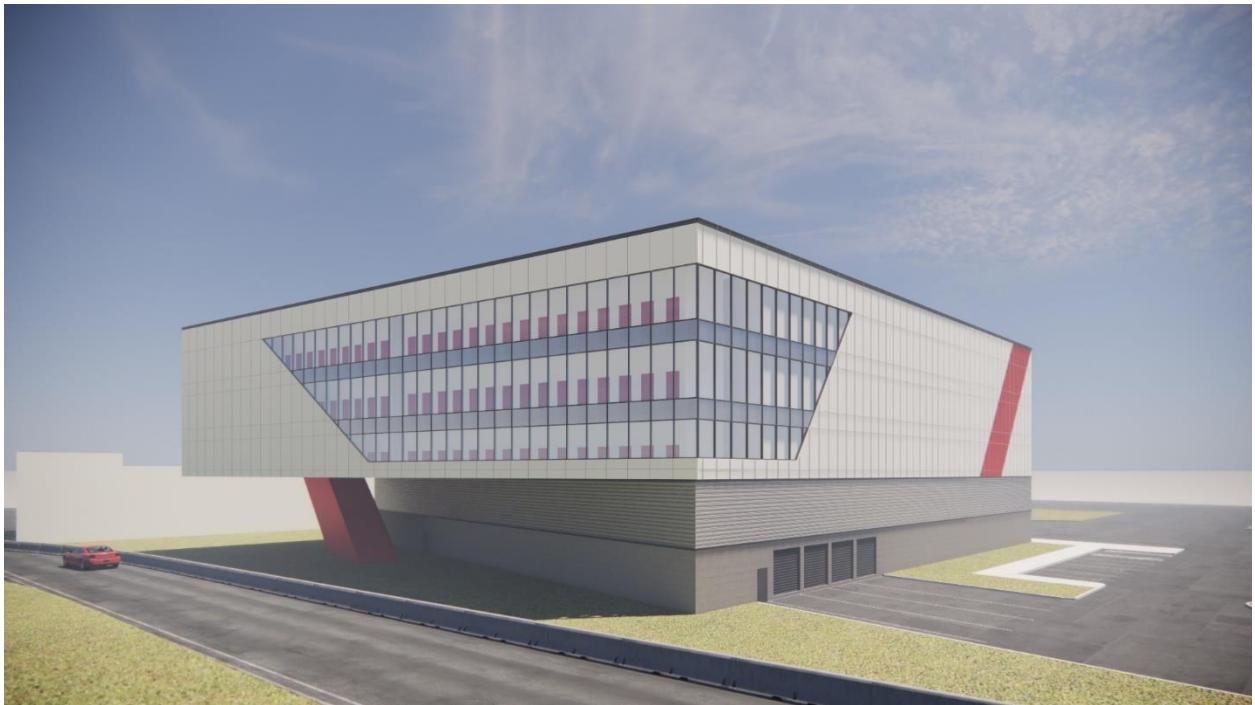


# 864 Lady Ellen Place Planning Rationale and Design Brief

Site Plan Control Application

December 2022





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## Site Plan Control Application

December 2022

**Prepared For:**

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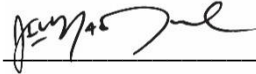
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# SIGNATURES

## PREPARED BY



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## APPENDIX

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# 1 Introduction

WSP was retained by Access Selfstorage Inc. to prepare a Planning Rationale and Design Brief (the “Report”), in support of a Site Plan Control application for the property municipally known as 864 Lady Ellen Place (the “Site”), in the City of Ottawa. The site has a land area of 1.36 hectares (3.35 acres) and is currently occupied by an existing 2.5-storey office building.

Access Selfstorage Inc. intends to develop two (2) low-rise self-storage/warehouse buildings at 864 Lady Ellen Place. The first of these will be a four-storey light industrial building with a gross floor area (GFA) of 15,913 m<sup>2</sup> (171,292 ft<sup>2</sup>), containing self-storage units on the upper three (3) storeys and warehouse space on the ground floor. The ground floor will also contain ancillary administrative office space to serve clients of the self-storage use. The second building will consist of a single-storey self-storage building with a GFA of 1,750 m<sup>2</sup> (18,844 ft<sup>2</sup>).

This Report is set up as follows:

- **Section 2** provides a description of the site location and community context;
- **Section 3** provides an explanation of the proposed development;
- **Section 4** includes our community engagement strategy;
- **Section 5** outlines the policy and regulatory framework applicable to the site, and provides a planning rationale and design brief for the proposed development;
- **Section 6** summarizes the planning opinion;
- **Appendix A** contains the Site Plan;
- **Appendix B** includes the Landscape Plan; and
- **Appendix C** includes the Building Elevations.

A number of technical studies have been prepared in support of the application and submitted to the City; the supporting studies are available under separate cover.

## 2 Site Location and Community Context

### 2.1 Site Location

The site is located at 864 Lady Ellen Place, and is legally described as 'Lots 9, 10, 11, 12 and Part of Lot 13, Registered Plan 387939 City of Ottawa', as per the Plan of Survey prepared by GeoVerra (ON) Ltd., dated May 19, 2022. It is located in Ward 16 – River Ward, at the northern terminus of Lady Ellen Place. The site is south of Ontario Highway 417 (Queensway), as illustrated in **Figure 2-1**.

The site has frontage along Lady Ellen Place (a local road running north-south). According to the Plan of Survey, the property is comprised of three (3) separate lots, each of which are municipally addressed as '864 Lady Ellen Place'. The site has a total lot area of approximately 3.348 acres (1.355 hectares) (13,548.875 m<sup>2</sup>). The site is irregular in shape and has vehicular accesses from Lady Ellen Place. It is fully serviced by municipal water and sewer.

**Figure 2-1: Site Location (geoOttawa, 2022)**



The southwest portion of the site is currently occupied by a 2.5-storey office building, formerly occupied by JL Richards Ltd and LCI Engineering Inc. (**Figure 2-2**). The existing building will be demolished prior to the construction of the new self-storage/warehouse buildings.

Currently, the site is primarily hardscaped as surface parking comprises most of the site. There are several trees within the site and along Lady Ellen Place. Additionally, there are hedges with small trees located along the west and east property lines. Several picnic benches that were used by the former office employees are located south of the existing building. Vehicular access to the site is from a driveway from the cul-de-sac on Lady Ellen Place.

A site visit was conducted on June 22, 2022. Photos contained herein are from WSP, unless otherwise stated.

**Figure 2-2: Existing three-storey commercial office building at 864 Lady Ellen Place**



---

## 2.2 Community Context

The site is located within the Carlington neighbourhood area, which is primarily comprised of a mix of low-density office and light industrial uses. There are some office uses and auto dealerships in the vicinity of the site, including the Volvo, Ford, Mazda and Workaway offices. Beyond Laperriere Avenue, Carlington Park and Recreation Centre, and Raven Park are located south of the site. These areas include J.A. Dulude Arena, Belleveue Manor Park, baseball diamonds, and a dog run.

Nearby amenities and services are shown in **Figure 2-3**. The surrounding site and community context is shown in **Figure 2-4**, and **Figures 2-5 to 2-12**.



Land uses adjacent to the site are as follows:

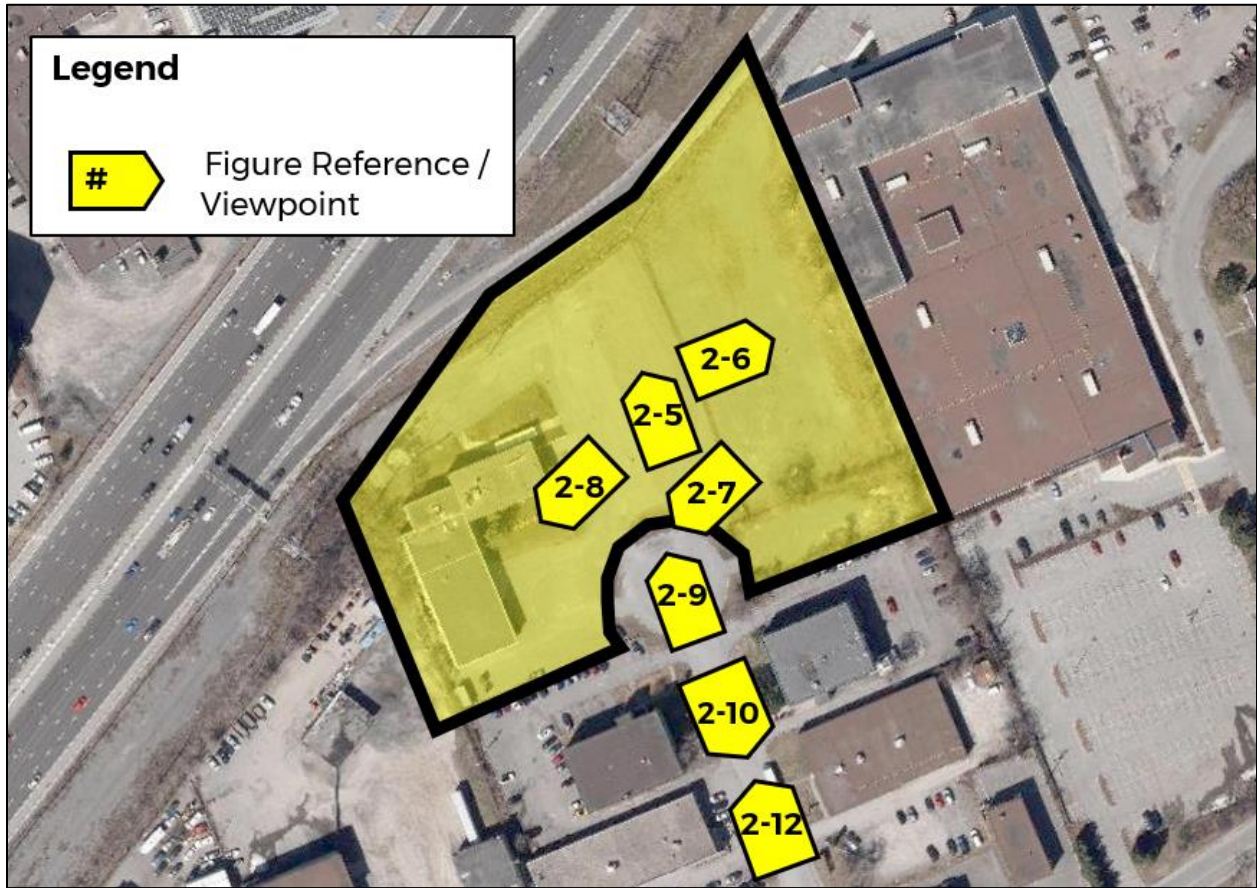
- **North:** Highway 417 and further north, a 3-storey Dymon Self-Storage building at 1554 Carling Avenue.
- **South:** Two 3-storey office buildings at 880 & 881 Lady Ellen Place and surface parking lots.
- **East:** Single-storey commercial and office building at 1550 Carling Avenue and surface parking areas.
- **West:** Single-storey warehouse buildings at 1523 Laperriere Avenue, surface parking lot, and outdoor storage area.

**Figure 2-3: Surrounding Community Context (WSP, 2022)**





**Figure 2-4: Site Photos - Existing Site Conditions and Context - Pinpoint Map (WSP, 2022)**



**Figure 2-5: Existing parking area on site looking north**





**Figure 2-6: Existing parking area on site looking east**



**Figure 2-7: Existing parking area and driveway to site, looking southwest**



**Figure 2-8: Existing picnic area on site**



**Figure 2-9: Existing driveway to site, looking northeast**



**Figure 2-10: Lady Ellen Place, looking south from site**



**Figure 2-11: Lady Ellen Place, looking north towards site**



## 2.2.1 Surrounding Development Activity

**Figure 2-12** identifies active development applications in the vicinity of the site. **Table 2-1** provides a summary of these development applications.

**Table 2-1: Development Activity within the Vicinity of the Site**

Address	Land Use	Max Height (Storeys)	Non Residential GFA (sq.m)	Development Status
885 Churchill Avenue South & 1551 Laperriere Avenue	Retail / Service	1	14,010 m <sup>2</sup>	ZBLA* approved in 2011; Built
1400 Carling Avenue	Institutional (Retirement Home)	12	25 units	ZBLA* approved in 2017; SPA** submitted in 2017
689 Churchill Avenue North	Residential	3	15 units	SPA** approved in 2018; Built
864 Lady Ellen Place	Office	5	4,290 m <sup>2</sup>	SPA** approved in 2020
701 Churchill Avenue	Residential	4	12 units	SPA** approved in 2020
1619-1655 Carling Avenue	Mixed-Use (Residential-Commercial)	18	370 units; 667.8 m <sup>2</sup> commercial GFA	ZBLA* approved in 2021; SPA **submitted in 2020
861 Clyde Avenue	Mixed-Use (Residential-Commercial)	39	1,933 units; 9,000 m <sup>2</sup> commercial GFA	OPA*** and ZBLA* approved in 2021

