



# Planning Justification Report

**Wellings of Stittsville Phase 2 and 3 Proposed Senior's Development  
20 Cedarow Court, Stittsville, ON**

**Site Plan Control Application  
City of Ottawa**

**Prepared for:**

Wellings of Stittsville Inc.  
Wellings of Stittsville Phase 2

**Prepared by:**

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## 1.0 INTRODUCTION

This Report has been prepared in support of an application for Site Plan Control approval on behalf of NLG 2011 Inc. for the property known municipally as 20 Cedarow Court.

The purpose of this application is to permit a mixed-use development comprised of a seniors apartment building and retail and offices uses to be built on this site. Although the site could be developed in as many as three phases, this application seeks approval for the entire project at this time.

On May 2<sup>nd</sup>, 2019 and July 10<sup>th</sup> 2019, Nautical Lands Group (NLG) met with City of Ottawa Planning and Growth Management staff to discuss the proposed site plan submission for 20 Cedarow Court.

On September 1, 2022 Nautical Lands, Chmiel Architects and Laurin Group met with the City of Ottawa Planning Department to discuss revisions to the Site Plan due to increase in costs and providing changes that would permit the development of affordable seniors residential to continue. Over the past months, NLG has worked with various consultants to prepare the revised requested documents. The site plan application reflects feedback received from City staff at the above-noted pre-application meetings as well as from the Urban Design Review Panel meeting on September 6<sup>th</sup>, 2019 and incorporates information from the requested studies.

Public consultation of the proposal will be in accordance with the City of Ottawa policy.

## 2.0 SITE LOCATION, SITE CONTEXT AND DEVELOPMENT PROPOSAL

The site is located on the north side of Hazeldean Road, between Huntmar Drive to the east and Cedarow Court to the west as shown on Figure 1 to this report. The site is rectangular in shape, having 124 meters of frontage on Hazeldean Road, a depth of 199 meters and an area of 22,124 square metres (2.21 Ha). Figure 1 is a Location Map.

The site fronts onto Hazeldean Road, an arterial road. Hazeldean Road is a four lane divided road. There is a signalized intersection at Fringewood Dr (Wellings Private).

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Wellings Private is a private road with access to Hazeldean Road and is shared with the Wellings of Stittsville Phase 1.



Figure 1 – Location Map

## 2.2 SITE CONTEXT

To the immediate east of the site is the Wellings of Stittsville Phase 1, and the vacant land proposed for Extendicare long term care facility. At the north west corner of Huntmar Drive and Hazeldean Road is the Keg Restaurant.

To the north are single detached dwellings fronting onto a cul-de-sac, Coriolis Court. Also to the north is City owned open space in the Poole Creek ravine and a public park, Tempest Park.

Further to the west is Cedarow Court that mostly contains automotive commercial uses and also a spa and a fitness studio.

Across Hazeldean Road, the site is under construction with a No Frills grocery store along with other commercial and office uses proposed. The vacant parcels are zoned to permit automotive commercial uses, townhouses and stacked townhouses

## 2.3 DEVELOPMENT PROPOSAL

The development proposal is a mixed use development that is capable of being built in two phases. The total number of units proposed are 344.

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The design of the site is a U shaped building fronting Hazeldean. There is a central courtyard which will provide an amenity area for the residents.

The building at the front of the site will be a mixed use building with commercial on the ground floor and residential above and has a proposed height of 21.15m (6 storeys). There is a proposed roof top patio. Both Phase 2 and 3 will have indoor and outdoor amenity areas.

Surface and below grade parking will be provided for the residents. There are 364 parking spaces proposed for the residents, staff and visitors. The proposed parking spaces for residents are 201 parking spaces below grade and 37 surface parking spaces for a total of 238 spaces. Commercial parking spaces of 57 have been proposed along with 69 visitors parking spaces.

The commercial/retail along Hazeldean Road will be conducive to pedestrian activity and will be situated close to the sidewalk with inviting entrances. There will be a pedestrian access from Hazeldean Road along Wellings Private.

A connection between Phase 1 and Phase 2 and 3 is proposed through a below grade path under Wellings Private.

Exterior finishes of the Phase 2 and 3 buildings will be a complimentary mix of masonry, metal siding and window-walls to work with the materials used in Wellings of Stittsville Phase 1 and other buildings in the area. The commercial and retail will have glass entrances and doors fronting Hazeldean Road. Each suite will have a private balcony.

Along with the courtyard and rooftop patios, Poole Creek will provide a park like setting and will connect to the UNA lands at the rear of the Phase 1 development with connective walking paths. With a majority of the parking below grade there will be more landscaping and more usable land for residents to enjoy.

The site plan complies with the zoning requirements of the AM9 [474] Zone however, a minor variances has been approved to permit and increased height for the building. The Arterial Mainstreet Zone is a zoning category in which the uses proposed in the site plan are permitted uses and which implement the Official Plan and approved Kanata West Concept Plan by allowing transit supportive land uses on the property.

