# 1815 MONTREAL ROAD

1815 MONTREAL ROAD

ISSUED FOR SITE PLAN CONTROL 3: 2024.01.05

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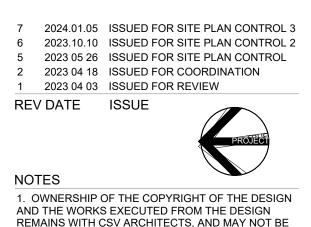
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CLIENT

OTTAWA ONTARIO, CANADA

PROJECT

### 1815 MONTREAL ROAD

1815 MONTREAL ROAD

TITLE

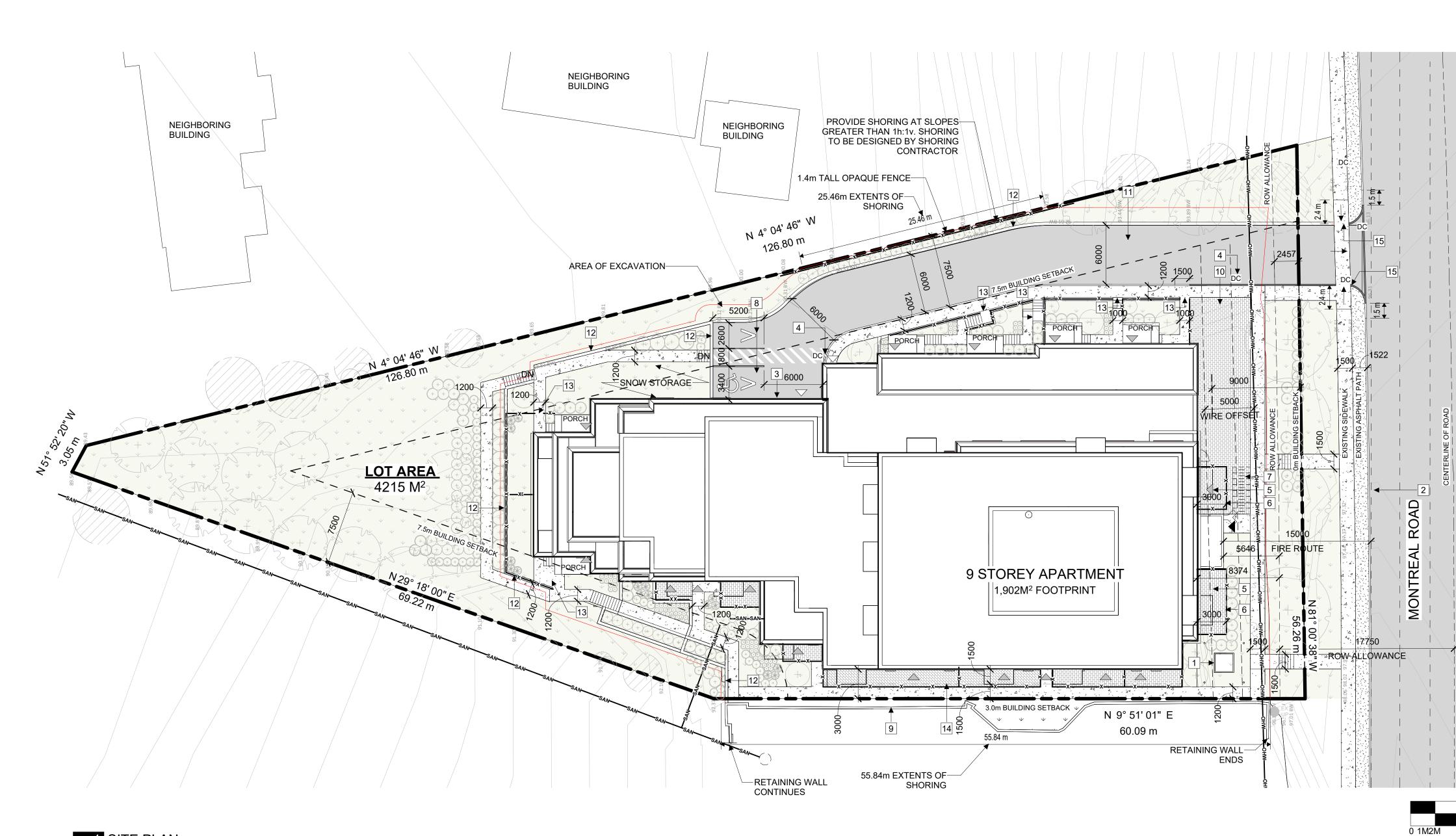
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R.R. 1 : 100

DRAWING NO.





1 SITE PLAN A100 1:250

#### LEGAL DESCRIPTION: LOT 141 REGISTERED PLAN 652 CITY OF OTTAWA

#### **REFERENCE SURVEY:**

BASED ON INFORMATION FROM A SURVEY PREPARED BY R.P., J.D. BARNES LIMITED. DATED: 3RD DAY OF OCTOBER, 2022.

#### MUNICIPAL ADDRESS:

1815 MONTREAL ROAD, CITY OF OTTAWA, ON

#### **DEVELOPMENT INFORMATION:**

SITE AREA	4,215 m <sup>2</sup>
BUILDING FOOTPRINT	1,902 m <sup>2</sup>
LOT COVERAGE	= 45%
GROSS FLOOR AREA (1st FLOOR - 9th FLOOR)	10,918 m <sup>2</sup>
BUILDING HEIGHT	+/-31m

ZONE

SCHEDULE 1:

SCHEDULE 1A:

SCHEDULE 2:

Proposed - AM10 Current - R1AA AREA C "URBAN AND GREEN BELT AREA" AREA C "SUBURBAN" DISTANCE EXCEEDS 600 m

	<u>UNIT MIX</u>	
	1 BED APT	54
	2 BED APT	55
	3 BED APT	2
	GFU*	6
	BACH	13
	TOTAL	130
	DENSITY	
	SITE AREA (m <sup>2</sup> )	4352
	SITE AREA (Ha)	0.4352
	DENSITY du/Ha	299
*GROU	ND FACING UNIT	

ZONING PROVI	SION	<u>REQUIRED</u>	PROVIDED	ZONING PRO	VISION	<u>R</u>		
MIN. LOT WIDTI	н	NO MINIMUM	54.9m AFTER ROW	Amenity	6m <sup>2</sup> per dwelling unit.			
MIN. LOT AREA		NO MINIMUM	4215m <sup>2</sup> AFTER ROW	Space Requirements	Min 50% communal, a			
MIN. FRONT YAR	RD SETBACK	3m	5.6m		areas of 54m <sup>2</sup> and who aggregated area is pro	ovide		
ROW DEDICATI REQUIREMENT		17.75m FROM CENTERLINE (2.39m)	2.4m		must be minimum 54n (Can count towards la requirements)			
HYDRO SETBA	СК	5m	5m					
MIN. REAR YAR	D SETBACK	7.5m	45.5m					
MIN. INTERIOR ABUTTING A RE	SIDE YARD ESIDENTIAL ZONE	7.5m	7.5m					
MIN. INTERIOR ABUTTING AN A		ОМ	3.0M					
Maximum Building Height (m)	Up to 20 meters from a R1, R2, R3 Zone	11m	East:24.8m West: 31.28m	PARKING QUE	EING + LOADING R	EQU		
(height is from average grade)	20m to 30m from R1-R3 Zone	20m	31.28m	RESIDENTIAL	SPACES	15		
-	30m from property line abutting R1-R4 Zone	30 m, max. 9 storeys.		VISITOR SPAC	CES	26		
	In all other cases	30m, max. 9 storeys.		BICYCLE PAR	KING	6		
Landscaped Area	Abutting a residential zone	Min 3 m; reduced to 1 m where 1.4 m	East: 1.0m. 1.4m opaque					
Анса		high opaque fence is provided	high opaque fence	high opaque fence	fence is provided.	Waste /	If outdoor: Located wit	thin 9
	In all other cases	No minimum		Garbage Collection	abutting a public stree lot line, and screened	from		
TOTAL LANDSC	APED AREA		1,916M <sup>2</sup>	Location	opaque screen of mini	mum		

