a collector Road) for the highest density of housing being a future retirement home and residential care facility. This building will be subject to a separate Site Plan Control application and will be designed to address this policy.

8. *To maintain a high quality, obstacle free pedestrian environment, all servicing, loading areas, and other required mechanical equipment and utilities should be internalized and integrated into the design of the base of the building where possible. If they cannot be internalized these services are to be screened from public view (i.e. trees, landscaping, decorative walls and fences etc.) and are to be acoustically dampened where possible. The location and operation these areas and equipment should be designed to maintain a pedestrian friendly environment and not impede public use of the sidewalk.*

Services and utilities are provided below ground throughout the development. Please refer to the “Notes and Details Grading” drawing (prepared by Novatech, Drawing No. 122040-ND2), submitted with these applications for detailed private way cross-sections. Servicing, mechanical equipment, and loading areas for the future retirement home will be determined through a separate Site Plan Control application.

9. *Roof-top mechanical or telecommunications equipment, signage, and amenity spaces should be incorporated into the design and massing of the upper floors of the building. [Amendment #150, LPAT July 19, 2019]*

This policy is not applicable to the proposed townhouse and back-to-back townhouse units. Roof-top mechanical or telecommunications equipment, signage, and amenity spaces for the future retirement home will be determined through a separate Site Plan Control application.



Figure 21: Conceptual rendering of the proposed townhouses (image credit: LightCache)



Figure 22: Conceptual rendering of the proposed back-to-back townhouses (image credit: Shapeshifter Digital Animation Inc.)

4.2.4 Scenic Route

Conroy Road is designated as a Scenic Route in both the existing Official Plan (Schedule I) and the new Official Plan (Schedule C13), as shown in **Figures 23 and 24**. As the intent of the Scenic Route policies is generally consistent between the current and new Official Plans, the policies of the new Official Plan are listed below with a description of how the proposed development has addressed them.

Per Policy 4 of Section 4.6.2 of the new Official Plan:

4. *Development abutting Scenic Routes, as identified on Schedule C13, shall contribute to conserving or creating a desirable context by such means as:*
 - a. *Preserving and restoring landscaping, including but not limited to distinctive trees and vegetation along the right of way;*
 - b. *Orienting buildings towards the Scenic Route and providing direct pedestrian access, where appropriate; and*
 - c. *Providing screening by way of opaque fencing or landscape buffers to hide surface parking lots or outside storage; and*
 - d. *Managing the intensity and spill-over of lighting on adjacent parcels.*

There are no natural or cultural heritage features on or adjacent to the Subject Site. The back-to-back units are oriented towards Conroy Road with appropriate separation provided with a widow street. Streetlights exist along Conroy Road and any on-site lighting will not impact Conroy Road or the adjacent commercial and office uses. Please refer to the Landscape Plan submitted with this application for proposed tree planting.

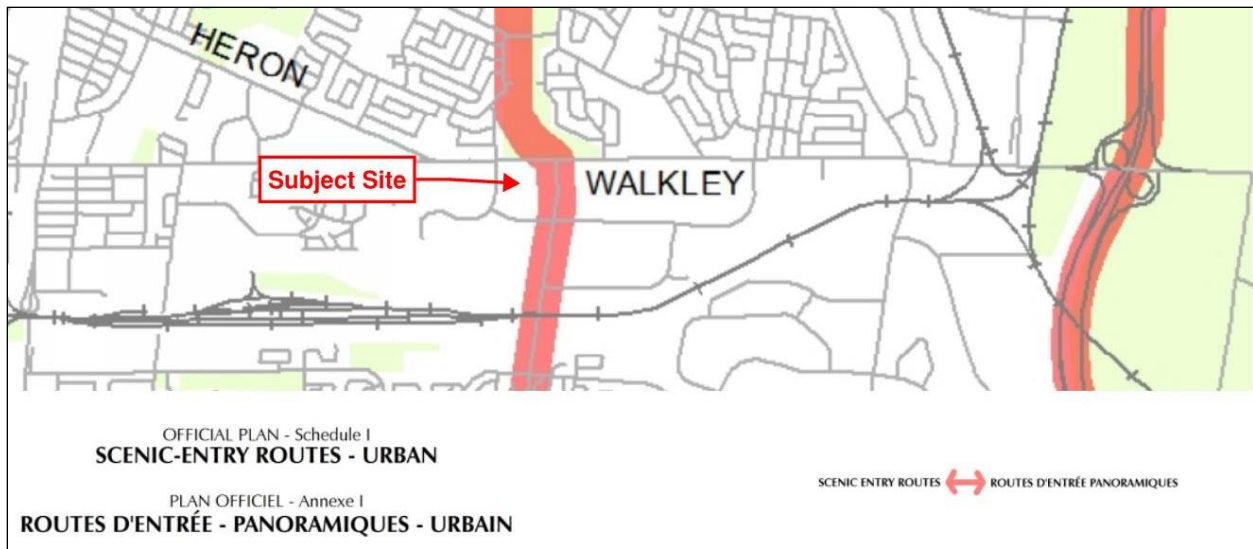


Figure 23: Excerpt of current Official Plan "Schedule I - Scenic Entry Routes"

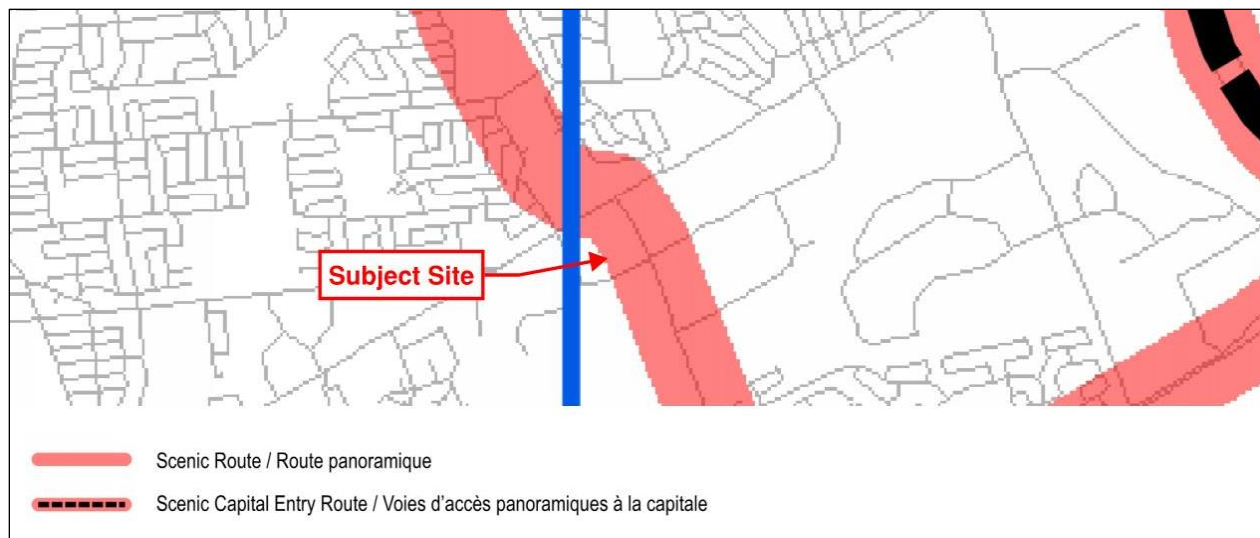


Figure 24: Excerpt of new Official Plan "Schedule C13 - Scenic Routes"

The proposed development conforms to the applicable policies of the existing City of Ottawa Official Plan and new City of Ottawa Official Plan.

4.3 Zoning By-law 2008-250

The Subject Site is zoned General Mixed Use, Urban Exception 1327 (GM[1327]) in the City of Ottawa's Zoning By-law 2008-250. The purpose of the GM zone is to:

- (1) *allow residential, commercial and institutional uses, or mixed use development in the General Urban Area and in the Upper Town, Lowertown and Sandy Hill West Character Areas of the Central Area designations of the Official Plan;*
- (2) *limit commercial uses to individual occupancies or in groupings in well defined areas such that they do not affect the development of the designated Traditional and Arterial Mainstreets as viable mixed-use areas;*
- (3) *permit uses that are often large and serve or draw from broader areas than the surrounding community and which may generate traffic, noise or other impacts provided the anticipated impacts are adequately mitigated or otherwise addressed; and*
- (4) *impose development standards that will ensure that the uses are compatible and complement surrounding land uses.*

Per Section 187(1) and (2) of the Zoning By-law, all the proposed uses ("Townhouse Dwelling", "Planned Unit Development", "Retirement Home", and "Residential Care Facility") are permitted and are consistent with purpose (1) of the GM zone.

