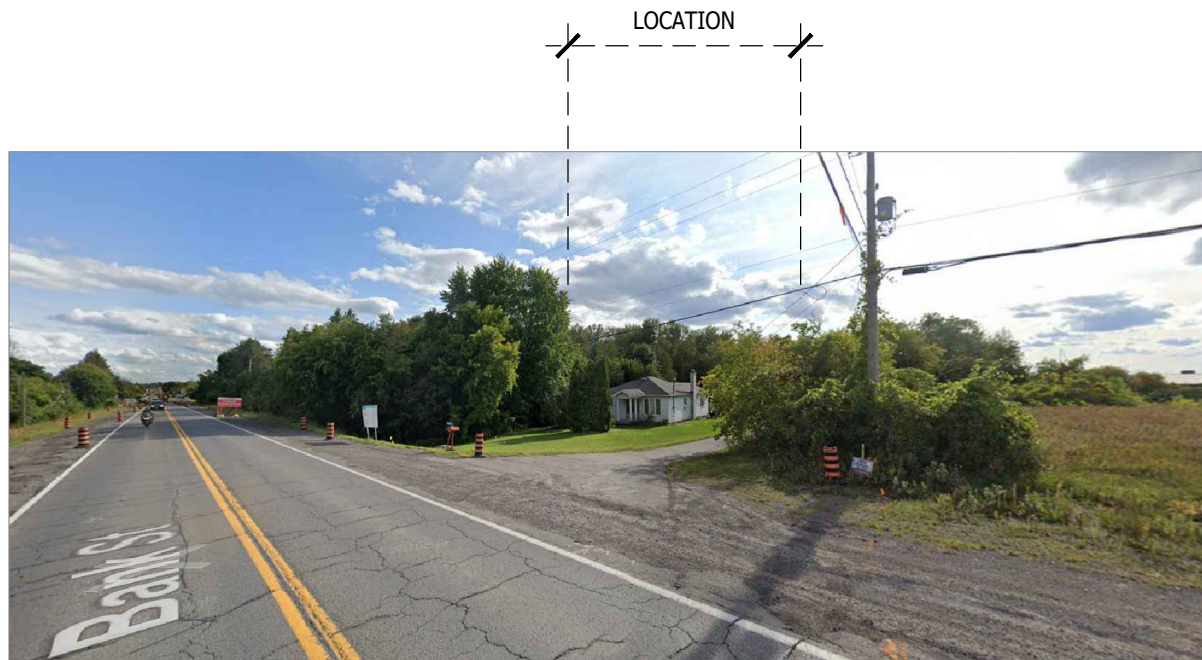


PLANNING RATIONALE

5254 Bank Street, Ottawa ON - Service Repair Facility



DRAWINGS

- 01 . APPLICATION SUBMISSION
- 02 . CONTEXT ANALYSIS
- 03 . COMPATIBLE LAND USE
- 04 - PROPOSAL - SITE
- 05 - PROPOSAL - 3D VIEWS
- 06. PROPOSAL - 3D VIEWS
- 07 . PROPOSAL - 3D VIEWS
- 08. PROPOSAL - 3D VIEWS
- 09 . PROPOSAL - ELEVATIONS
- 10. PROPOSAL - BUILDING SECTION

Planning Rationale - 5254 Bank Street, Ottawa ON - Service Repair Facility

APPLICATION SUBMISSION

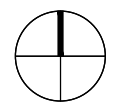
The purpose of this application is to release from 'hold' the previous approved zoning by-law amendment (reference ACS2021-pie-PS-0111) and for Site Plan Control Submission for development of a new Service Repair Facility. The by-law amendment is to alter the existing zoning from Rural Countryside (RC) to Rural General Industrial (RG).

This submission follows a pre-consultation with the city planning team on May 17, 2022 and subsequent comments attached to our pdf files on January 9th, 2024 by planning staff.