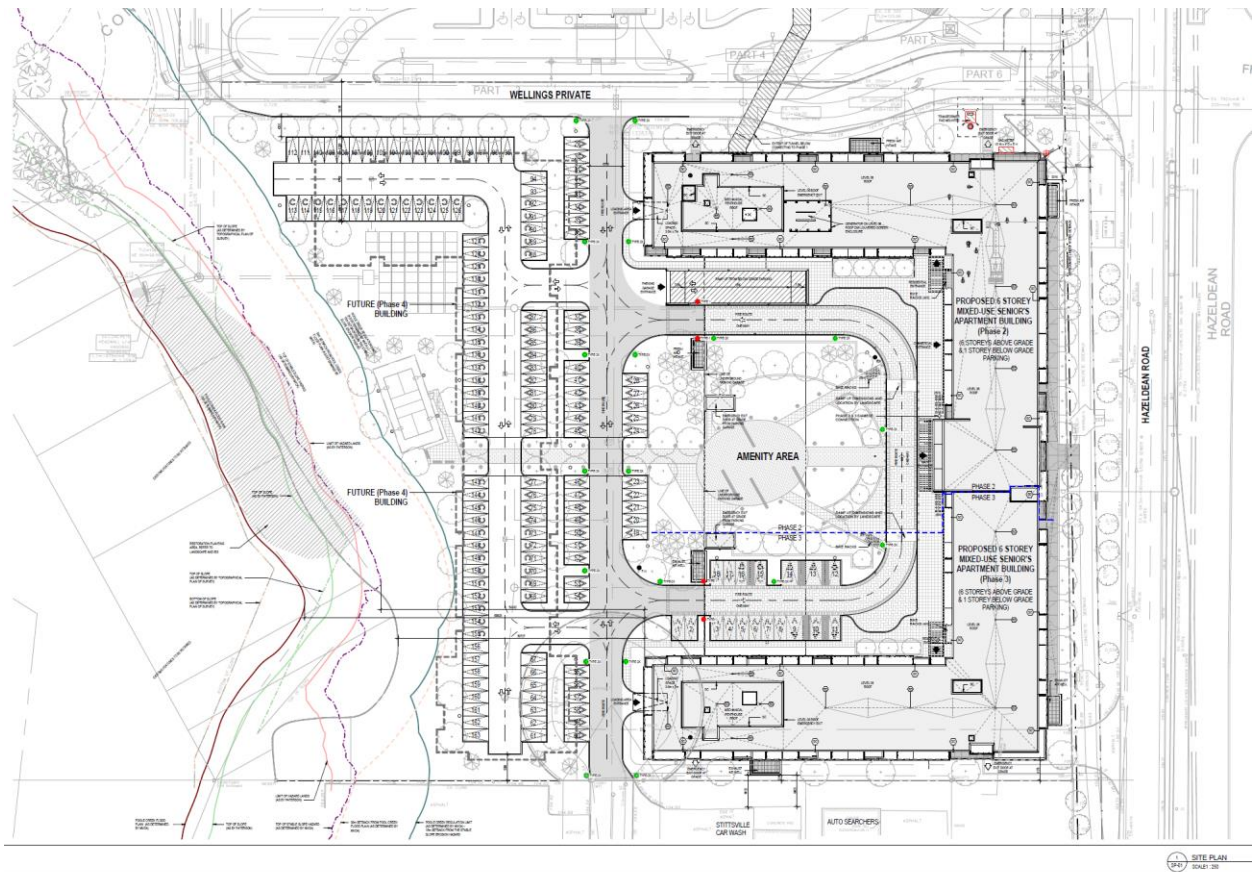
Figure 2 is the Site Plan and Elevation Drawings that show the proposed site development at build out and elevation drawings for all buildings.

At this point in time, urban services in the form of sanitary sewers and municipal water supply abut the site. Stormwater management measures including enhanced measures for water quality and quality controls are incorporated into the site engineering design and will tie into the LID system in Phase 1.

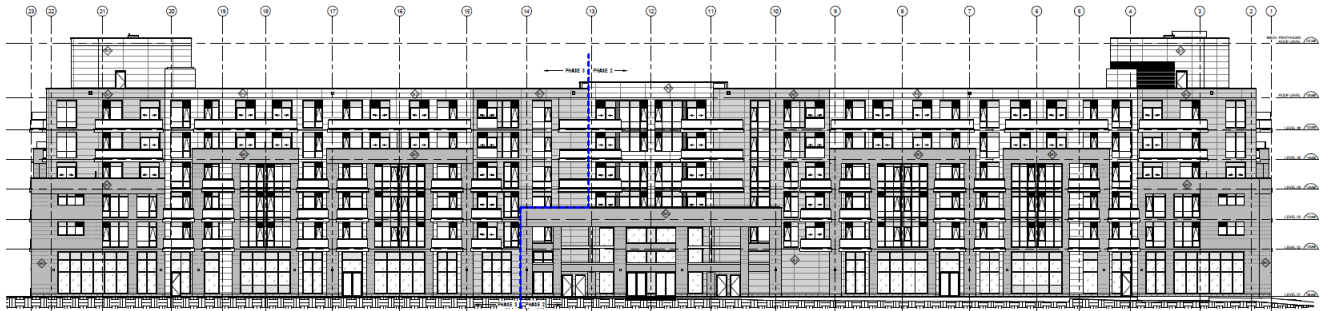
The subject property was approved to outlet stormwater into Poole Creek which ties into the Stormwater Management system in Phase 1.

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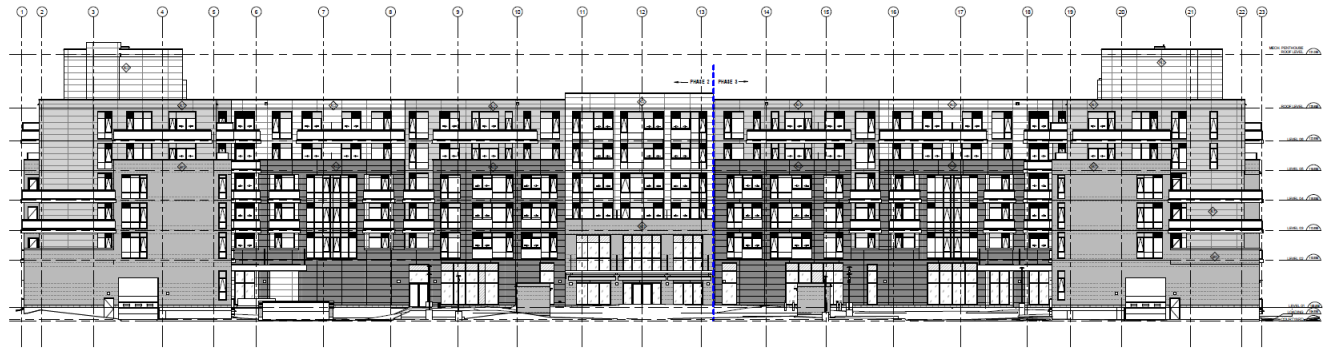
**Figure 2- Site Plan and Elevation Drawings**