#### SITE PLAN GENERAL NOTES:

- 1. ALL GENERAL SITE INFORMATION AND CONDITIONS COMPILED FROM EXISTING PLANS AND SURVEYS
- 2. DO NOT SCALE THIS DRAWING 3. REPORT ANY DISCREPANCIES PRIOR TO COMMENCING WORK. NO RESPONSIBILITY IS BORN BY THE CONSULTANT FOR UNKNOWN SUBSURFACE CONDITIONS
- 4. CONTRACTOR TO CHECK AND VERIFY ALL DIMENSIONS ON SITE AND REPORT ANY ERRORS AND/OR OMISSIONS TO THE CONSULTANT
- 5. REINSTATE ALL AREAS AND ITEMS DAMAGED AS A RESULT OF CONSTRUCTION ACTIVITIES TO THE SATISFACTION OF THE CONSULTANT
- 6. CONTRACTOR TO LAYOUT PLANTING BEDS, PATHWAYS ETC. TO APPROVAL OF CONSULTANT PRIOR TO ANY JOB EXCAVATION
- 7. THE ACCURACY OF THE POSITION OF UTILITIES IS NOT GUARANTEED - CONTRACTOR TO VERIFY PRIOR TO EXCAVATION
- 8. INDIVIDUAL UTILITY COMPANY MUST BE CONTACTED FOR CONFIRMATION OF UTILITY EXISTENCE AND LOCATION PRIOR TO DIGGING
- 9. ALL DISTURBED AREAS TO BE RESTORED TO ORIGINAL CONDITION OR BETTER UNLESS OTHERWISE NOTED

#### **SITE PLAN KEYNOTES:**

- 1 HYDRO-TRANSFORMER
- 2 6m WIDE FIRE TRUCK ACCESS
- 3 UNDERGROUND PARKING ACCESS RAMP
- 4 DEPRESSED CURB
- 5 PRIVATE PATIO
- 6 PRIVACY FENCE
- 7 8 X VISITOR BIKE RACKS 1800x600 TYP
- 8 VISITOR PARKING SPACE
- 9 EXISTING RETAINING WALL
- 10 INTERLOCK AREA/ GARBAGE PICKUP
- 11 NEW ASPHALT LANEWAY
- 12 RETAINING WALL
- 13 GATE
- 14 BALCONIES ABOVE
- 15 ACCESS TO SITE DESIGNED AS PER CITY OF OTTAWA "CURB RETURN ENTRANCE" DWG. NO. SC7.1 "CURB RETURN AT A PRIVATE OR COMMERCIAL ENTRANCE WITH BOULEVARD - UNSIGNALIZED INTERSECTION"

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OTTAWA ONTARIO, CANADA

PROJECT

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TITLE

### SITE PLAN

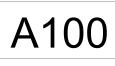
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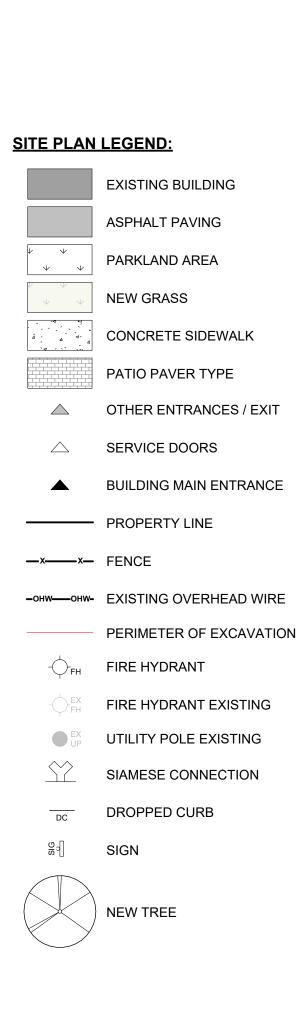
- BUILDING MAIN ENTRANCE

