

Proposed Mixed Use Building

3745 St. Joseph Boulevard

**Applications for Zoning By-law Amendment
and Site Plan Control Approval**

Planning Rationale

Revised February 2024

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Planning Rationale

3745 St. Joseph Boulevard
Ottawa, ON

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1.0 Introduction

13890767 CANADA INC. is proposing to construct a six-storey mixed-use building at 3745 St. Joseph Boulevard in Ottawa, Ontario. The site is located approximately 700 metres southwest of the intersection of Trim Road and Ottawa Regional Road 174 (Figure 1). The site is currently undeveloped.

Figure 1: Site Location (GeoOttawa, 2023)



A Zoning By-law Amendment and Site Plan Control approval are being sought at this time to enable the proposed mixed-use building. This Planning Rationale has been prepared as a supporting document to these applications. It is intended to be of assistance to the City of Ottawa in evaluating the proposal in the context of land use policies of the Provincial Policy Statement, the new Official Plan (2022), and the Comprehensive Zoning By-law.

This Application also includes the following supporting plans and studies:

1. Planning Rationale, prepared by Parsons Inc, dated February 2024
2. Site Plan Package, including Site Plan, Parking Level, Podium, Upper-Level Floor Plans and ground floor plan, Elevations and Renderings prepared by CSV Architects, dated February 2024
3. Design Brief, prepared by CSV Architects, dated May 2023
4. Tree Conservation Report and Landscape Plan, prepared by James B. Lennox & Associates Inc. Landscape Architects, dated February 2024
5. Site Servicing and Stormwater Management Report, prepared by McIntosh Perry, dated February 2024
6. Removals, Erosion & Sediment Control, and Lot Grading & Drainage Plan, prepared by McIntosh Perry, dated February 2024

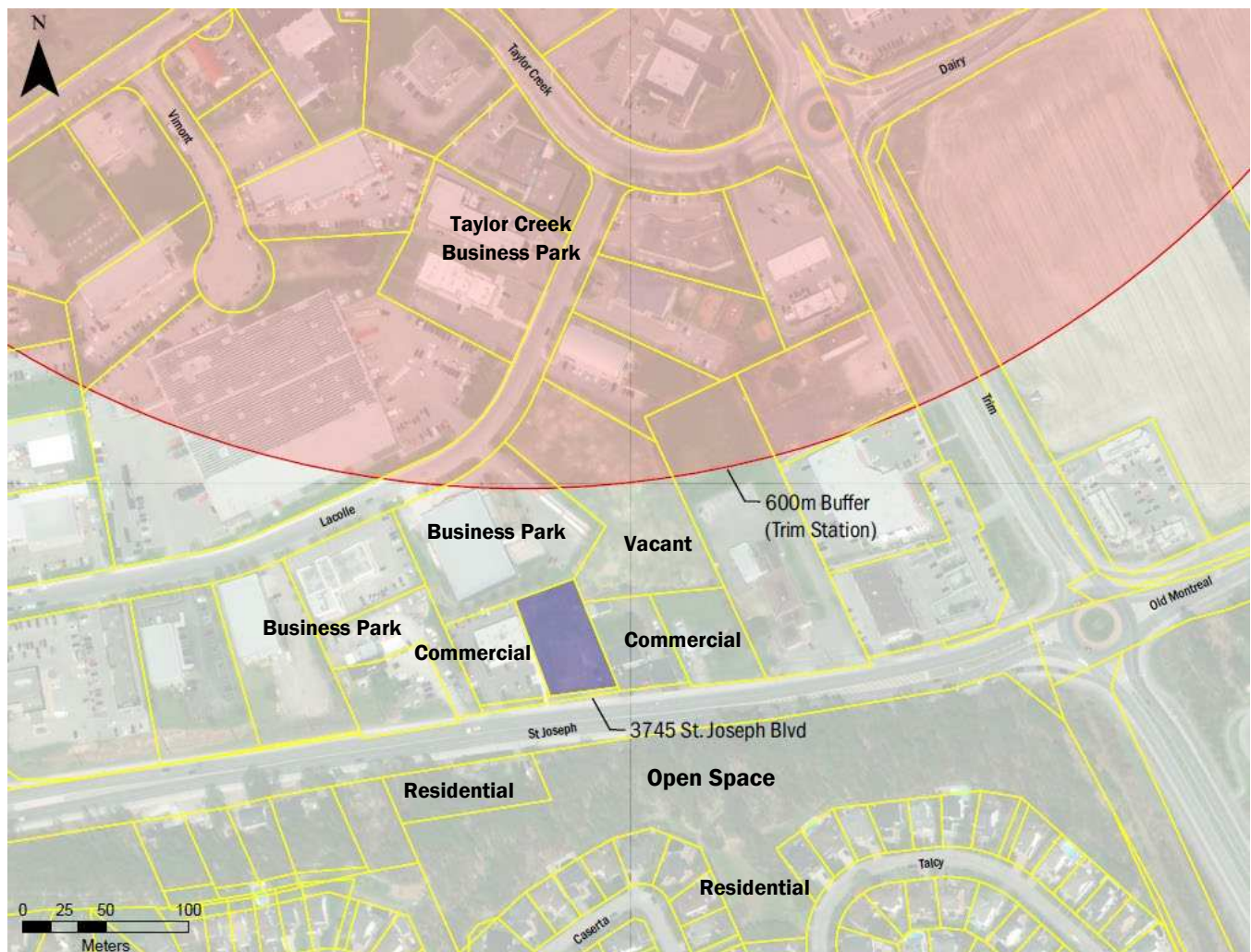
7. Site Servicing Plan, prepared by McIntosh Perry, dated February 2024
8. Noise and Vibration Assessment, prepared by Gradient Wind Engineers & Scientists, dated May 2023
9. Phase 1 Environmental Site Assessment, prepared by Paterson Group, May 2014 and an update provided in February 2023
10. Geotechnical Report, prepared by Paterson Group, dated August 2023
11. Grading and Servicing Plan Review, prepared by Paterson Group, dated August 2023
12. Scoped Transportation Impact Assessment, prepared by Parsons Inc, dated October 2023
13. Plan of Survey, prepared by Annis O’Sullivan Vollbeek, dated May 2022.

A Pre-consultation Meeting was held with City staff on January 12, 2023. Prior to this meeting a Transportation Impact Assessment (TIA) Screening Form was completed for the project. Correspondence from this meeting, including the list of studies and plans required as part of this application, are included in **Appendix A, Pre-consultation Correspondence**.

2.0 Site and Context

The site which is the subject of this Zoning By-Law Amendment and Site Plan Control application (the “Site”) is municipally addressed as 3745 St. Joseph Boulevard (**Figure 2**). The Site is legally described as PART LOT 31, CONCESSION 1, CUMBERLAND OLD SURVEY, PARTS 1, 2, 6, 7, 8, 9, 10, 11 AND 12, PLAN 4R-32177. The site is 2,502 m² (0.25 ha) in area. The Site has frontage of approximately 40 metres which provides access from St. Joseph Boulevard (**Figure 3**).

Figure 2: Surrounding Context for Site



The site is currently undeveloped. The Site is located just south of the Taylor Creek Business Park that house a range of light industrial and commercial and service uses. The immediate surroundings of the Site consist of other commercial uses as noted below.

- **West:** An existing commercial building.
- **East:** A converted residential building to commercial to east now housing a travel agency for the majority of the lot line and a vacant land at the north end of the lot line.
- **North:** A commercial warehouse building.
- **South:** A vegetated ridge which rises to the south bordering the neighbourhood of Fallingbrook and includes some single-family homes.

The images below illustrate the surrounding context.

Figure 3: View of the Site from St. Joseph Boulevard (Google 2021)



The image shows the site from St. Joseph Boulevard illustrating surrounding commercial, warehouse and vacant lands. A sidewalk and on-road bike lane are located along St. Joseph Boulevard. The image also illustrates the existence of overhead power lines.

Figure 4: View from Lacolle Way, looking South (Google 2021)



The image shows the site in behind the commercial warehouse building and vacant lands along Lacolle Way and illustrates the grade change from Lacolle Way sloping up towards St. Joseph Boulevard.

As part of the Urban Road Network, St. Joseph Boulevard is classified as an Arterial (Existing) (Figure 5). As part of the Transit Network, this segment of St. Joseph Boulevard is not designated, however the O-Train (Phase 2) will run to the north, and there is a Conceptual Future Transit Corridor indicated to the east (Figure 6).

Figure 5. Urban Road Network with Site location indicated, Official Plan, 2021, Schedule C4

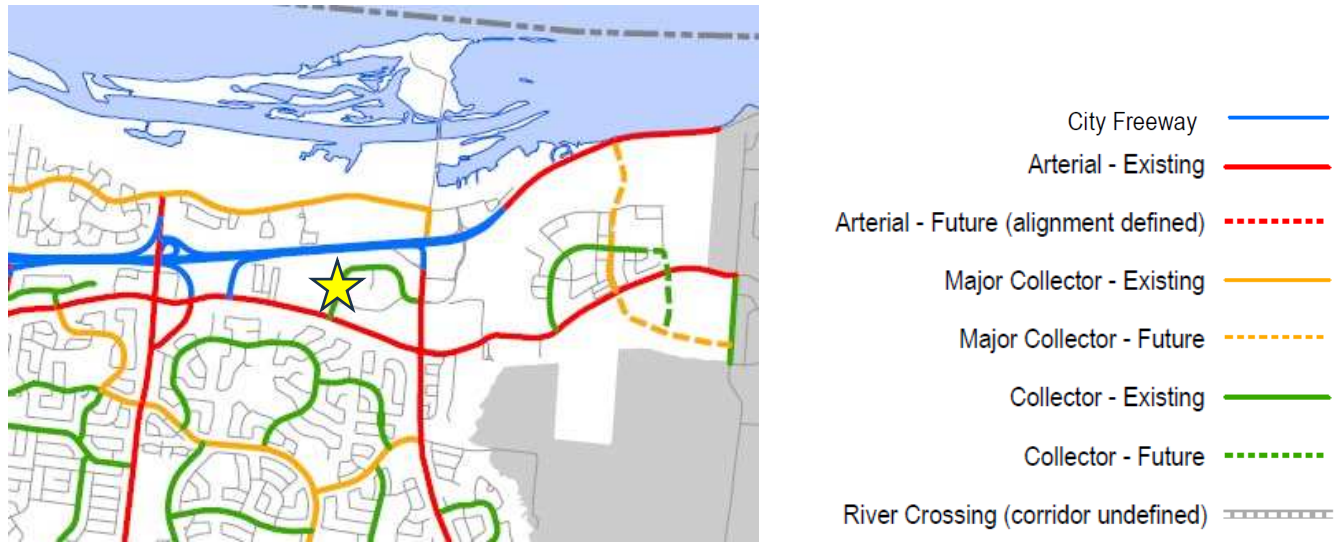
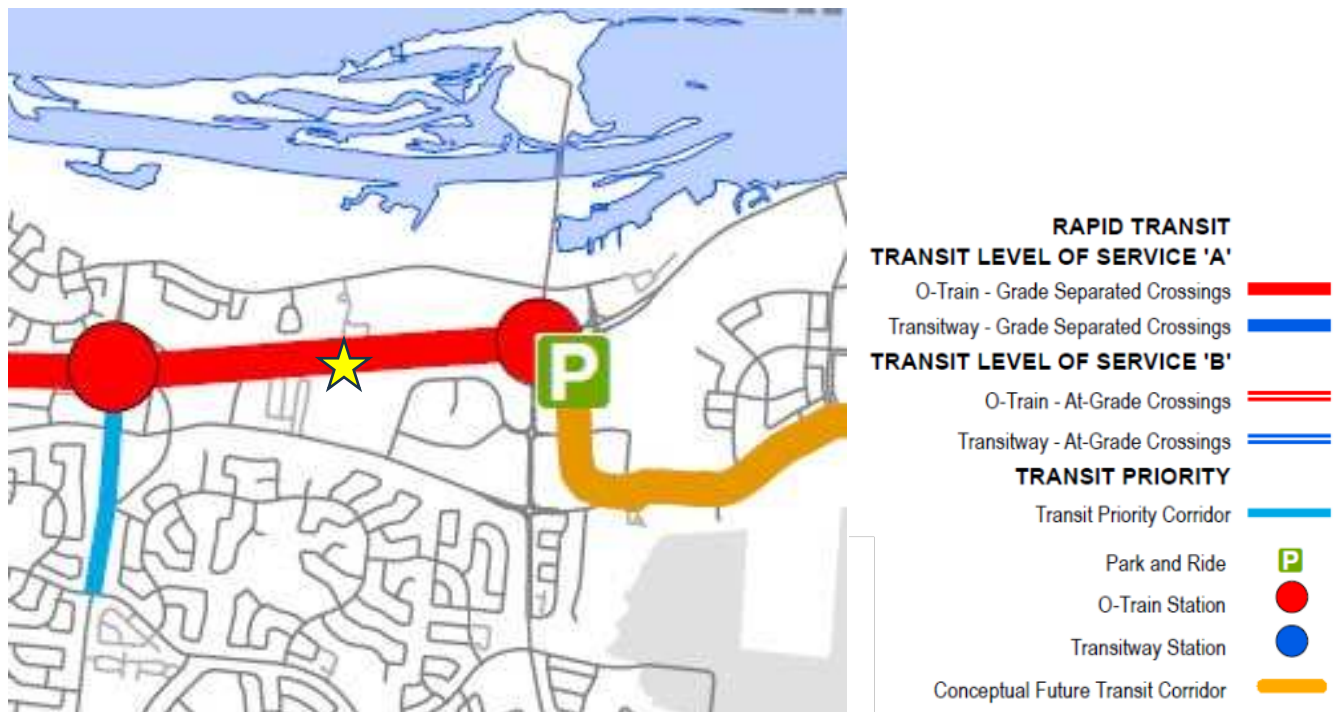