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**EAST BUILDING KEY ELEVATION**  
SCALE: 1/8"

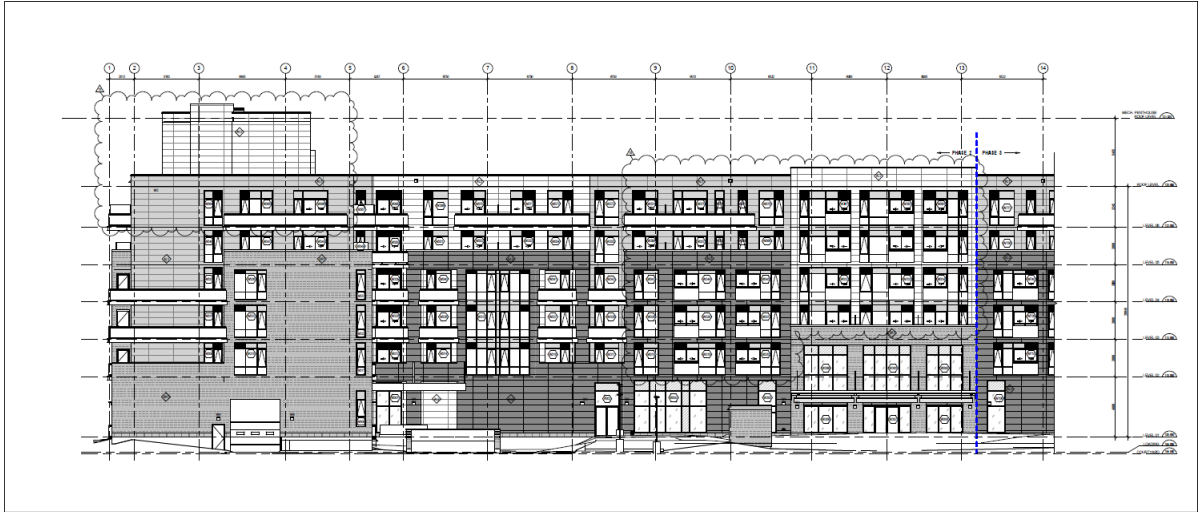


**WEST BUILDING KEY ELEVATION**  
SCALE: 1/8"

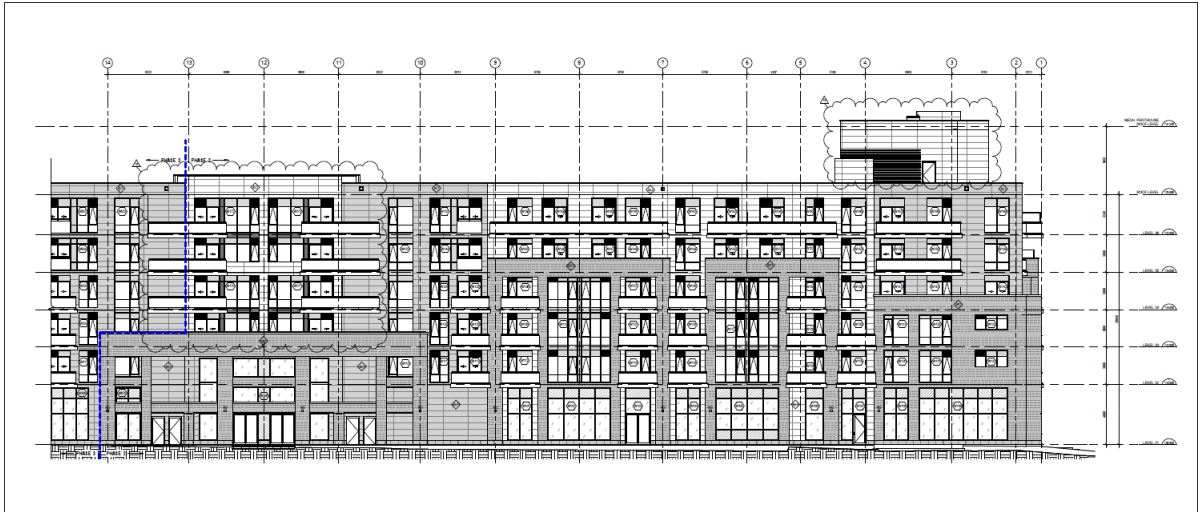
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WEST BUILDING ELEVATION - PHASE 2  
SCALE: 1/8"