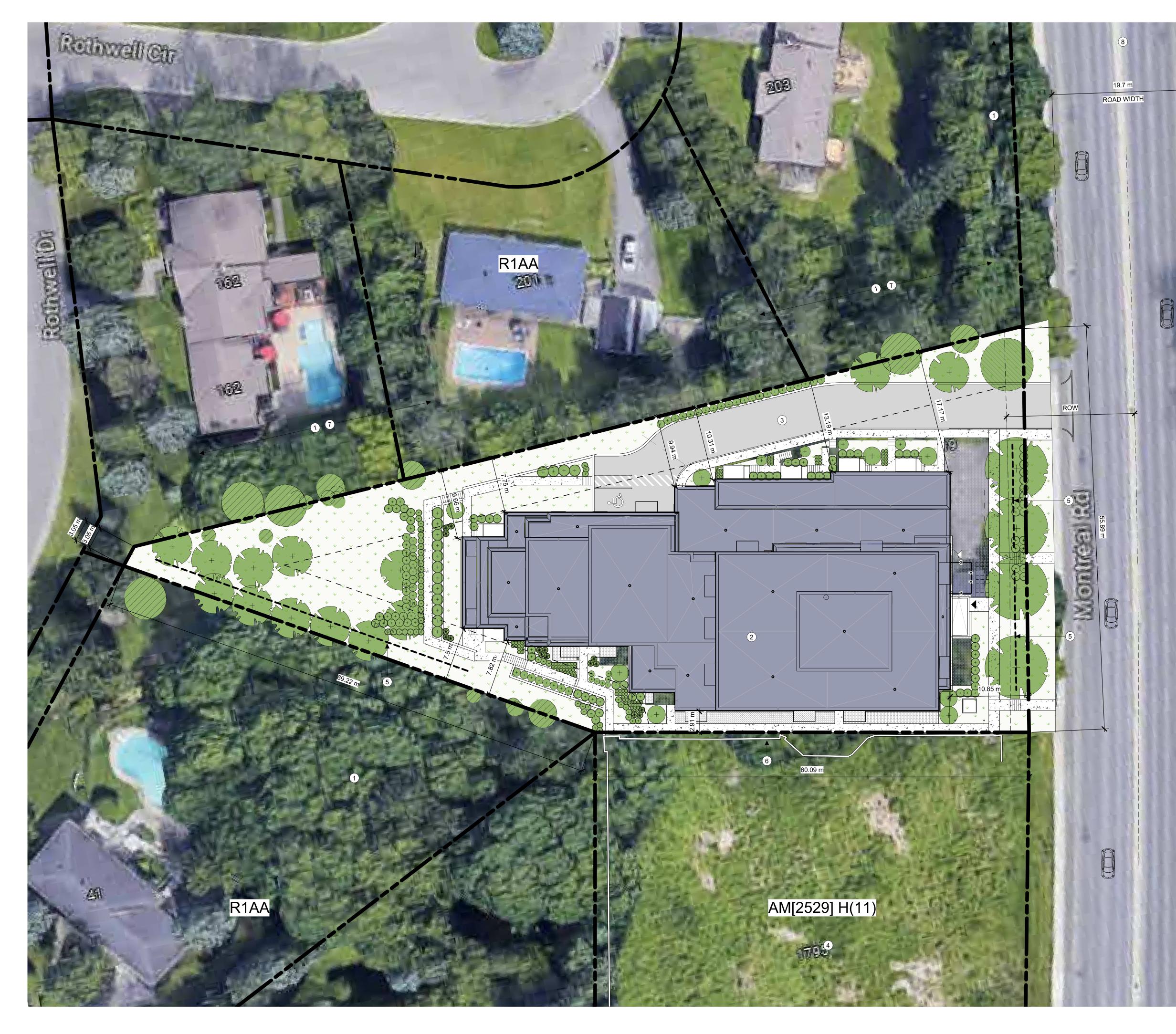
- SIAMESE CONNECTION

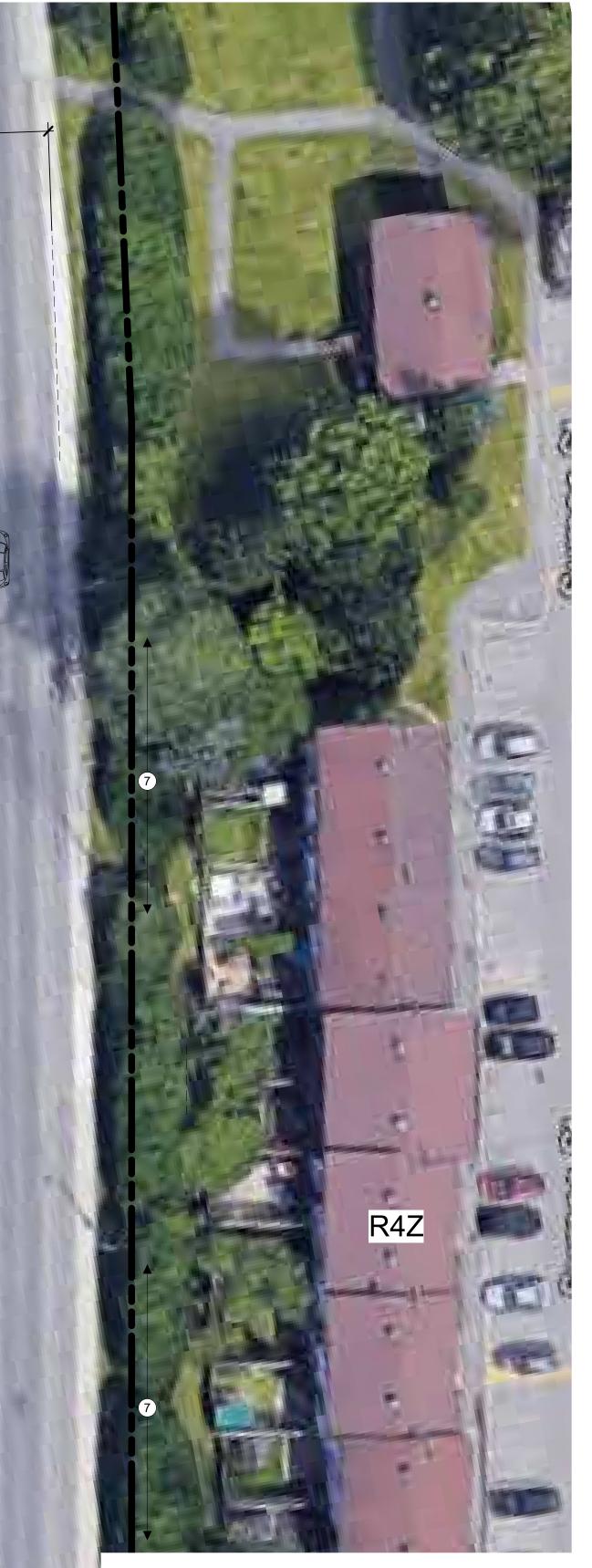
10M

<u>REQUIRED</u>	PROVIDED
it.	Balconies= 805.7 m²
I, aggregated into where more than one provided, at least one 4m <sup>2</sup> = 780 m <sup>2</sup>	Communal Rooftop Terrace (2 <sup>nd</sup> and 7 <sup>th</sup> floor): 135 m <sup>2</sup>
lanuscape	Outdoor Private Patio (ground floor): 127 m²
	Interior amenity space = 210.2 m <sup>2</sup>
	Rear Yard Area: 770.2 m <sup>2</sup>
	Total amenity area= 2048.1 m <sup>2</sup>
REQUIRED	PROVIDED
	89 TOTAL 8) 4.6m x 2.4m 71) 6m x 2.6m
(2)	18 TOTAL 6) 5.2m x 2.6m BF 6m x 3.40m .5m Transfer space
Res Res Vis	Total: 66 sidential: 12 @P2 sidential: 19 @P1 sidential: 27 @L1 sitors: 8 Outside ces are 1.8m x 0.6n
within 9m from lot line reet, 3m from any othe ed from view with an ninimum height of 2m.	Pickup staging at

public street.







#### <u>LEGEND</u>

- 1 WOODED AREAS
- 2 PROPOSED BUILDING
- 3 PROPOSED DRIVEWAY
- (4) VACANT SITE (APPROVED FOR WAREHOUSE + OFFICE BUILDING)
- 5 SWALE
- (6) EXISTING RETAINING WALL
- 7 TREES
- 8 PUBLIC TRAVELLED ROAD

<u>NOTE:</u> FOR DETAILED SITE LANDSCAPE PLEASE REFER TO A100, L1.1 FOR DETAILED GRADING PLEASE REFER TO C101, C102

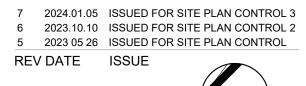
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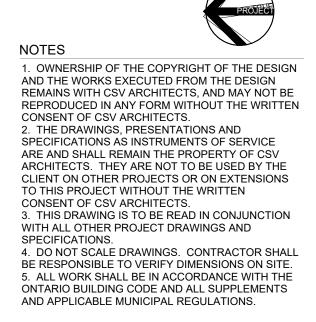
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### SITE PLAN CONTEXT