The proposed development adheres to all provisions of the GM Zone as listed in Table 187 of the Zoning By-law with respect to minimum lot area and width, minimum front, corner, interior, and rear yard setbacks, and maximum building height. A new Urban Exception (GM[XXXX]) is proposed to remove provision 187(g) which requires a maximum floor space index (FSI) of 2 and provision (h) which requires a minimum width of landscaped area abutting a street or a residential or institutional zone. Regarding provision 187(g), the Zoning By-law defines Floor Space Index as: *"the ratio of the gross floor area of a building to the total area of the lot on which the building is located"*. Since the proposed development is

a planned unit development consisting of townhouses on private streets, its built form and layout is already subject to and informed by the provisions of the GM zone and Planned Unit Development provisions of the Zoning By-law. Regarding provisions 187(h), the proposed units comply with the required GM zone yard setbacks abutting the surrounding public streets and the residential yards will be landscaped.

An amendment is also required to rezone the portion of the Subject Site to be dedicated as a public park to a Parks and Open Space (O1) zone. The purpose of the O1 zone is to:

- (1) *permit parks, open space and related and compatible uses to locate in areas designated as General Urban Area, General Rural Area, Major Open Space, Mixed Use Centre, Village, Greenbelt Rural and Central Area as well as in Major Recreational Pathway areas and along River Corridors as identified in the Official Plan, and*
- (2) *ensure that the range of permitted uses and applicable regulations is in keeping with the low scale, low intensity open space nature of these lands.*

The proposed public park is consistent with purpose (1) and will be planned in accordance with purpose (2) of the O1 zone.

The proposed development complies with the purposes and applicable provisions of Zoning By-law 2008-250.

5.0 PUBLIC CONSULTATION STRATEGY

It is proposed to consult with the public through the legislated public consultation requirements. The public consultation strategy will involve a variety of methods as follows:

- Signage posting on the Subject Site which provides the public with details of the proposed development and means of contacting the file lead to provide comments and/or questions.
- Digital copies of all required supporting studies and plans will be made available for public viewing through the City of Ottawa's Development Applications webpage (<https://devapps.ottawa.ca/en/>).
- Community organization(s) will be notified of the details of the proposed development through a 'heads up' by City of Ottawa staff.

6.0 CONCLUSION

This Planning Rationale has been prepared in support of Zoning By-law Amendment, Site Plan Control, and Plan of Condominium applications to facilitate the development of a planned unit development consisting of 192 back-to-back townhouses and 36 townhouses on private streets, and a public park. The Zoning By-law Amendment application also applies to the residual portion of 1900 Walkley Road outside of the proposed Site Plan at the southeast corner of Walkley Road and Don Reid Drive to facilitate the future development of a retirement home and residential care facility, which will be subject to a separate Site Plan Control application.

The Subject Property is zoned General Mixed Use, Urban Exception 1327 (GM[1327]) in the City of Ottawa's Zoning By-law 2008-250. Urban Exception 1327 contains provisions that were specific to a large-format retail use previously planned for the Subject Site. The proposed uses ("Planned Unit Development", "Townhouse Dwelling", "Retirement Home", and "Residential Care Facility") are permitted in the GM zone.

The proposed Zoning by-law Amendment seeks to remove Urban Exception 1327 and add a new Urban Exception to remove the “Maximum Floor Space Index” provision and “Minimum Width of Landscape Area” provisions from the GM zone. This application also seeks to rezone the proposed park from GM[1327] to Parks and Open Space (O1).

It is our assessment that the proposed development is consistent with the Provincial Policy Statement, conforms with the policies of the City of Ottawa’s current and new Official Plans, and complies with the City of Ottawa Zoning By-law 2008-250. The proposed Zoning By-law Amendment, Site Plan Control, and Plan of Condominium applications are appropriate for the development of the Subject Site and represents good land use planning.

NOVATECH

Prepared by:



Ellen Potts, BES(PI)
Planner

Reviewed by:

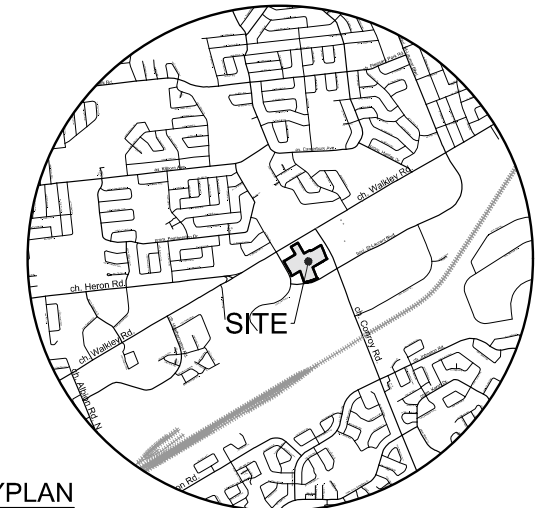
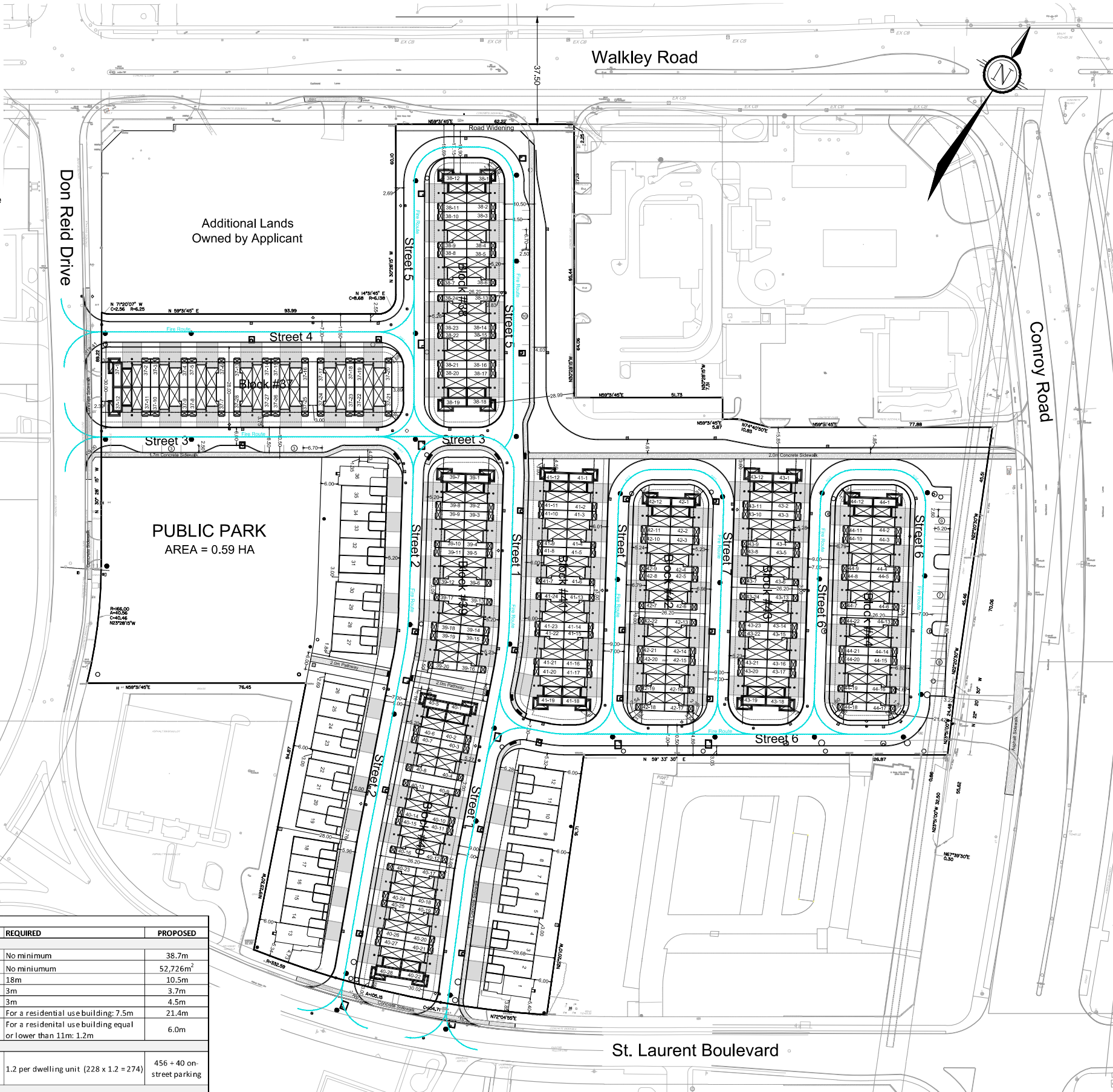