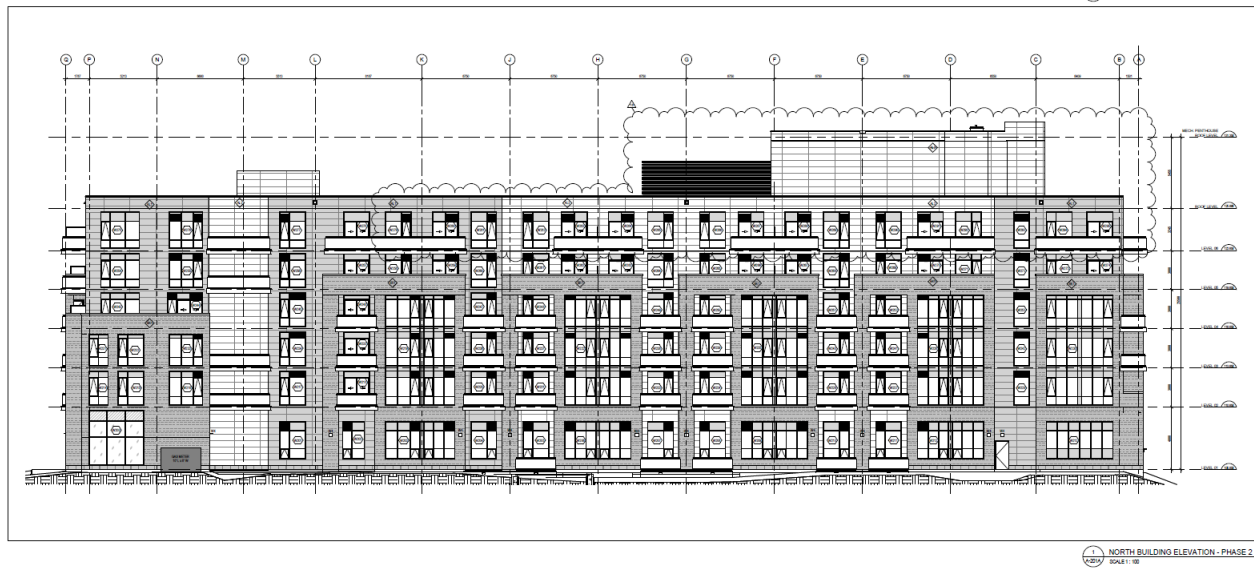
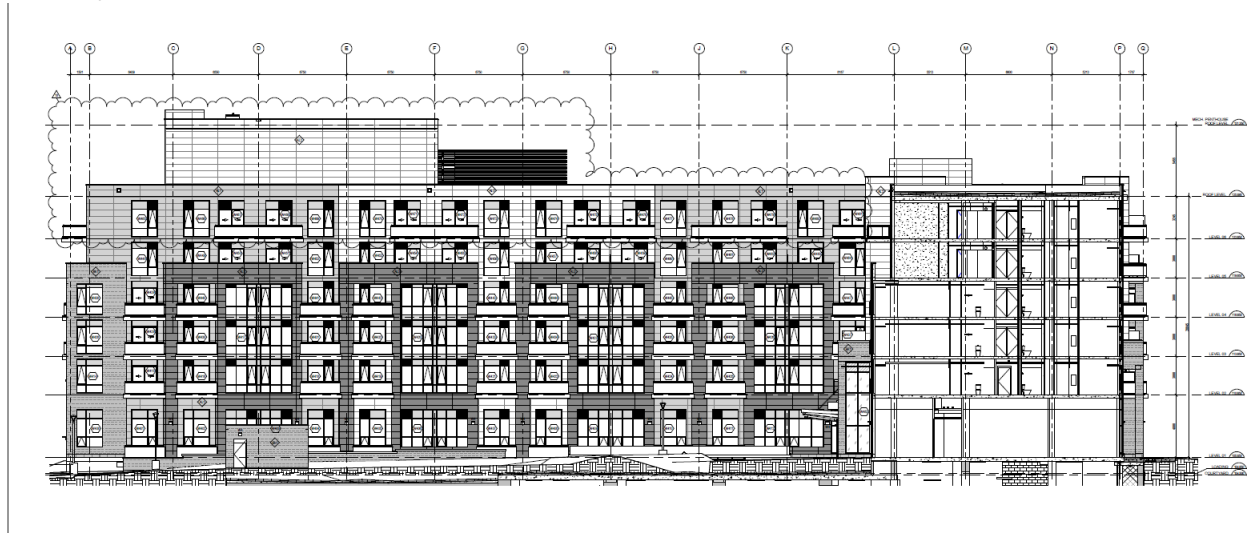
EAST BUILDING ELEVATION - PHASE 2  
SCALE: 1/8"



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### 3.0 POLICY AND REGULATORY FRAMEWORK

#### 3.1 PROVINCIAL POLICY STATEMENT 2020

The Provincial Policy Statement 2020 (PPS) under Section 3 of the *Planning Act* sets out key Provincial interests with regard to land use planning. Decisions made by all approval authorities in the Province of Ontario must be consistent with the policies of the Statement.

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#### **Section 1.0 Building Strong Healthy Communities**

Policy 1.1.1.b) indicates that healthy, livable and safe communities are sustained by accommodating an appropriate range and mix of residential (including second units, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs.

Policy 1.1.1.c) calls on municipalities to avoid development and land use patterns that may cause environmental concerns.

Policy 1.1.1.e) promotes cost-effective development patterns to minimize land consumption and servicing costs.

Policy 1.1.1.h) promotes development that conserves biodiversity.

Policy 1.1.3.1 advises that settlement areas shall be the focus of growth and development, and their vitality and regeneration shall be promoted.

Policies 1.1.3.2 and 1.1.3.3 support intensification where it can be accommodated with sufficient infrastructure. Municipalities are called upon to identify appropriate locations for intensification.

Having regard for the preceding policies, development of the site for a mix of uses including retail, office, and seniors residential facilities is an excellent combination as encouraged by the PPS. The conveyance of the land near Poole Creek to the City for protection and preservation is a measure to address environmental concerns and promote biodiversity.

The mix and density of land uses represents a cost-effective land use pattern that minimizes land consumption and servicing costs.

The urban area of the City of Ottawa is a settlement area as defined by the PPS and this site is within the urban area. Therefore, this site is in an area of the City in which growth and development is promoted.

The City has implemented the PPS by identifying in the Official Plan, areas that are a priority for intensification. One of the preferred areas is Arterial Mainstreets that includes Hazeldean Road.

This site is a priority for intensification and is supported by adequate infrastructure.

Seniors' apartments are in high demand in Ottawa, Wellings of Stittsville Phase 1 has had great success to the east of the proposed Phase 2.

By developing this site, we will also be providing much needed employment to this community. Employees will be required with the seniors' apartments and the commercial/office component.