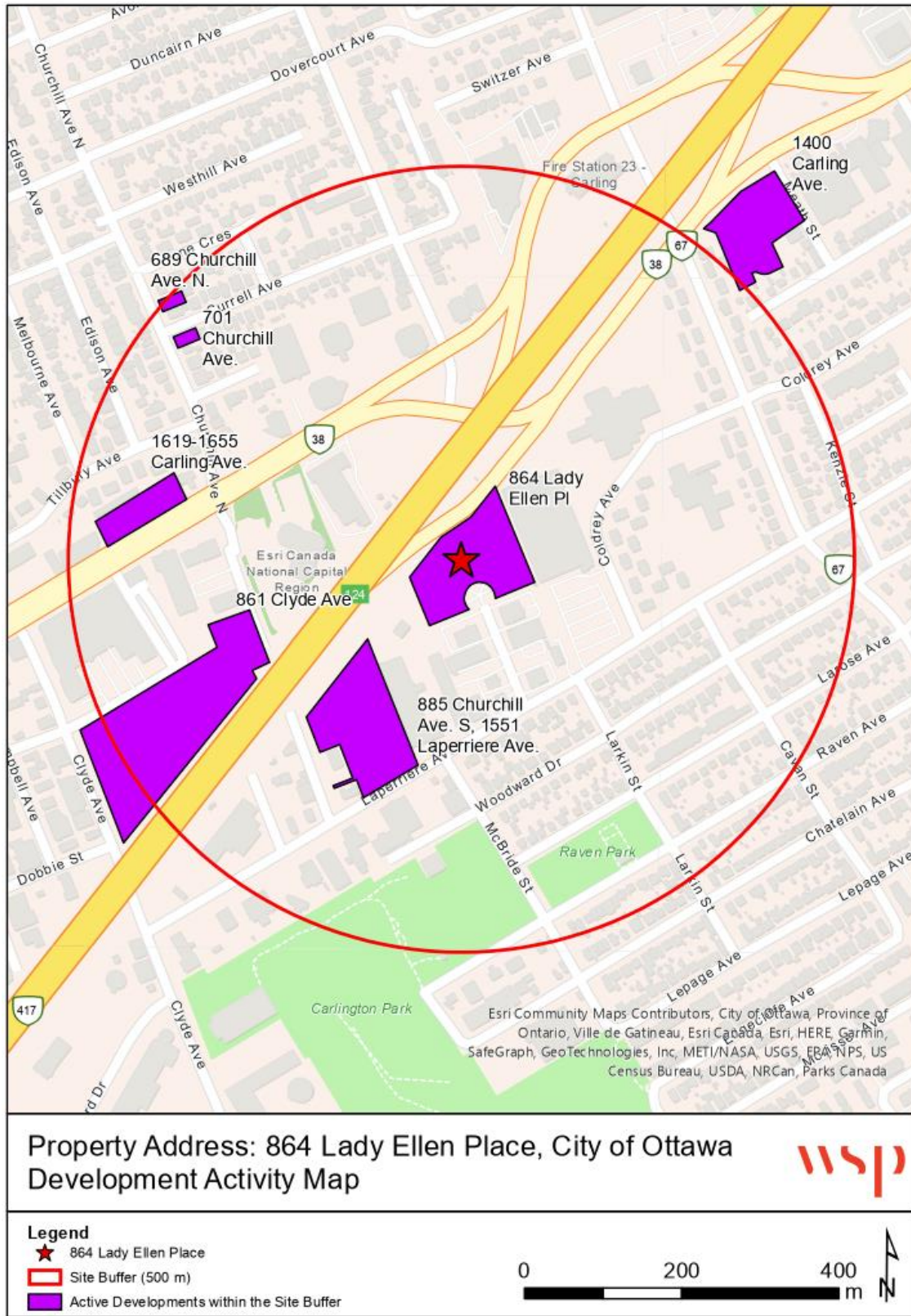
\*Zoning By-law Amendment Application

\*\*Site Plan Control Application

\*\*\*Official Plan Amendment Application



**Figure 2-12: Development Activity within the Vicinity of the Site**



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## 2.3 Transportation Network

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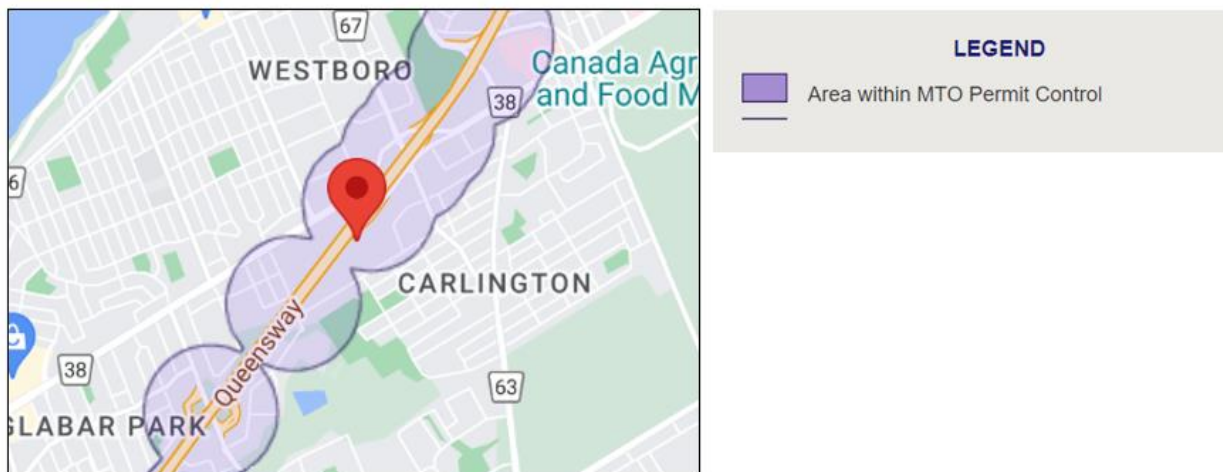
### 2.3.1 Roads

#### Ministry of Transportation Permit Control Area

Under the authority of the Public Transportation and Highway Improvement Act, the Ministry of Transportation (MTO) regulates the permit-controlled area for buildings, structures, roads, entrances and the placement of signs. Where a site is located within MTO Controlled Areas, MTO will also provide a review of any municipal planning applications as part of the land use planning process. As part of a pre-application consultation meeting, the City of Ottawa may engage MTO to provide initial input, or a separate pre-consultation meeting with MTO can be arranged.

As the lands at 864 Lady Ellen Place are adjacent to Ontario Highway 417, the site is part of MTO's Permit Control Area, as shown in **Figure 2-13**.

**Figure 2-13: MTO Permit Control Area (MTO, Highway Corridor Management, 2022)**



#### MTO Corridor Management Manual (April 2022)

The MTO Highway Corridor Management Manual (“Manual”) contains policies, guidelines, best practices, and specifications for the management of land use, buildings, encroachments, access, and signs within MTO’s controlled area under the Public Transportation and Highway Improvement Act.

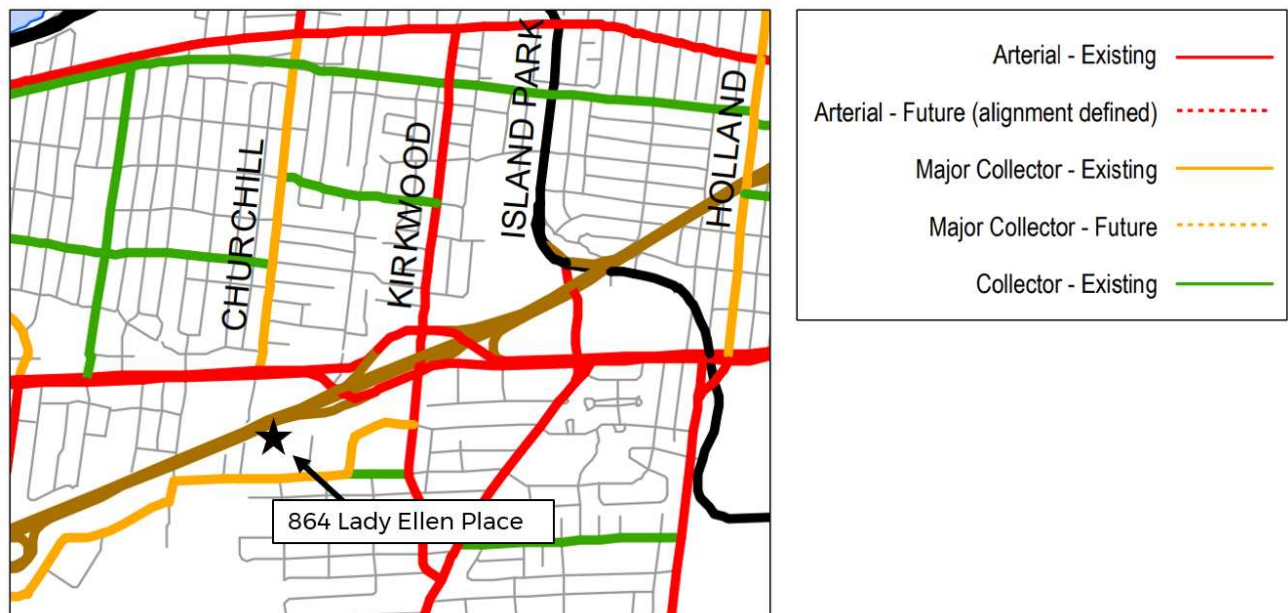
As per Section 2.13.1 Basic Setback Distances of the Manual, in built-up areas, the minimum setback for all buildings and structures adjacent to a Class 1 or 2 highway or a 400 series highway is 14 metres.

## City Road Network

The road network around the site as per Schedule C4 Urban Road Network of the City of Ottawa Official Plan (Council Adopted November 21, 2022, approved by MMAH on November 4, 2022 with modifications) as illustrated in **Figure 2-14**. Lady Ellen Place is designated as a Local Road (running north-south). Laperriere Avenue is identified as an Existing Major Collector Road (running east-west).

There are no sidewalks on Lady Ellen Place. There is a sidewalk on the south side of Laperriere Avenue, south of the site. There are no bike lanes on Lady Ellen Place or Laperriere Avenue.

**Figure 2-14: Schedule C4 - Urban Road Network (Excerpt) (City of Ottawa Official Plan (Adopted November 24, 2021, and approved by the Ministry of Municipal Affairs and Housing on November 4, 2022 with modifications))**



### 2.3.2 Public Transit

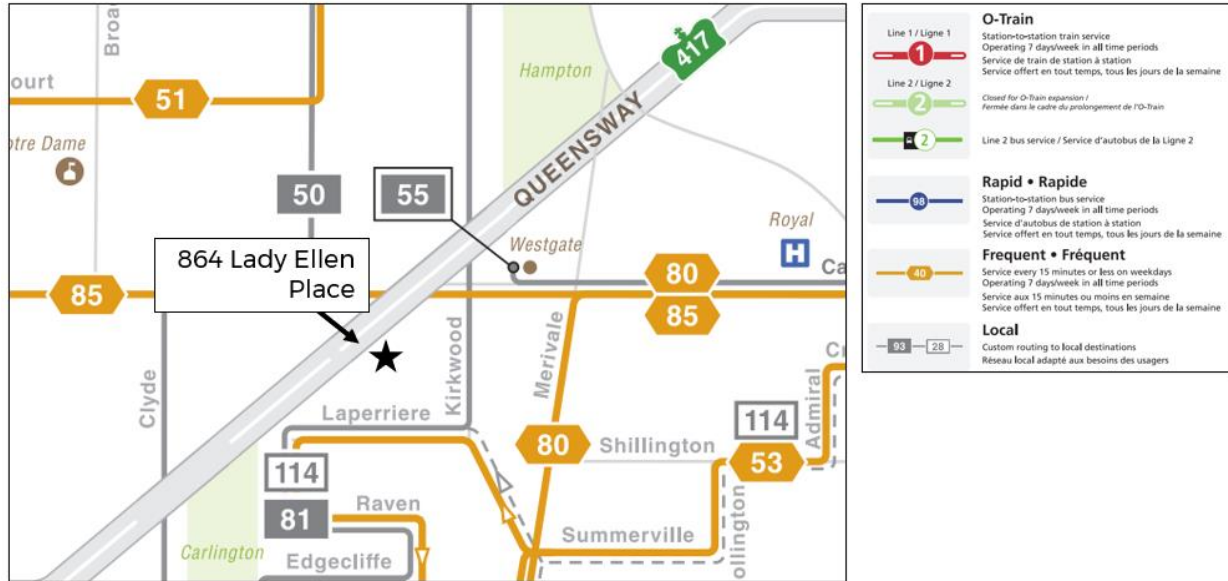
The site is also well-served by existing public transit, as shown in **Figure 2-15**. There are three (3) OC Transpo bus routes that run in close proximity to the site:

- Route 53 (Tunney's Pasture - Carlington);
- Route 81 (Tunney's Pasture - Clyde); and
- Route 114 (Rideau - Carlington).



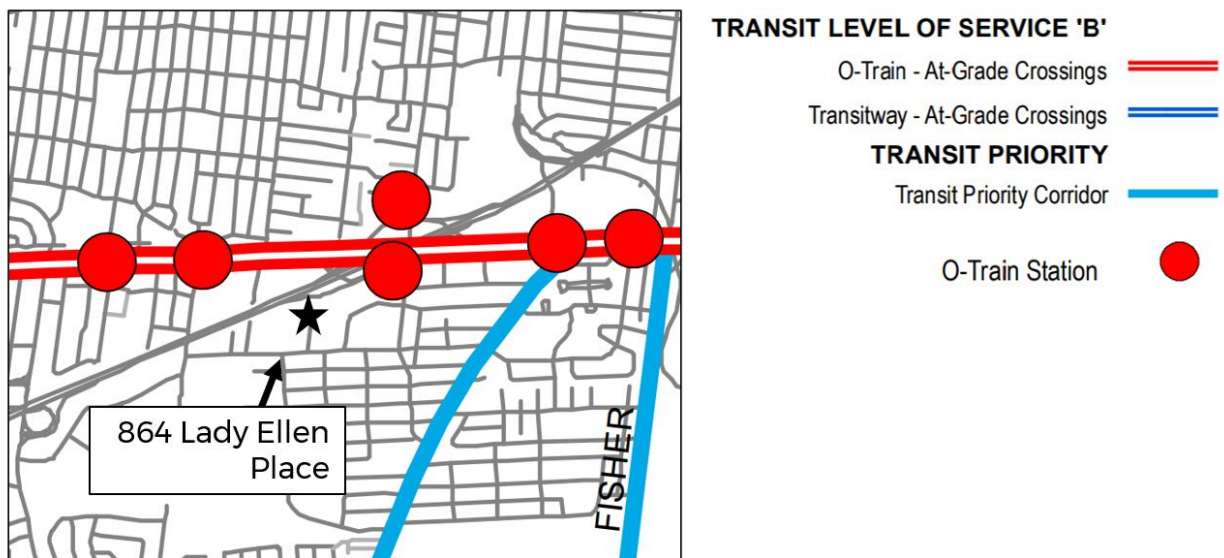
The OC Transpo bus stops that are closest to the site include those at Laperriere / McBride and Laperrière / Larkin.

**Figure 2-15: OC Transpo Network Map (September 4, 2022)**



Per the City's Official Plan, Schedule C2 - Transit Network (Ultimate), the site is in close proximity of the future O-Train Light Rail Transit (LRT) line on Carling Avenue, as shown in **Figure 2-16**.

**Figure 2-16: Schedule C2 - Transit Network (Ultimate) (City of Ottawa Official Plan (Adopted November 24, 2021, and approved by the Ministry of Municipal Affairs and Housing on November 4, 2022 with modifications))**



## 3 The Proposed Development

The proposed development consists of two (2) low-rise self-storage/warehouse buildings, with a proposed total gross floor area of 17,663 m<sup>2</sup> (190,122.95 ft<sup>2</sup>). As shown on the Site Plan, prepared by Architecture49 (dated November 29, 2022), 'Building A' will be four (4) storeys in height and will be located at the east side of the site. The proposed GFA of Building A is 15,913 m<sup>2</sup> (171,292 ft<sup>2</sup>). The single-storey 'Building B' will be located on the west side of the site and has a proposed GFA of 1,750 m<sup>2</sup> (18,844 ft<sup>2</sup>). The existing 2.5-storey office building on the site would be demolished to accommodate the proposed development.