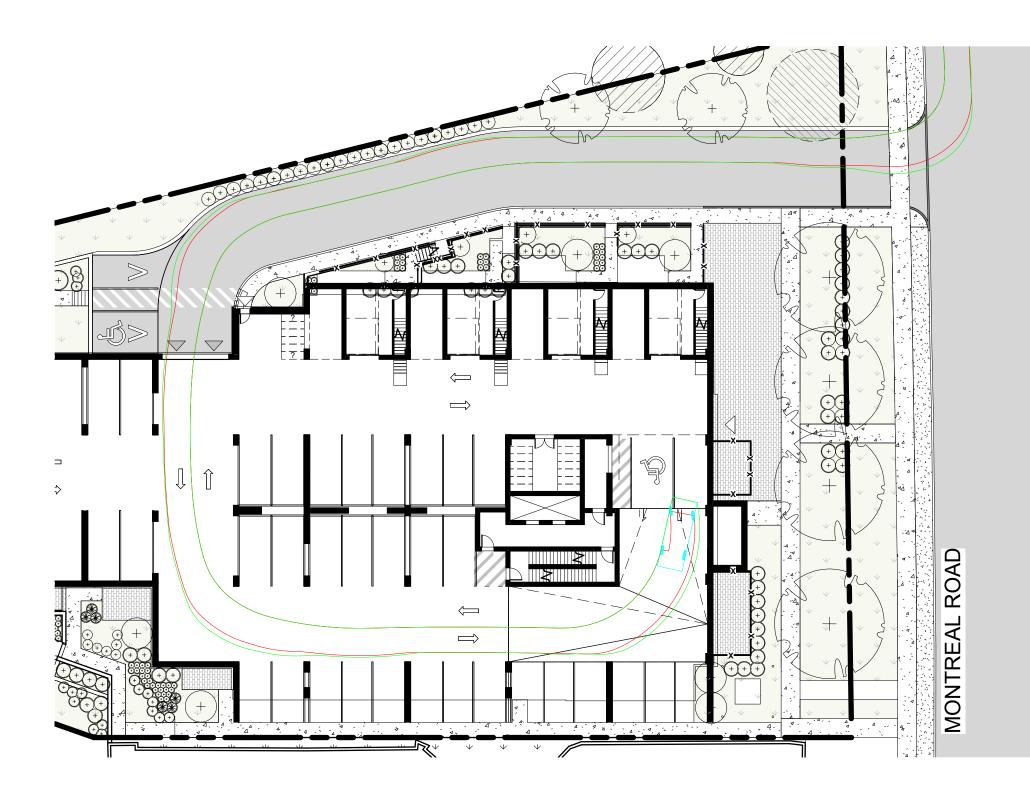
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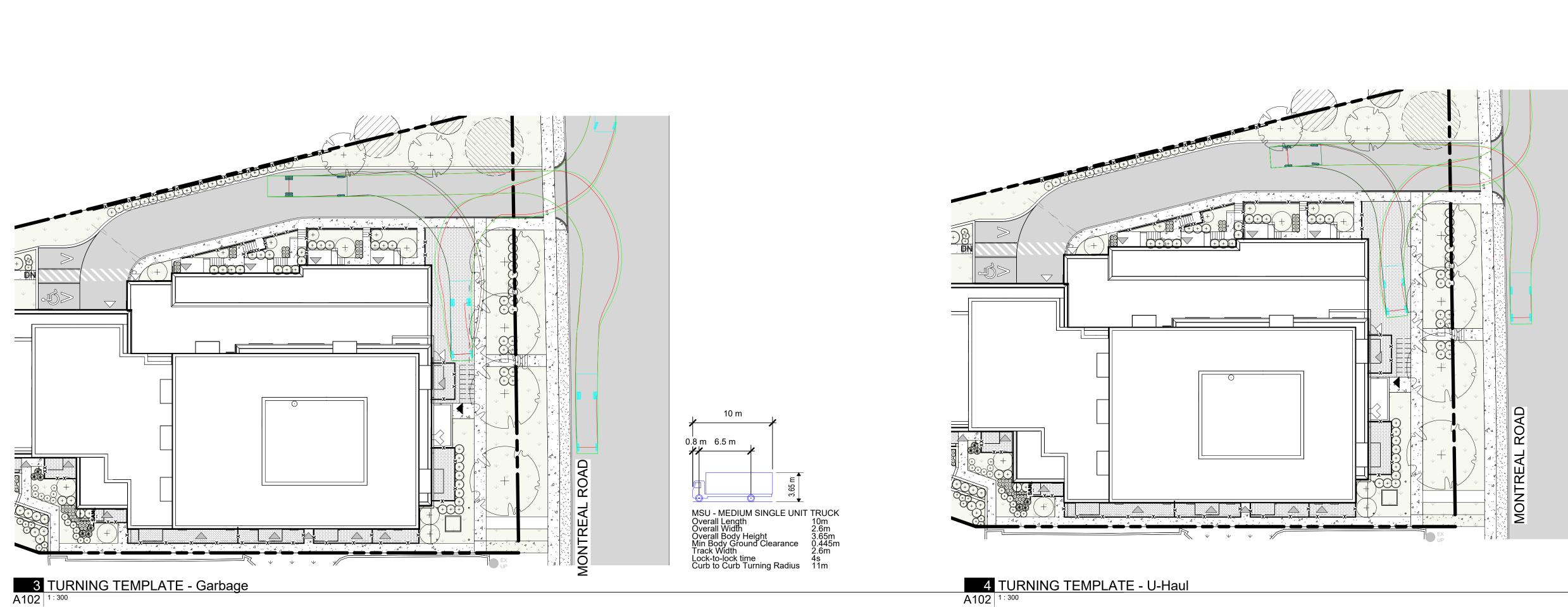
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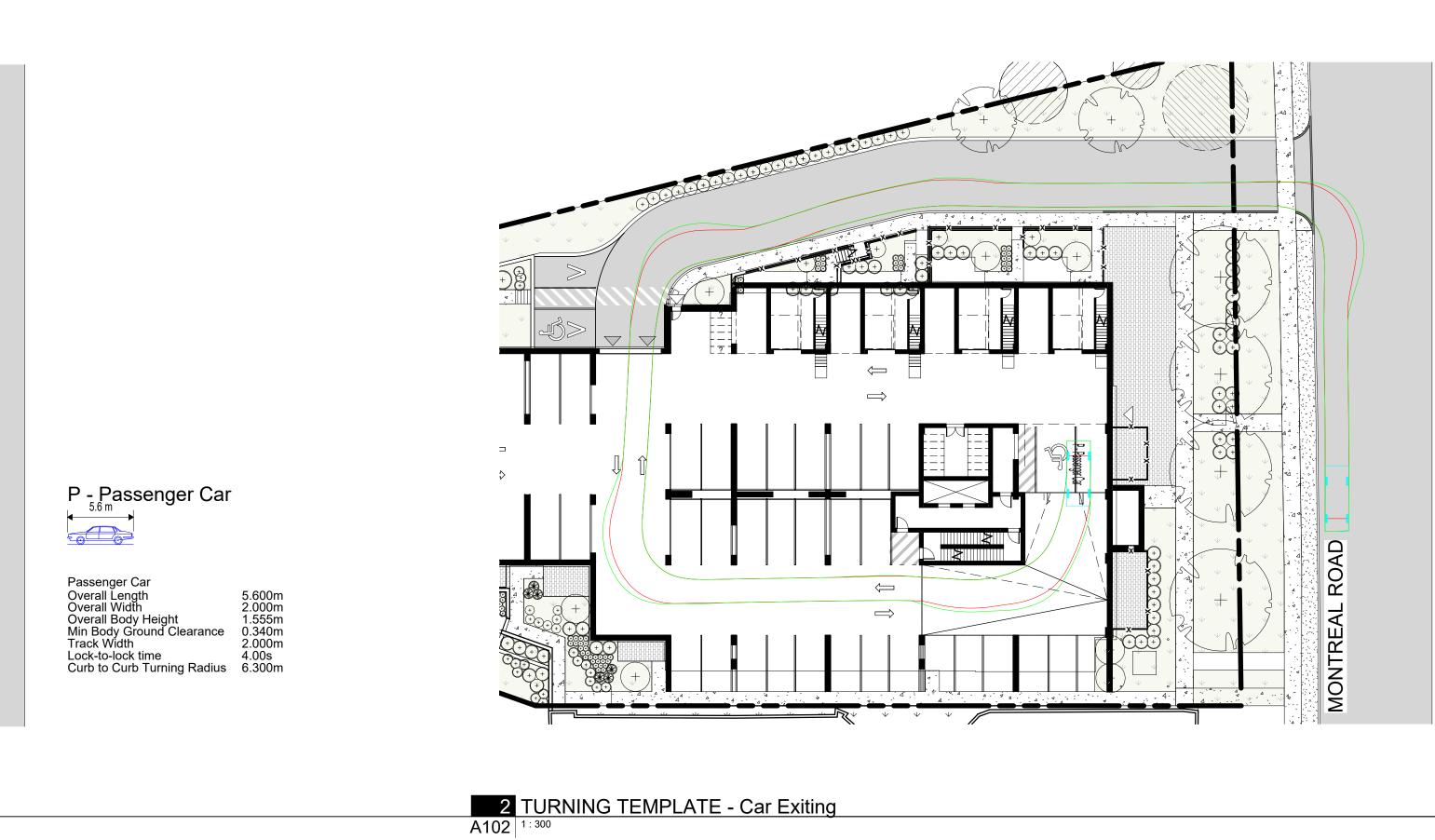
A101