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#### **Section 2.0 Wise Use and Management of Resources**

Section 2.1.1 states that natural features and areas shall be protected for the long term. Poole Creek is being protected from development in this proposal. The northern part of this site that includes these lands have been conveyed to the City by the previous land owner. In addition, an Environmental Impact Assessment has been prepared in support of this application that shows that development and site alteration will have no negative impacts on the natural features or their ecological functions. The Assessment satisfies the requirements of Sections 2.1.7 and 2.1.8 of the PPS.

The Parks and Open Space O1R Subzone on the northern part of the site places the necessary restrictions on development and site alteration to protect Poole Creek, a sensitive surface water feature. The zoning implements Section 2.2.1 e)2. of the PPS.

#### **Section 3.0 Protecting Public Health and Safety**

There are no natural hazards or human-made hazards on or near to this site.

In conclusion, approval of this application for Site Plan Control is consistent with and supported by the policies of the PPS 2014.

### **3.2 CITY OF OTTAWA OFFICIAL PLAN**

The Ministry of Municipal Affairs and Housing has issued a Notice of Decision to approve the City's New Official Plan, adopted by By-law 2021-386.

Volume 2C – Area Specific

#### **Section 2 – Kanata West**

##### **2.1 Landowners within Kanata West**

The owners of Wellings of Stittsville Phases 2 & 3 have entered into an agreement with the Kanata West Landowners Group with the Phase 1 Development

##### **2.3 Conditions for Development**

- a) The Ministry of Environment, Conservation and Parks will have approved the Carp River, Poole Creek and Feedmill Creek Restoration Class Environmental Assessment (EA); and
- b) The Kanata West Landowners Group, or the City, will have commenced the physical construction of Phase I of the Carp River restoration works; and
- c) Mississippi Valley Conservation Authority will have issued a permit under Section 28 of the Conservation Authorities Act for the placement and removal of fill in accordance with the Carp River Restoration Plan and the EA; and
- d) Filling of the property and an as-built survey will have been completed to demonstrate that the area is entirely removed from the flood plain; and



e) At all times, the flood storage capacity of the corridor will be maintained at or above existing conditions.

### **Suburban West Transect**



Schedule B5: Suburban West Transect

### **Section 5.4 Suburban Transects**

5.4.1c) Mid-rise along Mainstreet Corridors  
however the following policy direction applies;

- i) Where the lot fabric can provide a suitable transition to abutting Low-rise areas, High-rise development may be permitted;
- ii) The setback requirements for buildings shall be proportionate to the width of the abutting right of way, and consistent with the objectives in the urban design section on Mid-rise and High-rise built form in Subsection 4.6.6, Policies 7), 8) and 9); and

*The subject 6-storey mid-rise building abuts Hazeldean Road which is a Mainstreet Corridor. The building location is close to the road with parking locating below ground and at the rear of the building not at the road. The building location and design of the site is consistent with the requirements of the urban design guidelines for mid-rise buildings.*

5.4.1 3) In the Suburban Transect, this Plan shall support:

- a) A range of dwelling unit sizes
- b) In Hubs and on Corridors, a range of housing types to accommodate individuals not forming part of a household

*The development is for seniors residential rental apartments which a diverse housing type and accommodates individuals not forming part of a household*

5.4.3 Provide direction to the Hubs and Corridors located within the Suburban Transect

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5.4.3 3) Along Mainstreet Corridors, permitted building heights, except where a secondary plan or area-specific policy specifies different heights and subject to appropriate height transitions, stepbacks and angular planes, maximum building heights as follows:

- a) Generally, not less than 2 storeys and up to 9 storeys except where a secondary plan or area-specific policy specifies greater heights; however
- b) The wall heights directly adjacent to a street of such buildings, or the podiums of high-rise buildings shall be of a height proportionate to the width of the abutting right of way, and consistent with the objectives in the urban design section on mid-rise and high-rise built form in Subsection 4.6.6, Policies 7), 8) and 9);

*The proposed building is 6 storeys in height adjacent to a Mainstreet Corridor. This conforms to the requirements of section 5.4.3 of the OP.*

## 6.2 Corridors

6.2 2) Development within the Corridor designation shall establish buildings that locate the maximum permitted building heights and highest densities close to the Corridor, subject to building stepbacks where appropriate. Further, development:

- a) Shall ensure appropriate transitions in height, use of land, site design and development character through the site, to where the Corridor designation meets abutting designations;
- b) May be required to provide public mid-block pedestrian connections to nearby streets or abutting designations;
- c) For sites generally of greater than one hectare in area or 100 metres in depth: i) Shall be required to establish an enhanced circulation network throughout the site that prioritizes the needs of pedestrians, cyclists and transit users; and ii) Where development is proposed to occur in phases, may be required to build phases closest to the Corridor before phases located at the back of the site, subject to any overlay that may apply; and
- d) Shall be prohibited from including functions or uses causing or likely to cause nuisance due to noise, odour, dust, fumes, vibration, radiation, glare or high levels of heavy truck traffic.

*The proposed building will have a height of 6 storeys. This is the maximum height permitted for the site and was permitted through an minor variance approval. Wellings Private is a private road located between Wellings of Stittsville Phase 1 and the proposed Phase 2 and 3. This private road connects to the walking path running along pool creek.*

*The Private Road is an extension of Fringewood to the south of Hazeldean. It connects the Wellings community to the sidewalks and retail along Hazeldean. It also permits walking to the nature trails at the rear of the site along Pool Creek.*

*The construction will starting at the front of the site nearest to the Mainstreet and will continue to the back towards Pool Creek.*

*The Seniors Residential Development will not cause nuisances of odour, fumes or heavy truck traffic*

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6.2 3) Corridors will generally permit residential uses

*The proposed development is a seniors residential rental apartment*

6.2 4) Unless otherwise indicated in an approved secondary plan, the following applies to development of lands with frontage on both a Corridor and a parallel street or side street:

a) Development shall address the Corridor as directed by the general policies governing Mainstreet Corridors Minor Corridors, particularly where large parcels or consolidations of multiple smaller parcels are to be redeveloped; and

b) Vehicular access shall generally be provided from the parallel street or side street  
*Access to the site is provided through Wellings Private.*

6.2.2 Recognize Mainstreet Corridors as having a different context and setting out policies to foster their development

1) In the Mainstreet Corridor designation, this Plan shall permit a mix of uses including offices. These uses are permitted throughout the building, however the Zoning By-law may require active commercial or service uses on the ground floor, which include those that support cultural development in order to maintain, extend, or create a continuous stretch of active frontages along a Mainstreet.