### Self-Storage Warehouse Buildings

Buildings A and B are proposed to accommodate light industrial uses. Building A will contain self-storage units on the upper three (3) storeys, with an ancillary administrative space on the ground floor to support the self-storage use as well as warehouse space. Building B will contain self-storage units only. There will be approximately 5 -7 employees at the new Access Storage facility. With this proposed development, Access Selfstorage Inc. intends to expand upon its successful portfolio of quality storage facilities. With this development, Access Storage intends to expand upon its successful portfolio of quality storage facilities in the Ottawa area.

The primary four-storey Building A is intended to be a prominent feature along Highway 417, a prominent artery of the National Capital Region. The ground floor, which will be occupied by warehouse space will be a muted dark masonry volume that provides a functional podium to the loading bays and multiple entrances, which will serve the tenant from the west elevation and the upper-level self-storage from the south elevation. The upper three (3) storeys of Building A will be cantilevered from the base and its north façade will be aligned to Highway 417. The three-storey volume will be a combination of white gloss and matte aluminum composite panels and curtain wall, contrasting from the base as a seemingly floating volume from the site. The partial glazing along the north elevation will reveal single-loaded storage corridors, which will signal the function of the building for those passing by. A diagonal exit stair will connect the upper levels to the ground floor, not only serving life-safety functions but also acts to reinforce the branding of Access Storage on this key elevation.

### Landscaping and Amenity Areas

Existing vegetation on the site is primarily comprised of trees and low shrubbery) along Lady Ellen Place. As detailed in the Tree Conservation Report, prepared by WSP-Golder (dated December 16, 2022), fifteen (15) trees are proposed to be removed and sixteen (16) trees will be maintained on site. Impacts to the vegetation proposed for retention will be minimal, given that the vegetation is commonly found in areas that have been previously disturbed.

The frontage of the site is defined through new planting beds with a mixture of trees and shrubs, sod, and signage at the main entrance of the proposed buildings, to contribute to an

improved frontage and provide an attractive entry with a strong axial relationship from Lady Ellen Place. A sidewalk is proposed to provide access off to Lady Ellen Place that connects to the overall pedestrian circulation system of the site. A painted crosswalk is proposed to provide a main pedestrian crossing between the two (2) buildings.

The overall landscape treatment around the perimeter of the site includes tree planting with a native grass seed mix to promote carbon sequestration, water infiltration and the establishment of habitat. This includes landscaping along the 417 Highway to screen the parking, loading and waste receptacles. Limited tree planting can occur along the 417 Highway corridor with the overhead Hydro lines present. A rock mulch bed would be used under the canopy of Building A.

### **Parking and Circulation**

The site currently has four (4) vehicular accesses from Lady Ellen Place. The proposed development would maintain the existing 4 accesses to the proposed parking areas and site.

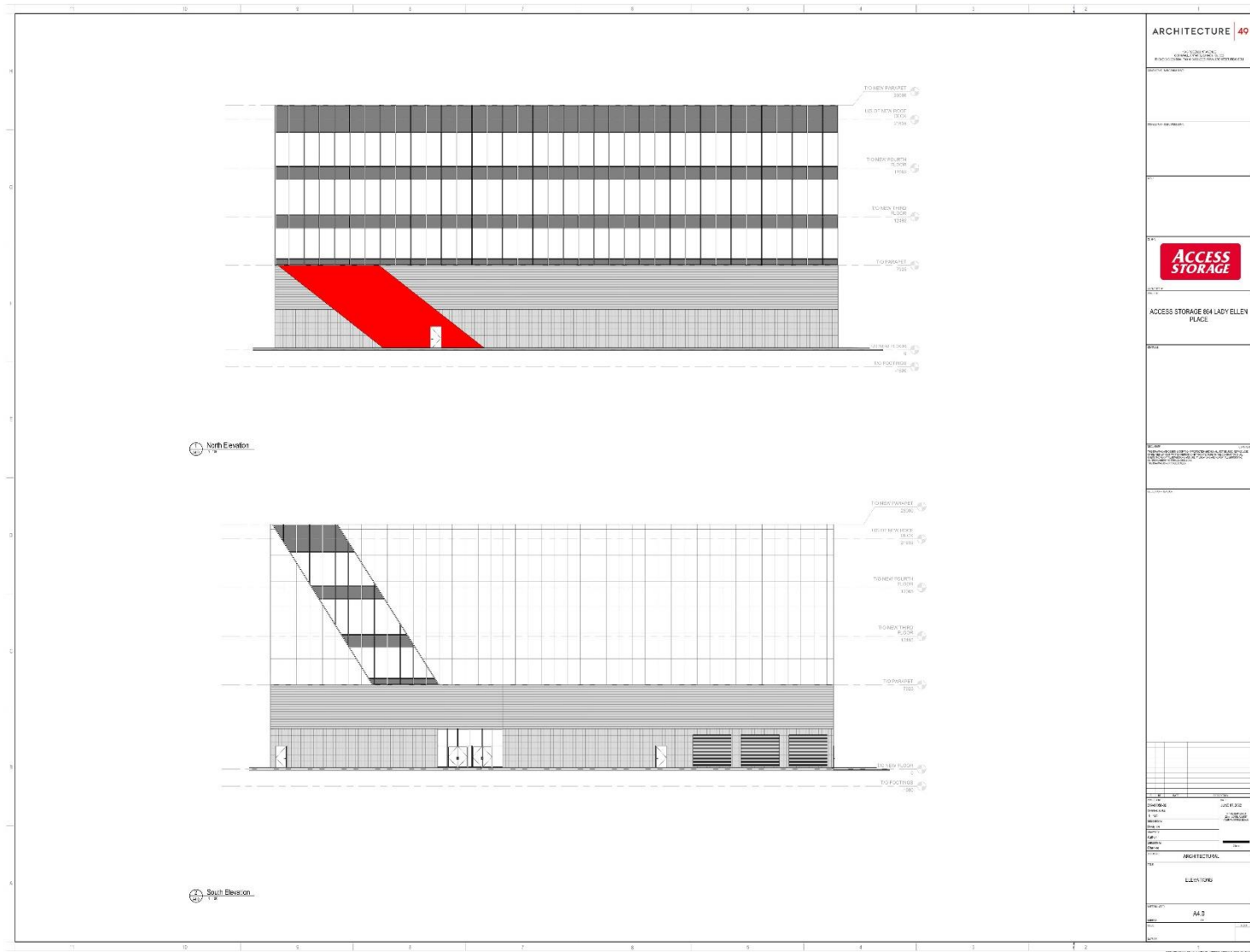
A total of 74 parking spaces are proposed for the site, which meets the requirements of the Zoning By-law. Of these, four (4) parking spaces would be barrier-free, meeting the Type A and Type B, and access aisle requirements established in the Accessibility for Ontarians with Disabilities Act (AODA). These spaces are proposed to be located near the main entrances of both buildings. Visitor parking is not proposed for the site as the proposed use does not require visitor parking, however visitors may use any of the parking areas for the proposed buildings at 864 Lady Ellen Place. Further, exterior covered bicycle parking would be provided by the front entrances of Buildings A and B to encourage active transportation.

As per the Servicing Report, prepared by WSP (dated December 16, 2022) and included with the Site Plan Control application, municipal services currently exist on the site, including water, wastewater and sanitary servicing.

A detailed Site Plan, prepared by Architecture49 (dated November 29, 2022) is shown in **Figure 3-1**, and is also included in Appendix A. Proposed building elevations are shown in **Figure 3-2 to 3-4**. A preliminary rendering of Building A is shown in **Figure 3-5 and 3-6**.



**Figure 3-2: Proposed Building Elevations - Building A (prepared by Architecture49, dated June 17, 2022)**

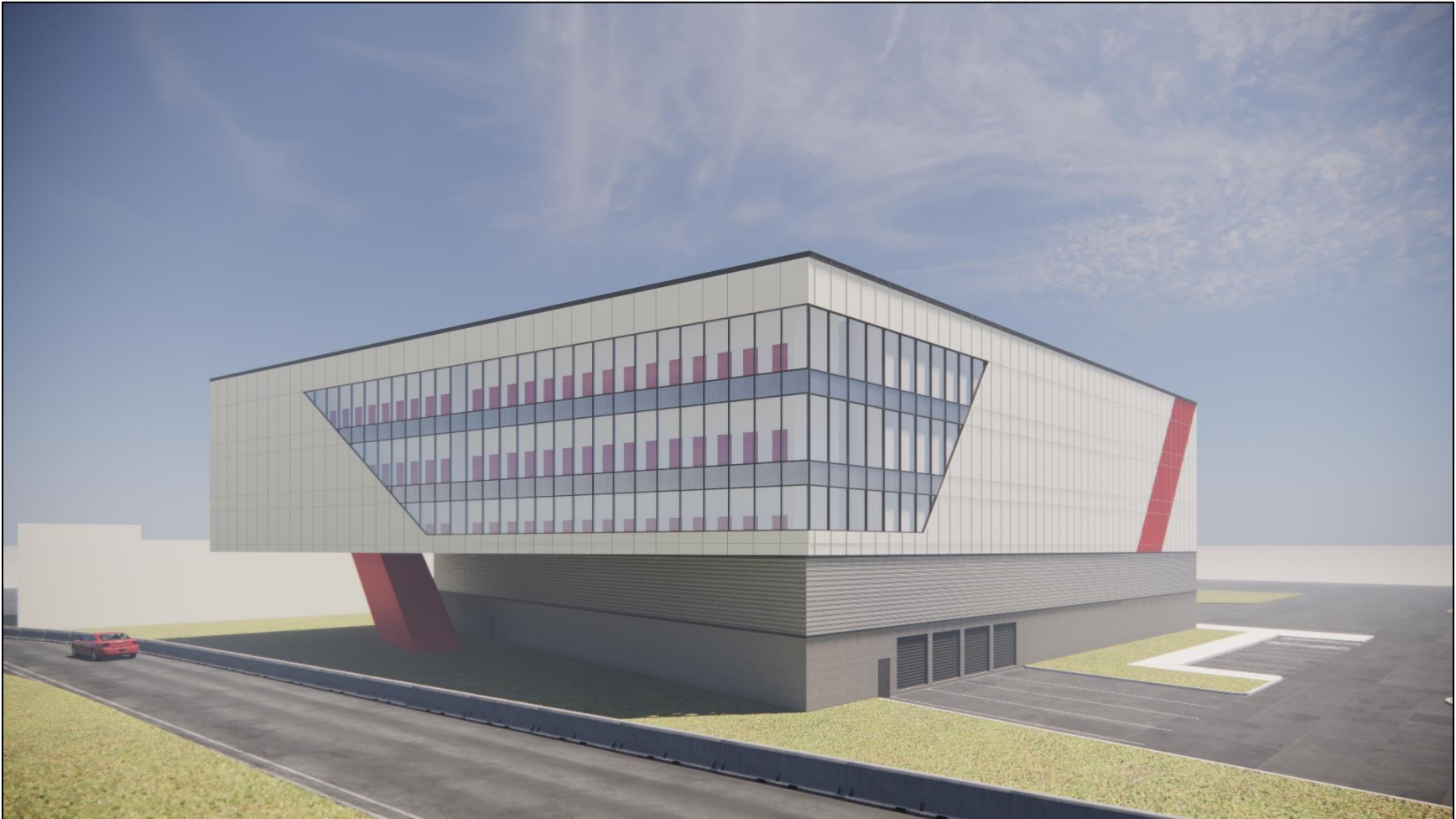








**Figure 3-5: Preliminary Rendering - Building A - Looking southeast (prepared by Architecture49, December 2022)**



**Figure 3-6: Preliminary Rendering - Building A - Looking northeast (prepared by Architecture49, December 2022)**



## 4 Community Engagement and Outreach (Public Consultation Strategy)

The public engagement for the Site Plan Control application will follow the City of Ottawa public consultation process and practises pursuant to the City's [website](#).

Access Selfstorage Inc. has undertaken preliminary community engagement in support of the proposed development. A meeting was held on December 12, 2022 with representatives of the following offices, organizations, and associations to present the proposed development and provide a head's up of the Site Plan Control application submission:

- Carlington Community Association; and
- Local Councillor Riley Brockington's office.

## 5 Policy and Regulatory Framework

This section describes the provincial, and local policy framework that is relevant or applicable to the proposed development of the site, including: the Provincial Policy Statement, 2020, the City of Ottawa Official Plan (Adopted November 2021 and approved by MMAH on November 4, 2022 with modifications), and the City of Ottawa Zoning By-law 2008-250 (Consolidation September 8, 2021).

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### 5.1 Provincial Policy Statement, 2020

The 2020 PPS provides policy direction on matters of provincial interest related to land use planning and development. As a key part of Ontario's policy-led planning system, the PPS sets the policy foundation for regulating development and use of land.

The PPS seeks to strike a balance between the Province's economic, social, and environmental interests through the following:

- Promoting cost-effective development patterns which stimulate economic growth;
- Protecting resources for their economic use and/or environmental benefits; and
- Directing development away from areas where there is a risk to public health and safety or of property damage.

Section 1.1 Managing and Directing Land Use to Achieve Efficient and Resilient Development and Land Use Patterns includes policies to sustain healthy, liveable, and safe communities by promoting efficient and cost-effective development and land use patterns and standards, accommodating an appropriate affordable and market-based range and mix of residential

types, including affordable housing, and other uses to meet long-term needs, and improving accessibility for persons with disabilities and older persons by addressing land use barriers, and promoting cost-effective development patterns and standards, among other considerations.

Policy 1.1.3.1 directs that settlement areas shall be the focus of growth and development. Policy 1.1.3.2 states that land use patterns within settlement areas shall be based on densities and a mix of land uses which:

- a) efficiently use land and resources;
- b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;
- c) minimize negative impacts to air quality and climate change, and promote energy efficiency;
- d) prepare for the impacts of a changing climate;

Policy 1.1.3.2 further states that land use patterns within settlement areas shall be based on a range of uses and opportunities for intensification and redevelopment in accordance with the criteria in Policy 1.1.3.3, where this can be accommodated.

Policy 1.3.1 states that planning authorities shall promote economic development and competitiveness by:

- a) providing for an appropriate mix and range of employment, institutional and broader mixed uses to meet long-term needs;
- b) providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses;
- e) ensuring the necessary infrastructure is provided to support current and projected needs.

Policy 1.3.2.1 states that planning authorities shall plan for, protect and preserve employment areas for current and future uses and ensure that the necessary infrastructure is provided to support current and projected needs.

Policy 1.3.2.3 states that within employment areas planned for industrial or manufacturing uses, planning authorities shall prohibit residential uses and prohibit or limit other sensitive land uses that are not ancillary to the primary employment uses in order to maintain land use compatibility.