Greg Winters, RPP, MCIP
Director, Planning and Development

Appendix A – Site Plan

LEGEND

- Site Plan Boundary
- 5.2m Garage Setback
- 1.8m Building Setback
- Proposed Sanitary Manhole
- Proposed Storm Manhole
- Proposed Box Manhole
- Proposed Catchbasin Box Manhole
- ⊙ Proposed Catchbasin Manhole
- Proposed Catchbasin
- ◇ Proposed Fire Hydrant
- Proposed Tactile Walking Surface Indicator (TWSI)
- Proposed Landscape Drain

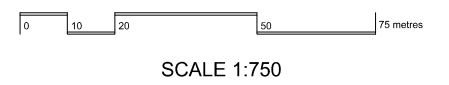


KEYPLAN
NOT TO SCALE

SITE PLAN

2510 ST. LAURENT BOULEVARD

PART OF LOTS A AND I
CONCESSION 4 (RIDEAU FRONT)
Geographic Township of Gloucester
CITY OF OTTAWA



ZONING PROVISIONS - ZONING BY-LAW 2008-250		
PROVISION	REQUIRED	PROPOSED
GENERAL MIXED USE (GM) [XXXX]		
MIN LOT WIDTH	No minimum	38.7m
MIN LOT AREA	No minimum	52,726m ²
MAX BUILDING HEIGHT	18m	10.5m
MIN FRONT YARD SETBACK	3m	3.7m
MIN CORNER SIDE YARD SETBACK	3m	4.5m
MIN REAR YARD SETBACK	For a residential use building: 7.5m	21.4m
MIN INTERIOR SIDE YARD SETBACK	For a residential use building equal or lower than 11m: 1.2m	6.0m
PARKING RATES		
RESIDENT/VISITOR COMBINED (townhouse)		
Resident (1.0/dwelling unit)	1.2 per dwelling unit (228 x 1.2 = 274)	456 + 40 on-street parking
Visitor (0.2/dwelling unit)		
LANDSCAPED AREA AROUND PARKING LOTS		
MIN WIDTH OF LANDSCAPED BUFFER	Abutting a street - 11 to 99 spaces: 3m	3.2m
PLANNED UNIT DEVELOPMENT (PUD)		
MIN WIDTH OF PRIVATE WAY	6m	6.0m
MIN SETBACK FOR ANY WALL OF A RESIDENTIAL USE BUILDING TO A PRIVATE WAY	1.8m	2.4m
MIN SETBACK FOR ANY GARAGE OR CARPORT ENTRANCE TO A PRIVATE WAY	5.2m	5.2m
MIN SEPARATION BETWEEN BUILDINGS WITHIN A PUD WHERE THE HEIGHTS OF ABUTTING BUILDINGS IS LESS THAN OR EQUAL TO 14.5m	1.2m	2.7m

SOURCE REFERENCE:
 Legal Information: *Topographical Plan of Survey Annis, O'Sullivan, Vollebakk Ltd. / August, 2022 / MTM Zone 9 / NAD 83 ORIG*
 Topographic Information: *Topographical Plan of Survey Annis, O'Sullivan, Vollebakk Ltd. / August, 2022 / MTM Zone 9 / NAD 83 ORIG*
 Topographic Information: 1:1000
 City of Ottawa / 2017 / MTM Zone 9 / NAD 83 ORIG

No.	REVISION	DATE	BY
3	ISSUED FOR CITY REVIEW	NOV 01/22	EP
2	ISSUED FOR CLIENT REVIEW	OCT 11/22	EP
1	PREPARED FOR DISCUSSION	SEPT 09/22	EP

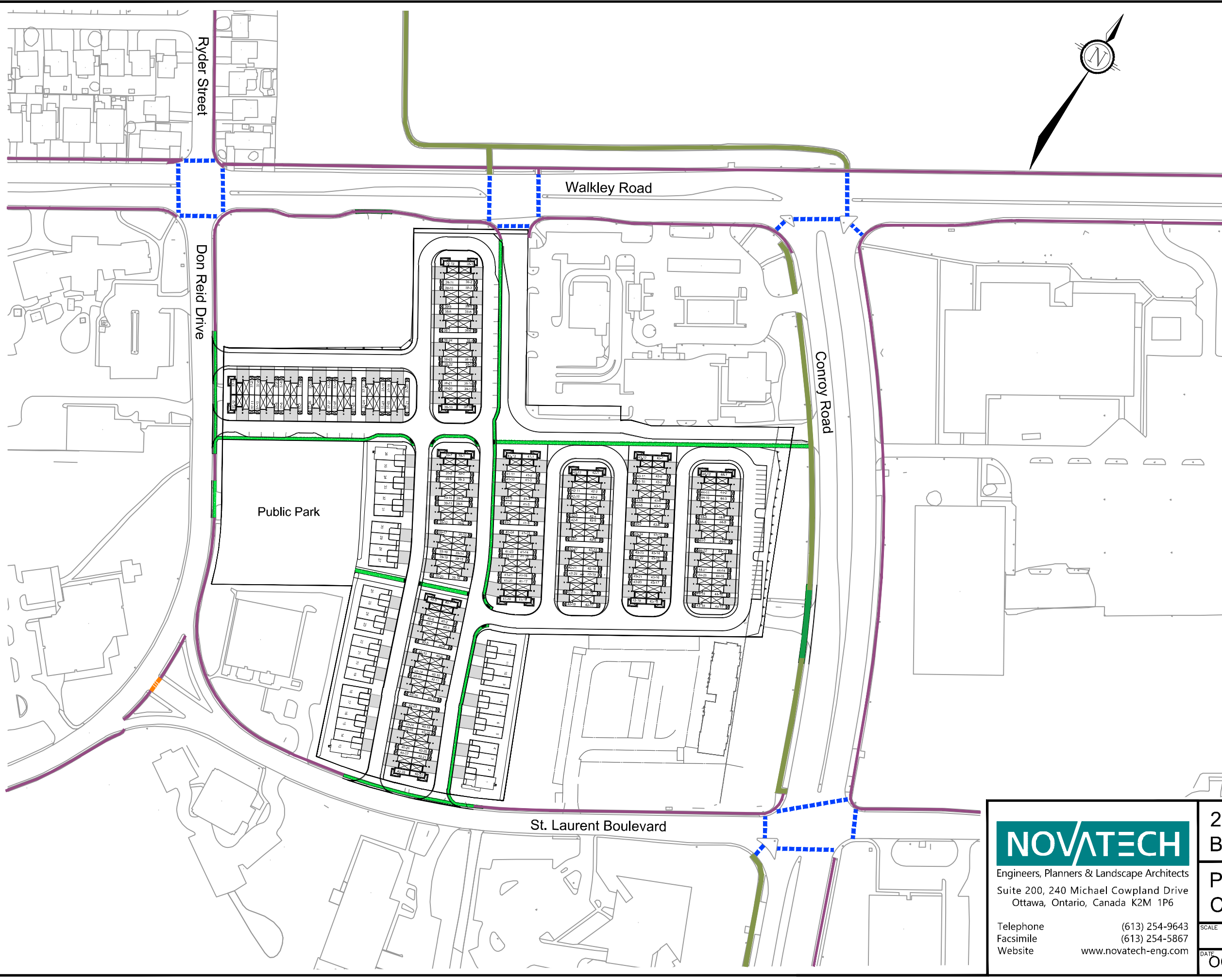
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 Facsimile: (613) 254-5867
 Website: www.novatech-eng.com