*The residential building will have ground floor commercial uses including a restaurant, pub, personal service. This will maintain the intent of the Mainstreet Corridor.*

### 3.3 KANATA WEST CONCEPT PLAN

The site is within the limits of the Kanata West Concept Plan that has been approved by City Council for implementation.

The Kanata West Concept Plan is directed toward providing a full complement of urban services for people to live, work, play and learn by allowing for approximately 25,000 jobs, 5,000 residential units and supporting services within a cohesive framework of land development. Permitted uses include office, housing, retail, institutional, entertainment and leisure activities. The Concept Plan implements the Provincial Policy Statement and City Official Plan by making efficient use of land and services and by promoting the use of public transit.

The Land Use Schedule to the Kanata West Concept Plan designates this site as Community-Level Retail, Residential Area B and the north-west corner of the site, River / Creek Corridors. Figure 6 is the Kanata West Concept Plan Land Use Schedule with the site highlighted.

The policies for the Community Level Retail are directed toward creating a retail area where parking is not the main feature, a pedestrian friendly format and a "green corridor" along Hazeldean Road. The range of permitted uses include personal and

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community-serving uses, leisure and entertainment facilities, retail and commercial uses including offices, institutional uses and residential uses including retirements homes and apartments. All of the uses proposed on the site plan are listed as permitted uses on lands designated Community Level Retail. Parking is underground.

The Residential Area B is directed toward providing a diversity of housing accommodation that is in a transit-integrated and pedestrian friendly environment. Residential uses include apartment buildings, retirement homes and planned unit developments and lower density forms of housing.

The River / Creek Corridor lands are intended to be conveyed to the City for public park and open space purposes.

The application for Site Plan Control is in conformity to the Kanata West Concept Plan.

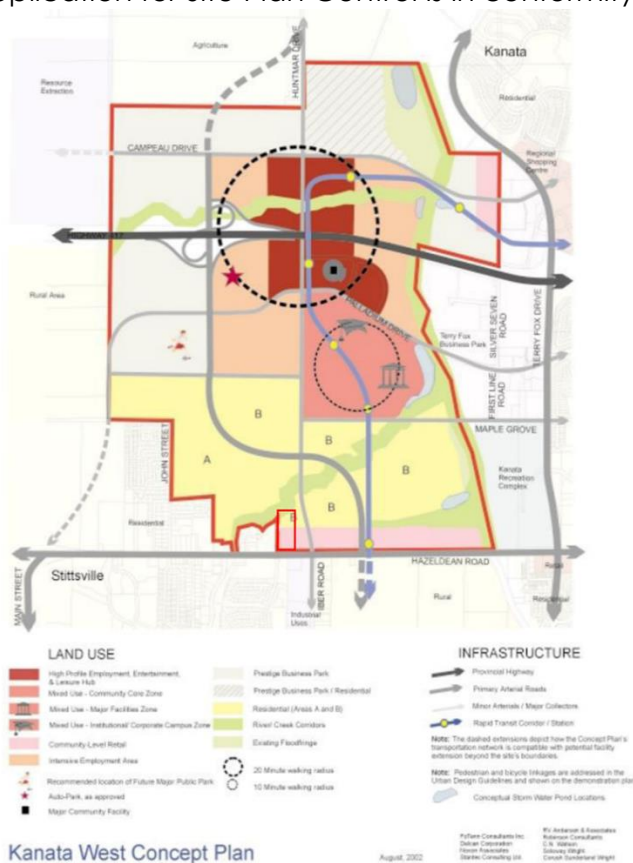


Figure 3-1 - Concept Plan  
August 2002

Figure 6 – Kanata West Concept Plan – Land Use Schedule

### 3.5 DESIGN GUIDELINES FOR ARTERIAL MAINSTREETS

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In May 2006, City Council adopted Design Guidelines for Arterial Mainstreet. As Hazeldean Road is considered as a Mainstreet, the policies apply to this site.

The objectives of the guidelines are;

- To foster compatible development that will contribute to the recognized or planned character of the streets
- To promote a comfortable pedestrian environment and create attractive streetscapes
- To achieve high-quality built form and establish a strong street edge along Arterial Mainstreets
- To facilitate a gradual transition to more intensive forms of development on Arterial Mainstreets
- To accommodate a broad range of uses including retail, services, commercial, office, institutional and higher density residential
- To enhance connections that link development sites to public transit, roads and pedestrian walkways.

There are seven sections to address in the Guidelines:

#### **1. Streetscape**

Guidelines 1, 2 and 3 call for new buildings to be located close to the street with a planted boulevard and a sidewalk in the road allowance. The sidewalk is already installed by the City and the site plan proposes to locate the retail/office building within 3m of the front lot line. The landscape plan provides tree planting in the road allowance.

Guideline 4 encourages the creation of continuous streetscape by the use of buildings and landscaping. The proposed retail/office building occupies more than 50% of the street frontage and the landscaping between the street and the proposed building will result in a continuous streetscape.

Guideline 6 recommends that new buildings be setback within 3m from the front lot line and that is what the site plan proposes.

#### **2. Built Form**

Guideline 13 requires that buildings occupy the majority of the street frontage and that is the case in the proposed development. There are two pedestrian connections to provide pedestrian access to buildings at the rear of the site and to the central courtyard. These pedestrian accesses help to break up the front façade of the building.