Figure 6. Transit Network with Site location indicated, Official Plan, 2021, Schedule C2



3.0 Development Proposal

The current development proposal is to enable construction of a new six-storey mixed-use building to contain an approximately 61-room hotel with associated co-working spaces, multi-purpose room and gym. The building will also contain opportunities for commercial spaces including restaurant uses and retail. Site modifications associated with the proposed development include:

- Construction of a 6-storey mixed-use building of 3,828 square meters of gross floor area;
- Construction of 2 and ½ levels of underground parking containing 76 underground vehicle spaces and one (1) passenger drop-off space. The total vehicle parking spaces includes five (5) accessible spaces located within the parking garage. Access to the underground parking is via a driveway on the west side of the building, leading to an entrance ramp to the underground parking garage from the north side of the building;
- Provision of 20 covered outdoor bicycle parking spaces. Ten spaces are located at the north end of the building, near the entrance to the parking garage, and 10 are located at the main entrance to the building.
- Landscaping buffers surrounding the surface parking areas, along with a landscaped outdoor amenity area at the rear and east side of the building. The plan also includes patio areas at the front of the building along St. Joseph Boulevard as well as a rooftop terrace;
- Installation of a black chain link fence with vegetation on the east side of the building; and
- Installation of water, sanitary and storm infrastructure to service the new building.

Site Development Statistics are provided in **Table 3-1** below.

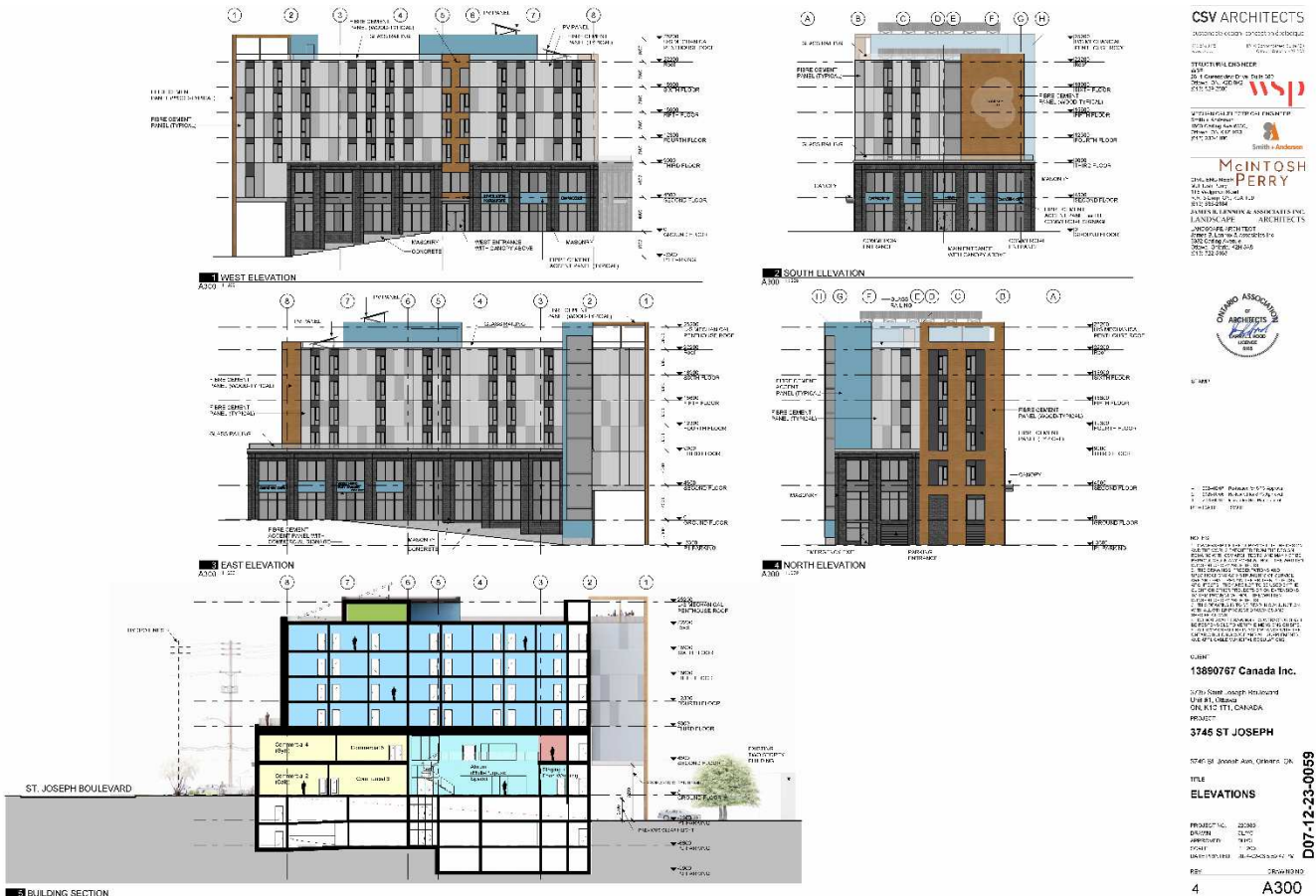
Table 3-1. Site Development Statistics

Use	Proposed Area
Site Area	2,502 m ² (0.25 ha)
• Hotel	
– 61 Hotel Rooms	2,565 m ²
– Gym	186 m ²
– Co-Working Space	253 m ²
– Multi-Purpose Event Space	175 m ²
• Day Program (Instruction Facility)	171 m ²
• Commercial 1 (Retail)	81 m ²
• Commercial 2 (Restaurant/Café)	82 m ²
• Commercial 3 (Retail)	93 m ²
• Commercial 5 (Retail)	84 m ²
• Restaurant (Full Service)	102 m ²
Total Gross Floor Area	3,791 m²
Passenger Drop-Off Parking Space (surface)	1
Standard Parking Spaces (below ground)	76
Total Vehicle Parking Spaces Provided	77
Total Barrier Free Spaces included in Total	5
Bicycle Parking Spaces	20

Figure 10. Building Rendering – Street View from St. Joseph Boulevard, Looking West, prepared by CSV Architects



Figure 11: Development Proposal - Building Elevations, prepared by CSV Architects



4.0 Planning Rationale

4.1 Requested Planning Approvals

To implement the modifications to the site associated with the development proposal, applications for a Zoning By-law Amendment and Site Plan Control Approval are required. This Planning Rationale has been prepared to review the provincial and municipal land use planning policies that guide development on the Site. It provides our professional land use planning opinion on compliance with applicable policy.

4.2 Provincial Policy Statement

The Provincial Policy Statement (PPS), issued under Section 3 of the *Planning Act* (revised 2020), provides policy direction on matters of Provincial interest and sets the rules for land use planning in Ontario. It includes policies on managing growth, use and management of natural resources, protecting the environment, and public health and safety. The *Planning Act* requires that decisions affecting planning matters “shall be consistent with” policy statements.

The proposed commercial node development meets the following policies of the PPS:

1.1.1 a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;

1.1.1 b) accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;

1.1.1 c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;

1.1.3.2 Land use patterns within settlement areas shall be based on densities and a mix of land uses which:

a) efficiently use land and resources;

b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;

f) are transit-supportive, where transit is planned, exists or may be developed;

1.1.3.6 New development taking place in designated growth areas should occur adjacent to the existing built-up area and should have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities.

The *Planning Act* requires that decisions affecting planning matters “shall be consistent with” policy statements.

Planning Response: The development proposal represents an efficient development pattern and is underpinned by broad principles of sustainability, by optimizing the use of land via intensification on an existing site. The development contributes to the success of an existing mix of various commercial uses along an arterial road in proximity to residential neighbourhoods and the Taylor Creek business park offering hotel and conference opportunities for the area. Sidewalk and cycling connections from the future Trim Road LRT Station will encourage and support the use of active and sustainable forms of transportation to/from the proposed land use. The development will also make use of existing municipal services along St. Joseph Boulevard and to the rear of the site without required upgrades to the municipal systems.

The supporting plans and studies demonstrate that the proposed development can be accommodated on the Site without any anticipated impacts to the health and safety of the surrounding community or natural environment. The site does not contain nor is anticipated to impact any resources such as natural heritage or water, agriculture, minerals and petroleum, mineral aggregate or cultural heritage and archaeological resources.

It is our opinion that the development proposal is consistent with the *Provincial Policy Statement*.

4.3 City of Ottawa Official Plan (2022)

A new Official Plan was adopted by Council on November 24th, 2021, as by-law 2021-386. The OP was approved by the Ontario Ministry of Municipal Affairs and Housing with 30 amendments and entered into full force and effect on November 4th, 2022.

The City of Ottawa Official Plan directs how the city will grow over time and sets out policies to guide the development and growth of the city to the year 2046 and beyond. The OP is laid out in thirteen sections. Section 8 (Greenbelt Designations) and Section 9 (Rural Designations) are not applicable to this application. Sections 11, 12 and 13 are regarding Implementation, Local Plans and Definitions. The remaining sections of the OP are reviewed below and planning responses are presented.