ISSUED
NOVEMBER, 2022
 PROJECT No.
122040
 DRAWING No.
122040-SP

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Appendix B – Pedestrian Circulation Plan

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- LEGEND**
- Property Line
 - Proposed Sidewalk
 - Proposed Multi-Use Pathway
 - Existing Sidewalk
 - Existing Multi-Use Pathway
 - - - Existing Signalized Crosswalk
 - Existing Crosswalk

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 Website www.novatech-eng.com

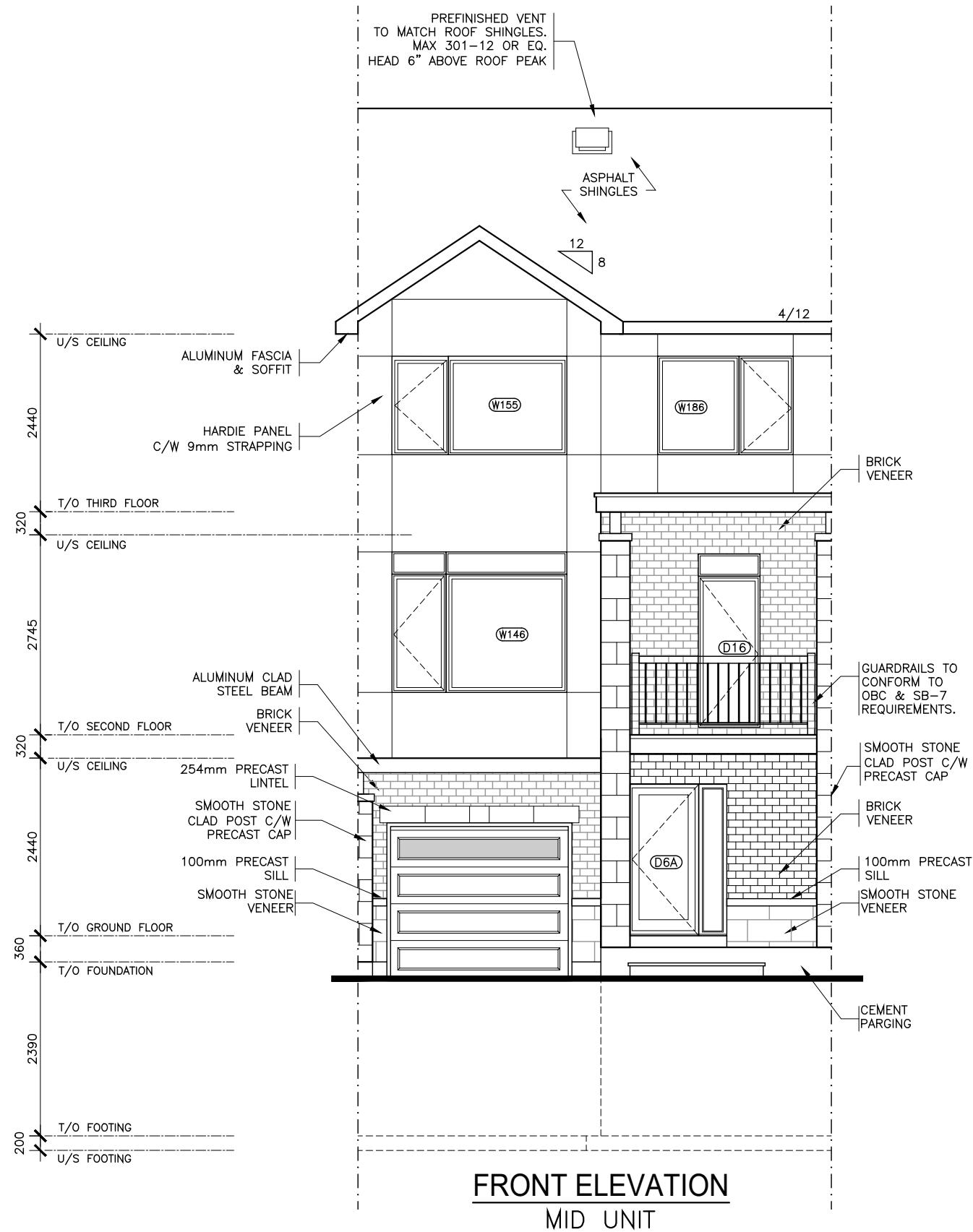
2510 ST. LAURENT BOULEVARD

PEDESTRIAN CIRCULATION PLAN

SCALE 1 : 2000

DATE OCT, 2022 JOB 122040 FIGURE 122040-PED

Appendix C – Building Elevations

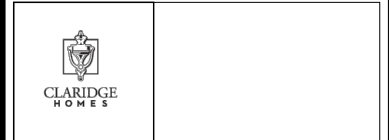


**FRONT ELEVATION
MID UNIT**

NOTE:

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No.	REVISION	DATE
4	THIRD FLOOR REDESIGN	OCT.18.21
3	STRUCTURAL REVIEW	MAR.23.20
2	DESIGN REVIEW	OCT.15.19
1	STRUCTURAL REVIEW	AUG.22.19



Model Title:
ALGONQUIN 2020
FLOOR AREA: 1395 SQ.FT = 129.58 SQ.M
BSMT: 0 SQ.FT = 0 SQ.M
OTB: 17 SQ.FT = 1.57 SQ.M

Project:
**PROJECT NAME
BLOCK # UNIT #**

Title:
**FRONT ELEVATION
MID UNIT**

AS-BUILT RELEASE
No. 3 JUN.06.2022

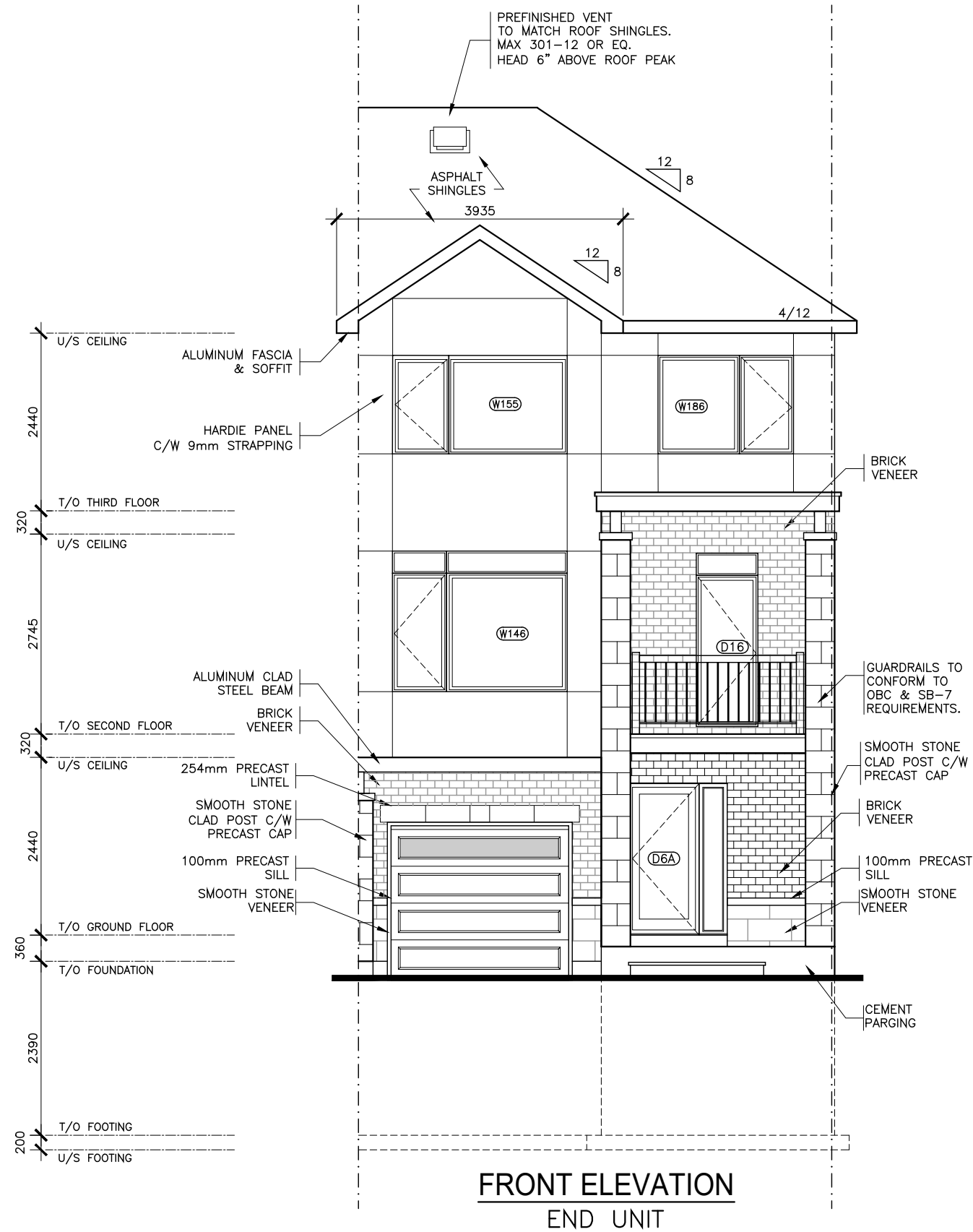
BROCHURE DATE
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Drawing No.