To meet Guidelines 16, 17 and 18, the front façade of the retail/office building facing Hazeldean Road and has doors that are visible and accessible from the public sidewalk



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using sidewalks to the front door. As well there is a proposed patio from the restaurant facing Hazeldean Road. Clear windows and doors are incorporated into the site design and the retail/office building has a rich design to create public interest.

#### **3. Pedestrians and Cyclists**

To meet Guideline 19, direct, safe continuous and clearly defined pedestrian access is provided from public access to building entrances. There are two openings for pedestrians proposed these proportionately located at either end of building fronting Hazeldean Road and links the buildings at the rear of the site to the sidewalk on Hazeldean Road through the courtyard.

Direct access is provided to the retail / office building entrances from the sidewalk on Hazeldean Road to meet Guideline 20.

Unobstructed pedestrian walkways connect parking areas to the entrances to buildings to satisfy Guideline 21.

#### **4. Vehicles and Parking**

To meet Guideline 25, vehicular access is shared with the adjacent site to the east.

Surface parking spaces are at the rear of the proposed buildings as recommended by Guideline 27.

A consistent width of landscaping and pedestrian areas is provided across the front of the site to implement Guideline 30.

#### **5. Landscaping and Environment**

To meet Guideline 32, the vegetation chosen for planting on-site considered tolerance to urban conditions such as heat and salt tolerance. Native plant materials are incorporated into the landscape design.

Tree planting and street lighting will be coordinated in the landscape plan as recommended by Guideline 34.

The area of the site that is not occupied by buildings, parking and driveways is landscaped to satisfy Guideline 37.

To meet Guideline 41, the side yards and rear yards have at least a 2.5m width of landscaping.

#### **6. Signs**

To meet Guideline 47, signs will be located on the building in a uniform manner that respects the scale of the buildings. They will be designed to complement the character of the area.

#### **7. Servicing and Utilities**

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All utility equipment will be screened from adjacent properties and streets to satisfy Guideline 50.

To meet Guideline 51, lighting is designed to be directed downward, without glare or spillage onto abutting uses.

As recommended by Guideline 53, secondary doors have been designed to blend in with the building façade.

In conclusion, the design of the site meets the relevant Urban Design Guidelines for Development along Arterial Mainstreets.

## 4.0 ZONING BY-LAW

The site is regulated by City of Ottawa 2008-250. The site has two zoning designations.

The site is zoned Arterial Mainstreet AM9 [474]Subzone. This Zone permits retail, service commercial, offices, residential and institutional uses either in mixed-use buildings or side by side. An additional intent of the AM Zone is to promote intensification while achieving compatible development. The site plan complies with the special development standards of the AM9 Subzone with the exception of the proposed six storey apartment building that will have a height of 2.15m which exceeds the maximum building height permitted. A minor variance for the height has been approved by the Committee of Adjustment.

The Zoning By-law also contains requirements for parking of automobiles and bicycles. The minimum required parking is 0.25 spaces per unit plus 0.2 spaces for unit for visitor parking. For the 344 units proposed, we exceed the parking requirement. There are 364 parking spaces proposed with 201 spaces below grade for residents and the remainder above grade for the residents, staff and visitors. The proposed parking spaces for residents are 238. With the residents of Phase 1 and Phase 2 and 3 being the primary customers of the retail/office and restaurant, the parking provided will be sufficient and will promote pedestrian flow. The additional parking will be used by staff. There is sufficient parking for visitors.

In conclusion, site plan has been designed to comply with the Zoning By-law.

## OVERVIEW OF INDIVIDUAL TECHNICAL STUDIES

### 5.1 PHASE 1 ENVIRONMENTAL SITE ASSESSMENT

A Phase 1 Environmental Site Assessment (ESA) was completed in general accordance with the Ontario *Environmental Protection Act R.S.O. 1990, C e. 19* and Ontario

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*Regulation 153/04.* The purpose of the ESA was to assess the potential presence of environmental impacts at the site due to activities, past and present, on or near to the site.

The report concluded that nothing was found that would likely have resulted in impacts to the soil and groundwater on site. A Phase 2 ESA would not be required to be completed.

#### 5.2 GEOTECHNICAL STUDY AND SLOPE STABILITY

On March 7, 2019 a Geotechnical Investigation of the site was completed by drilling 29 boreholes to a maximum depth of 4m and taking soil samples from them.

The flat, grass and tree covered surface conditions were underlain by a layer of stiff to very stiff silty clay, followed by a grey very stiff to stiff silty clay layer. Glacial till was encountered below the silty clay layer consisting of compact silty sand to sandy silt with clay, gravel, cobbles and boulders. Bedrock was encountered at depths ranging from 1.6 m to 4.0m.

Groundwater level is anticipated at a depth ranging from 2.5m to 3.5m below existing grade.

A slope stability analysis was also conducted. This was carried out to determine the required stable slope allowance setback from the top of slope. It was concluded that a stable slope allowance was not required. Existing vegetation on the slope should not be removed as it contributes to the stability of the slope and reduces erosion

From a geotechnical perspective the subject site is suitable for the proposed development.

#### 5.3 TRANSPORTATION IMPACT STUDY

A Traffic Impact Assessment (TIA) was conducted in accordance with the City of Ottawa "*Transportation Impact Assessment Guidelines.*"

The TIA concluded that the proposed development can be supported and should be permitted to proceed from a transportation perspective.

#### 5.4 SERVICING AND STORMWATER MANAGEMENT

A Servicing and Stormwater Management Brief was prepared that examined water, sanitary and stormwater servicing to the site.

Based on the supplied boundary conditions for existing watermains and estimated domestic and fire flow demands for the subject site, it is anticipated that the proposed

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servicing in this development will provide sufficient capacity to sustain both the required domestic demands and emergency fire flow demands of the proposed site. Fire flows greater than those required per the FUS Guidelines are available for this development. Pressure reducing valves are recommended for all proposed buildings within the development in order to satisfy the design guidelines/OBC requirements for average day operating pressures.