Policy 1.8.1 provides that planning authorities shall support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and preparing for the impacts of a changing climate through land use and development patterns which:



- a) focus major employment, commercial and other travel-intensive land uses on sites which are well served by transit where this exists or is to be developed, or designing these facilitate the establishment of transit in the future;

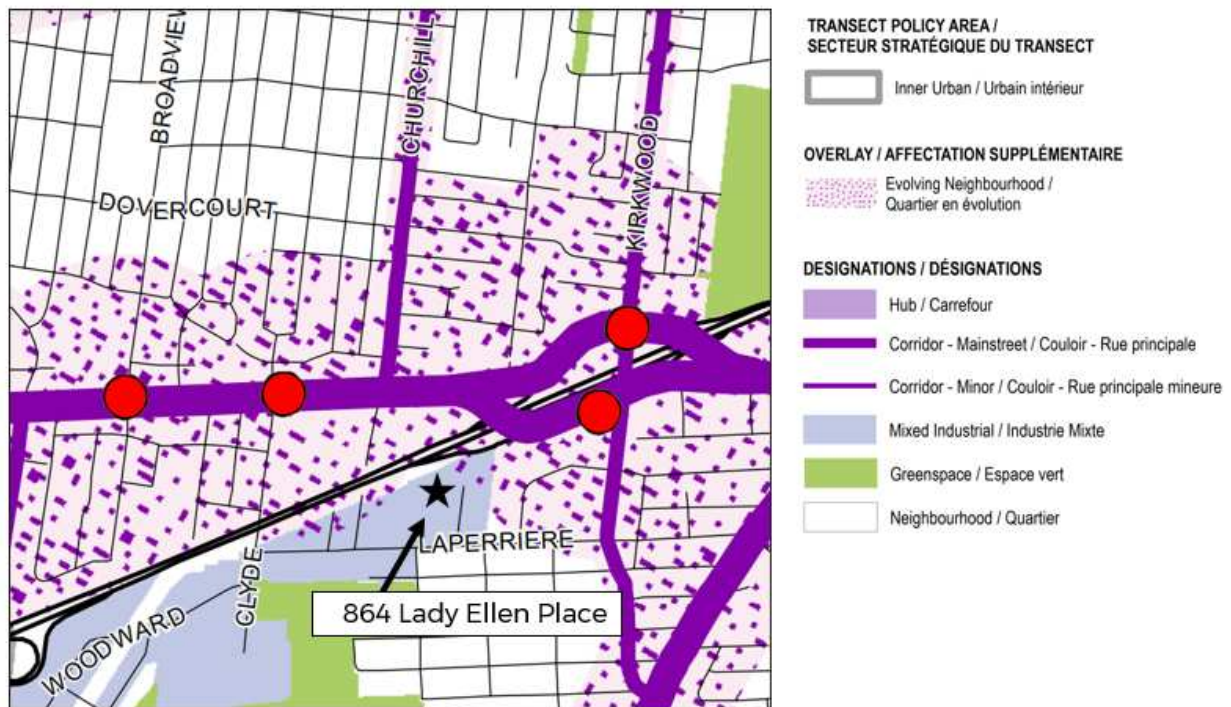
**The proposed development is consistent with the 2020 PPS, as it represents the protection and intensification of an employment area and is not located in close proximity to sensitive land uses. It provides for economic activities and ancillary uses that will support the surrounding industrial/employment area. The new development will result in approximately 5 -7 new jobs.**

## 5.2 City of Ottawa Official Plan (Adopted November 24, 2021 and approved by MMAH on November 4, 2022 with modifications)

Ottawa City Council adopted the City of Ottawa Official Plan (“OP”) on November 24, 2021 and the OP was approved by the Ministry of Municipal Affairs and Housing on November 4, 2022 with modifications. The OP sets the vision for how the city will grow and develop to 2046.

In the OP, the site is located within the Inner Urban Transect as per Schedule A Transect Areas. It is designated as Mixed Industrial and is subject to the Evolving Neighbourhood Overlay as per Schedule B2 Inner Urban Transect, as shown in **Figure 5-1**.

**Figure 5-1: Schedule B2 - Inner Urban Transect (Excerpt) (City of Ottawa Official Plan (Adopted November 24, 2021, and approved by the Ministry of Municipal Affairs and Housing on November 4, 2022 with modifications))**



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## 5.2.1 Strategic Directions

Section 2 of the OP, Strategic Directions, outlines the broad policies that will govern the growth and development of Ottawa over the next 25 years.

**Section 2.1 – The Big Policy Moves** outlines five broad policy directions to shape Ottawa as a liveable, mid-sized city.

- **Big Policy Move 1:** Achieve, by the end of the planning period, more growth by intensification than by greenfield development.
- **Big Policy Move 2:** By 2046, the majority of trips in the city will be made by sustainable transportation.
- **Big Policy Move 3:** Improve our sophistication in urban and community design and put this knowledge to the service of good urbanism at all scales, from the largest to the very small.
- **Big Policy Move 4:** Embed environmental, climate and health resiliency and energy into the framework of planning policies.
- **Big Policy Move 5:** Embed economic development into the framework of planning policies.

**Section 2.2 – Cross Cutting Issues** identifies six policy direction categories that span a range of topics related to achieving the City of Ottawa’s goal of becoming a livable City. The relevant cross-cutting issues are described as follows.

**Section 2.2.1** directs residential growth within the built-up urban area towards 15-minute neighbourhoods. This direction supports the creation of 15-minute neighbourhoods by locating daily and weekly needs in proximity to Hubs, Corridors and surrounding Neighbourhoods. Promoting intensification is a key part of achieving this goal.

**Section 2.2.3** promotes the development of a compact urban form with a mix of land uses and housing options to ensure both energy efficient and sustainable patterns of development are created.

**Section 2.2.4** is intended to support healthy and inclusive communities through the development of walkable 15-minute neighbourhoods that feature a range of housing options, supporting services and amenities. 15-minute neighbourhoods support cultural expression and community identity as well as ensuring access to goods and services within peoples’ communities using active transportation modes.



The proposed development supports the Strategic Directions of the OP by accommodating growth and new employment uses within the urban area, through the development of warehouse storage buildings in an existing mixed industrial area. The proposed development will contribute to employment opportunities within the area and the broader City of Ottawa.

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## 5.2.2 Growth Management Framework

Section 3 of the OP contains the City's Growth Management Framework, which ensures that there are sufficient development opportunities and a range of choices of where to locate growth and how to design it. The Growth Management Framework supports growth that increases sustainable mode shares and uses existing infrastructure efficiently while reducing greenhouse gas emissions.

The intent of the Growth Management Framework policies is to:

- a) "provide an appropriate range and mix of housing that considers the geographic distribution of new dwelling types and/or sizes to 2046;
- b) provide a transportation network that prioritizes sustainable modes over private vehicles, based on the opportunities for mode shifts presented by each transect area context;
- c) prioritize the location of residential growth to areas with existing municipal infrastructure, including piped services, rapid transit, neighbourhood facilities and a diversity of commercial services;
- d) reduce greenhouse gas emissions in the development and building sectors and in the transportation network; and
- e) establish a growth management framework that maintains a greater amount of population and employment inside the Greenbelt than outside the Greenbelt".

Employment growth is anticipated in the City, with the Downtown Core remaining the primary employment hub in Ottawa and other areas such as the Inner Urban Transect will be significant areas of employment growth moving forward.

**Section 3.2 - Support Intensification**, Policy 6) states that the majority of employment growth and intensification are to be focused in areas including the Mixed Industrial designation as shown on the B-Series Schedules of the OP.

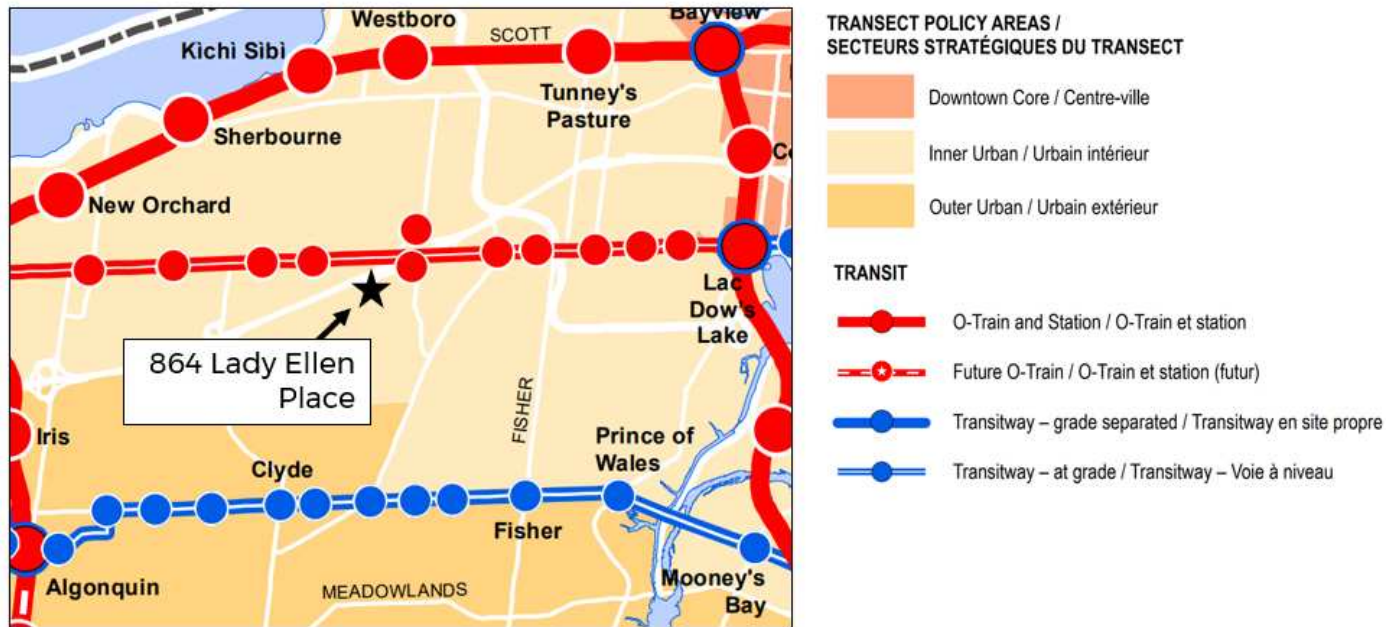
**Section 3.5 - Meet employment needs** includes policies that address employment growth shall occur and where employment areas as defined by the PPS, 2020 are to be disaggregated. Policy 3.5(3) directs that business parks containing a mix of industrial and non-industrial uses, such as small-scale office and service-oriented locations are located in lands designated as Mixed Industrial.

The proposed development conforms with the Growth Management Framework policies by serving as a new light industrial use within the Mixed Industrial designation in the inner urban area. The self-storage / warehouse use will be located in an area already containing similar light industrial and office uses and will serve the broader community.

### 5.2.3 Transect Policy Area

In the OP, the site is located within the “Inner Urban” Transect, as shown on Schedule A – Transect Policy Areas (**Figure 5-2**). The OP states that the Inner Urban Transect includes the pre-World War II neighbourhoods that immediately surround the Downtown Core, and the earliest post-World War II areas directly adjacent to them. Generally, the older neighbourhoods reflect the urban built form characteristics described in the OP, while the post-war neighbourhoods reflect suburban characteristics.

**Figure 5-2: Schedule A - Transect Policy Areas (Excerpt) (City of Ottawa Official Plan (Adopted November 24, 2021, and approved by the Ministry of Municipal Affairs and Housing on November 4, 2022 with modifications))**



The goals of the Inner Urban Transect designation include establishing an urban pattern of built form, site design and mix of uses. Prioritizing walking, cycling and transit within, and to and from, the Inner Urban Transect.

The proposed development conforms to the Inner Urban Transect policies as the proposed employment use will support the development of the Inner Urban Transect as a mixed-use environment and will contribute to the existing and planned mature light industrial area.

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## 5.2.4 Land Use Designation

As per Schedule B2 – Inner Urban Transect (**Figure 5-1**), the site is designated as Mixed Industrial. Section 6.5 – Mixed Industrial states that the Mixed Industrial designation includes areas that are clusters of economic activity and include a broad range of less impactful non-residential uses than that of the Industrial designation. It is intended that Mixed Industrial areas provide a transition between Industrial and Logistics lands and the residential, mixed-use Neighbourhood, Hubs, and Corridor designations. Section 6.5 directs that Mixed Industrial areas will be characterized by a broad mix of uses, including small-scale office, light industrial, wholesale, small contractors, small-scale commercial service uses, and non-residential sensitive uses.

Section 6.5.1 states that the Mixed Industrial designation shall preserve land for uses that require a business park environment. Policy 6.5.1(2)(a) permits low-impact light industrial uses, including light manufacturing, warehousing, distribution, and storage. Therefore, the proposed warehouse development is a permitted use.

**The proposed development conforms to the Mixed Industrial policies of the OP. The proposed warehouse use is permitted in the Mixed Industrial designation. In addition, the proposed development represents a low-impact light industrial use that will contribute to employment opportunities in the City. The proposed development conforms to the surrounding context as the uses in the immediate vicinity of the site can be primarily characterized by light industrial and low-density office uses.**

**The proposed development conforms to the policies of the Official Plan policies including Strategic Directions, Growth Management, Transect Areas, and Land Use Designation policies.**

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## 5.2.5 Road Classification and Rights-of-Way

As per Section 4.1.7 of the OP, the City may acquire land for rights of way or the widening of rights of way through conditions of approval for a plan of subdivision, severance (severed and retained parcels), site plan or a plan of condominium.

Schedule C16, Table 1 of the OP sets forth the ROW widths that the City may require. Table 1 contains specific ROW protection requirements for arterial streets. **There is no ROW protection for Lady Ellen Place.**

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## 5.3 Design Brief

The Planning Act gives municipalities the authority to require an Urban Design Brief to be prepared. Under Section 34(10.2) and Section 41(4) of the Planning Act, Council has the authority to request other information or material that the authority needs to evaluate and decide on an application. Section 11.1 of the OP sets out the information and/or reports that may be required in support of development applications, which includes an Urban Design Brief. As a part of the Site Plan Control application, the City has requested a scoped Urban Design Brief be included.

The City of Ottawa has a framework in place to guide urban design in accordance with a series of policies and guidelines documents. The following sections identify the urban design policies which are applicable to the site, as well as responses to comments provided by Planning and Urban Design staff at the pre-application consultation meeting held on November 3, 2022.