The property subject to this submission is 5254 Bank Street, Part of Lot 28, Concession 4 (Rideau Front), Geographical Township of Gloucester, City of Ottawa.



key plan



true north

Planning Rationale - 5254 Bank Street, Ottawa ON - Service Repair Facility

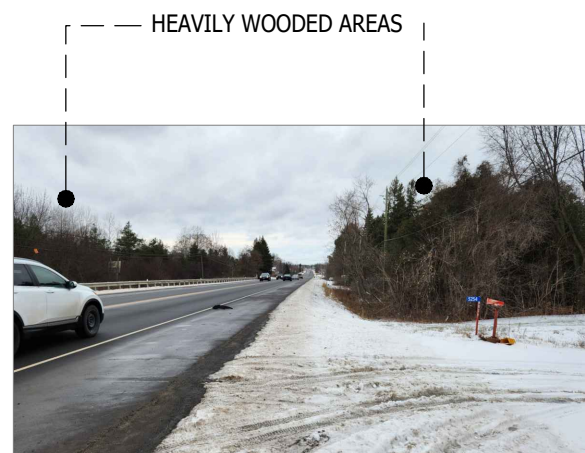
CONTEXT ANALYSIS

The 1704 sq.m, rural lot is surrounded by an Rural General Industrial (RG)1 zoned property to the north and Mineral Extraction (ME) properties to the south and east. Other nearby properties are zoned as Rural Commercial (RC) and Rural Countryside (RC).

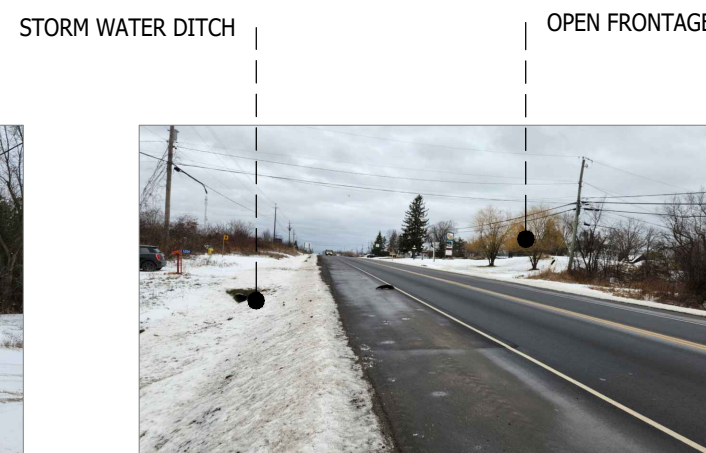
The property located on Bank Street is defined as a Scenic Entry Route to the city in the City of Ottawa Official plan. It's currently a 2 lane road edged by open ditches, dense wooded areas and rural properties. Buildings along this section of Bank Street are setback from the street, typically not visible from the road and spaced intermittently. There are no roadway provisions for cyclists nor sidewalks for pedestrians. It's a vehicle centric context.



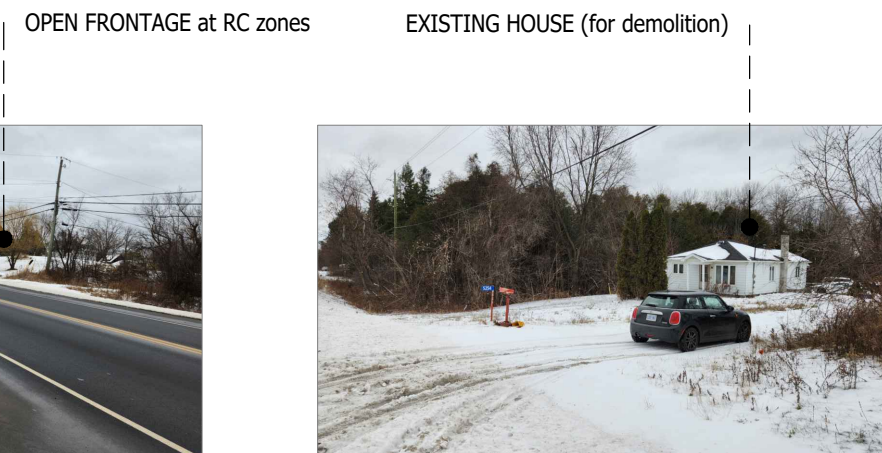
zoning plan



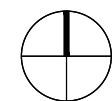
at site entry looking south down Bank Street



at site entry looking north down Bank Street



site entrance



true north

Planning Rationale - 5254 Bank Street, Ottawa ON - Service Repair Facility

COMPATIBLE LAND USE

The compatibility between the proposed zoning of this property as Rural General Industrial and the surrounding properties have been reviewed in consultation with the Ontario's current draft Land Use Compatibility Guideline.

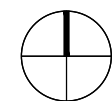
The property located on Bank Street is to be built as an industrial facility for storage and auto service repairs and does not contain any uses typically associated as a *sensitive* such as residential, daycare, hotel, community centres, commercial, or places of worship. It is however surrounded on the south, east and 350m to the north by aggregate extraction facilities. These facilities come with characteristics of noise, dust and vibration during operational hours - and - in reference to Ontario's guide, and multiple visits on site at different times of day, these characteristics can be summarized as Glass 1 or low Class 2 indicating a very low or infrequent chance of these characteristics being perceived on this property.