The proposed sanitary sewer network is sufficiently sized to provide gravity drainage of the site. The proposed site will be serviced by a gravity sewer which will direct the wastewater flows (approx. 8.8 L/s) to the existing 675mm dia. Hazeldean Road sanitary sewer. The proposed drainage pattern is in accordance with the Kanata West Master Servicing Report for the Hazeldean Road sewer.

The proposed stormwater management plan is in compliance with the criteria established for the site. Rooftop and subsurface storage has been designed to limit outflows from the subject site to calculated predevelopment levels. Poole Creek is located downstream of the site and has sufficient capacity to receive runoff volumes from the site based on anticipated peak flows and detention times for the subsurface storage tank servicing the development.

Grading for the site has been designed to provide an emergency overland flow route as per City requirements and reflects the grade raise restrictions recommended in the Supplemental Geotechnical Investigation prepared by Paterson Group (March, 2019) . Erosion and sediment control measures will be implemented during construction to reduce the impact on existing facilities.

Utility infrastructure exists within the Hazeldean Road ROW at the south property boundary of the proposed site. Overhead poles are located along the south side of Hazeldean Road. It is anticipated that existing infrastructure will be sufficient to provide a means of distribution for the proposed site. Exact size, location and routing of utilities will be finalized after design circulation.

MECP Environmental Compliance Approval is not expected to be required for the proposed site works. A Permit to Take Water is not anticipated to be required for pumping requirements for sewer installation, however, will likely be a requirement for building excavation. The Mississippi Valley Conservation Authority will need to be consulted in order to obtain municipal approval for site development. No other approval requirements from other regulatory agencies are anticipated.

## **5.5 NOISE FEASIBILITY STUDY**

A Noise Feasibility Study was prepared to compare the predicted sound levels in comparison to the guidelines of Ontario Ministry of the Environment, The Conservation and Parks (MECP) and the City of Ottawa Environmental Noise Control Guidelines . The

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investigation determined the noise levels will exceed MECF guideline sound levels and will require noise control measures.

The future daytime and nighttime sound levels at the façade with exposure to Hazeldean Road will exceed the MECF guideline sound levels and will require noise control measures. Central air conditioning is required for all dwelling units in the building. Upgraded building construction will be required for the south façade facing Hazeldean Road to provide acoustical insulation for indoor spaces.

As well, the sound emissions from the existing commercial facilities to the west has potential to exceed the applicable noise limits of the MECF at the exposed ground level façade at the northwest corner of the proposed building facing the commercial facilities. An acoustic barrier may be required.

- Central air conditioning will be required for Phase 2 dwelling units
- For the dwelling units with exposure to roadways, window glazing construction should be refined on actual window to floor ratios
- The use of warning clauses in the property and tenancy agreements is recommended to inform future residents of traffic noise issues
- An acoustic barrier in front of the residences at the west property line
- An additional warning clause is required to inform future occupants of the presence of existing commercial facilities and the installation of the barrier.

## 5.6 ENVIRONMENTAL IMPACT STATEMENT

An Environmental Impact Statement was prepared to summarize the existing natural environment, ecological functions and significant features on-site to support the Environmental Impact Statement that identifies potential impacts and makes recommendations for measures to protect the natural environment.

The following are the recommended mitigation and enhancement measures based on the impact assessment according to EIS guidelines provided by the City of Ottawa and the MVCA:

- The wetland to the north of the site is currently unidentified and therefore has no status
- The Poole Creek as cool/cold watercourse that provides critical nursery habitat for all species within the creek, mitigation measures will be required for any potential negative impacts. Exclusion fencing to the sensitive natural features should be established and protected
- Within the exclusion zone established there is no grading or construction proposed

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Tree and vegetation removal:

- Tree and vegetation removal is limited to the construction foot print
- A butternut agreement is in place between the City of Ottawa and the previous owner of the site. The agreement has been included with this submission.
- Minimize or avoid impacts to breeding and nesting birds specifically during April 15 and August 15
- Wild Parsnip was confirmed to be present, this is toxic and should be handled carefully

Based on the findings mitigation measures will be put in place for the protection of species, trees and vegetation.

## 5.7 STAGE 1 ARCHAEOLOGICAL ASSESSMENT

A Stage 1 Archaeological Assessment was completed and will be filed with the Ontario Ministry of Tourism, Culture and Sport. It was determined that a Stage 2 similar to that in Wellings of Stittsville Phase 1 will be required. A Stage 2 Archaeological Assessment was prepared for the Wellings Phase 1 site and it supports the conclusion found in this assessment.

## 5.0 CONCLUSION

The application for Site Plan Control by NLG 2011 Inc. for a mixed-use development at 20 Cedarow Court has been fully reviewed from a land use planning perspective.

It is our professional planning opinion that the application for Site Plan Control should be approved for the following reasons;

1. The approval of the application is consistent with the Provincial Policy Statement 2020.
2. The site plan is in conformity with the City of Ottawa Official Plan.
3. The site plan is in conformity with the Kanata West Concept Plan and its Design Guidelines for Community Retail District.
4. The site plan is in conformity with the intent of the Zoning By-law.
5. The site plan, landscape plan and elevation drawings meet the relevant Arterial Mainstreet Design Guidelines.
6. The site plan represents good land use planning.
7. The site plan will assist the City of Ottawa in meeting the need for seniors' accommodation and provide commercial, retail and office and employment.

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In summary, the proposed development is based on good planning supported by applicable policies, studies and reports. As outlined throughout this report, it is our opinion that the proposed development represents good planning and should proceed through the process prescribed by the *Planning Act*.

Respectfully,

*Angela Mariani*

Angela Mariani  
Development Planner

## APPENDIX 1 – BUILDING RENDERINGS



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