Section 2.1 - The Big Policy Moves. Section 2.1 – The Big Policy Moves. Section 2 of the OP outlines the City’s five broad policy directions as the foundation to becoming the most livable mid-sized city in North America over the next century. The five big moves are:

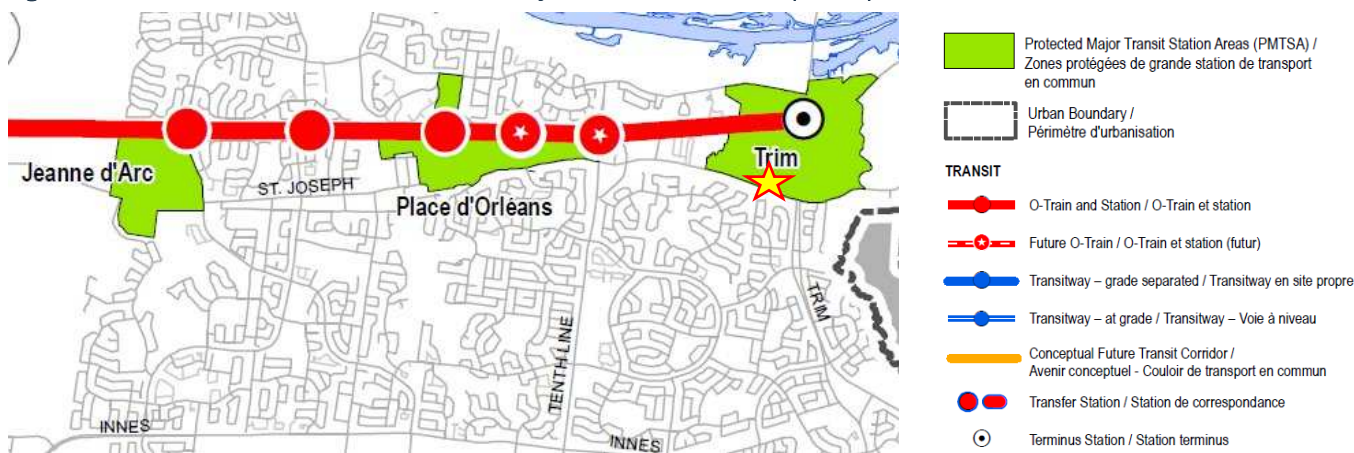
1. *Achieve, by the end of the planning period, more growth by intensification than by greenfield development.*
2. *By 2046, the majority of trips in the city will be made by sustainable transportation.*
3. *Improve our sophistication in urban and community design and put this knowledge to the service of good urbanism at all scales, from the largest to the very small;*
4. *Embed environmental, climate and health resiliency and energy into the framework of our planning policies;*
5. *Embed economic development into the framework of our planning policies.*

Section 2.2 - Cross-cutting Issue. Six cross-cutting issues essential to the achievement of a liveable city are identified in the new OP and are implemented through the policies in multiple sections of the Plan:

1. *Intensification*
2. *Economic Development*
3. *Energy and Climate Change*
4. *Healthy and Inclusive Communities*
5. *Gender Equity*
6. *Culture*

Section 3 – Growth Management Framework – Section 3 of the OP directs that “most growth will occur within the urban area of the City, with a majority of residential growth to be within the built-up area through intensification, increasing over time during the planning horizon.” This section contains projected population, households and employment for the City of Ottawa to the year 2046 as well as residential and employment density targets for certain areas of the City. Focus areas for intensification are directed to lands designated as *Downtown Core, Corridor, Industrial and Logistics, Mixed Industrial and Special District Designations*. The site is located with the **Trim Protected Major Transit Station Area (Figure 12)** and designated as **Mixed Industrial (Figure 14)**. *Protected Major Transit Stations Areas (PMTSA)* outside *Hubs*, do not have specific density targets and are not considered PMTSA’s as it relates to the PPS.

Figure 12: Official Plan, Schedule C1 – Protected Major Transit Station Areas (PMTSA)



Section 3.5 – Meet Employment Needs. The OP outlines where different employment sectors are intended to occur and how employment is defined in the Provincial Policy Statement. Lands within the *Industrial and Logistics* and *Rural Industrial and Logistics* areas are considered employment areas for the purposes of the Provincial Policy Statement. Business Parks that have a mixture of industrial and non-industrial uses such as small-scale office and other service-oriented uses are located within the *Mixed Industrial* areas.

Section 4 – City-Wide Policies. Section 4 of the OP contains City-wide policies. Those that relate to the proposed development include the following.

Section 4.1 – Mobility. The City’s transportation policies reflect councils commitment towards more equitable, safe and healthy communities and climate action. The City will rely on space- and cost-effective modes of transportation to accommodate the projected growth and intensification targets of the plan and support a shift towards sustainable modes of transportation and multi-modal travel.

Section 4.6 – Urban Design. As described in the OP, “*Urban Design is the process of giving form and context to our city to create the theatre of public life. It concerns both the built form and the public realm*” The Plan encourages innovative design practices and technologies in site planning and building design.

Section 4.7 – Drinking Water, Wastewater and Stormwater Infrastructure. Policies related to drinking water, wastewater and stormwater infrastructure have been established to ensure that infrastructure in the city is safe, affordable, environmentally sound and meets the need of the future. Development applications are required to mitigate the impacts of additional runoff resulting from increased imperviousness through measures such as site-specific stormwater management.

Planning Response: The proposed development represents intensification of a currently vacant and under-utilized site in the built-up area of Ottawa for a new mixed-use building containing community servicing uses that contribute to a 15-minute neighbourhood serving both the Taylor Creek Business Park and surrounding residential neighbourhoods. The site will be connected by existing pedestrian and cycling facilities to the future Trim LRT Station, provides sheltered facilities for bicycle parking and provides more than the minimum of bicycle parking spaces, and takes a balanced approach to providing sufficient parking but reduced being in proximity to a future transit station.

The development also follows principles of sustainability and resiliency including mass timber construction and solar panels on the roof. The site is currently vacant and will require on-site stormwater management in order to meet release rate requirements into the City’s storm capture systems. The site design includes underground storage in the form of underground storage to meet this requirement.

The proposed development is in keeping with the broad-based, City-wide objectives of the Official Plan.

Section 5 – Transects. Section 5 of the OP divides the City into six concentric policy areas called transects. Each transect represents a different gradation in the type and evolution of the built environment. The site is located within the **Suburban Transect (Figure 13)**. Section 5.4 describes the Suburban Transect as being comprised of neighbourhoods which generally reflect the conventional suburban model, characterized by the separation of land uses, stand-alone buildings, generous setbacks and low-rise building forms. The objective for this area is to recognize the suburban pattern and built form while supporting an evolution towards 15-minute neighbourhoods by introducing transit and active mobility options, support employment opportunities and more urban type development. The suburban transect is generally characterized by low- to mid-density development.

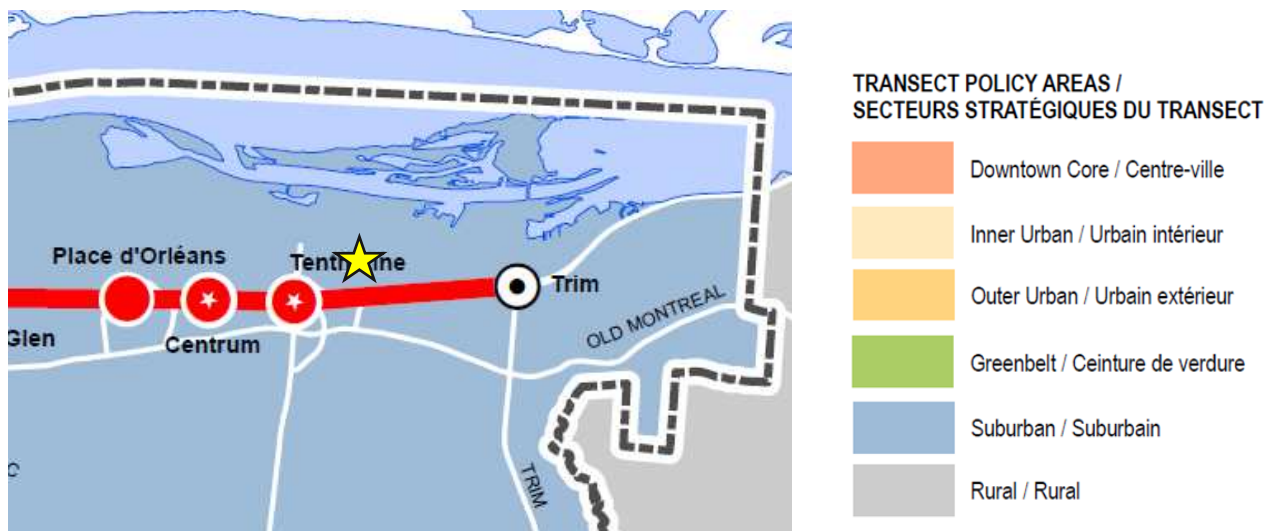
Section 5.4.4 provides direction for new development within the Suburban Transect and will contribute to the evolution towards 15-minute neighborhoods. Policies include:

Traffic flow and capacity may be permitted provided it minimizes negative impacts on the public realm, and maintains the priority of sustainable modes of transportation, and the safety of vulnerable road users (s.5.4.4.1 c);

Active transportation linkages that safely and efficiently connect residential areas to schools, places of employment, retail and entertainment, parks, recreational facilities, cultural assets and transit (s. 5.4.4.1 d);

Treed corridors, including arterial roads and collector streets that are lined with building typologies containing small-scale, street-oriented convenience and neighbourhood commercial services and other neighbourhood-oriented uses, including medium-density residential uses (s.5.4.4.1 g).

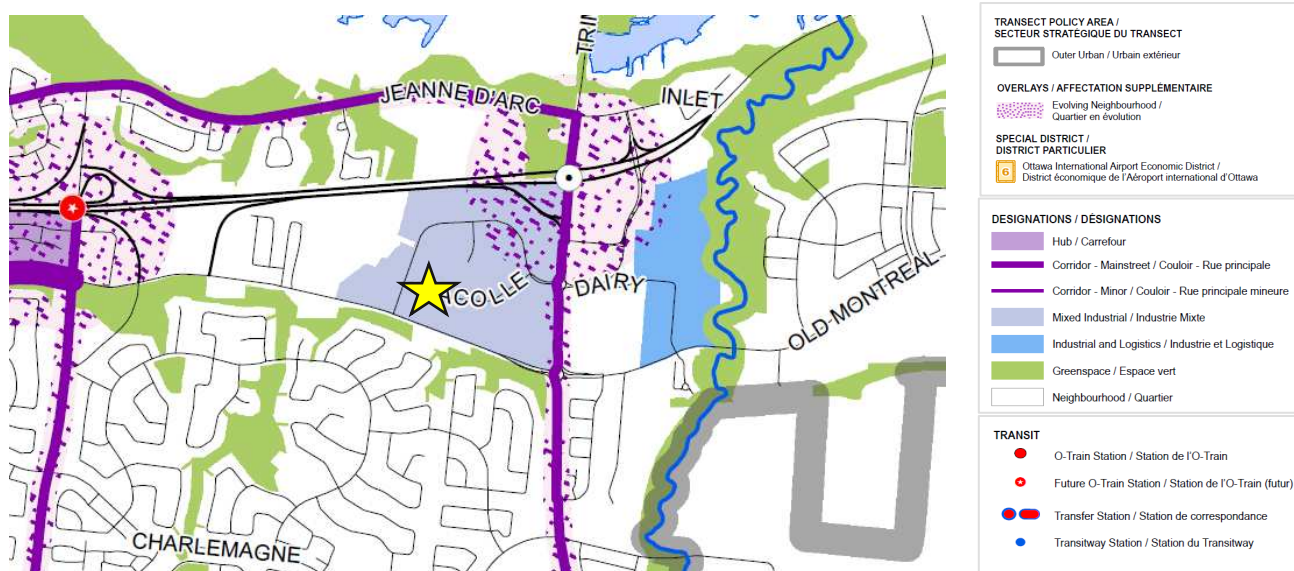
Figure 13. Official Plan Schedule A – Transect Policy Areas



Section 6 – Urban Designations. As noted previously, the site is designated *Mixed Industrial* (Figure 14). *Mixed Industrial* areas can provide a transition between *Industrial and Logistics Areas* and *Neighbourhoods, Hubs or Corridors* and provide a supply of land for non-residential sensitive uses and smaller scale light industrial and commercial uses.” Further, “these areas can contribute to 15-minute neighbourhoods through the location of neighbourhood-based uses.”