**<sup>1</sup>** TURNING TEMPLATE - Car Entering A102 1:300





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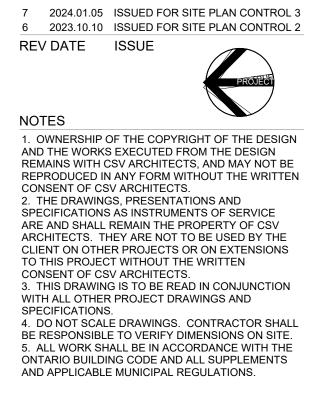


# Passenger CarOverall Length5.600mOverall Width2.000mOverall Body Height1.555mMin Body Ground Clearance0.340mTrack Width2.000mLock-to-lock time4.00sCurb to Curb Turning Radius6.300m





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PROJECT

### 1815 MONTREAL ROAD

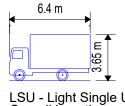
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### SITE ACCESS/ **TURNING TEMPLATES**

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LSU - Light Single Unit Truck Overall Length Overall Width Overall Body Height Min Body Ground Clearance Track Width Lock-to-lock time Curb to Curb Turning Radius



REV



Resident	
17	COMPACT
72	NORMAL
Visitor	
2	BF PARKING
16	NORMAL
107	

Bike Parking 66





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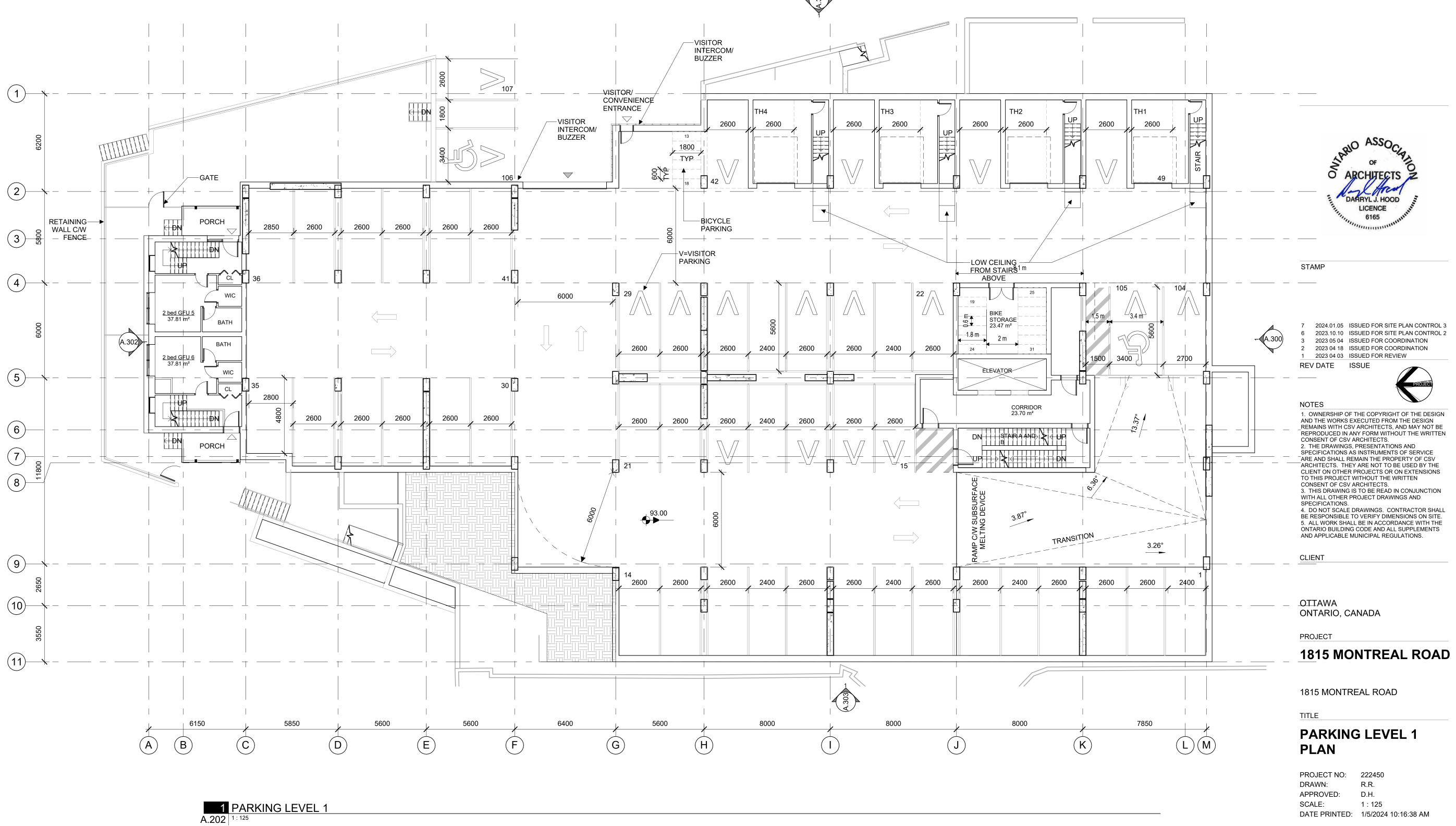
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REV

COMPACT
NORMAL
BF PARKING
NORMAL
·

Bike Parking 66



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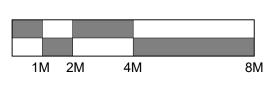
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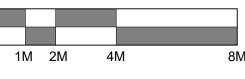


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7

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A.213



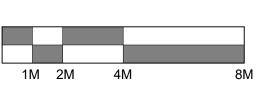
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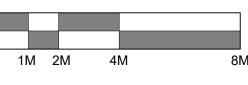


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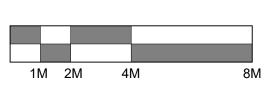