Drawn by: 50

A-6

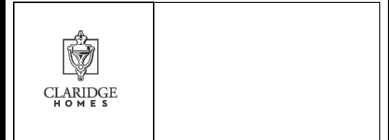


FRONT ELEVATION
END UNIT

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No.	REVISION	DATE
4	THIRD FLOOR REDESIGN	OCT.18.21
3	STRUCTURAL REVIEW	MAR.23.20
2	DESIGN REVIEW	OCT.15.19
1	STRUCTURAL REVIEW	AUG.22.19



Model Title:
ALGONQUIN 2020
FLOOR AREA: 1395 SQ.FT = 129.58 SQ.M
BSMT: 0 SQ.FT = 0 SQ.M
OTB: 17 SQ.FT = 1.57 SQ.M

Project:
PROJECT NAME
BLOCK # UNIT #

Title:
FRONT ELEVATION
END UNIT

AS-BUILT RELEASE
No. 3 JUN.06.2022

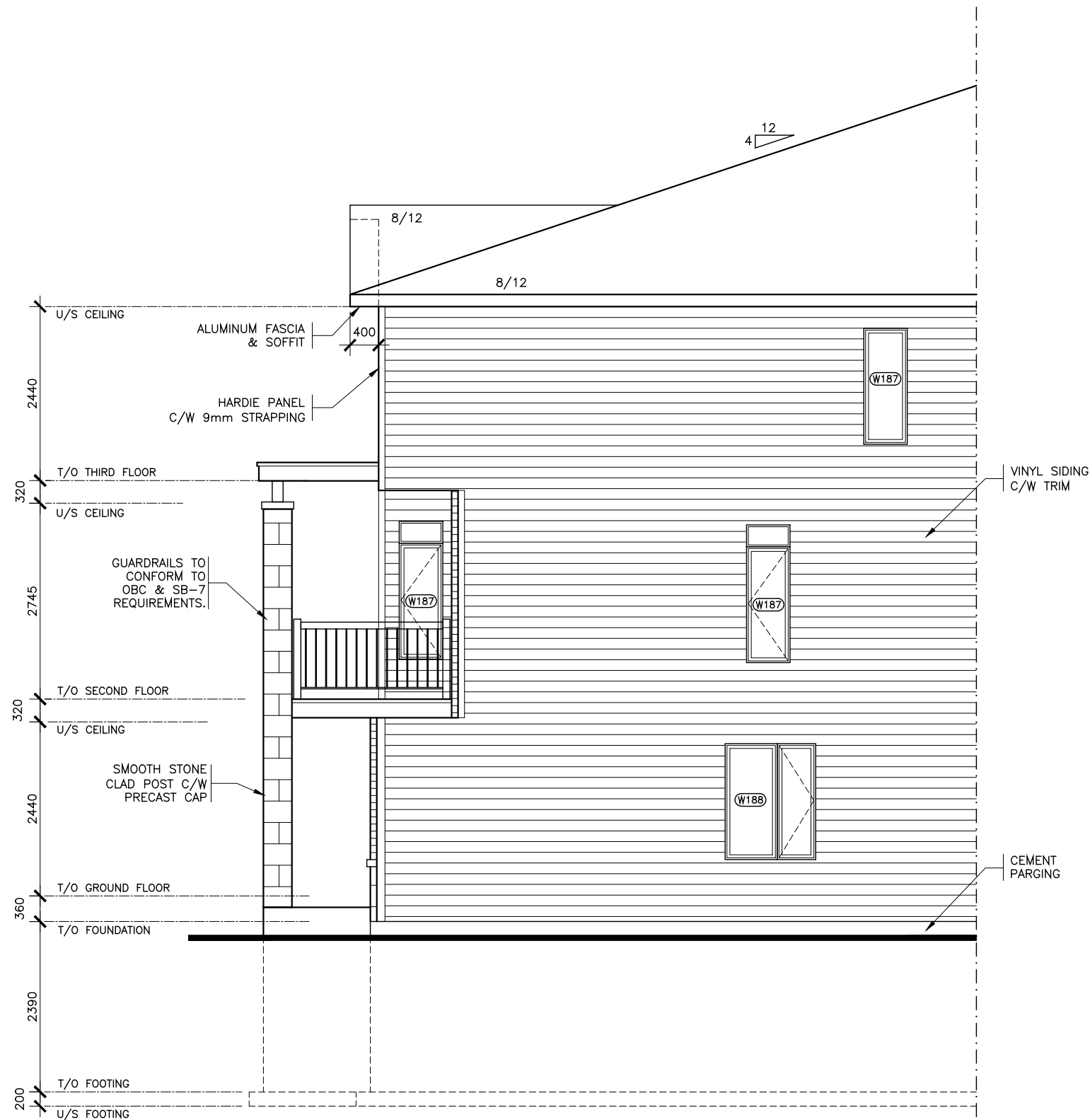
BROCHURE DATE
XX

Scale: 1:75

Drawing No.

Drawn by: 50

A-6



SIDE ELEVATION
END UNIT

EXPOSED BUILDING AREA: 71.60 SQM
 TOTAL UNPROTECTED OPENINGS: 4.25 SQM
 PERCENTAGE OF UNPROTECTED OPENINGS: 5.94 %
 MAX. ALLOWABLE UNPROTECTED OPENINGS AT 1.2 m SIDEYARD: 7.0 %

NOTE:

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No.	REVISION	DATE
4	THIRD FLOOR REDESIGN	OCT.18.21
3	STRUCTURAL REVIEW	MAR.23.20
2	DESIGN REVIEW	OCT.15.19
1	STRUCTURAL REVIEW	AUG.22.19