Mixed Industrial areas are characterized by a broad mix of uses including small-scale office, light industrial, wholesale, small contractors, small-scale commercial service uses and non-residential sensitive uses such as places of worship, indoor recreational uses and stand-alone licensed care centres that would otherwise not be permitted on lands designated *Industrial and Logistics*. Because the primary uses are not considered a nuisance or have high impacts, these areas can diversify more than Industrial and Logistics areas and can blend more into surrounding neighbourhoods (s. 6.5)

Figure 14. Location of Site on Official Plan (2022), Schedule B8 - Suburban (East) Transect



Pertinent policies within the *Mixed Industrial* area include:

Preserve land for uses that require a business park environment for their operation (s.6.5.1);

Act as a transition between Neighbourhoods and Industrial Areas (s. 6.5.2); and

Permit small scale neighbourhood-based uses along the edge of Mixed Industrial areas where they interface with neighbourhoods (s. 6.5.3).

- The types of permitted services are those of a scale that cater to a local neighbourhood clientele and to the employees working on lands designated Mixed Industrial, including small scale grocery stores, recreational, health and fitness uses and service commercial uses (e.g., convenience retail, doctor and dentist office, coffee shop, restaurant, bank, service station or gas bar) (s.6.5.3.1 a);*
- Commercial service uses shall be located along the edge of the Mixed Industrial area abutting residential neighbourhoods but no more than 600 metres from a residential zone (s.6.5.3.1 c)*

As listed in Section 3.5.3, and as it relates to the proposed hotel use, sensitive land uses may be considered through site-specific amendments to the Zoning By-law provided all the following criteria are met.

- a) There are no adverse health impacts from adjacent uses and the sensitive institutional use does not impede an existing Mixed Industrial use's ability to operate via a Ministry of the Environment, Conservation and Parks permit. The assessment will be guided by the Ministry of the Environment Conservation and Parks Land Use Compatibility Guidelines, as amended,*
- b) There are no traffic impacts that would impede a Mixed Industrial use's operations;*
- c) Is located within 600 metres of a residential zone; and*
- d) Is located a minimum of 300 metres away from zones where heavy industrial land uses are permitted.*

Section 10 – Protection of Health and Safety. – Environmental conditions, whether naturally occurring or not, can result in hazards to human life or health and damage or loss of value to property. One of the objectives is to minimize incompatible land uses. Section 10.2 contains policies to minimize incompatible land uses including policies related to noise control (Section 10.2.1).

*Development proposals for a new or expanded noise sensitive land-use shall require a noise feasibility study and/or detailed noise study pursuant to the Environmental Noise Control Guidelines in the following locations...a
iii) lands designated as Industrial and Logistics or Mixed Industrial or lands zoned for industrial uses.*

Planning Response: The proposed development represents intensification of a currently vacant and under-utilized site in the built-up area of Ottawa for new mixed-use building that will include a hotel and a range of commercial/service uses that are located to serve both the Mixed Industrial uses in the Taylor Creek Business Park and also the adjacent residential neighbourhoods. The proposed development also serves as a transition between the residential neighbourhoods to the south and the Mixed Industrial designation.

The accompanying Noise Report and Transportation Impact Assessment conclude that no adverse effects are anticipated on the hotel use from the surrounding Taylor Creek Business Park Uses or impacts to the adjacent road network or the uses within the Business Park. Further, the site is located less than 50 metres from residential uses on the south side of St. Joseph Boulevard and more than 500 metres from the Industrial and Logistics designation on the east side of Trim Road where heavy industrial uses may be permitted.

It is our opinion that the development proposal is in general conformance with the associated land use designation policies of the Official Plan.

4.4 Orleans Corridor Secondary Plan (2023)

The site is located within the Orleans Corridor Secondary Plan Area. The Orleans Corridor Secondary Plan was approved by Council on February 22nd, 2023. The appeal period expired on March 20, 2023.

A secondary plan is part of the Official Plan that provides detail, based on local context, on how the broader policies of the ‘parent’ Official Plan can be implemented. The plan area generally represents an 800 metre distance or approximately a 10-minute walking distance from the future LRT stations. The site is designated as *Trim Minor Corridor*.

Section 4 – Policies that Apply to the Entire Plan Area. Policies within Section 4 of the plan address the vision, goals and objectives that all apply across the Secondary Plan Area. Those that influence the site design are noted below.

Section 4.2 Built Form and Public Realm Policies.

Development shall minimize conflict between vehicles, pedestrians and cyclists and improve the attractiveness of the public realm by internalizing all servicing, loading areas, mechanical equipment, and utilities, where possible, into the design of the building (s.4.2.3);

Co-location of cultural, institutional, and recreational uses in mid-rise and high-rise buildings is encouraged (s.4.2.4).

New buildings shall, where possible, include active frontages facing the public realm, such as along public or private streets, multi-use pathways, City parks (including linear parks and the Voyageur Creek Greenway) and Privately-Owned Public Spaces (POPS) (s.4.2.6).

Buildings will locate the main entrance fronting an adjacent street with a direct connection to the nearest sidewalk (s.4.2.7).

New development shall frame their adjacent streets and parks to animate public spaces and create comfortable pedestrian environments in the public realm and avoid long expanses of blank walls (s.4.2.10).

4.4 Corridors. The Secondary Plan describes *Corridors* as “bands of land along specified streets whose planned function combines a higher density of development, a greater degree of mixed uses and a higher level of street transit service than abutting Neighbourhoods, but lower density than nearby Hubs” and are intended to become walkable places that give priority to pedestrians and sustainable modes of transportation. Active frontages are required to enhance the public realm and the street. The objective of the Secondary Plan is to advance the objectives of the OP by providing specific contexts.

4.7 Mid-Rise Guidelines. The guidelines apply to developments that are between 5 and 9 storeys in height.

New mid-rise development should have:

- a) *A base that relates to the sidewalk and pedestrian realm, and depending on location, incorporate commercial uses; and*
- b) *b. A middle portion, with a maximum height of the building that is approximately equivalent to the width of the right-of-way, to form part of the streetwall and relate to adjacent buildings and the street; and*
- c) *A middle or top portion that incorporates building form articulations such as stepbacks and/or elevation treatments to break up building mass and allow skyview, sunlight and transition (s.4.7.1)...*

The relationship between the new development and the abutting existing and future residential buildings shall be carefully examined and addressed to ensure liveability for existing and future residents through adequate provisions for privacy, sunlight, and cross ventilation (s. 4.7.2)

4.8 Active Transportation Policies. The objective of the plan is “increase the availability of safe and convenient sustainable transportation options within neighbourhoods, and between neighbourhoods and key destinations like mainstreets and O-Train stations”. The following policies have been incorporated into the proposed development.

- 1) *Plan and design new development to prioritize sustainable transportation.*
- 2) *Create new active transportation connections to key community destinations.*

4.11 Vehicular Parking Policies. The Secondary Plan states that:

There shall be no minimum vehicular parking space rate requirements for development within the Secondary Plan Boundary identified on Schedule A – Designation Plan, with the exception of required visitor and accessible parking (s. 4.11 1); and further;

Surface parking lots will generally be placed at the rear of properties, or otherwise visually screened from the public realm (s.4.11.3)

Underground vehicular parking is encouraged for mid-rise and high-rise developments (s.4.11.5).

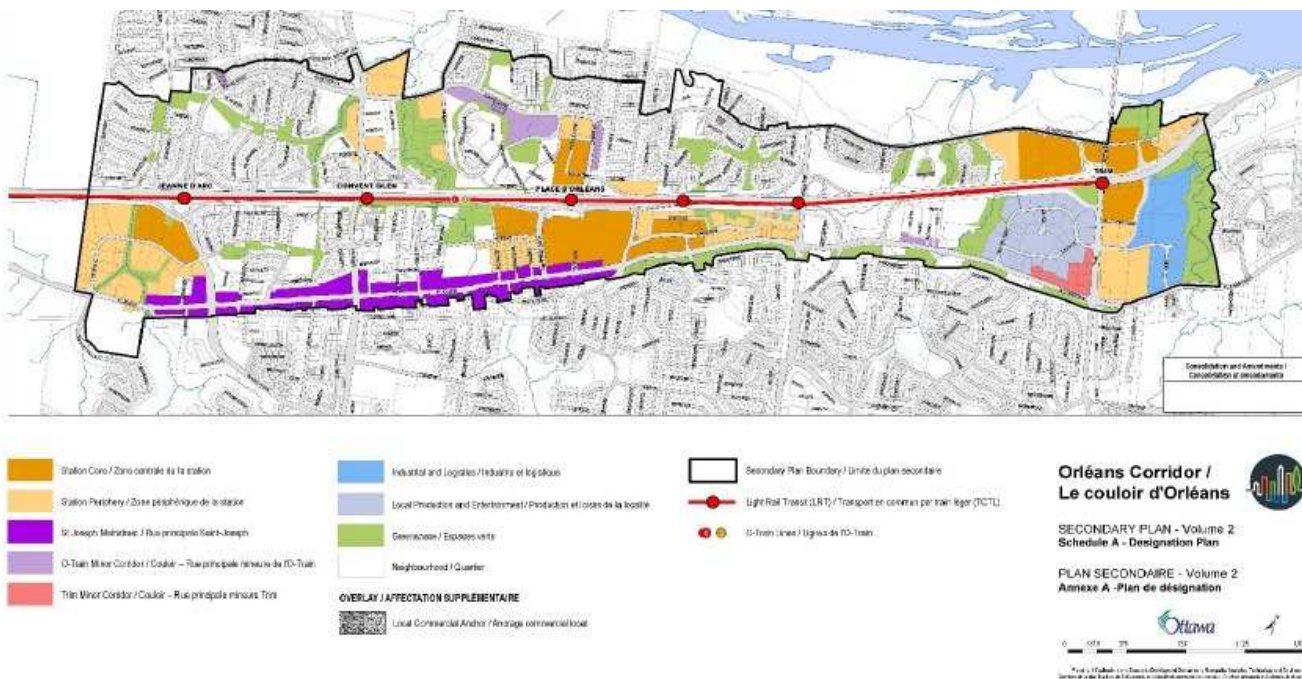
4.19 Sensitive Land Uses. The development which proposes a hotel use, that may be considered a sensitive land use, is directly adjacent to the Local Production and Entertainment designation and as such, the Secondary Plan requires a Noise and Vibration Assessment to be completed. A Noise and Vibration Study was prepared by Gradient Wind Engineers and Scientists and concludes that the uses in proximity to the proposed development in the Taylor Creek Business Park are anticipated to have negligible effects on the proposed hotel use as the proposed development is not in proximity to any large mechanical equipment or any significant sources of ground-borne vibrations. Additional measures are required to mitigate noise from St. Joseph Boulevard on the hotel use that are included in the building design.

Section 5 – Designations. The site is designated *Trim Minor Corridor* in the Secondary Plan (**Figure 15**). The policies are contained in Section 5.6 of the Secondary Plan. The Trim Minor Corridor is intended to support the adjacent *Local Production and Entertainment* designation and the *Trim O-Train Station*. Permitted uses include those that are allowed within the *Local Production and Entertainment* designation as well as commercial, restaurant, and hotel uses (s.5.6.1). For context, the mixed industrial uses [described in the Official Plan], excluding noise and vibration sensitive land uses, will be permitted in addition to uses associated with nightlife, entertainment production, social interaction, and recreation.