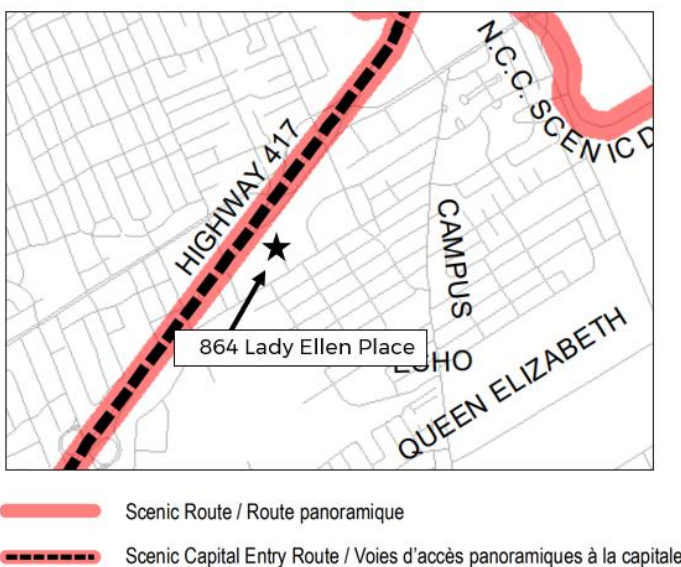
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### 5.3.1 Section 4.6 – Urban Design

Section 4.6 contains policies related to Urban Design that apply to development sites in the City of Ottawa. Policy 4.6.1 of the OP states that Design Priority Areas (DPAs) are identified to promote design excellence through the development review process. The site is not within a Design Priority as per Schedule C7-A – Design Priority Areas.

The site abuts Highway 417, which is identified as a Scenic Capital Entry Route as per Schedule C13 – Scenic Capital Routes, as shown in **Figure 5-3**.

**Figure 5-3: Schedule C13 - Scenic Routes (Excerpt) (City of Ottawa Official Plan (Adopted November 24, 2021, and approved by the Ministry of Municipal Affairs and Housing on November 4, 2022 with modifications))**



Policy 4.6.2.4 states that development abutting Scenic Routes shall contribute to conserving or creating a desirable context through:

- a) “Protecting the opportunity to view natural and cultural heritage features;
- b) Preserving and restoring landscaping, including but not limited to distinctive trees and vegetation along the right of way;
- c) Orienting buildings towards the Scenic Route and providing direct pedestrian access, where appropriate; and
- d) Providing screening by way of opaque fencing or landscape buffers to hide surface parking lots or outside storage; and
- e) Managing the intensity and spill-over of lighting on adjacent parcels”.

Additionally, where Scenic Routes are also identified as Scenic Capital Entry Routes on Schedule C13, Policy 4.6.2.5 directs that development should also:

- a) “Enhance the opportunity for views and vistas towards national symbols, cultural landscapes and other features of the Capital; and
- b) Contribute to the image of Ottawa as the Capital city by providing landscape and aesthetic improvements, including buildings that enhance the urban character, where possible”.

Policy 4.6.6.6 states that low-rise buildings shall be designed to respond to context, transect area policies, and shall incorporate soft landscaping, main entrances at-grade, and front porches or balconies where appropriate. Buildings shall be integrated to complement the surrounding context.

**The proposed development conforms to the applicable Urban Design policies in the OP. All main building entrances will be provided at-grade. The proposed Building A will be visible from Highway 417, a Capital Scenic Route. As such, as shown in the Exterior Elevations and preliminary rendering, the proposed building design and materiality for Building A has been carefully considered in order to improve the urban landscape along this corridor. Additionally, Building A is oriented towards Highway 417, with coloured panels and windows visible from the road. Landscaped areas and tree plantings are proposed throughout the site to provide screening from neighbouring properties.**

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### **5.3.2 Response to Preliminary Planning and Urban Design Comments**

Comments were provided by City Planning and Urban Design staff at the pre-application consultation meeting held on November 3, 2022. These comments are summarized below, and a response has been provided indicating where the proposed development addresses staff's comments.

## Planning

- 1. Please consider providing a landscaping buffer between the large asphalted area serving the loading spaces and the northern property line to reduce unneeded asphalt.**
  - A landscape buffer has been added and is shown on the Site Plan.
- 2. If possible, provide small tree plantings within the front planting bed where it does not conflict with proposed signage for the building.**
  - Small trees have been added in the planting beds in front of both Buildings A and B.
- 3. Indicate on the Site Plan the pathway connections of the side and rear doors with pedestrian pathways around the parking lot.**
  - The side and rear doors are for emergency egress only. Concrete pads will be added once the door locations have been finalized in detailed design.
- 4. Ensure that anticipated pedestrian crossings through the parking lot are considered and protected for pedestrian safety.**
  - The site design includes a painted crosswalk at the pedestrian crossing on site.

## Urban Design

- 1. Note that the adjacent portion of Highway 417 is designated as a Scenic Capital Entry Route in the Official Plan (Schedule C13). As such, adjacent development should contribute to the image of Ottawa as the Capital city through landscape and aesthetic improvements.**
  - **Ensure that the architecture properly responds to views from the highway by ensuring visual interest, providing animated facades, incorporating glazing, accent lighting, etc.**
    - The landscape design focuses on providing framed views from the highway towards important views of the new building facades. Key visual elements of the façade of the new buildings are directed towards the view from the highway.
  - **Minimize the visual impact of loading docks, garbage enclosures, utilities, etc.**
    - The visual impact of the loading docks and waste receptacles will be minimized through maintaining the existing grade of the site where the height of the highway provides a natural visual barrier. Additionally, a landscape buffer has also been added.
- 2. Please connect the internal pedestrian pathways to the street.**
  - A sidewalk connection has been added as per the City's request.
- 3. Explore the possibility of reducing the amount of paved surface in the rear of the site. If this is possible, please use this opportunity to plant additional trees.**
  - Tree planting in this area is challenged by the existing location of hydro lines. Additional landscape buffer areas have been added as per the City's request.



**4. An Urban Design Brief is required as a part of your submission. This may be combined with your Planning Rationale report.**

- Noted. The Scoped Design Brief is incorporated into this Planning Rationale report - please refer to Sections 2, 3, and 4. The Site Plan, Landscape Plan, and Elevations are included as Appendices to this Report.

**5. This application is not subject to review by the Urban Design Review Panel.**

- Acknowledged.

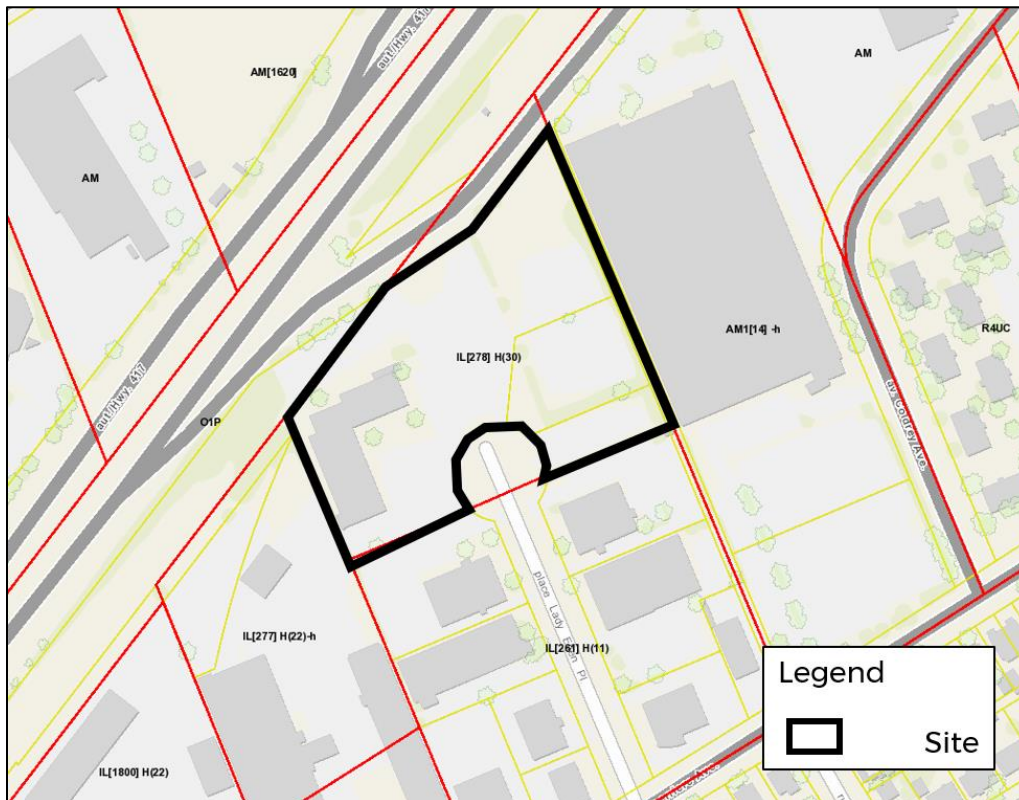
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## 5.4 City of Ottawa Comprehensive Zoning By-law 2008-250 (Consolidation September 8, 2021)

Under the City of Ottawa Comprehensive Zoning By-law 2008-250 (Consolidation September 8, 2021), the site is zoned **Light Industrial, Urban Exception 278, Height Limit 30 (IL[278] H(30))**, as shown in **Figure 5-4**.

Properties which are immediately adjacent to the site are zoned Parks and Open Space, Subzone P (O1P), Light Industrial, Urban Exception 261, Height Limit 11 (IL[261] H(11)), Light Industrial, Urban Exception 277, Height Limit 22 with Holding Symbol (IL[277] H(22)-h), and Arterial Mainstreet Subzone 1, Urban Exception 14 with Holding Symbol (AM[14] -h).

**Figure 5-4: Current Site Zoning (geoOttawa, 2022)**



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## 5.4.1 Zoning Provisions

The general purpose of the IL Zone is to:

- permit a wide range of low impact light industrial uses, as well as office and office-type uses in a campus-like industrial park setting, in accordance with the Employment Area designation of the Official Plan or, the General Urban Area designation where applicable;
- allow in certain Employment Areas or General Urban Areas, a variety of complementary uses such as recreational, health and fitness uses and service commercial (e.g. convenience store, personal service business, restaurant, automobile service station and gas bar), occupying small sites on individual pads or in groupings as part of a small plaza, to serve the employees of the Employment or General Urban Area, the general public in the immediate vicinity, and passing traffic;
- prohibit retail uses in areas designated as Employment Area but allow limited sample and showroom space that is secondary and subordinate to the primary use of buildings for the manufacturing or warehousing of the product;
- prohibit uses which are likely to generate noise, fumes, odours, or are hazardous or obnoxious; and
- provide development standards that would ensure compatibility between uses and would minimize the negative impact of the uses on adjacent non-industrial areas.

**The IL Zone permits mixed office, service, and light industrial uses, including warehouses. Per Section 54 - Definitions, a Warehouse means: “a building used for the storage and distribution of goods and equipment including self-storage units and mini-warehouses and may include one accessory dwelling unit for a facility manager.”**

Urban Exception 278 applies to the site and includes the following provisions:

- Required parking is permitted to be located off-site provided that it is located within the IL[278] H(30) zone; and
- A yard that abuts the zone boundary separating the IL[278] H(30) zone from the adjacent IL2[277]H(22)-h zone may be reduced to 3.6 m.

The maximum permitted building height for the site is 30 metres as per its current site zoning (H(30)).

Section 93 of the Zoning By-law contains provisions which permit the application of one lot for zoning purposes in certain circumstances. Section 93 provides that a group of occupancies in an AM, GM, LC, MC, MD, IG, IH, IL, IP, or RC Zone shall be considered as one lot for the purposes of applying zoning provisions and regulations provided that that they:

- a) are designed, developed, and managed, including site access and infrastructure servicing, as a unit by a single owner, group of owners, or tenants acting in collaboration;



- b) are made up entirely of permitted or lawfully non-conforming uses on the site, and has either:
  - i. a common parking lot or parking garage of combination thereof; or
  - ii. a group of parking lots or parking garages or a combination thereof which are managed as a unit by the same owner, owners or tenants of the occupancies required in clause (a) above, and are on the same lot or lots as the occupancies required in clause above.

The site at 864 Lady Ellen Place is being designed, developed, and managed by Access Selfstorage Inc., **As such, “one lot for zoning purposes” applies to the site as it is located in an IL Zone.**

Based on the Site Plan, prepared by Architecture49 (dated November 29, 2022), **Table 5-1** provides a detailed compliance analysis of how the proposed development meets the Zoning By-law provisions for the IL Zone. As discussed above, “one lot for zoning purposes” will apply for the evaluation of the proposed development against the zoning provisions of the IL Zone.

**Table 5-1: Zoning Compliance for Light Industrial, Urban Exception 278, Height Limit 30 (IL[278] H(30))**

Zoning Provision	Requirement - (Warehouse)	Calculation (if applicable)	Compliance (Yes or No)
<b>Zone Requirement - IL Zone (Table 203)</b>			
<b>Minimum lot area - Sec. 203(3), Table 203(a)</b>	2,000 m <sup>2</sup>	N/A	Yes - 13,575.6 m <sup>2</sup>
<b>Minimum lot width - Sec. 203(3), Table 203(b)</b>	No minimum	N/A	Yes - No requirement for minimum lot width.
<b>Maximum lot coverage - Sec. 203(3), Table 203(c)</b>	65%	Proposed building ground floor area:  <b>Building A:</b> 3,978.25 m <sup>2</sup> <b>Building B:</b> 1,750 m <sup>2</sup> Total = 5,725.25 m <sup>2</sup>  (5,725.25 m <sup>2</sup> / 13,575.6 m <sup>2</sup> ) x 100% = 42.19%	Yes - 42%
<b>Minimum front and corner side yard setback- Sec. 203(3), Table 203(d)</b>	7.5 m	N/A	<b>Building A:</b> Yes - 7.5 m  <b>Building B:</b> Yes - 7.5 m

<b>Zoning Provision</b>	<b>Requirement - (Warehouse)</b>	<b>Calculation (if applicable)</b>	<b>Compliance (Yes or No)</b>
<b>Part 15 - Exceptions - Urban Exception 278</b>	A yard that abuts the zone boundary separating the IL[278] H(30) zone from the adjacent IL2[277]H(22)-h zone may be reduced to 3.6 m.	N/A	N/A - Building B is setback 7.5 m from the adjacent IL2[277]H(22)-h zone.
<b>Minimum interior side yard setback- Sec. 203(3), Table 203(e)</b>	(ii) all other cases: 7.5 m		<b>Building A:</b> Yes - 15 m  <b>Building B:</b> Yes - 7.5 m
<b>Minimum rear side yard setback- Sec. 203(3), Table 203(f)</b>	(ii) abutting hydro or railway right-of-way: 3.5 m  (iii) all other cases: 7.5 m	N/A	Yes - 8.88 m
<b>Maximum floor space index- Sec. 203(3), Table 203(g)</b>	2	<b>Building A GFA:</b> 15,913 m <sup>2</sup>  <b>Building B GFA:</b> 1,750 m <sup>2</sup>  Total GFA = 17,663 m <sup>2</sup>  <b>FSI:</b> = 17,663 m <sup>2</sup> / 13,575.6 m <sup>2</sup> = 1.30	Yes - FSI of 1.30
<b>Maximum building height - Sec. 203(3), Table 203(h)</b>	(i) within 20 metres of a residential zone: 11 m  (ii) all other cases: 18 m	N/A	N/A - Maximum building height of 30 m applies - H(30).  <b>Building A:</b> Yes - 23 m  <b>Building B:</b> Yes - 8 m
<b>Minimum width of landscaped area - Sec. 203(3), Table 203(i)</b>	(iii) all other cases: No minimum	N/A	Yes - landscaped areas and tree plantings are proposed around the perimeter of the site.