Model Title:
ALGONQUIN 2020
 FLOOR AREA: 1395 SQ.FT = 129.58 SQ.M
 BSMT: 0 SQ.FT = 0 SQ.M
 OTB: 17 SQ.FT = 1.57 SQ.M

Project:
PROJECT NAME
BLOCK # UNIT #

Title:
SIDE ELEVATION
END UNIT

AS-BUILT RELEASE No. 3 JUN.06.2022

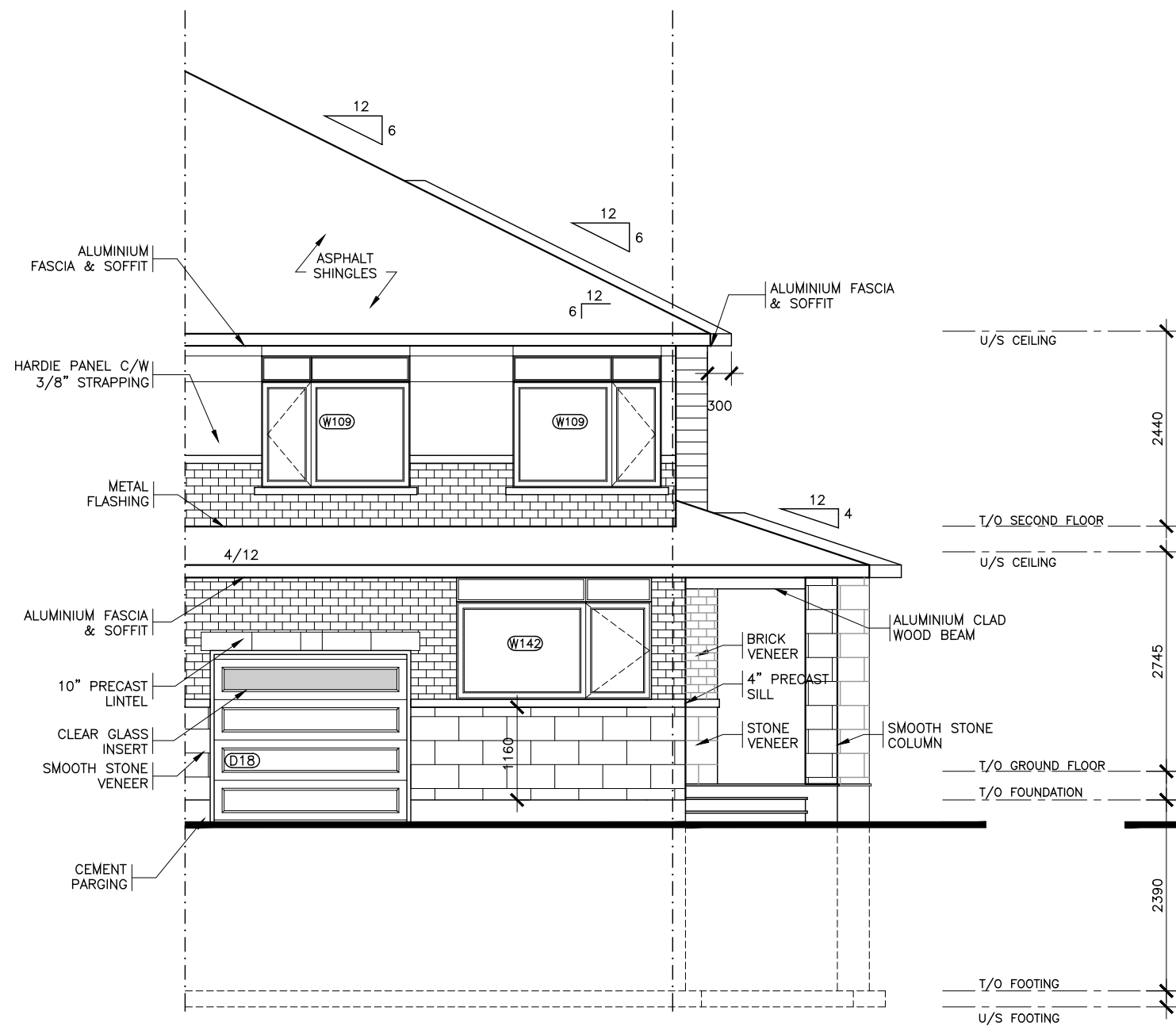
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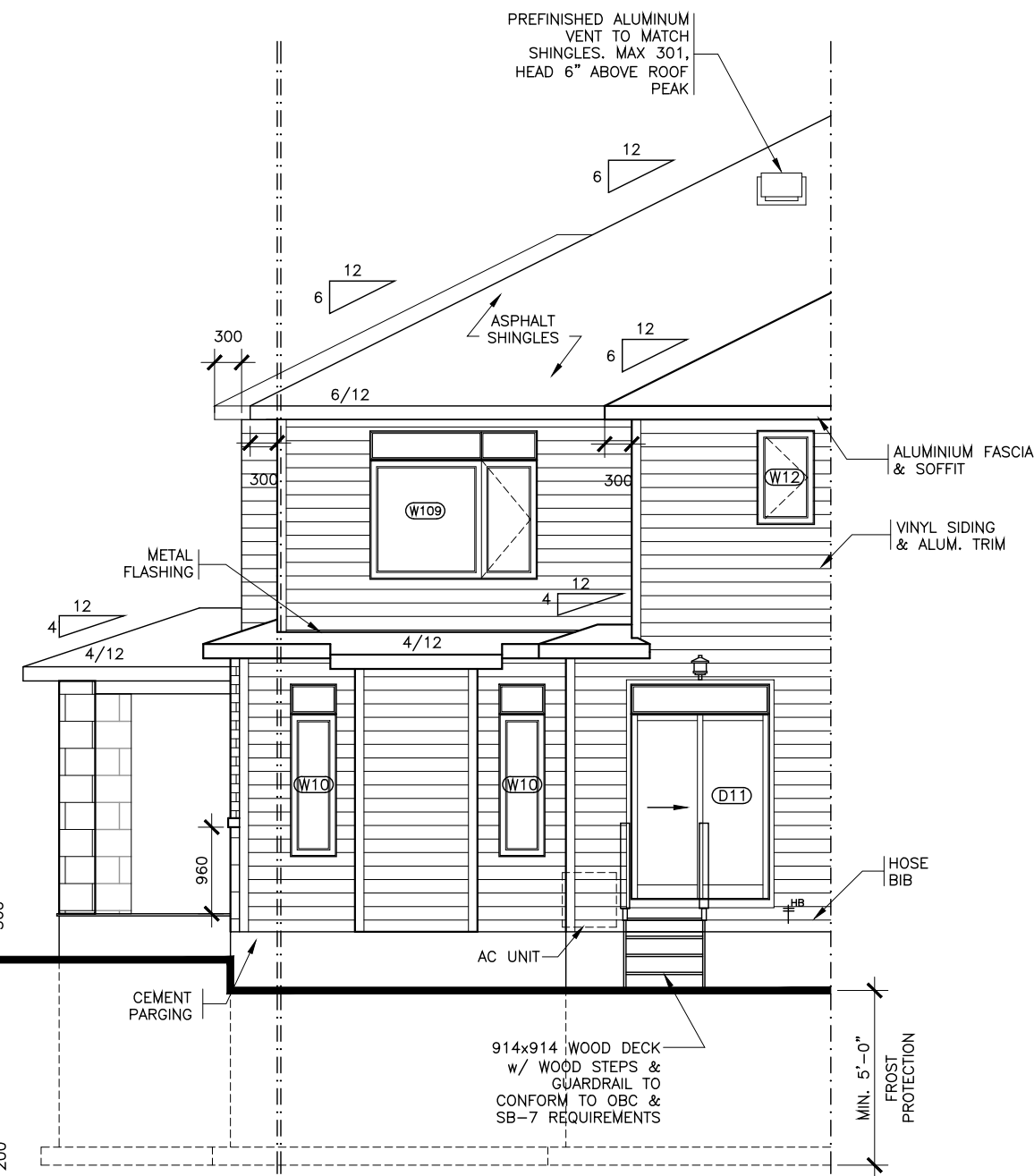
Drawing No.

Drawn by: 50

A-7



FRONT ELEVATION
STREET END
CONTEMPORARY



REAR ELEVATION
STREET END
CONTEMPORARY

NOTE:

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6	X		X
5	X		X
4	X		X
3	X		X
2	X		X
1	STRUCTURAL REVIEW		AUG.01.22
No.	REVISION		DATE