Should the surrounding properties expand their operations closer to their property line limits and noise, dust and vibration increases in the future, the nature of the *non-sensitive*, automotive repair facility proposed here will not be adversely affected in its operations nor the health of the occupants - who work under a variation of these similar conditions. Given this, this rationale sees no issues of compatibility between this RG site and the surrounding ME zone.



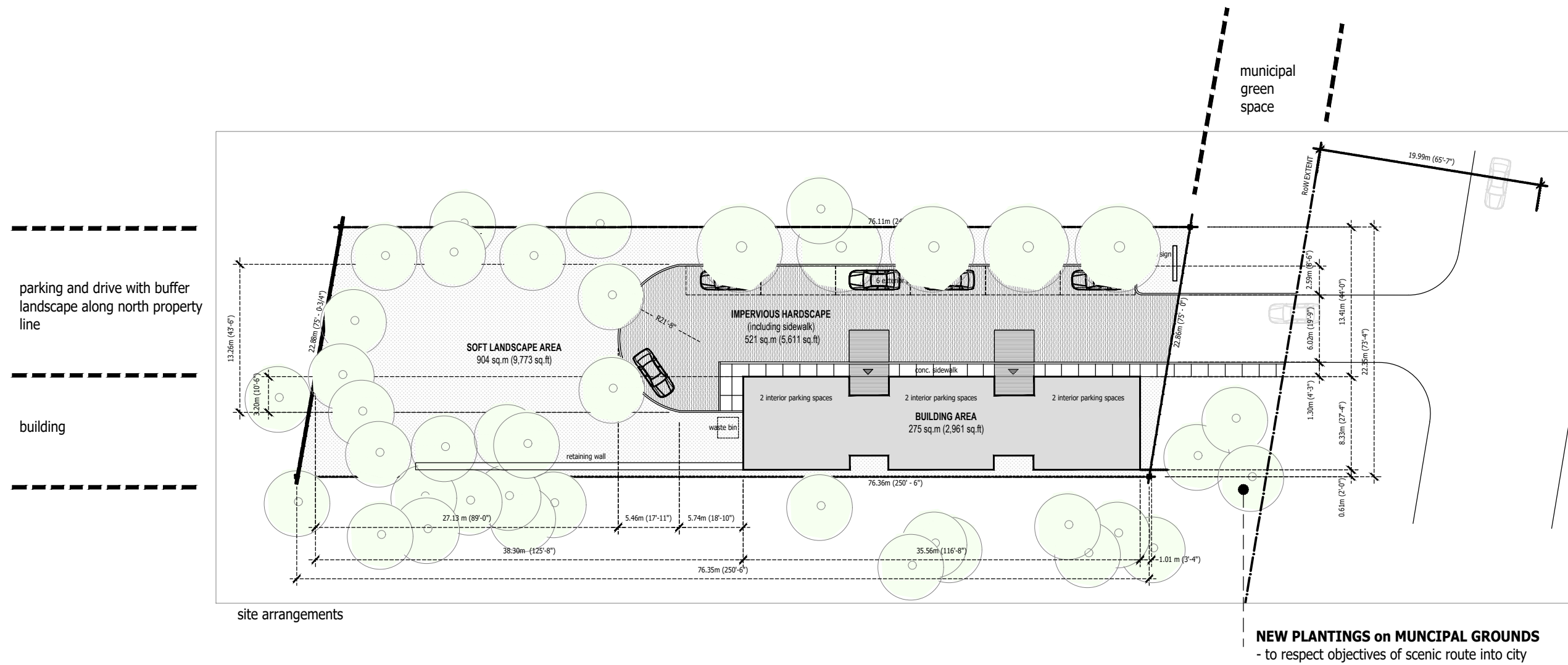
land use plan

LOCATION OF PROPERTY



true north

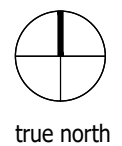
Planning Rationale - 5254 Bank Street, Ottawa ON - Service Repair Facility



PROPOSAL

The proposal is to demolish the existing buildings on the site and construct a new single storey with mezzanine service repair facility along the south property line. Vehicle entry, circulation, parallel parking and planted green space is planned on the northern side of the property.

The rear of the property has a substantial drop in grade of approximately 3m and the development of the property maintains this drop through terraced re-grading which will also accommodate an underground septic system. Natural plantings will be comprised of non-invasive species and a retaining wall along the south property line will be included to support the terracing of the grade.