## 5.4.2 Permitted Projections Above the Height Limit and Permitted Projections into Required Yards Provisions

The Zoning By-law contains provisions for permitted projections above the height limit and permitted projections into required yards, as set out in **Table 5-2**. There are no proposed projections for the proposed building.

**Table 5-2: Provisions for Permitted Projections Above Height Limit / into Required Yards**

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
<b>Permitted projections above the height limit - Sec. 64</b>	Except in the case of buildings or structures located within the area shown on Schedules 11 to 88 (Central Area Height Schedules), the maximum height limits do not apply to the structures listed below or to any other similar structures that may require a height in excess of maximum height limits in order to serve their intended purpose, unless otherwise specified in the by-law and provided these structures are erected only to such height or area as is necessary to accomplish the purpose they are to serve and that is necessary to operate effectively and safely: <ul style="list-style-type: none"> <li>– Mechanical and service equipment penthouse, elevator or stairway penthouse</li> </ul>	N/A	N/A
<b>Chimney, chimney box and fireplace box, Sec. 65, Table 65(1)</b>	1 m, but not closer than 0.6 m to a lot line	N/A	N/A
<b>Eaves, eaves-troughs and gutters, Sec. 65, Table 65(2)</b>	1 m, but not closer than 0.3 m to a lot line	N/A	N/A

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
<b>Ornamental elements such as sills, belt courses, cornices, parapets and pilasters, Sec. 65, Table 65(3)</b>	0.6 m, but not closer than 0.6 m to a lot line	N/A	N/A
<b>Canopies and awnings, Sec. 65, Table 65(4)</b>	(a) Residential use buildings other than low-rise apartment dwellings and mid-high rise apartment dwellings: 1.8 m, but not closer than 0.6 m to a lot line	N/A	N/A
<b>Fire escapes, open stairways, stoop, landing, steps and ramps - Sec. 65, Table 65(5)</b>	(b) Other features: i) where at or below the floor level of the first floor: 1. in the case of the interior side yard or rear yard: no limit, and  ii) other cases: 1. In the case of any yard: 1.5m, but not closer than 1 m to a lot line	N/A	N/A
<b>Covered or uncovered balcony, porch, deck, platform and verandah, with a maximum of two enclosed sides, excluding those covered by canopies and awnings, Sec. 65, Table 65(6)</b>	a) uncovered, unenclosed features such as decks or platforms where the walking surface is not higher than 0.6 m above adjacent grade: (i) in the interior side yard and rear yard: no limit (ii) in the front yard and corner side yard - the greater of 2m or 50% of the required front yard or corner side yard, but no closer than 1m to a property line	N/A	N/A

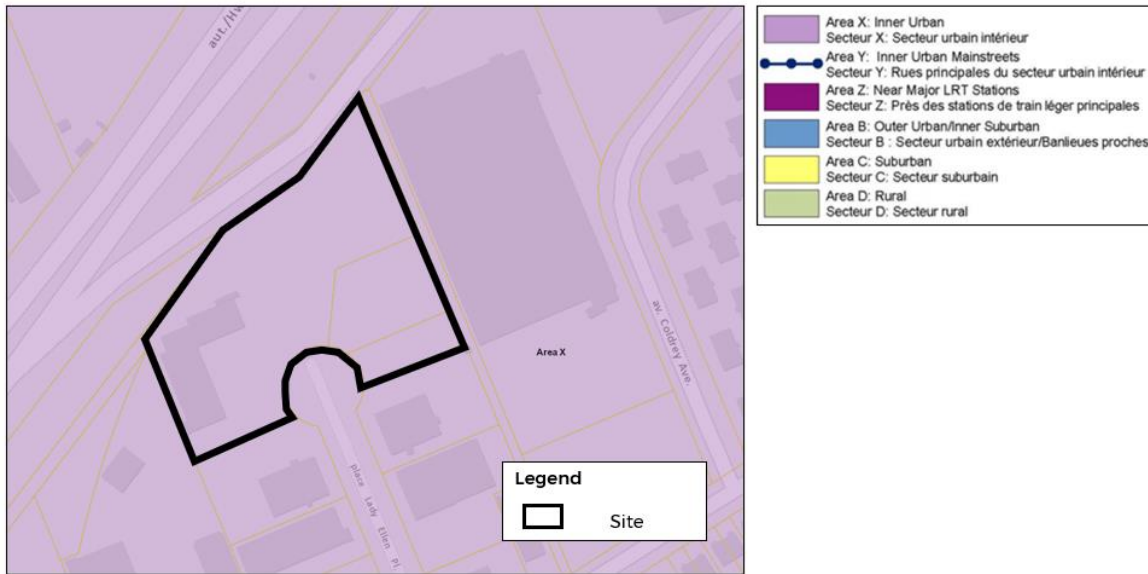
Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
	c) In all other cases: 2 metres, but no closer than 1 metre from any lot line.		
<b>Bay window where window faces a lot line, Sec. 65, Table 65(7)</b>	1 m, but not closer than 1.2 m from a lot line	N/A	N/A
<b>Air conditioner condenser, heat pump or similar equipment, Sec. 65, Table 65(8)</b>	1 m, but not closer to a lot line than 0.3 m, and may not be located in a front yard or a corner side yard	N/A	N/A
<b>Exit stairs – Sec. 161(13)(m)</b>	Exit stairs providing required egress under the Building Code may project a maximum of 2.2 metres into the required rear yard.	N/A	N/A



### 5.4.3 Parking Provisions

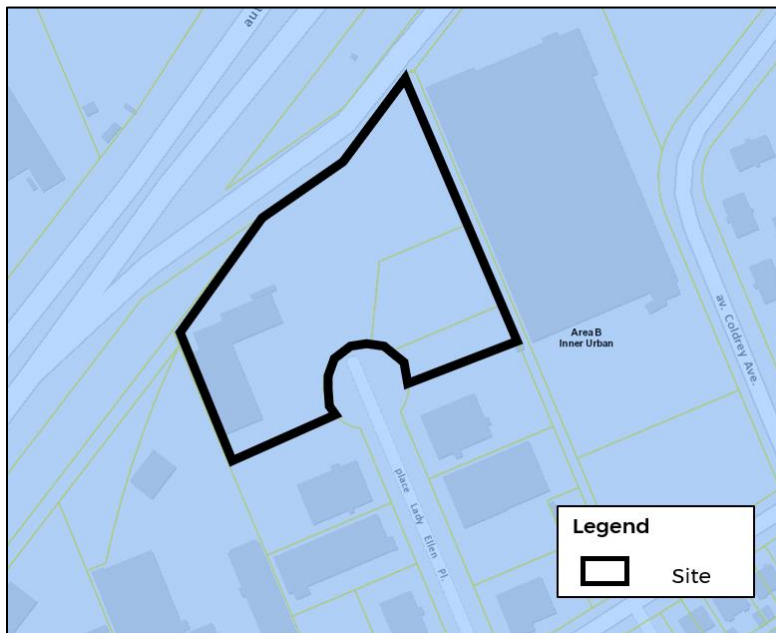
The site is within Area X: Inner Urban on Zoning By-law Schedule 1A, as illustrated in **Figure 5-5**.

**Figure 5-5: Schedule 1A - Areas for Minimum Parking Space Requirements, City of Ottawa Zoning By-law (geoOttawa, 2022)**



For the purposes of applying the provisions of Section 103 of the Zoning By-law, the site is located in Area B of the Zoning By-law, as illustrated in **Figure 5-6**.

**Figure 5-6: Schedule 1 - Boundaries of the Central, Inner Urban, Suburban and Rural Areas (Excerpt), City of Ottawa Zoning By-law (geoOttawa, 2022)**



The Zoning By-law contains provisions for parking, as set out in **Table 5-3**.

**Table 5-3: Parking and Loading Space Provisions**

<b>Zoning Provision</b>	<b>Requirement</b>	<b>Calculation (if applicable)</b>	<b>Compliance (Yes or No)</b>
<b>Schedule 1A</b>	The Site is designated as Area X “Inner Urban” in Schedule 1A of City of Ottawa Zoning By-law 2008-250.	N/A	N/A
<b>Schedule 2A and 2B</b>	The Subject Site is not within 300 metres of a rapid transit station in Schedule 2A or Schedule 2B.	N/A	N/A
<b>Minimum Parking Space Rates – Sec. 101, Table 101</b>	<p><b>Area X (Schedule 1A):</b></p> <p><b>Warehouse:</b> 0.4 per 100 m<sup>2</sup> for the first 5000 m<sup>2</sup> of gross floor area; 0.4 per 100 m<sup>2</sup> above 5000 m<sup>2</sup> of gross floor area</p>	<p><b>Proposed GFA:</b> 17,663 m<sup>2</sup></p> <p><b>Required Parking:</b> 0.4 (5,000 m<sup>2</sup> / 100 m<sup>2</sup>) = 20 spaces + 0.4 (12.663 m<sup>2</sup> / 100 m<sup>2</sup>) = 50.65 spaces</p> <p>20 spaces + 50.65 spaces = 70.65 spaces = 71 spaces (rounded)</p> <p><b>Total Parking Required:</b> 71 spaces</p>	Yes – 74 parking spaces.
<b>Minimum visitor parking spaces – Sec. 102, Table 102</b>	None required for the proposed use.	N/A	Yes – 0 spaces provided; however visitors may use parking spaces for the proposed buildings.

Zoning Provision		Requirement	Calculation (if applicable)	Compliance (Yes or No)
<b>Dimension requirements for a motor vehicle parking space - Sec. 106(1)</b>	<b>(a) Width</b>	Minimum width of 2.6 m; maximum width of 3.1 m	N/A	Yes - Parking spaces are proposed to have a minimum width of 2.6 m and a minimum length of 6 m.
	<b>(b) Length</b>	5.2 m	N/A	
<b>Minimum Driveway Width to Parking Lot - Sec. 107(1)</b>		(ii) in the case of a parking lot, 6.0 metres for a double traffic lane;	N/A	Yes - 7 m for a double traffic lane
<b>Minimum Required Aisle Width - Table 107</b>		6.7 m for angle of parking that is 90 degrees	N/A	Yes - 6.7 m
<b>Accessible Parking Spaces (Traffic and Parking By-law, Part C - Parking required for persons with disabilities, Section 111)</b>		For a public parking area that contains 20-99 parking spaces, one (1) accessible spaces are required.	N/A	Yes - 4 spaces  2 Type A spaces are provided; 2 Type B spaces are provided.
<b>City of Ottawa Accessibility Design Standards - Section 3.1.3(f)</b>		Minimum width of 3400 mm for "Type A" wide van accessible spaces Minimum width of 2400 mm for "Type B" standard parking spaces	N/A	Yes - Type A spaces are 3.4 m in width; Type B spaces are 2.4 m in width.
<b>City of Ottawa Accessibility Design Standards - Section 3.1.3(g)</b>		Provide an access aisle adjacent and parallel to each accessible parking space:  i. 1500 mm wide (minimum; ii. extend the full length of the space;	N/A	Yes - 1.5 m

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
	<ul style="list-style-type: none"> <li>iii. clearly indicated by high colour contrast diagonal pavement markings;</li> <li>iv. where two accessible parking spaces are provided adjacent to each other, they may share an access aisle;</li> <li>v. connect with adjacent accessible path of travel and centre curb ramp on access aisle</li> </ul>		
<p><b>Outdoor loading and refuse collection areas within a parking lot - Sec. 110(3)</b></p>	<p>Requirements:</p> <ul style="list-style-type: none"> <li>- 9 m from a lot line abutting a public street</li> <li>- 3 m from any other lot line</li> </ul> <p>Screened from view by an opaque screen with a min. height of 2 m</p>	N/A	<p>Yes - 14.68 m. In-ground waste bins are proposed at north side of Building B and are set back 10.29 m from the lot line.</p>
<p><b>Minimum number of bicycle parking spaces- Sec. 111(2), Table 111A</b></p>	<p>(h) warehouse</p> <ul style="list-style-type: none"> <li>- 1 per 2,00 m<sup>2</sup> of gross floor area</li> </ul>	<p><b>Proposed GFA:</b> 17,663 m<sup>2</sup></p> <p>17,663 m<sup>2</sup> /2,000 m<sup>2</sup> = 8.83 =9 (rounded)</p> <p><b>Required bicycle parking spaces: 9</b></p>	<p>Yes - 9 bicycle parking spaces.</p>
<p><b>Bicycle parking space location - Sec. 111 (3)</b></p>	<p>Must be located on the same lot as the use or building for which it is provided</p>	N/A	<p>Yes - bicycle parking is located on the same lot.</p>

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
<b>Bicycle parking space location - Sec. 111 (4)</b>	Must be located in order to provide convenient access to main entrances or well-used areas	N/A	Yes - bicycle parking is located in close proximity to the main entrances of Buildings A and B.
<b>Bicycle parking spaces location in landscaped area - Sec. 111 (7)</b>	Max. of 50% or 15 spaces, whichever is greater	N/A	Yes - 0 bicycle parking spaces proposed in landscaped area.
<b>Minimum bicycle parking space dimensions - Sec. 111 (8), Table 111B</b>	<b>(a) Horizontal:</b> Width: 0.6 m Length: 1.8 m	N/A	Yes - bicycle parking spaces will have a width of 0.6 m; length of 1.8 m
<b>Minimum bicycle parking aisle width - Sec. 111 (9)</b>	A bicycle parking space must have access from aisle having a min. width of 1.5 m	N/A	Yes - 1.5 m
<b>Minimum number of vehicle loading spaces required - Sec. 113, Table 113A</b>	a) Heavy industrial use, light industrial use, truck transport terminal, warehouse, hospital, museum, place of worship, post secondary educational institution, school, sports arena, theatre, cannabis production facility  • 15,000 m <sup>2</sup> - 24,999 m <sup>2</sup> - 2 loading spaces	N/A	Yes - 6 loading spaces.
<b>Minimum width of driveway accessing</b>	(i) - Single traffic lane - 3.5 m (ii) Double traffic lane -		Yes - 17 m



Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
<b>loading space – Sec. 113, Table 113B(a)</b>	6 m		
<b>Minimum width of aisle accessing loading space, by angle of loading Space – Sec. 113, Table 113B(b)</b>	For a standard space: (ii) Between 60 and 90 degrees – 9 m	N/A	Yes – 17 m
<b>Minimum width of loading space – Sec. 113, Table 113B(c)</b>	3.5 m	N/A	Yes – 3.5 m
<b>Minimum length of loading space – Sec. 113, Table 113B(d)</b>	(ii) Other cases: 7 m	N/A	Yes – 13 m
<b>Minimum vertical clearance for loading space – Sec. 113, Table 113B(e)</b>	4.5 m	N/A	N/A – Loading spaces are proposed in the surface parking areas.
<b>Permitted location of loading space – Sec. 113, Table 113B(f)</b>	Permitted in all locations other than in a required front yard or required corner side yard, or in a required yard abutting a residential zone.	N/A	Yes – loading spaces are not located in the front or corner side yard.