Model Title:
REED 2022
FLOOR AREA: 2029 SQ.FT = 188.47 SQ.M
BSMT: 418 SQ.FT = 38.80 SQ.M
OTB: 14 SQ.FT = 1.23 SQ.M

Project:
PROJECT NAME
BLOCK # UNIT #

Title:
FRONT & REAR ELEVATIONS

AS-BUILT RELEASE No. X DATE

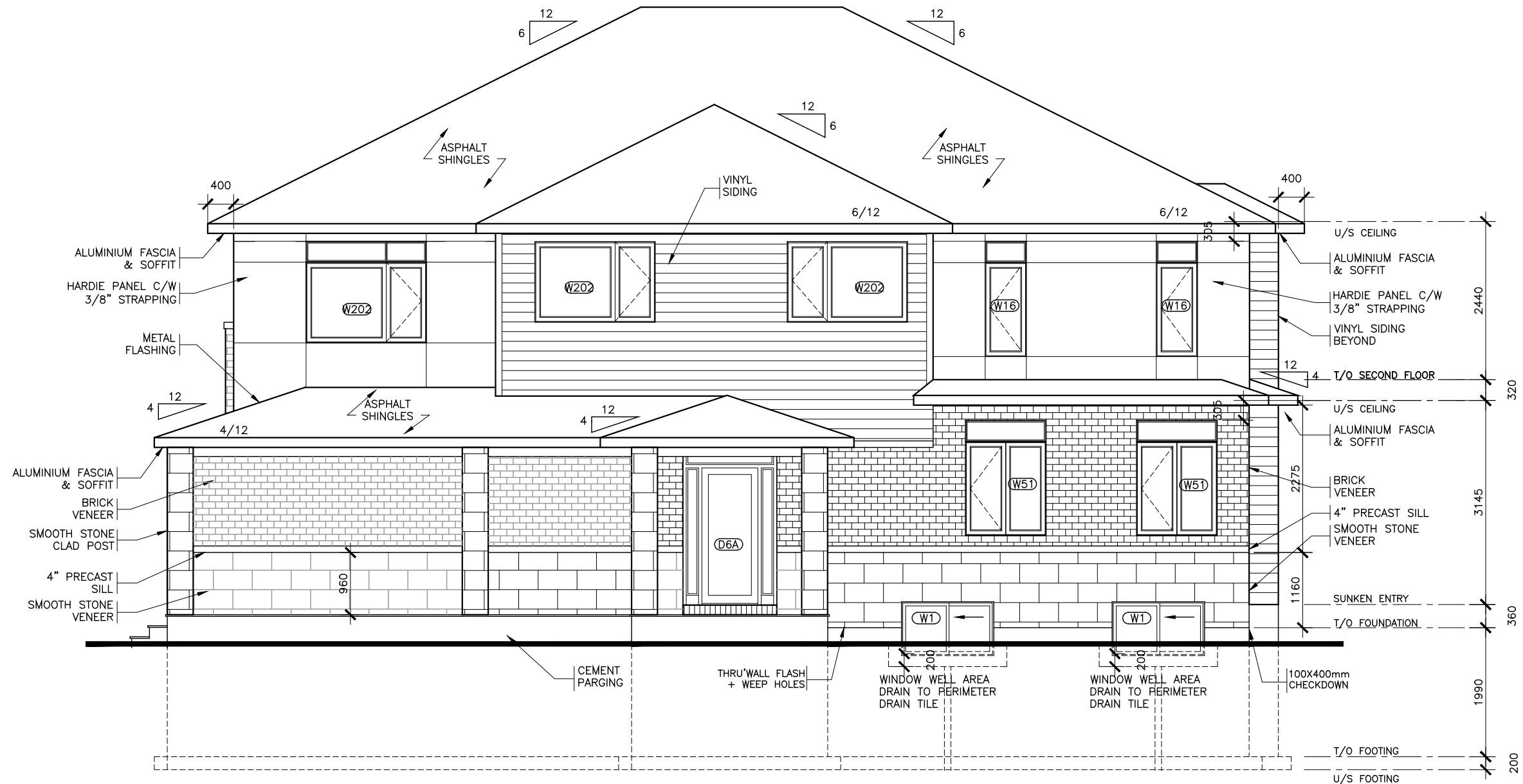
BROCHURE DATE DATE

Scale: 1 : 75

Drawing No.

Drawn by: DR/IM

A-5



SIDE ELEVATION
STREET END
CONTEMPORARY

NOTE:

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6	X		X
5	X		X
4	X		X
3	X		X
2	X		X
1	STRUCTURAL REVIEW		AUG.01.22
No.	REVISION		DATE



Model Title:
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Project:
PROJECT NAME
BLOCK # UNIT #

Title:
SIDE ELEVATION
STREET END

AS-BUILT RELEASE No. X DATE

BROCHURE DATE DATE

Scale: 1 : 75

Drawing No.

Drawn by: DR/IM

A-6

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2	X		X
1	STRUCTURAL REVIEW		AUG.01.22
No.	REVISION		DATE



Model Title:
WHITNEY II 2022
 FLOOR AREA: 1462 SQ.FT = 135.86 SQ.M
 BSMT: 340 SQ.FT = 31.54 SQ.M
 OTB: 9 SQ.FT = 0.85 SQ.M