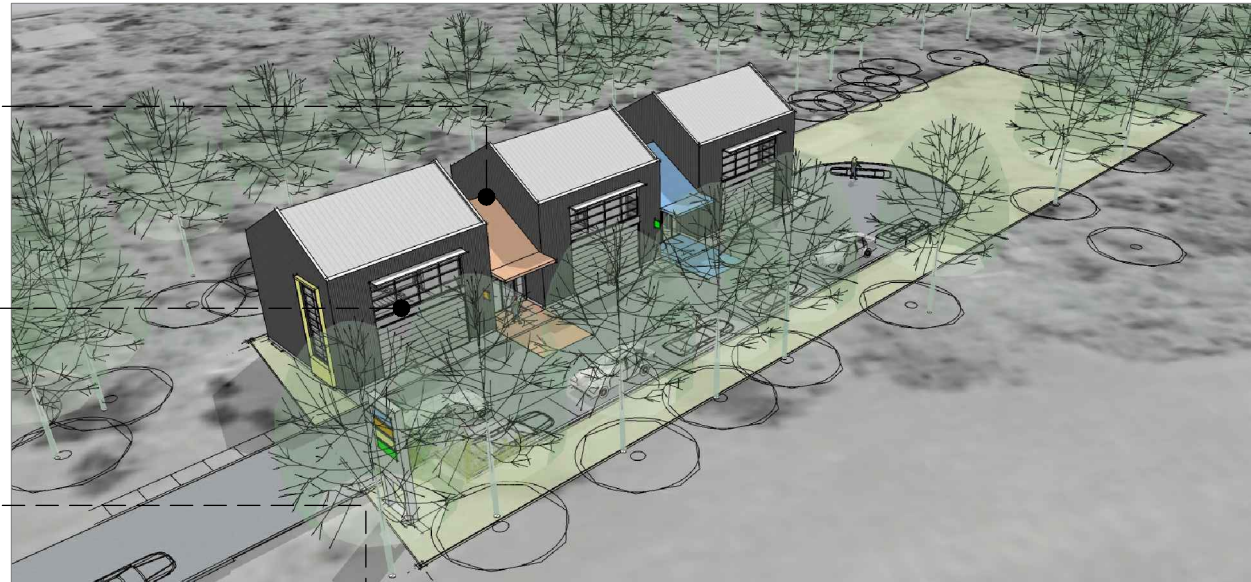


Planning Rationale - 5254 Bank Street, Ottawa ON - Service Repair Facility

BUILDING MASSING
- 3 volumes with entry points at connection points

ARTICULATED FACADES
- with overhead doors, transoms and canopies

PEAKED ROOFS
- in keeping with surrounding small scale buildings



LINE of TREES
- along north edge

PARALLEL PARKING

TURNING CIRCLE



3D views

5254 Bank Street, Ottawa ON - Service Repair and Personal Services Building

3D VIEWS



NATURAL LANDSCAPING
- with buried septic system and naturally grown foliage

ACCENT WINDOW SURROUNDS
- to provide interest along Bank Street scenic route

ENTRANCES
- at building breaks

3d view

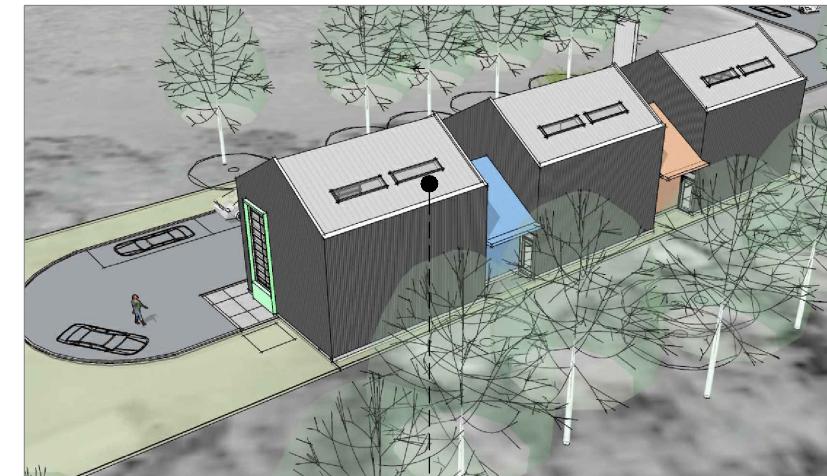
5254 Bank Street, Ottawa ON - Service Repair and Personal Services Building