Development of sensitive uses such as a hotel is conditional on the submission of a Noise and Vibration study, and an analysis of existing or potential land use conflicts demonstrating compatibility (s.5.6.4).

Design strategies shall be implemented which may include locating non-residential sensitive land uses in a manner that shields them from nearby Class I or Class II industrial uses, framing the building to the adjacent street, maximizing setbacks in accordance with Provincial land use compatibility guidelines, and installing walls, fences, or landscaping to mitigate nuisances, where there is a current, or anticipated need (s.5.6.5).

Figure 15: Orleans Corridor Secondary Plan, Schedule A - Designation Plan



Section 7 – Implementation. Implementation policies that specifically apply to the development applications include:

If a policy or schedule conflicts with other policies in the Official Plan, this Secondary Plan will be deemed to supersede. (s 7.2).

Land designations will be in accordance with Schedule A, Designation Plan (s. 7.7).

Maximum permitted building heights will be in accordance with Schedule B, Maximum Building Heights (s 7.8).

Possible transportation infrastructure, including pathways, bridges and cycle tracks are as illustrated on Schedule C – Public Realm and Mobility Improvements (s7.9).

According to Schedule B, the maximum building height on the site is six storeys (**Figure 16**). A physically separated cycling facility is envisioned for St. Joseph Boulevard (**Figure 17**).

Figure 16: Orleans Corridor Secondary Plan, Schedule B - Maximum Building Heights

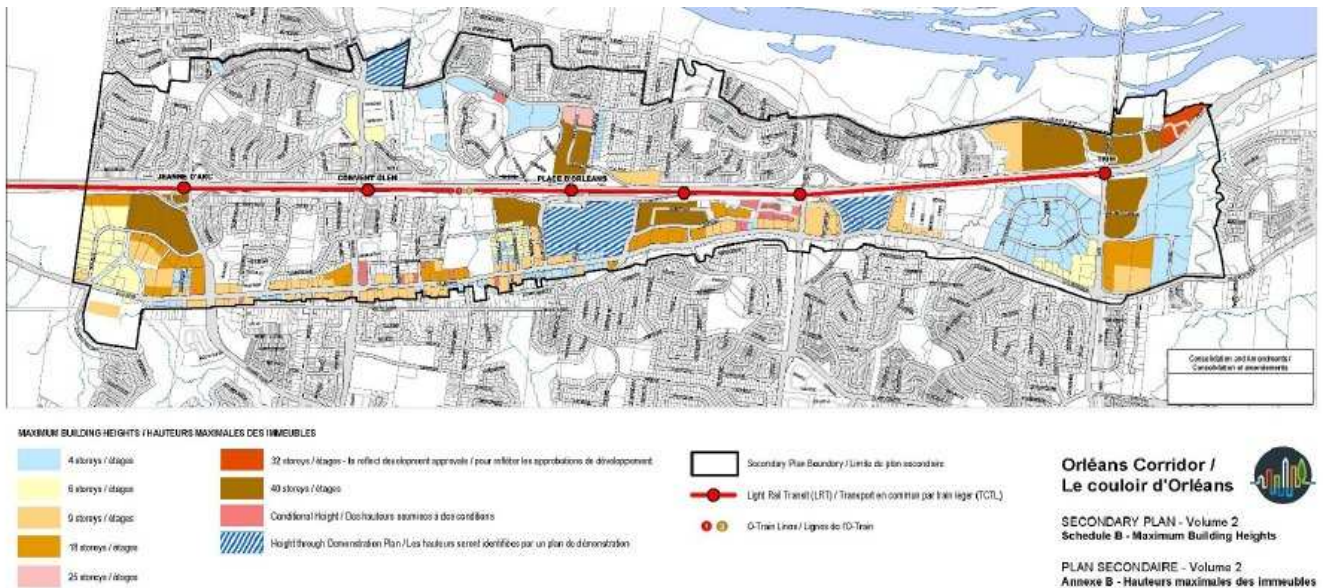
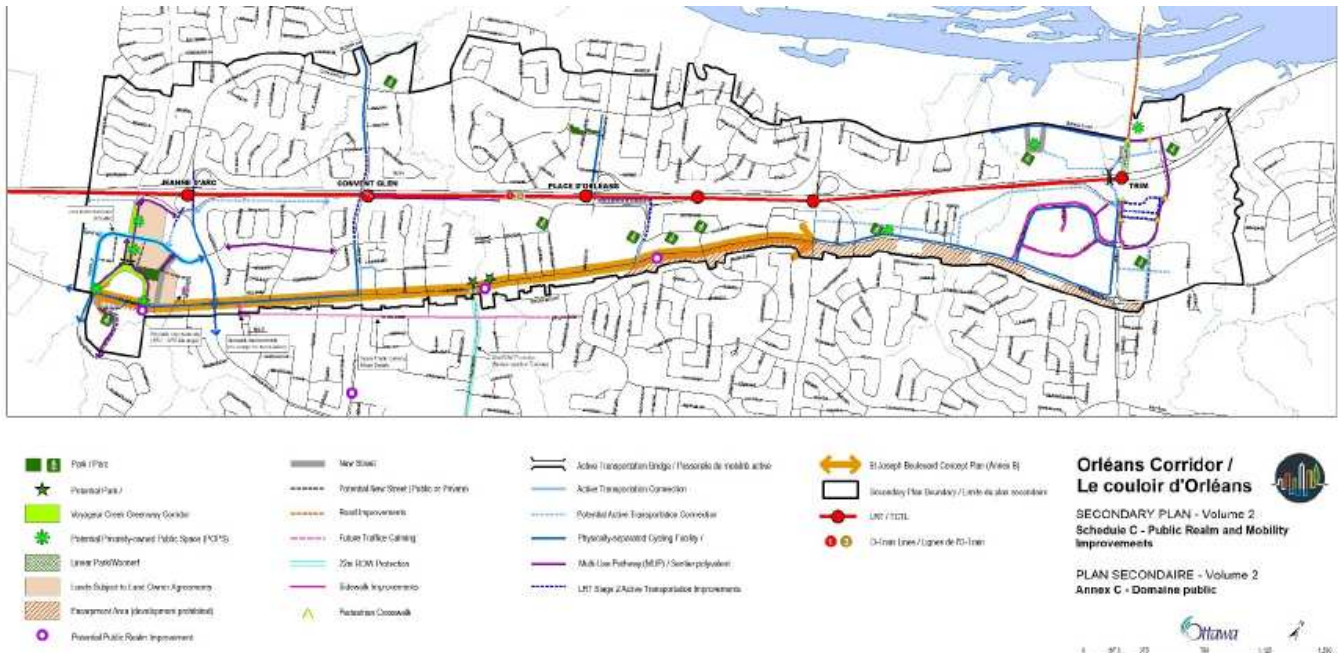


Figure 17: Orleans Corridor Secondary Plan, Schedule C - Public Realm and Mobility Improvements



Planning Response: The building includes a mixture of commercial uses and hotel use with main floor commercial uses located along the St. Joseph with outdoor patio spaces to animate the public realm. The proposed mixture of uses will all serve the adjacent business park as well as the surrounding residential communities. The site is designed to have the main entrance and pedestrian access from St. Joseph Boulevard and consolidates access for vehicles and service away from main entrance to avoid conflicts and includes suitable setbacks from the adjacent commercial properties. The proposed development provides a balanced number of parking spaces to support the mixture of uses and more than the minimum required bicycle parking spaces at entrances to the building designed and designed to be covered spaces. Sidewalk and cycling connections from the future Trim Road LRT Station will encourage and support the use of active and sustainable forms of transportation to/from the proposed land use and the reduced parking rate for the site that is being proposed. The six-storey building design is in accordance with maximum height (six-storeys) permitted within the Secondary Plan.

The proposed hotel use is a conditional permitted use that is supported by the accompanying Noise and Vibration Report that makes recommendations on building design to mitigate noise from St. Joseph Boulevard. Noise and vibration impacts are not anticipated from nearby businesses in the Taylor Creek Business Park. The hotel rooms have been oriented east to west which minimizes views to the adjacent business park.

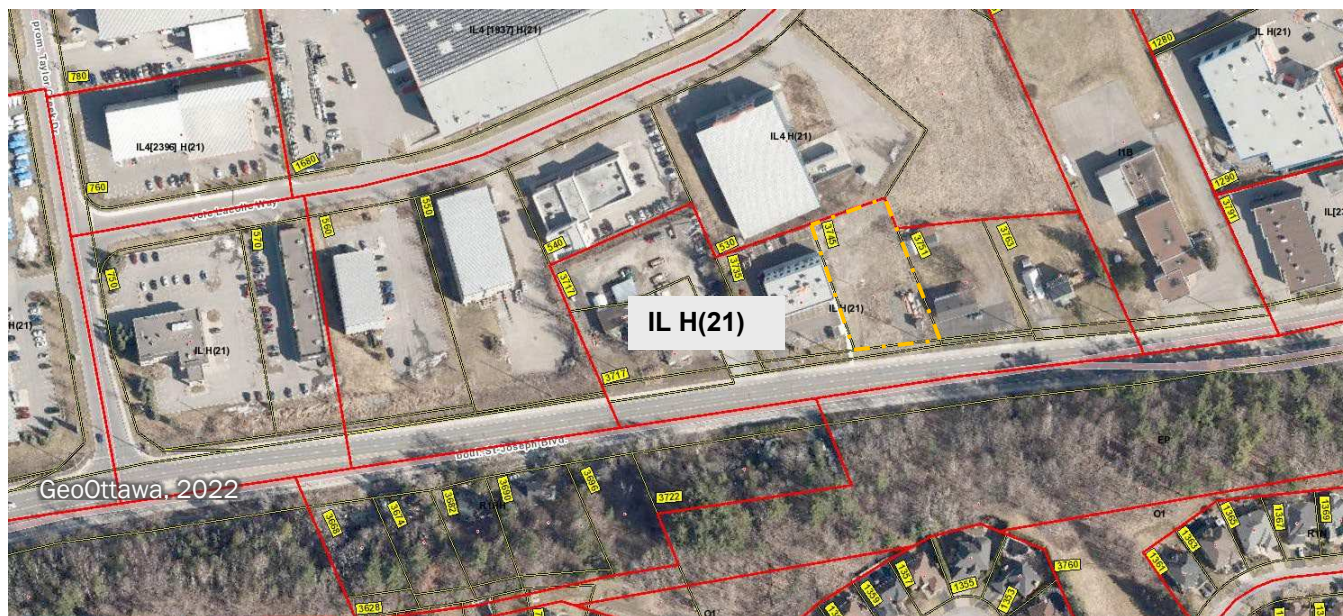
It is our opinion that the proposed development is in keeping with the policies objectives of the Orleans Corridor Secondary Plan.

4.5 City of Ottawa 2008-250 Consolidated Zoning By-law

The objective of the City of Ottawa 2008-250 Consolidated Zoning By-law (as amended) is to establish and regulate land use by implementing the policies of the City’s Official Plan into site-specific development performance standards. Notwithstanding, the Zoning By-law has not yet been brought into conformance with the New Official Plan (November 2022) or the Secondary Plan (March 2023). As such, amendments to the Zoning By-law are required to implement the project.

The Site is zoned Light Industrial with a maximum height of 21 metres (IL H(21)) as illustrated in **Figure 18**.