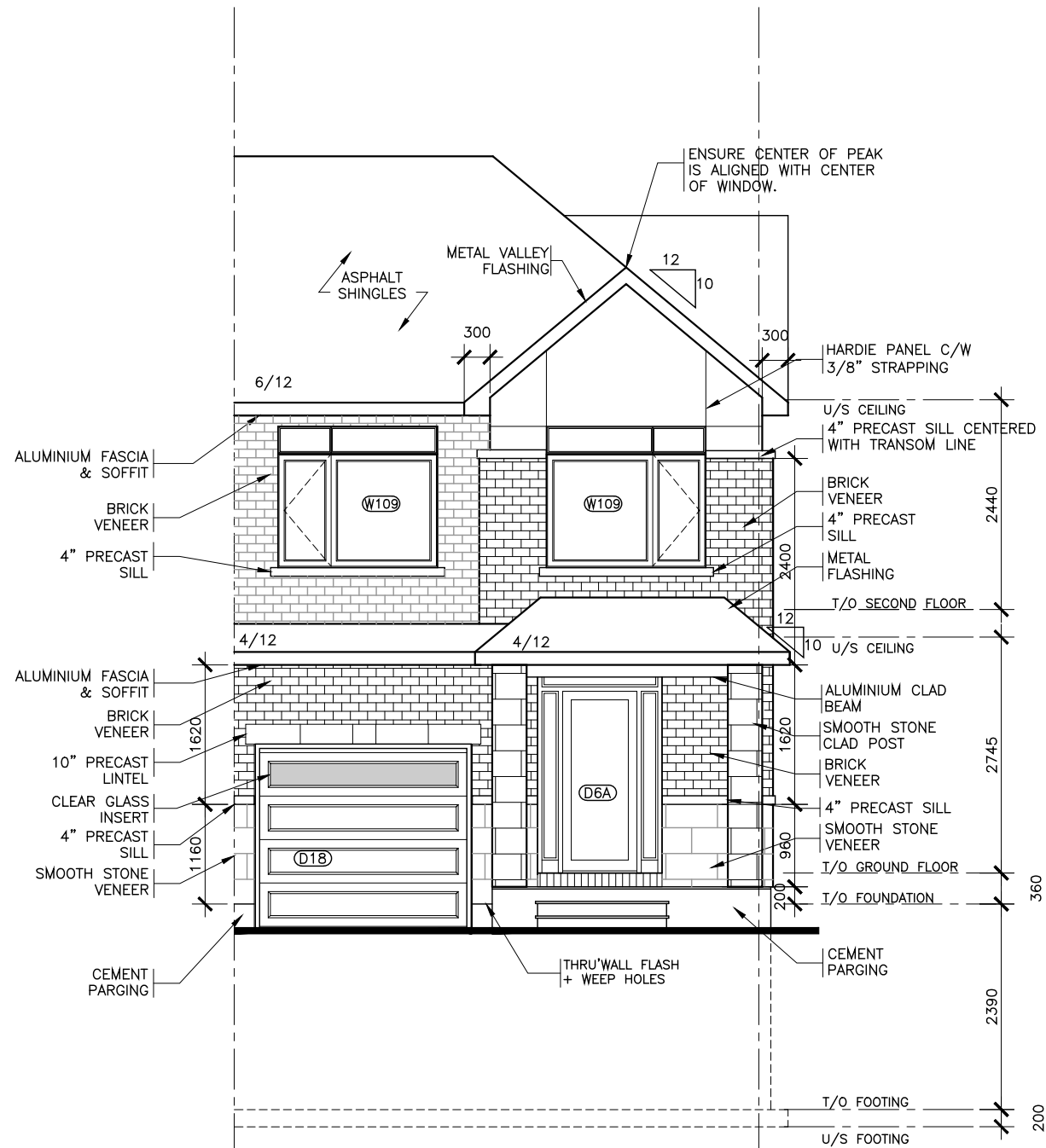
Project:
PROJECT NAME
BLOCK # UNIT #

Title:
FRONT & REAR ELEVATIONS
STREET END

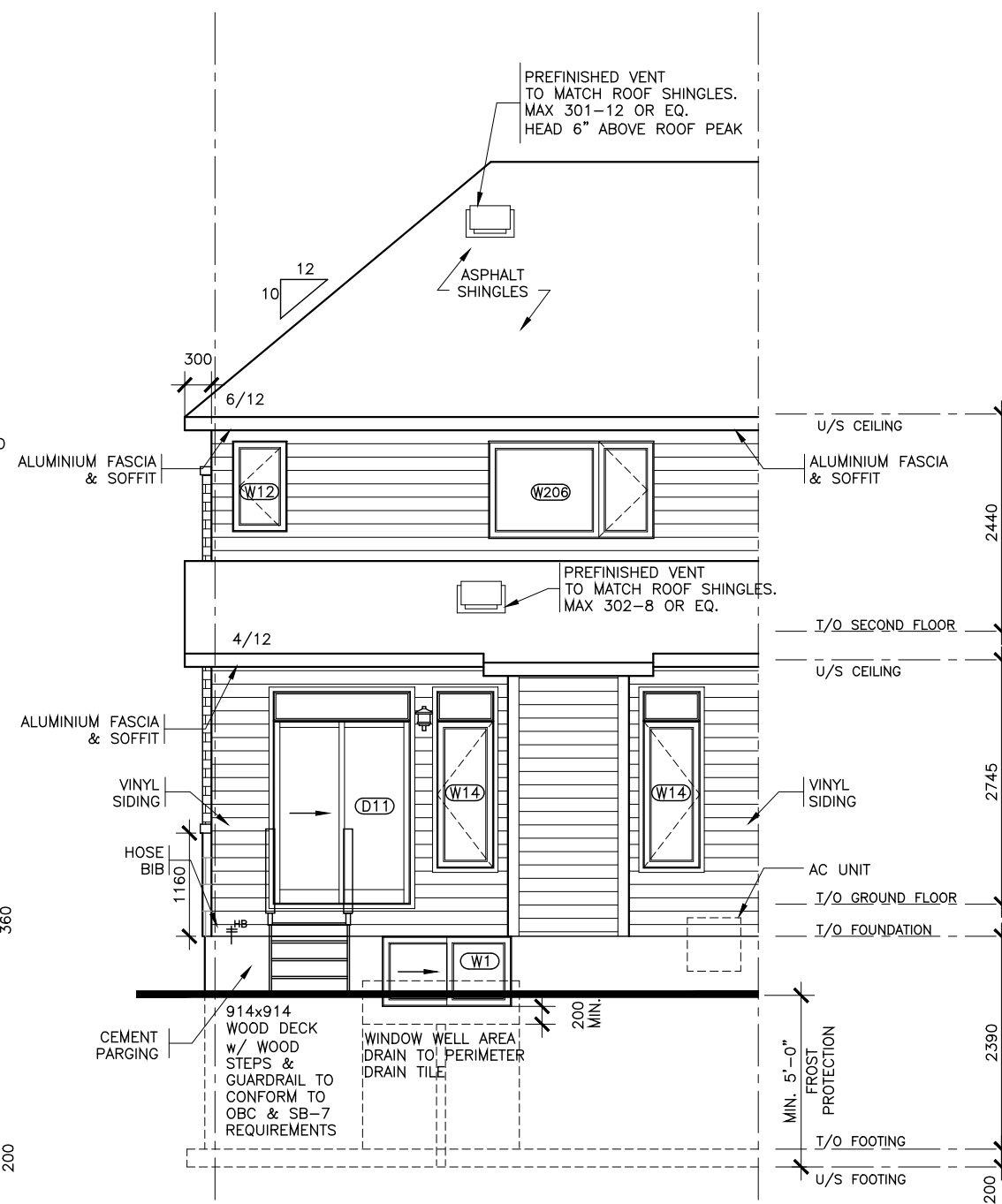
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 BROCHURE DATE XX

Scale: 1 : 75
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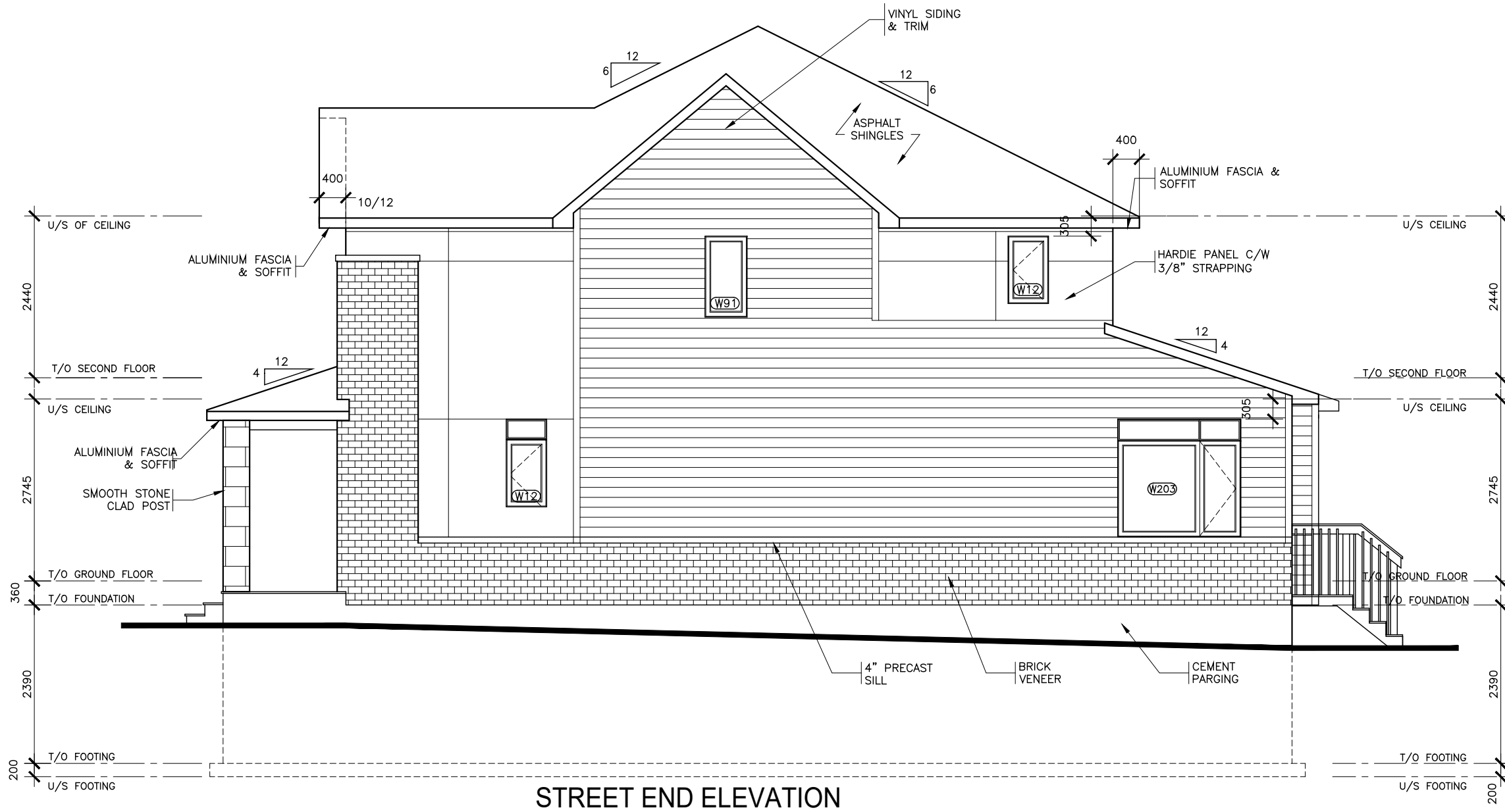
Drawn by: NC



FRONT ELEVATION
 STREET END UNIT
 CONTEMPORARY



REAR ELEVATION
 STREET END UNIT
 CONTEMPORARY



STREET END ELEVATION

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2	X		X
1	STRUCTURAL REVIEW		AUG.01.22
No.	REVISION		DATE



Model Title:
WHITNEY II 2022
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 BSMT: 340 SQ.FT = 31.54 SQ.M
 OTB: 9 SQ.FT = 0.85 SQ.M

Project:
PROJECT NAME
BLOCK # UNIT #

Title:
SIDE ELEVATION
STREET END

AS-BUILT RELEASE
 No. X DATE

BROCHURE DATE
 XX

Scale: 1 : 75

Drawing No.

Drawn by: NC

A-6