## 6 Summary of Opinion

It is the professional opinion of WSP that the proposed (4) four-storey storage warehouse building at 864 Lady Ellen Place represents good land use planning, and is appropriate for the site for the following reasons:

- The proposed development supports and is consistent with the 2020 Provincial Policy Statement.
- The proposed development conforms to the strategic directions and policies of the Official Plan by supporting redevelopment and intensification in the urban area, with an employment use that will serve the surrounding mixed industrial area and greater community.

- The proposed development meets several policies under the Urban Design section in Section 4.6 of the OP.
- The proposed development complies with the general intent and purpose of the Zoning By-law.

In conclusion, the Site Plan Control approval being sought to support the proposed development at 864 Lady Ellen Place represents good planning and is in the public interest.

Please feel free to contact me at [Nadia.De-Santi@wsp.com](mailto:Nadia.De-Santi@wsp.com) or 613-690-1114, if you have any questions or require additional information.

Yours truly,

**WSP**



Nadia De Santi, MCIP, RPP  
Practice Lead



Jill MacDonald, MCIP, RPP  
Project Planner

# APPENDIX

## A SITE PLAN

CONSULTANT - SUB CONSULTANT

CONSULTANT - SUB CONSULTANT

CONSULTANT - SUB CONSULTANT

CONSULTANT - SUB CONSULTANT

CLIENT

CLIENT REF. #  
 PROJECT

**PROPOSED SELF-STORAGE  
 LADY ELLEN PLACE**

KEY PLAN

DISCLAIMER: THE DRAWING AND DESIGN IS COPYRIGHT PROTECTED BY ARCHITECTURE 49. IT SHALL NOT BE USED, REPRODUCED OR RELEASED WITHOUT WRITTEN PERMISSION BY ARCHITECTURE 49. CHECK AND VERIFY ALL DIMENSIONS AND UTILITY LOCATIONS AND REPORT ALL ERRORS AND OMISSIONS PRIOR TO COMMENCING WORK. THIS DRAWING IS NOT TO BE SCALED.

ISSUED FOR: REVISION

NO.	DATE	ISSUES FOR RPA	DESCRIPTION

PROJECT NO.	219-00058-09	DATE	2022-11-29
DISIGNED BY:	AH		
DRAWN BY:	SG		
CHECKED BY:	AH		

DISCIPLINE:  
 LANDSCAPE ARCHITECTURE

**SITE PLAN**

SHEET NUMBER	A100
SHEET #	OF
DATE	2022-11-29

**LAYOUT NOTES:**

- CONTRACTOR TO CONFIRM ALL DIMENSIONS AND REPORT ANY DISCREPANCIES TO CONTRACTOR ADMINISTRATOR PRIOR TO CONSTRUCTION OR REMOVALS.
- LAYOUT TO BE APPROVED BY CONTRACT ADMINISTRATOR PRIOR TO ANY CONSTRUCTION OR REMOVALS.
- ALL DIMENSIONS ARE IN METRIC UNLESS OTHERWISE NOTED.
- CONTRACTOR IS RESPONSIBLE FOR ALL EXCAVATIONS, REMOVALS, DISPOSALS AND ROUGH GRADING AS REQUIRED TO CONSTRUCTION ALL WORKS AS SHOWN ON ALL PLANS, DETAILS AND SPECIFICATIONS.
- LOCATION OF ALL UTILITIES SHOWN FOR ILLUSTRATION ONLY. CONTRACTOR MUST CONTACT ALL UTILITIES REGARDING RULES FOR WORKING IN THE AREA OF THE UTILITIES PRIOR TO COMMENCEMENT OF ANY WORK. CONTRACTOR MUST CONFIRM LOCATION OF ALL UTILITIES PRIOR TO CONSTRUCTION.
- ALL EXISTING ROADS, SIDEWALKS, CURBS, FENCING, PAVING, SODDED AREAS, AND APPROACHES, ETC. TO REMAIN TO BE PROTECTED DURING CONSTRUCTION TO CONTRACT ADMINISTRATOR'S APPROVAL AT THE CONTRACTOR'S OWN COSTS.
- ALL EXISTING TREES, SHRUB BEDS, MULCH BEDS, AND SOD TO REMAIN TO BE PROTECTED DURING CONSTRUCTION. AREAS DAMAGED DURING CONSTRUCTION TO BE REPAIRED TO CONTRACT ADMINISTRATOR'S APPROVAL AT THE CONTRACTOR'S OWN COST.
- USE SPECIFIED BACKFILL IN ALL TRENCHES RUNNING BELOW ALL STRUCTURES, PAVING, WALKWAYS, ETC.
- FILL ALL HOLES AND LOW AREAS TO DESIGN SUBGRADE WITH COMPACTED FILL (SUITABLE TO SURFACE FINISH), FOR SODDED/PLANTED AREAS USE COMPACTED CLEAN EARTH FILL SUITABLE FOR PLANT GROWTH. FOR PAVED AREAS USE COMPACTED GRANULAR BASE.
- ALL TREES WITHIN OR IMMEDIATELY ADJACENT TO AREA OF WORK TO BE PROTECTED TO CITY OF OTTAWA TREE PROTECTION STANDARDS.

**PARKING REQUIREMENTS:**

- WAREHOUSE:
  - 0.4 PER 100m<sup>2</sup> FOR THE FIRST 5000m<sup>2</sup> GROSS FLOOR AREA,
  - 0.4 PER 100m<sup>2</sup> ABOVE 5000m<sup>2</sup> GROSS FLOOR AREA.
  - = 71 SPACES
- ACCESSIBLE PARKING REQ.:
  - 1 TYPE A
  - 2 TYPE B
- TOTAL PARKING PROVIDED:
  - 74 SPACES PROVIDED (71 REQ.)
  - 7 LOADING SPACES PROVIDED (3 REQ.)
- BICYCLE PARKING:
  - 1 STALL PER 2000m<sup>2</sup> GROSS FLOOR AREA
  - 10 SPACES PROVIDED (9 REQ.)

**LEGEND:**

- TOPSOIL AND SOD
- PLANT BED
- CONCRETE SIDEWALK
- HEAVY DUTY ASPHALT PAVEMENT
- LIGHT DUTY ASPHALT PAVEMENT
- PAINTED LINES
- SNOW REMOVAL AREA
- TOPSOIL AND NATIVE GRASS SEED MIX
- TOPSOIL AND DEEP ROOTING GRASS SEED MIX
- TOPSOIL AND POLLINATOR SEED MIX
- 3/4" RIVER STONE MULCH
- 2-4" RIVER STONE MULCH
- PROPERTY LINE
- EASEMENT LINE
- BUILDING OVERHANG
- EXISTING CHAINLINK FENCE
- APPROXIMATE AREA OF WORK
- EXISTING CURB
- NEW CURB
- ACCESSIBLE PARKING STALL

**KEYNOTES:**

- CONCRETE SIDEWALK. REFER TO DETAIL 4/L300
- ACCESSIBLE RAMP WITH TACTILE WARNING SURFACE INDICATOR (TWS)
- 150mm HT CONCRETE CURB. REFER TO DETAIL 5/L300
- ASPHALT PARKING LOT. REFER TO CIVIL FOR DETAIL.
- TYPE A PARKING STALL
- TYPE B PARKING STALL
- PROPOSED GARBAGE ENCLOSURE. REFER TO DETAIL 11/L300
- SNOW REMOVAL STORAGE AREA
- EXISTING FIRE HYDRANT
- 3/4" RIVER STONE MULCH.
- 2-4" RIVER STONE MULCH.
- 316 STAINLESS STEEL BICYCLE RACKS. REFER TO DETAIL 10/L300
- SODDED AREA. REFER TO DETAIL 6/L300
- PAINTED CROSSWALK
- PAINTED ISLAND
- SIGNAGE
- LOADING SPACE
- SEEDDED AREA. REFER TO DETAIL 6/L300

**ZONING STATISTICS**

**SITE INFORMATION:**  
 LEGAL DESCRIPTION:  
 Lots 9, 10, 11, 12, and Part of Lot 13,  
 Registered Plan 387939, City of Ottawa

**EASEMENTS:**  
 Inst. CR388144, Plan 387939  
 Inst. CR621418, Part 8 Plan 4R-29611  
 Inst. OC201490, By-law 2003-162  
 Inst. N430098 (L1609535)

**MUNICIPAL ADDRESS:** 864 Lady Ellen Place, Ottawa, Ontario

**LOT AREA:** 13,576.785 m<sup>2</sup> (146,144.07 sq.ft.)

**BUILDING GROSS FLOOR AREAS:**  
 Existing 3,529 m<sup>2</sup>  
 Proposed 17,663 m<sup>2</sup>

**ZONING INFORMATION:**  
 ZONE: Light Industrial Zone, Urban Exception 278,  
 Height Limit 30 (L1 (278) H(30))  
 Sec. 203(3), Table 203

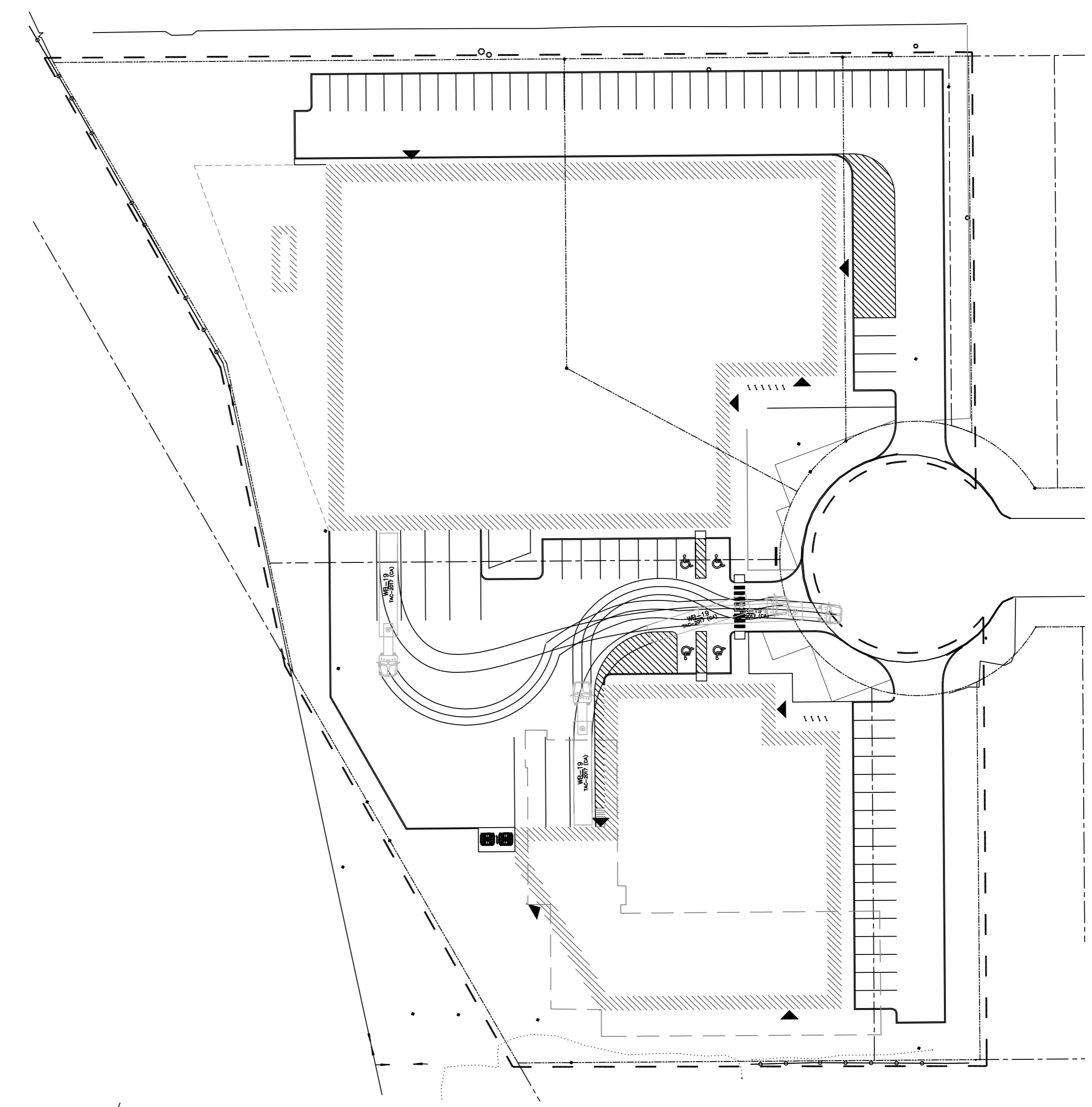
**PROPOSED LOT COVERAGE:** 39.3% (Maximum allowable 65%)