Figure 18: City of Ottawa Comprehensive Zoning By-Law (2008, as amended) – Excerpt from GeoOttawa



The IL Zone permits the following non-residential uses:

- | | | |
|---------------------------------|------------------------|-----------------------------|
| animal care establishment | broadcasting studio | emergency service |
| animal hospital | catering establishment | heavy equipment and vehicle |
| automobile dealership | day care | sales, rental and servicing |
| automobile rental establishment | drive-through facility | light industrial uses |

medical facility	personal brewing facility	service and repair shop
municipal service centre	place of assembly	technology industry
office	post office	training centre
park	printing plant	truck transport terminal
parking garage	production studio	warehouse
parking lot	research and development centre	

The following uses are also permitted provided they do not exceed a cumulative gross floor area of 2,999 m² and each use not exceeding 300 m² (except amusement centre, amusement park, and recreational and athletic facility.

athletic facility;	bank	gas bar
animal care establishment	bank machine	instructional facility
amusement centre	bar	personal service business
amusement park	car wash	recreational and athletic facility
automobile service station	convenience store	restaurant

The permitted uses and development standards that apply to the IL zone and their compliance as part of the proposed development are outlined in **Table 4-1** below.

Table 4-1. Zoning Compliance Table

Sec.	Zone Provision	Zone Provision	Proposed Development	In Compliance (Yes/No)
1	List of Permitted Uses	Does not include Hotel or Retail Store	Add Hotel and Retail Store	No
2	(b) Cumulative Gross Floor Area of Additional Permitted Uses	2,999m ² maximum	403 m ²	Yes
	(c) Each Additional Permitted Use (included in proposal) not exceeding 300 m ²			
	Day Program (Instructional Facility)	300 m ²	208 m ²	Yes
	Restaurant (1 - Café)	300 m ²	83 m ²	Yes
	Restaurant (2 - Full Service)	300 m ²	112 m ²	Yes
	(a) Minimum Lot Area (m ²)	No minimum	2,502 m ²	N/A
	(b) Minimum Lot Width	No minimum	40.14 m	N/A
	(c) Maximum lot coverage	65%	43%	Yes
	(d) Minimum Front Yard Setback St. Joseph Boulevard	7.5 m	7.5 m	Yes
	(e) Minimum Interior Side Yard Setback			
	East Lot Line	7.5 m	3.90m	No
	West Lot Line	7.5 m	9.68 m	Yes
	(f) Minimum Rear Yard Setback All other cases	7.5 m	8.5 m	Yes
	(g) Maximum floor space index	2	1.52	Yes
	(h) Maximum Building Height	21 m	24 m	No
	(i) Minimum width of landscaped area			
	Abutting a street	3.0 m	7.5 m	Yes
	All other cases	No minimum	N/A	N/A

Part 4 of the Zoning By-law outline the provisions for Parking, Queuing and Loading. The Site is currently located within Area C (Suburban) of Schedule 1A of the Zoning By-law, which identifies areas for minimum parking space requirements. The general provisions for vehicle, including shared parking requirements, and bicycle parking are outlined in Table 4-2.

Table 4-2 Required and Provided Parking

Vehicle Parking (Section 101 and 104)	Proposed Gross Floor Area	Required Parking Ratio	Number of Spaces Required	Shared Parking Provisions
Hotel includes:				
<ul style="list-style-type: none"> Commercial 6 (Co-Working Space) Atrium/Multi-Purpose Space Commercial 4 (Gym Space) 	61 Units	1 per guest unit	61	61
Commercial 1 (Retail)	81 m ²	3.4 per 100 m ² GFA	3	
Commercial 2 (Restaurant Café, Fast Food)	82 m ²	5 per 100 m ² of GFA	4	
Commercial 3 (Retail)	93 m ²	3.4 per 100 m ² GFA	3	19
Commercial 5 (Retail)	84 m ²	3.4 per 100 m ² GFA	3	
Restaurant (Full-Service)	102 m ²	10 per 100 m ² of GFA	10	
Day Program (Instructional Facility)	171 m ²	3.4 per 100m ² of GFA	6	6
Vehicle Parking Required (Part 4, Section 101)			90 spaces	
Vehicle Required Parking – Shared Parking Provisions (Part 4, Section 104)			86 spaces	
Total Provided for All Uses			77 spaces	
Bicycle Parking (Section 111)				
Hotel	2,516 m ²	1 per 1,500 m ² of GFA	1.7	
Commercial 1 (Retail)	81 m ²	1 per 250 m ² of GFA	0.3	
Commercial 2 (Restaurant Café, Fast Food)	82 m ²	1 per 250 m ² of GFA	0.3	
Commercial 3 (Retail)	94 m ²	1 per 250 m ² of GFA	0.4	
Commercial 5 (Retail)	85 m ²	1 per 250 m ² of GFA	0.4	
Restaurant (Full-Service)	102 m ²	1 per 250 m ² of GFA	0.5	
Day Program (Instructional Facility)	171 m ²	1 per 1,500 m ² of GFA	1	
Total Required Bike Spaces			5	
Total Provided			20	

Between the surface and underground parking, 77 vehicle parking spaces are provided on site. Notwithstanding the Secondary Plan policy that parking is not required within the Secondary Plan Area, a zoning amendment to reduce the requirement for on-site parking to 75 spaces from the current by-law, recognizing a balanced approach to parking is required while the Trim LRT Station is constructed and the modal shift to transit and active transportation choices is realized. Recognizing that restaurant uses have the highest ratio of required spaces in the existing by-law, a cap of 300 m² for restaurant uses is proposed and a site specific parking rate of 75 spaces total. While the current on-site total is 77 spaces, a site specific parking rate of 75 is requested to provide flexibility through completion of the Site Plan Control Application.

As it relates to bicycle parking, there is a site requirement for 5 bicycle parking spaces, whereas a total of 20 spaces are provided (10 at each entrance to the building). Further, bicycle parking will be weather protected to further support active transportation uses on the site.

Section 64 of the Zoning By-law provides provisions surrounding *Permitted Projections Above the Height Limit* which permits certain building elements or structures to which the maximum height limits to not apply. This includes, as it relates to the development:

- Mechanical and service equipment penthouse, elevator or stairway penthouse
- Landscaped areas, roof-top gardens and terraces and associated guards and access structures; and
- Solar Panels

Planning Response: Zoning By-law 2008-250 has not yet been updated to consider the new policies of the City Official Plan or the more recent Secondary Plan. The majority of the uses are permitted within the existing IL zone save for hotel as a main permitted uses and retail as an additional permitted use that will be limited to a total gross floor area of 300 m² as a supporting use in the zone and surrounding community. The site will also require a reduced yard setback on the east side of the building however varies along the length of the property between 3.9 m to greater than five (5) metres. The proposed setback better aligns with the intended higher density built form for the *Trim Minor Corridor* designation, while still providing sufficient space between the proposed building and the adjacent commercial use to the east to minimize potential conflicts.

The building height is designed in accordance with the approved Secondary Plan of six-storeys. A minor increase in height from the maximum stipulated in the Zoning By-law is required due to the grade differences on the site, building height measurement from average grade, and the use of mass timber construction to support sustainability principles which requires additional height between floors to accommodate construction.

To implement the proposed development, a Zoning By-law amendment will require a reduced parking requirement. The proposed parking supply represents a balance with the Secondary Plans objective to not provide parking and providing sufficient parking that will not result in spill over onto the adjacent streets. The requested amendment proposes an site specific supply of 75 spaces and a cap on restaurant uses to 300m² for the development recognizing that this use is typically assigned the greatest parking ratio. The proposed development provides bicycle parking in excess of the minimum to support active transportation uses envisioned in the OP and Secondary Plan.

The proposed amendments, as outlined below, are considered appropriate and desirable to implement the proposed development that will result in a positive change for the area and surrounding community. Based on the foregoing zoning analysis, a Zoning By-law Amendment is being sought to:

- Rezone the lands from Light Industrial Zone with a height limitation of 21 metres (IL H(21)) to a Light Industrial Special Exception Zone.
- Addition of Hotel and Retail as additional permitted uses
- Reduce interior side yard setback along the east property line from 7.5 metres to 3.5 metres.
- Increase height from 21 m to 24 m.
- Total on-site parking requirement of 75 spaces.
- Maximum Gross Floor Area for restaurant uses not to exceed 300m².

5.0 Public Notification and Consultation Strategy

In addition to the requirements for Public Notification contained within the Planning Act, the City of Ottawa has a Public Notification and Consultation Policy for Development Applications. The following consultation steps have or will be undertaken in accordance with the Act and Policy.

Pre-Application Consultation

- A Pre-Application meeting was coordinated with the area Councillor and Planning Staff to review the proposed application on June 23, 2022.
- A Pre-Application Consultation Meeting was held with City Staff and the Applicant Team on January 12, 2023.
- Prior to application submission, a notification email will be sent to the Ward 1: Orleans East – Cumberland Councillor, Matthew Luloff. Follow-up with the Councillor office will be undertaken to determine how best to engage the surrounding community.

Post-Application Consultation

- As part of the City’s circulation requirements, a community “heads up” will be sent to registered community associations. The Applicant Team will be available to answer questions as required.
- If requested by the Ward Councillor, the Applicant Team will participate in a Community Information Session to discuss the proposed development.

Planning Committee and Council

- Notification of the Statutory Public Meeting for the Zoning By-law Amendment will be undertaken by the City of Ottawa. The Applicant Team will be in attendance and make representations in support of the application at Planning Committee.

6.0 Planning Conclusion

The preparation of this Planning Rationale has required a thorough understanding of the site, the development proposal and the supporting studies as well as a thorough review of the land use planning policy framework that apply to the site. Approval of the Zoning By-law Amendment and Site Plan Control Application are recommended on the following basis:

IT IS CONSISTENT WITH THE PROVINCIAL POLICY STATEMENT

The proposed development supports the broad goals and objectives of the Provincial Policy Statement. The proposed development will contribute to building a strong and healthy community by being in an existing built-up area of the city and providing a combination of uses that will meet the long-term needs of the adjacent business park and surrounding community. The proposed development is supported by additional studies that demonstrate how it can be serviced by existing systems, provide new pedestrian and cycling facilities, is separated from and will not result in impacts to surrounding resources, and protects public health and safety.

IT CONFORMS TO THE NEW OFFICIAL PLAN AND SECONDARY PLAN

The site is designated *Mixed Industrial* and the new Official Plan and *Trim Minor Corridor* in the recently approved Secondary Plan for the Orleans Corridor. The proposed development represents intensification of a currently vacant and under-utilized site in the built-up area of Ottawa for new mixed-use building that will include a hotel and a range of commercial/service uses that are located to serve both the Mixed Industrial uses in the Taylor Creek Business Park and also the adjacent residential neighbourhoods. The proposed development also serves as a transition between the residential neighbourhoods to the south and the Mixed Industrial designation which contributes to the realizing a 15-minute neighbourhood. The site has been designed to encourage the use of existing and active transportation facilities and is located near the future Trim LRT Station by providing sufficient parking to serve the needs of the uses while supporting the OP's objective of more sustainable transportation use. The proposed six-storey building meets the maximum building height stipulated in the Secondary Plan.

IT REQUIRES AN AMENDMENT TO THE PROVISIONS OF THE ZONING BY-LAW.

As the site zoning has not yet been updated to be in accordance with the new Official Plan and Secondary Plan guidance for the area, a Zoning By-law Amendment is required to implement the project.

On this basis, it is our professional planning opinion that approval of the proposed development will result in good land use planning and is recommended for approval.