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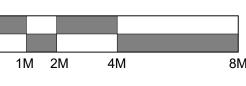
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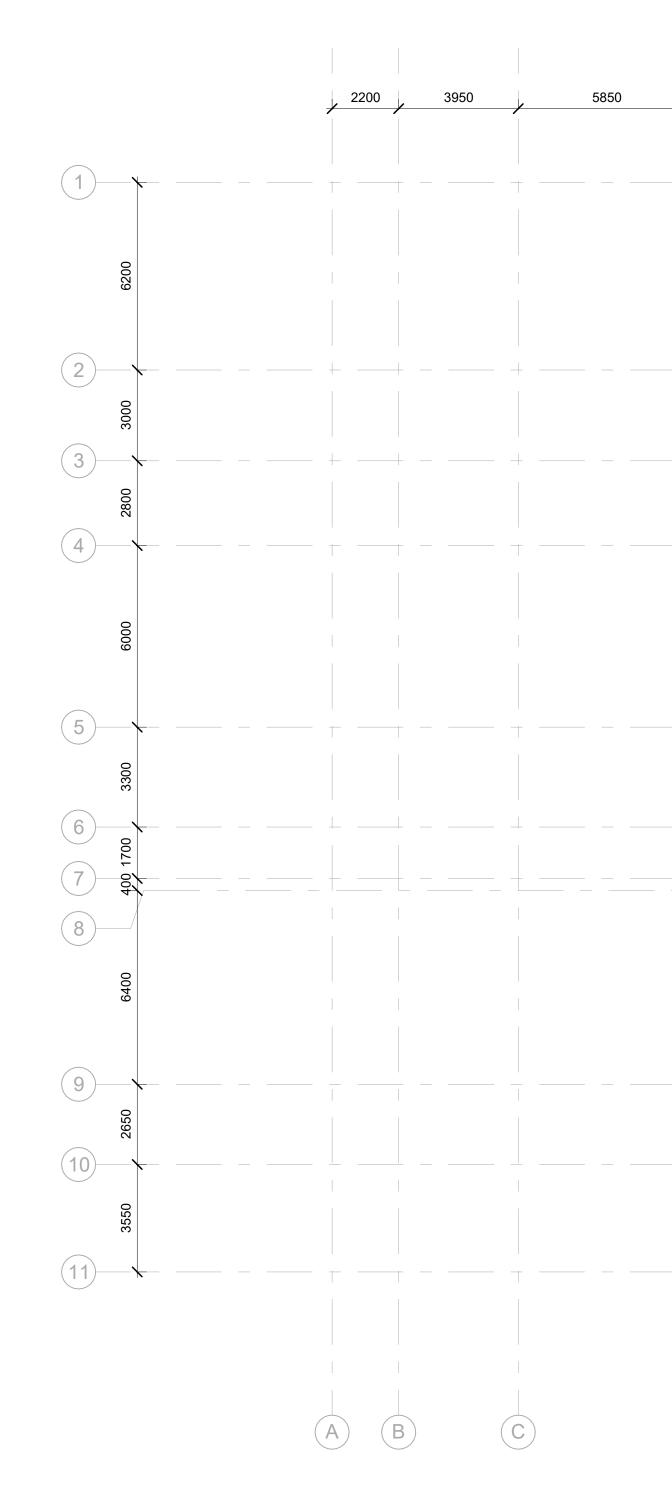
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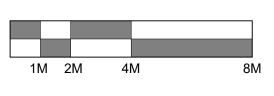


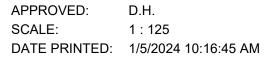
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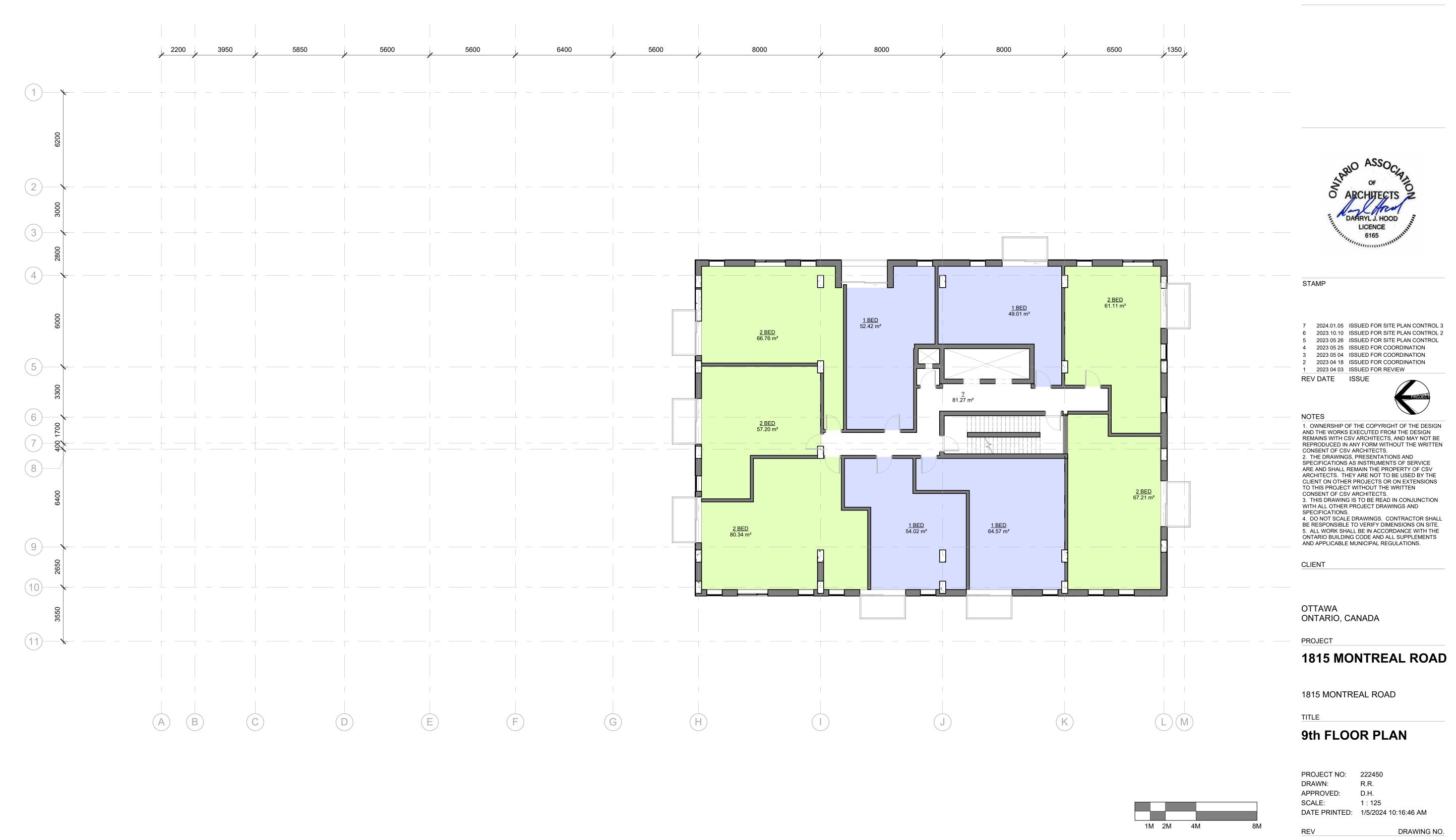


DRAWING NO.



SCALE:

REV



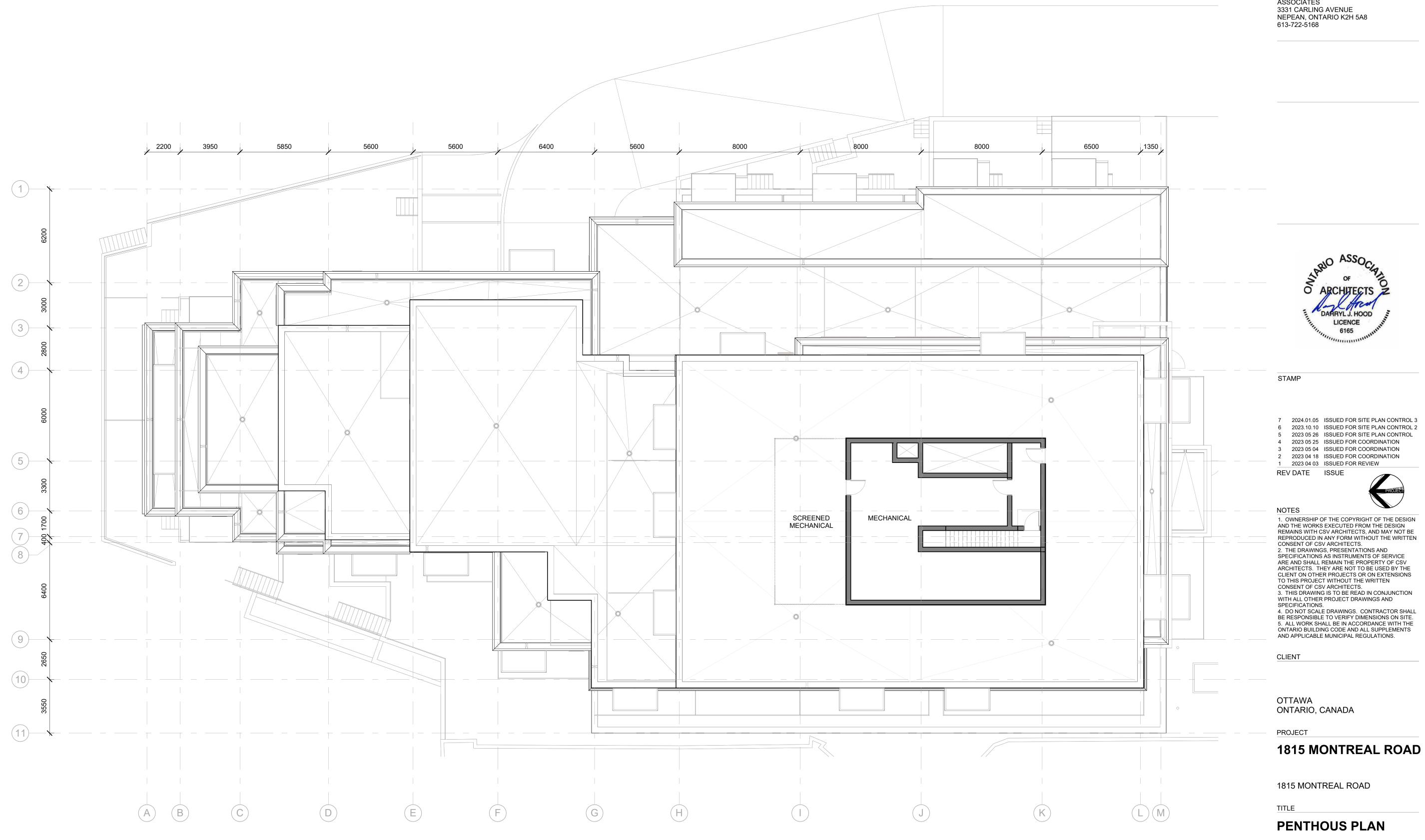
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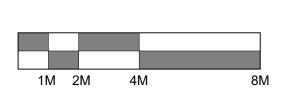




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REV	DATE	ISSUE
1	2023 04 03	ISSUED FOR REVIEW
2	2023 04 18	ISSUED FOR COORDINATION
3	2023 05 04	ISSUED FOR COORDINATION
4	2023 05 25	ISSUED FOR COORDINATION
5	2023 05 26	ISSUED FOR SITE PLAN CONTROL
6	2023.10.10	ISSUED FOR SITE PLAN CONTROL 2
7	2024.01.05	ISSUED FOR SITE PLAN CONTROL 3

### 1815 MONTREAL ROAD

PROJECT NO: 222450 R.R. DRAWN: APPROVED: D.H. SCALE: 1 : 125 DATE PRINTED: 1/5/2024 10:16:48 AM

REV

7







ELEVATION NOTES:

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BIRD-SAFE GLASS TO BE USED AT A MIN. 90% OF ALL GLASS WITHIN THE FIRST 16M ABOVE AVERAGE GRADE (OR TO THE HEIGHT OF THE ADJACENT MATURE TREES)

#### **ELEVATION KEYNOTES:**

- # SAMPLE TEXT
- # SAMPLE TEXT
- # SAMPLE TEXT
- # SAMPLE TEXT MULTI LINE
- # SAMPLE TEXT

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#### ELEVATION LEGEND:

HATCH PATTERNS SHOWN ARE FOR GRAPHIC PURPOSES ONLY AND SHOULD BE COORDINATED WITH ACTUAL SPECIFIED MATERIALS.

HATCH	SYMBOL	DESCRIPTION
		BRICK VENEER MANUFACTURE COLOUR:
		STONE VENEEF MANUFACTURE COLOUR:
		FIBERCEMENT MANUFACTURE COLOUR:

BRICK VENEER MANUFACTURER: COLOUR: STONE VENEER - BLOCK MANUFACTURER: COLOUR:

FIBERCEMENT CLADDING MANUFACTURER: COLOUR:



STAMP

7	2024.01.05	ISSUED FOR SITE PLAN CONTROL 3
6	2023.10.10	ISSUED FOR SITE PLAN CONTROL 2
5	2023 05 26	ISSUED FOR SITE PLAN CONTROL
4	2023 05 25	ISSUED FOR COORDINATION
3	2023 05 04	ISSUED FOR COORDINATION
2	2023 04 18	ISSUED FOR COORDINATION
1	2023 04 03	ISSUED FOR REVIEW
REV DATE		ISSUE

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CLIENT

OTTAWA ONTARIO, CANADA

PROJECT

### 1815 MONTREAL ROAD

1815 MONTREAL ROAD

TITLE

#### SOUTH ELEVATION

PROJECT NO: 222450 DRAWN: APPROVED: D.H. SCALE:

R.R. As indicated DATE PRINTED: 1/5/2024 10:16:58 AM

DRAWING NO.



REV

A.300



A.301 1:125

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#### **ELEVATION KEYNOTES:**

- # SAMPLE TEXT
- # SAMPLE TEXT
- # SAMPLE TEXT
- # SAMPLE TEXT MULTI LINE
- # SAMPLE TEXT

ELEVATION LEGEND:

### 32660 PENTHOUSE ROOF

29420 ROOF

26180 LEVEL 9

22940 LEVEL 8

19700 LEVEL 7

16460 LEVEL 6

9980 LEVEL 4

6740 LEVEL 3

3500 LEVEL 2

0 LEVEL 1

AVERAGE GRADE

-3500 PARKING 1

-7000 PARKING 2

-8800 TOP OF FOOTING

#### ACTUAL SPECIFIED MATERIALS. HATCH SYMBOL DESCRIPTION BRICK VENEER MANUFACTURER: COLOUR:

PURPOSES ONLY AND SHOULD BE COORDINATED WITH

HATCH PATTERNS SHOWN ARE FOR GRAPHIC

STONE VENEER - BLOCK MANUFACTURER: COLOUR: FIBERCEMENT CLADDING

MANUFACTURER: COLOUR:

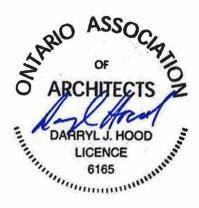
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7	2024.01.05	ISSUED FOR SITE PLAN CONTROL 3
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CLIENT

OTTAWA ONTARIO, CANADA

PROJECT

### 1815 MONTREAL ROAD

1815 MONTREAL ROAD

TITLE

### EAST ELEVATION

PROJECT NO: 222450 DRAWN: APPROVED: D.H. SCALE:

R.R. As indicated DATE PRINTED: 1/5/2024 10:17:08 AM

DRAWING NO.