**BUILDING HEIGHT:**  
 Building 'A' 23m (Maximum allowable 30m)  
 Building 'B' 7m (Maximum allowable 30m)

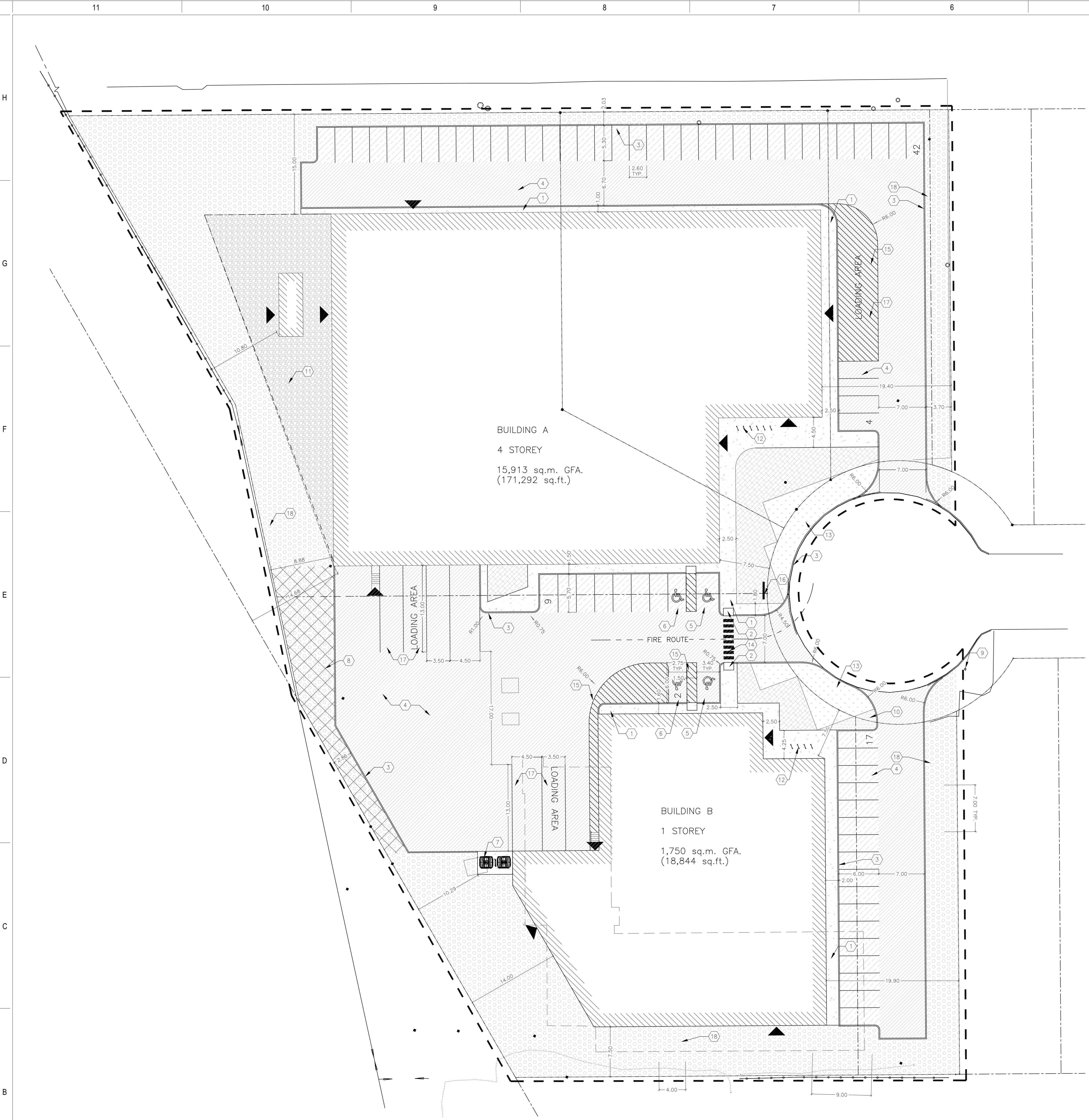
**FLOOR SPACE INDEX:** 1.3 (Maximum allowable 2)

**BUILDING SETBACKS:**  
 Building 'A' Setbacks:  
 Min. Front Yard: 7.5 m (7.5 m required)  
 Min. Interior Side Yard: 15 m (7.5 m required)  
 Min. Rear Side Yard: 8.88 m (3.5 m required)

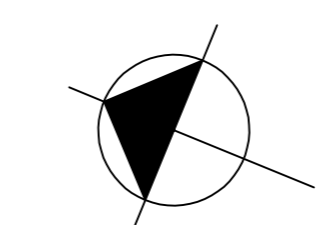
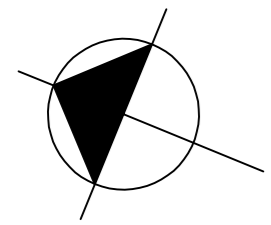
Building 'B' Setbacks:  
 Min. Front Yard: 7.5 m (7.5 m required)  
 Min. Interior Side Yard: 7.5 m (7.5 m required)  
 Min. Rear Yard: 14 m (3.5 m required)



**1 VEHICLE TURNING MOVEMENTS**  
 L100 1:500



**1 SITE PLAN**  
 L100 1:250



# APPENDIX

# B

## LANDSCAPE PLAN



CONSULTANT - SUB CONSULTANT

CONSULTANT - SUB CONSULTANT

CONSULTANT - SUB CONSULTANT

CONSULTANT - SUB CONSULTANT

SCALE:

CLIENT:

CLIENT REF. #  
 PROJECT:

PROPOSED SELF-STORAGE  
 LADY ELLEN PLACE

KEY PLAN

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ISSUED FOR: REVISION:

REV.	DATE	ISSUES FOR RFA	DESCRIPTION
1	11/09/2022		

PROJECT NO.: 219-00028-09  
 DATE: 2022-11-29

ORIGINAL SCALE: As indicated  
 DESIGNED BY: AH  
 DRAWN BY: SG  
 CHECKED BY: AH  
 DISCIPLINE: LANDSCAPE ARCHITECTURE  
 TITLE: LANDSCAPE PLAN

SHEET NUMBER: L100  
 OF

DATE: 2022-11-04

LAYOUT NOTES:

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- ALL TREES WITHIN OR IMMEDIATELY ADJACENT TO AREA OF WORK TO BE PROTECTED TO CITY OF OTTAWA TREE PROTECTION STANDARDS.

LEGEND:

- TOPSOIL AND SOD
- PLANT BED
- CONCRETE SIDEWALK
- HEAVY DUTY ASPHALT PAVEMENT
- LIGHT DUTY ASPHALT PAVEMENT
- PAINTED LINES
- SNOW REMOVAL AREA
- TOPSOIL AND NATIVE GRASS SEED MIX
- TOPSOIL AND DEEP ROOTING GRASS SEED MIX
- TOPSOIL AND POLLINATOR SEED MIX
- 3/4" RIVER STONE MULCH
- 2-4" RIVER STONE MULCH
- PROPERTY LINE
- EASEMENT LINE
- BUILDING OVERHANG
- EXISTING CHAINLINK FENCE
- APPROXIMATE AREA OF WORK
- EXISTING CURB
- NEW CURB
- ACCESSIBLE PARKING STALL
- PROPOSED DECIDUOUS TREE
- PROPOSED CONIFEROUS TREE
- PROPOSED DECIDUOUS SHRUB
- PROPOSED CONIFEROUS SHRUB
- EXISTING TREE TO REMAIN, TREES TO REMAIN ARE TO BE PROTECTED TO CITY OF OTTAWA STANDARDS
- EXISTING TREE TO BE REMOVED
- PLANT KEY
- # - PLANT QUANTITY
- Ps - PLANT SPECIES

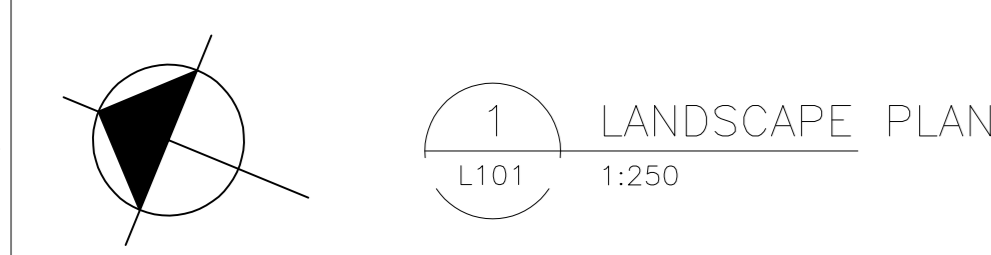
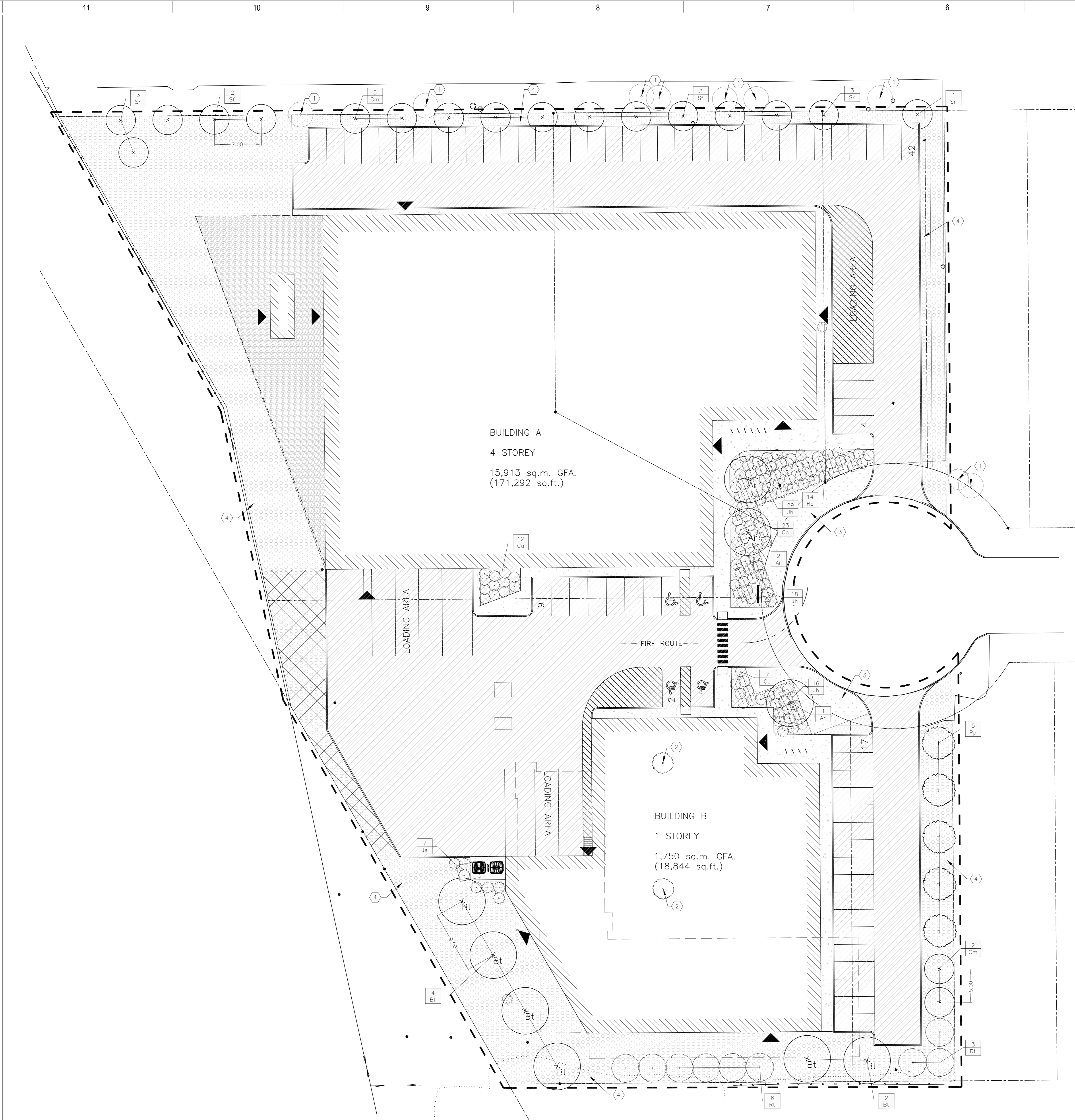
KEYNOTES:

- EXISTING TREE TO REMAIN
- EXISTING TREE TO BE REMOVED
- SODDED AREA. REFER TO DETAIL 6/L300
- SEEDED AREA. REFER TO DETAIL 6/L300

PLANTING NOTES:

- LOCATION OF ALL PLANTS MATERIAL TO BE MARKED OUT ON SITE AND APPROVED BY CONTRACT ADMINISTRATOR PRIOR TO PLANTING
- TREE SPACING, UNLESS OTHERWISE NOTED:
  - DECIDUOUS TREES: 6.0m MIN.
  - CONIFEROUS TREES: 3.0m MIN.
- SHRUB SPACING, UNLESS OTHERWISE NOTED:
  - SMALL DECIDUOUS: 600mm O.C.
  - LARGE DECIDUOUS: 900mm O.C.
  - CONIFEROUS: 750mm O.C.
  - GRASSES: 450mm O.C.
- ALL PLANT MATERIAL TO BE BEST QUALITY, REVIEWED ON SITE BY LANDSCAPE ARCHITECT PRIOR TO PLANTING. UNACCEPTABLE PLANTS TO BE REPLACED AND NEW PLANTS APPROVED PRIOR TO PLANTING. REPLACEMENTS ARE AT CONTRACTOR'S OWN COST.
- ALL PLANTING EXCAVATIONS ARE TO BE REVIEWED AND APPROVED BY LANDSCAPE ARCHITECT PRIOR TO PLACEMENT OF DRAINAGE COURSE, FILTER FABRIC AND/OR SOIL.
- SHRUB BEDS AT PARKING AREA TO HAVE PLANTS SET BACK SO THAT THERE IS A 450mm WIDE MULCH EDGE CURB WITH FULL GROWTH OF PLANTS.

KEY	QUANTITY	BOTANICAL NAME	COMMON NAME	SPACING (mm) (75%)	SIZE	COMMENTS
<b>TREES - Deciduous</b>						
Ar	3	Acer rubrum	Red Maple	as shown	75mm Cal. B&B	TREES TO BE BALL & BURLAP. SOURCED IN SAME GROWING ZONE. ALL TREES SHOULD HAVE 5 TO 7 MAIN BRANCHES WITH DOMINANT LEADER BRANCH AND WITHOUT DEFECT. MIN ROOT ZONE VOLUME PER TREE: 30 CU. M.
Bt	5	Betula x hybrida 'Fastigiata'	Columbian Paper Birch	as shown	50mm Cal. B&B	
Cm	7	Cornus x mordenensis 'Snowbird'	Snowbird Hawthorn	as shown	50mm Cal. B&B	
Bt	6	Betula papyrifera	Paper Birch	as shown	75mm Cal. B&B	
Sr	7	Syringa reticulata 'Ivory Silk'	Ivory Silk Japanese Tree Lilac	as shown	50mm Cal. B&B	
Ra	14	Rhus aromatica	Fragrant Sumac	1100	5 gal.	
Rt	9	Rhus typhina	Staghorn Sumac	as shown	5 gal.	
Jh	63	Juniperus horizontalis 'Prince of Wales'	Prince of Wales Juniper	1200	5 gal.	SHRUBS TO BE IN POTS. SOURCED IN SAME GROWING ZONE. ALL SHRUBS SHOULD HAVE DENSE GROWTH, WELL ROOTED AND WITHOUT DEFECT. MIN ROOT ZONE VOLUME PER SHRUB: 5 CU. M.
Ca	42	Cornus alba 'Balthaz'	Ivory Halo Dogwood	900	5 gal.	
Js	7	Juniperus sabina	Savin Juniper	900	5 gal.	
<b>HYDROSEEDED AREAS</b>						
REFER TO WATCH LEGEND	200sq.2	Native grass seed mix: Eastern Ectopye by DLF Pickseed		17.25g/ha	NA	PREPARE SOIL. INSTALL HYDROSEED. AND MAINTAIN SEEDING AREAS AS PER MANUFACTURER'S INSTRUCTIONS.



# APPENDIX

## C ELEVATIONS