3D VIEWS

CLEARSTORY WINDOWS
- above overhead doors



SKYLIGHTS
- along south side



5254 Bank Street, Ottawa ON - Service Repair and Personal Services Building

3D VIEWS



SIGN STRUCTURE - - - - -

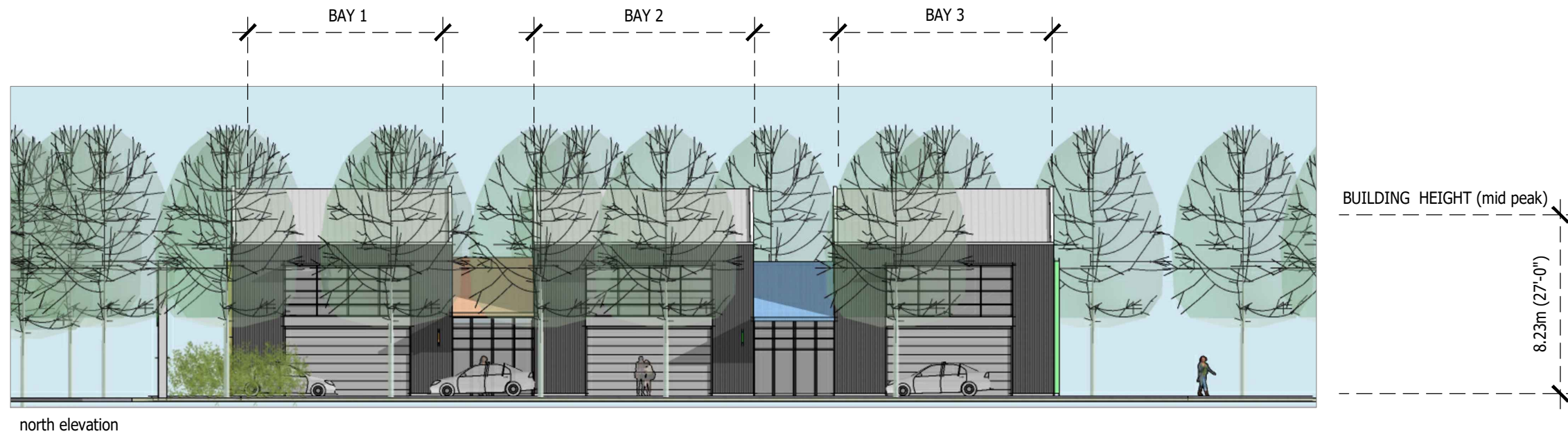
CANOPY DETAIL
- above clearstory windows



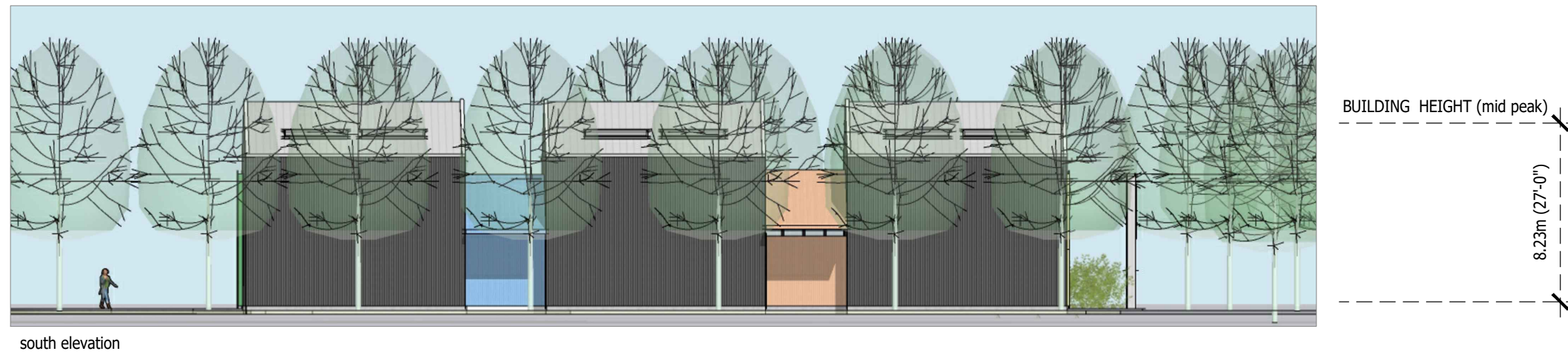
PAVERS
- in roadway to visually identify entrances

5254 Bank Street, Ottawa ON - Service Repair and Personal Services Building

ELEVATIONS



north elevation



south elevation