Respectfully Submitted,



Pamela Whyte, MCIP, RPP
Manager of Planning
Parsons Ottawa



Ivan Ho, M.Plan
Planner
Parsons Ottawa

Appendix A: Pre-consultation Correspondence

From: [Murshid, Shoma](#)
To: [McGirr, Emily \[NN-CA\]](#)
Cc: [Charlie, Kelsey](#); [Paudel, Neeti](#); [Rehman, Sami](#); [Wang, Randolph](#); [Maloney, David](#); [Richardson, Mark](#); [Whyte, Pamela \[NN-CA\]](#); [Hofstetter, Payton](#); [Camille Lewis](#); sisemajuste@gmail.com; [leonard.goddard](#); [Yigit Cuhacibasi](#)
Subject: [EXTERNAL] 3745 St. Joseph Boulevard Pre-Consultation Follow-up for Zoning By-law Amendment & Site Plan Control
Date: Tuesday, January 24, 2023 3:58:37 PM
Attachments: [image001.png](#)
[image002.png](#)
[preapplication_3745 St Joseph.pdf](#)
[220980_3745 St. Joseph Blvd Site Plan.pdf](#)
[design_brief_submission_requirements_3745 St. Joseph .pdf](#)

Good afternoon Emily,

Thank you for meeting with us on January 12, 2023 to discuss a 6-storey building at 3745 St. Joseph Boulevard, as per the attached concept. It is understood a hotel use will occupy the top four storeys, whereas the first two storeys will be comprised of co-working spaces and potentially a café, gym and/or other retail or office spaces. The subject land is zoned IL H(21). You wish to rezone the lands to permit the hotel use and to amend a few of the zoning provisions, such as side yard interior setbacks.

I offer the **following planning notes and comments** for your consideration when preparing a final formal submission for zoning by-law amendment and site plan control:

- Please provide shade/canopy trees along the proposed MUP and/or street frontage and consider any existing hydro wires and setbacks.
- Please bring the building as close as possible to the street to provide a more pedestrian friendly public realm along St. Joseph Boulevard.
- Ensure this proposal takes into account the new Official Plan as well as the Orleans Corridor Secondary Plan policies.
- When seeking reduced setbacks, first ensure there is adequate space between the existing shared property lines as well as its buildings in order that there be adequate maintenance access both properties.
- Please do not forget to design and identify locations of temporary (and/or permanent) snow storage, garbage/green-bin/recyclable pick-up, emergency and protective service turn-arounds (in other words, demonstrate truck turning movements) on your submission documents.
- At time of site plan control approval, private easements and a copy of the JUMA between 3735 and 3745 St. Joseph Boulevard will be required, via a condition of site plan approval, for the City's review. This JUMA will include details on the shared infrastructure services (service lines for 3735 St. Joseph that run through the northern segment of 3745 St. Joseph Blvd.) as well as the required western side yard maintenance access corridor at 3745 St. Joseph, in favour of 3735 St. Joseph, etc.
- At time of site plan approval, cash in lieu of parkland will apply
- At time of site plan approval, the MUP conveyance to the City will be required.
- Bird safe design guidelines are applicable.

Site Plan Control/Zoning By-law Amendment Categories, Required Plans/Studies and Fees for Submission:

I understand the intent is to submit the Site Plan Control and Zoning By-law Amendment applications concurrently. Note, when submitted concurrently, there will be a 10% deduction to the planning fee component of each application.

This proposal triggers Application for “New Development, Complex” site plan control, as it is approximately 1620 m² GFA in total for proposed retail uses requiring approximately 55 required parking spaces under the Zoning By-law. This site plan control category has a submission fee requirement that is (planning fee component) \$49,964.88 + the Initial Design Review and Inspection Fee, based on the value of Infrastructure and Landscaping (sliding scale between \$1,000 to \$10K) plus an Initial Conservation Authority Fee of \$1,065.00.

This proposal also triggers “Zoning By-law Amendment (Major)”. This development application has a submission fee requirement that is (planning fee component) \$22,472.80 + another Initial Conservation Authority Fee of \$400.00.

Plans and reports required at the time of zoning by-law amendment/site plan control submission (PDFs only) shall be:

Site Plan
Grade Control and Drainage/Ponding Plan
Existing Condition Storm Drainage Plan
Post Development Storm Drainage Plan
Landscape Plan (can be combined with TCR)
TCR, if applicable
Legal Survey Plan
Topographical Plan of Survey Plan with a published Bench Mark
Planning Rationale
Erosion and Sediment Control Plan
Site Servicing Plan
Site Servicing Study (can be combined with Stormwater Management Report)
Stormwater Management Report
Geotechnical Study
Noise Study
Plan Showing Parking Garage Layout
Elevations
Floor Plans
Phase 1 ESA

Comments from Policy Review, David Maloney:

The property at 3745 St. Joseph Boulevard is designated as ‘Trim Minor Corridor’ in the Orléans Corridor Secondary Plan. The applicable policies from the Orléans Corridor Secondary Plan are as follows:

The Trim Minor Corridor is intended to support the adjacent Local Production and Entertainment designation and the Trim O-train Station.

- 1) In addition to the uses permitted in the Local Production and Entertainment designation, commercial, restaurant and hotel uses may be permitted.
- 2) Residential is not permitted.
- 3) New or enhanced cycling and pedestrian connections will be pursued through tools such as: Site Plan Control, Community Benefits Agreements and traffic calming, in association with proposals for new development.
- 4) Development of sensitive uses such as a hotel is conditional on the submission of a Noise and Vibration study, and an analysis of existing or potential land use conflicts demonstrating compatibility.
- 5) Design strategies shall be implemented which may include locating non-residential sensitive land uses in a manner that shields them from nearby Class I or Class II industrial uses, framing the building to the adjacent street, maximizing setbacks in accordance with Provincial land use compatibility guidelines, and installing walls, fences, or landscaping to mitigate nuisances, where there is a current, or anticipated need.

The height schedule indicates a maximum height of six stories.

Comments:

The proposal is generally consistent with the intent of the applicable Orléans Corridor Secondary Plan. The proposed uses could meaningfully support the current activities of Taylor Creek, and its planned evolution. The proposed rooftop amenity space and restaurant, with views toward Petrie Island, Ottawa River and the Gatineau Hills is notable as this takes advantage of the site's locational assets and sloping grade. The co-working space, hotel, café, gym and the retail or office space will add important vitality to this area that will likely transform considerably with the arrival of the eastern terminus of the O-Train at Trim Station.

Policy Planning has a few specific comments to support the evolution of the project to achieve the active transportation objectives of the Secondary Plan. Other comments and to support efficient project design, with the understanding that existing zoning provisions have not been updated to align with the Secondary Plan.

- Dedication of land along the eastern property line for the purposes of a connection to Lacolle Way is important to establish a pedestrian and/or cycling shortcut from St. Joseph Boulevard into the Taylor Creek Business Park
 - Providing this connection to the 524 Lacolle Way property, entirely on the subject site would ensure its completion in the shorter term, as the City can leverage a potential easement on the vacant land to complete the link
 - Alternatives to a 6m wide multi-use pathway could be explored given the short length of this connection
 - Ensure the pathway / MUP connection is separated from vehicular traffic and buffered with landscaping to improve safety and enjoyability for users
- Reducing front and side yard setbacks are encouraged to frame the building on St. Joseph Boulevard and mitigate land use conflicts

- Understanding there are restraints caused by above grade infrastructure, but contributing to an urban edge along the north side of St. Joseph Boulevard is encouraged
- Reducing the side yard setback opposite the driveway is encouraged to allow for increased rear yard setback without losing potential GFA
- Flexible zoning provisions for permitted projections are encouraged to encourage optimal building placement and rear yard land use mitigation measures

Comments from Engineering, Kelsey Charie:

Please see the below engineering comments from the pre-consultation meeting for 3745 St. Joseph Boulevard which occurred on January 12, 2023.

List of Reports and Plans (Concurrent Site Plan Control and Rezoning applications):

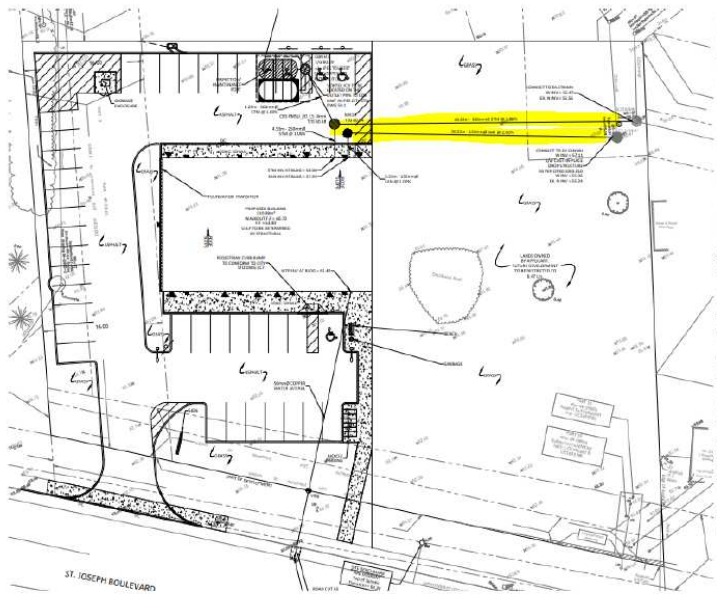
1. Site Servicing Plan
2. Site Grading and Ponding Plan
3. Erosion and Sediment Control Plan
4. Existing Condition Storm Drainage Plan
5. Post Development Storm Drainage Plan
6. Stormwater Management and Site Servicing Report
7. Geotechnical Investigation Report

List of Reports and Plans (If applicant elects to pursue Rezoning in advance of SPC):

1. Assessment of Adequacy of Public Services Report
2. Geotechnical Report

The applicant should also note that their submission should be coordinated with the plans and reports that were completed for the Phase 1 portion of this site: 3735 St. Joseph. The applicant's engineering consultant can obtain these plans and reports using the contact listed below and referencing Plan #16921.

One key item to note is the servicing for 3735 St. Joseph crosses the property of 3745 to connect to the public services at the North corner of the lot, see below snapshot of Phase 1 servicing plan:

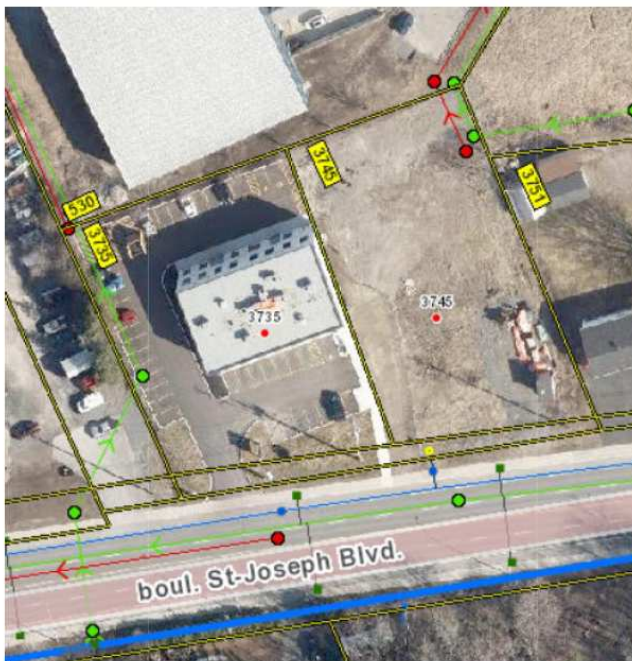


These services may need to be redesigned/relocated based on the proposed footprint that is shown in the preliminary plans for 3745 St. Joseph.