A.302 1:125

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- # SAMPLE TEXT
- # SAMPLE TEXT
- # SAMPLE TEXT MULTI LINE
- # SAMPLE TEXT

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#### ELEVATION LEGEND:

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BRICK VENEER MANUFACTURER: COLOUR: STONE VENEER - BLOCK MANUFACTURER: COLOUR:

FIBERCEMENT CLADDING MANUFACTURER: COLOUR:



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CLIENT

OTTAWA ONTARIO, CANADA

PROJECT

### 1815 MONTREAL ROAD

1815 MONTREAL ROAD

TITLE

### NORTH ELEVATION

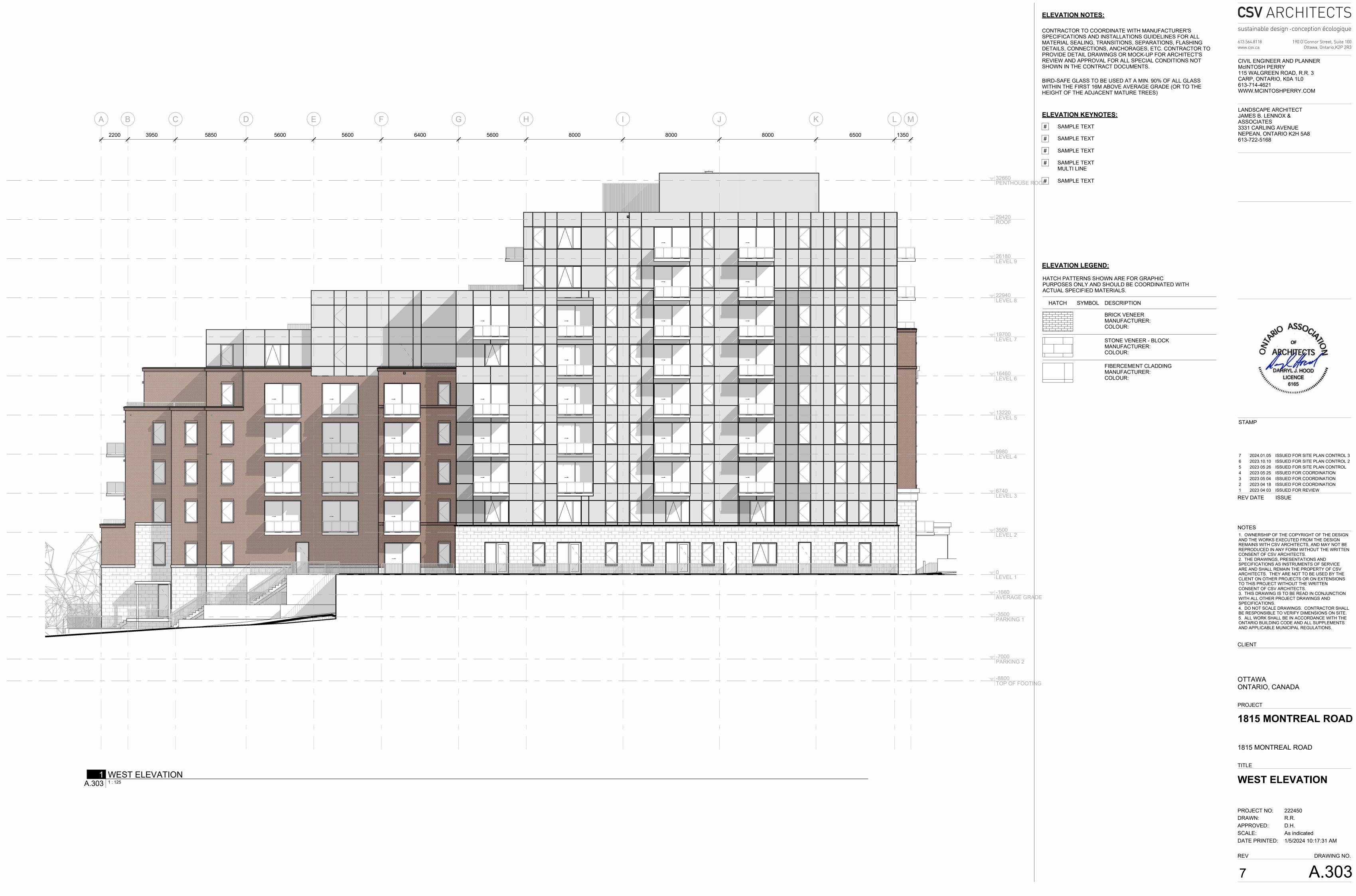
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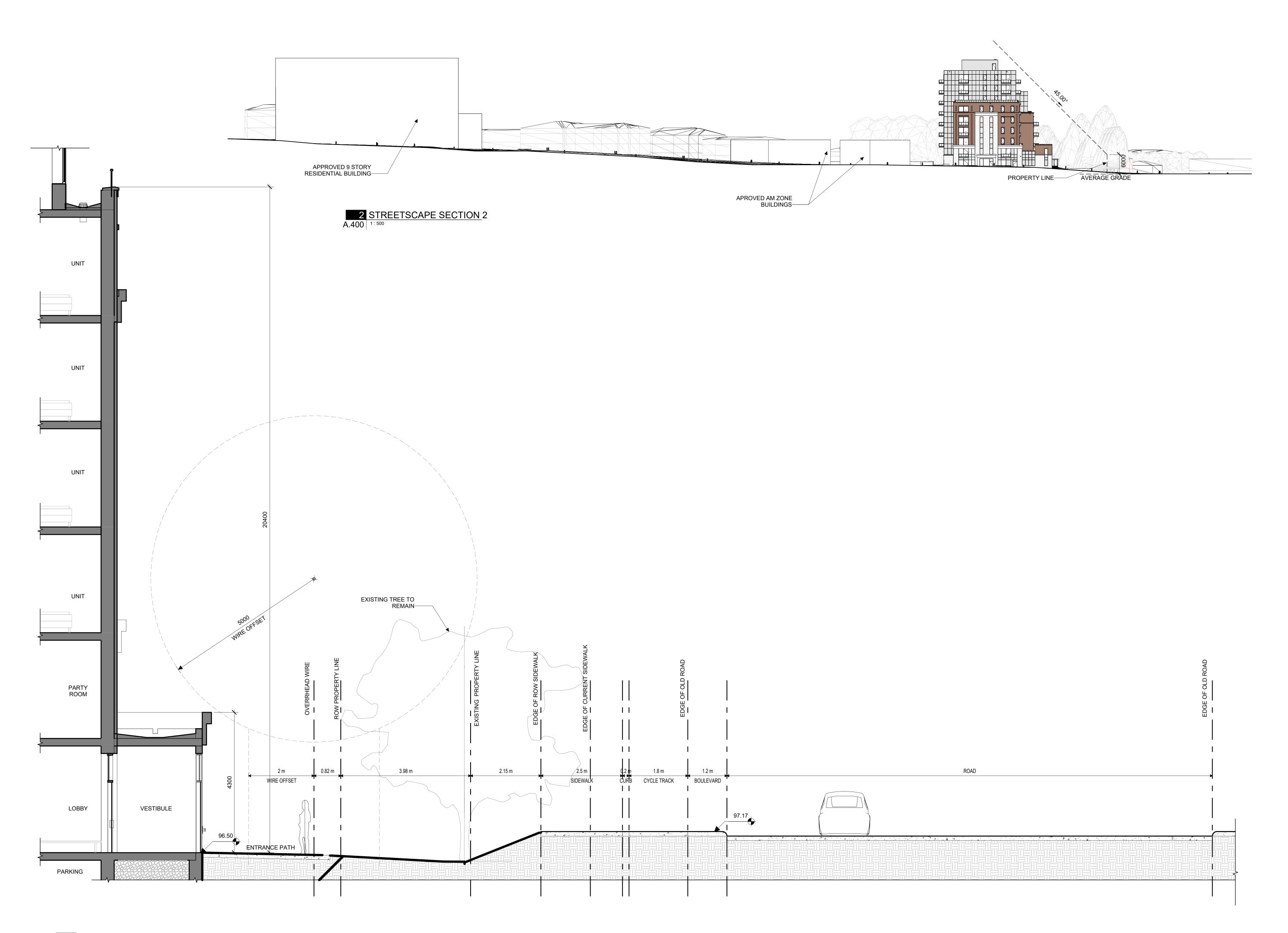
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A.302







A.400 1:50

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PROJECT

### 1815 MONTREAL ROAD

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TITLE

### STREETSCAPE SECTIONS

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