Please note the following information regarding the engineering design submissions for the above noted site:

1. The Servicing Study Guidelines for Development Applications are available at the following address:
<https://ottawa.ca/en/city-hall/planning-and-development/how-develop-property/development-application-review-process-2/guide-preparing-studies-and-plans> [ottawa.ca]
2. Servicing and site works shall be in accordance with the following documents:
 - Ottawa Sewer Design Guidelines, Second Edition, (October 2012), including Technical Bulletins, ISDTB-2014-01, PIETB-2016-01, ISTB 2018-01, ISTB-2018-04, and ISTB-2019-02
 - Ottawa Design Guidelines – Water Distribution, First Edition, (July 2010), including Technical Bulletins ISD-2010-2, ISDTB-2014-02, ISTB-2018-02, and ISTB-2021-03
 - Geotechnical Investigation and Reporting Guidelines for Development Applications in the City of Ottawa (Revised 2008)
 - City of Ottawa Slope Stability Guidelines for Development Applications (Revised 2012)
 - City of Ottawa Environmental Noise Control Guidelines (January, 2016)
 - City of Ottawa Hydrogeological and Terrain Analysis Guidelines (March 2021)
 - City of Ottawa Park and Pathway Development Manual (2012)
 - City of Ottawa Accessibility Design Standards (2012)
 - Ottawa Standard Tender Documents (latest version)
 - Ontario Provincial Standards for Roads & Public Works (2013)
3. Record drawings and utility plans are also available for purchase from the City (Contact the City's Information Centre by email at InformationCentre@ottawa.ca or by phone at (613) 580-2424 x 44455

4. The Stormwater Management Criteria for the subject site is to be based on the following:
 - The stormwater management for the subject site shall be designed per the “Guidelines for On-Site Detention Design in the Taylor Creek Business Park Township of Cumberland”
 - Flows to the storm sewer for the 5-Year and 100-Year storm events must be controlled on-site to a maximum release rate of 26.4 l/s/ha.
 - Ensure no overland flow for all storms up to and including the 100-year event. Provide adequate emergency overflow conveyance off-site
 - Quality control requirements to be provided by Rideau Valley Conservation Authority (RVCA).
5. Deep Services:



- i. A plan view of the approximate services may be seen above.
 - a. Available infrastructure for future connections:
 - i. 406 mm dia. PVC water main in St. Joseph Blvd.
 - ii. 250 mm dia. PVC sanitary sewer with sanitary manhole at the North corner of the property
 - iii. 1050 mm dia. PVC storm sewer with storm manhole at the North corner of the property
 - ii. Provide existing servicing information and the recommended location for the proposed connections. Services should ideally be grouped in a common trench to minimize the number of road cuts.
 - iii. Provide information on the monitoring manhole requirements – should be located in an accessible location on private property near the property line (ie. Not in a parking area).
 - iv. Provide information on the type of connection permitted

Sewer connections to be made above the springline of the sewermain as per:

- a. Std Dwg S11.1 for flexible main sewers – *connections made using approved tee or wye fittings.*
 - b. Std Dwg S11 (For rigid main sewers) – *lateral must be less than 50% the diameter of the sewermain,*
 - c. Std Dwg S11.2 (for rigid main sewers using bell end insert method) – *for larger diameter laterals where manufactured inserts are not available; lateral must be less than 50% the diameter of the sewermain,*
 - d. Connections to manholes permitted when the connection is to rigid main sewers where the lateral exceeds 50% the diameter of the sewermain. – Connect obvert to obvert with the outlet pipe unless pipes are a similar size.
 - e. *No submerged outlet connections.*
 - v. *Please provide estimated sanitary flows with the first submission, to allow the City to confirm whether there are any downstream capacity constraints.*
6. Civil consultant must request boundary conditions from the City's assigned Project Manager prior to first submission. Water Boundary condition requests must include the location of the service and the expected loads required by the proposed development. Please provide the following information:
- i. Location of service(s)
 - ii. Type of development and the amount of fire flow required (as per FUS, 2020).
 - iii. Average daily demand: ___ l/s.
 - iv. Maximum daily demand: ___ l/s.
 - v. Maximum hourly daily demand: ___ l/s.
 - vi. Hydrant location and spacing to meet City's Water Design guidelines.
 - vii. Water supply redundancy will be required for more than 50 m³/day water demand.
- Please note that a boundary condition request should be made to the City as early as possible, in order to identify any water supply constraints (if any exist).***
7. Phase 1 ESAs and Phase 2 ESAs must conform to clause 4.8.4 of the Official Plan that requires that development applications conform to Ontario Regulation 153/04.
8. All development applications should be considered for an Environmental Compliance Approval (ECA) by the Ministry of the Environment, Conservation, and Parks (MECP);
- a. The consultants determine if an approval for sewage works under Section 53 of OWRA is required and determines what type of application. The City's project manager may help confirm and coordinate with the MECP as required.
 - b. The project will be either transfer of review (standard), transfer of review (additional), direct submission, or exempt as per O. Reg. 525/98.
 - c. Pre-consultation is not required if applying for standard or additional works (Schedule A

of the Agreement) under Transfer Review.

- d. Pre-consultation with local District office of MECP is recommended for direct submission.
- e. Consultant completes an MECP request form for a pre-consultation. Send request to moecottawasewage@ontario.ca
- f. ECA applications are required to be submitted online through the MECP portal. A business account required to submit ECA application. For more information visit <https://www.ontario.ca/page/environmental-compliance-approval> [can01.safelinks.protection.outlook.com]

NOTE: Site Plan Approval, or Draft Approval, is required before an application is sent to the MECP.

9. General Engineering Submission requirements:

- a. As per section 53 of the Professional Engineers Act, O. Reg 941/40, R.S.O. 1990, all documents prepared by engineers must be signed and dated on the seal.
- b. All required plans are to be submitted on standard A1 size sheets (594mm x 841mm) sheets, utilizing a reasonable and appropriate metric scale as per City of Ottawa Servicing and Grading Plan Requirements: title blocks are to be placed on the right of the sheets and not along the bottom. Engineering plans may be combined, but the Site Plans must be provided separately. Plans shall include the survey monument used to confirm datum. Information shall be provided to enable a non-surveyor to locate the survey monument presented by the consultant.
- c. All required plans & reports are to be provided in *.pdf format (at application submission and for any, and all, re-submissions)

Should you have any questions or require additional information, please contact me directly at kelsey.charie@ottawa.ca

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Design Review Comments, Randolph Wang:

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Here are urban design comments on this pre-consultation:

- 1. Design Brief is required for these applications. The Terms of Reference is attached.
- 2. Urban design has no concern on the proposed uses.
- 3. The building placement and massing appear to be appropriate for the proposed uses and the relationship with the abutting properties also appears to be appropriate. The proposed MUP is in keeping with the Secondary Plan. However, the proposed site plan is very tight and leaves very little room for on-site vehicle maneuvering:
 - a. Have pick-up and drop-off been considered for hotel guests?
 - b. How is garbage storage and pick up arranged?
 - c. Are pick-up and drop-off required for the function of the community facility?
- 4. Given the history and the said close relationship between owners of this property and the owner of the one-storey building to the west, considerations may be given to create a loop at

the back both properties to allow for better vehicular circulation.

5. Continuous tree canopy and landscaping should be provided along St. Joseph and the proposed MUP.

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Comments from PIED Forestry, Mark Richardson:

- A Tree Conservation report is not needed unless there are trees on site that are greater than 10cm in diameter

LP tree planting requirements:

Minimum Setbacks

- Maintain 1.5m from sidewalk or MUP/cycle track or water service laterals.
- Maintain 2.5m from curb
- Coniferous species require a minimum 4.5m setback from curb, sidewalk or MUP/cycle track/pathway.
- Maintain 7.5m between large growing trees, and 4m between small growing trees. Park or open space planting should consider 10m spacing, except where otherwise approved in naturalization / afforestation areas. Adhere to Ottawa Hydro’s planting guidelines (species and setbacks) when planting around overhead primary conductors.

Tree specifications

- Minimum stock size: 50mm tree caliper for deciduous, 200cm height for coniferous.
- Maximize the use of large deciduous species wherever possible to maximize future canopy coverage
- Tree planting on city property shall be in accordance with the City of Ottawa’s Tree Planting Specification; and include watering and warranty as described in the specification (can be provided by Forestry Services).
- Plant native trees whenever possible
- No root barriers, dead-man anchor systems, or planters are permitted.
- No tree stakes unless necessary (and only 1 on the prevailing winds side of the tree)

Hard surface planting

- Curb style planter is highly recommended
- No grates are to be used and if guards are required, City of Ottawa standard (which can be provided) shall be used.
- Trees are to be planted at grade

Soil Volume

- Please document on the LP that adequate soil volumes can be met:

Tree Type/Size	Single Tree Soil Volume (m3)	Multiple Tree Soil Volume (m3/tree)
Ornamental	15	9
Columnar	15	9

Small	20	12
Medium	25	15
Large	30	18
Conifer	25	15

Sensitive Marine Clay

- Please follow the City’s 2017 Tree Planting in Sensitive Marine Clay guidelines

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Comments from Environmental Policy, Sami Rehman:

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Given the vacant nature of the subject property, and distance between the it and the urban natural feature, I don’t think there is much environmental concerns with this proposed development.

Because the proposed development is going to be over 4-storeys, my major recommendations for the Site Plan Control application would be:

- a. to ensure the applicant reviews and draws relevant design elements from the City’s Bird-safe Design Guidelines:
[Bird-Safe Design Guidelines | City of Ottawa \[ottawa.ca\]](#)
- b. plant as many locally appropriate native trees/shrubs/plans as possible.

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Comments from Transportation Engineering Review, Neeti Paudel:

- Noise Impact Studies required for the following:
 - Road.
 - Stationary (if there will be any exposed mechanical equipment due to the proximity to neighbouring noise sensitive land uses)
- Right of Way Protection on St Joseph along the frontage is **37.5m** (18.75m from the centerline). This needs to be protected and shown on the site plan.
- Ensure there is sufficient throat length at the access as per TAC.
- Access should be designed as per City standards SC7.1. Recommend moving the access as far as possible from the intersection.
- As the proposed site is for the general public use, AODA legislation applies.
 - Clearly define accessible parking stalls and ensure they meet AODA standards (include an access aisle next to the parking stall and a pedestrian curb ramp at the end of the access aisle, as required).
 - Please consider using the City’s Accessibility Design Standards, which provide a summary of AODA requirements. <https://ottawa.ca/en/city-hall/creating-equal-inclusive-and-diverse-city/accessibility-services/accessibility-design-standards-features#accessibility-design-standards> [ottawa.ca]

Closing Thoughts:

Sometime after June 2023, the regime of reviewing development review applications will change. It is advisable to submit before then so that these pre-consultation notes are still applicable. Also to note, sometime after May/June 2023, the High Performance Design Standards will become applicable, some of which will become mandatory and some of which will be voluntary measures. Community Benefits Charge By-law may also be applicable.

***Minimum Drawing and File Requirements- All Plans** -Please note that Plans are to be submitted on standard **A1 size** (594mm x 841mm) sheets, saved as PDFs, utilizing an appropriate Metric scale (1:200, 1:250, 1:300, 1:400, or 1:500). **Provide individual PDF of the DWGs (plans – i.e. not 2 or more sheets per PDF) and for reports please provide one PDF file of the reports. All PDF documents are to be unlocked and flattened.**

If there is a need for clarifications or a need for more information, please do not hesitate to contact me.

Best wishes,

Shoma Murshid, MCIP, RPP

(she/ her/ elle)

File Lead, Planner II

Responsable de dossier, urbaniste II

City of Ottawa/ Ville d'Ottawa

Development Review (Suburban Services, East)/ Examen des projets d'aménagement (Services suburbains Est)

Planning, Real Estate and Economic Development Department / Direction générale de la planification, des biens immobiliers et du développement économique

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