

1083-1095 Merivale Road

Planning Rationale and Design Brief

Site Plan Control Application June 2023



1083-1095 Merivale Road

Planning Rationale and Design Brief

Site Plan Control Application

June 2023

Prepared For:

Stephen Bartolo, Senior Vice President
Shepherds of Good Hope
233 Murray Street
Ottawa, ON K1N 5M9
T: 613-899-0721
E: sbartolo@sghottawa.com

Prepared By:

WSP Canada Inc.
2611 Queensview Drive, Suite 300
Ottawa, ON, K2G 8K2

Nadia De Santi, MCIP, RPP
Practice Lead
T: 613-690-1114
E: Nadia.De-Santi@wsp.com

and

Jill MacDonald, MCIP, RPP
Project Planner
T: 613-690-3936
E: Jill.MacDonald@wsp.com

SIGNATURES

PREPARED BY



Nadia De Santi, MCIP, RPP
Practice Lead



Jill MacDonald, MCIP, RPP
Project Planner

This Planning Rationale was prepared by WSP Canada Inc. (“WSP”) for CSV Architects (“Client”) on behalf of the Shepherds of Good Hope in accordance with the agreement between WSP and the Client. This Planning Rationale is based on information provided to WSP which has not been independently verified.

The disclosure of any information contained in this Report is the sole responsibility of the Client. The material in this Report, accompanying documents and all information relating to this activity reflect WSP’s judgment in light of the information available to us at the time of preparation of this Report. Any use which a third party makes of this Report, or any reliance on or decisions to be made based on it, are the responsibility of such third parties. WSP accepts no responsibility for damages, if any, suffered by a third party as a result of decisions made or actions based on this Report.

WSP warrants that it performed services hereunder with that degree of care, skill, and diligence normally provided in the performance of such services in respect of projects of similar nature at the time and place those services were rendered. WSP disclaims all other warranties, representations, or conditions, either express or implied, including, without limitation, warranties, representations, or conditions of merchantability or profitability, or fitness for a particular purpose.

This Standard Limitations statement is considered part of this Report.



Table of Contents

1	INTRODUCTION.....	1
2	SITE LOCATION AND COMMUNITY CONTEXT	2
2.1	Site Location.....	2
2.2	Community Context.....	3
2.2.1	Surrounding Development Activity.....	7
2.3	Transportation Network.....	8
3	THE PROPOSED DEVELOPMENT	9
4	COMMUNITY ENGAGEMENT AND OUTREACH (PUBLIC CONSULTATION STRATEGY).....	18
5	POLICY AND REGULATORY FRAMEWORK ...	18
5.1	Provincial Policy Statement, 2020	18
5.2	City of Ottawa Official Plan (Adopted November 24, 2021 and Approved by MMAH on November 4, 2022 with Modifications).....	20
5.2.1	Road Classification and Rights-of-Way.....	25
5.3	Design Brief.....	26
5.3.1	Design Priority Areas.....	26
5.3.2	Section 4.6 - Urban Design	27
5.3.3	Urban Design Guidelines for Development along Traditional Mainstreets (May 2006).....	28
5.3.4	Merivale Road North Community Design Plan (June 2018).....	33
5.3.5	Response to Preliminary City Urban Design Comments	35
5.4	City of Ottawa Comprehensive Zoning By-law 2008-250 (Consolidation September 8, 2021).....	38
5.4.1	Zoning Provisions.....	39

5.4.2 Permitted Projections Above the Height Limit and Permitted Projections into Required Yards Provisions..... 43

5.4.3 Parking Provisions..... 46

5.5 Amenity Area Provisions.....55

6 SUMMARY OF OPINION57

Figures

Figure 2-1: Site Location (GeoOttawa, 2023) 2

Figure 2-2: Rendering of 1095 Merivale Road (CSV Architects, 2022) 3

Figure 2-3: Surrounding Community Context (WSP, 2023) 4

Figure 2-4: Site Photos – Existing Site Conditions and Context – Pinpoint Map (WSP, 2023) 5

Figure 2-5: Site, looking east – Existing single-detached dwelling utilized as an informal office. 5

Figure 2-6: Site, looking northeast along Merivale Road..... 5

Figure 2-7: Four-storey apartment building (1077 Merivale, north of the site. 6

Figure 2-8: Four-storey apartment building (1900 Merivale), south of the site. 6

Figure 2-9: Existing three-storey SGH building at 1095 Merivale Road, looking east..... 6

Figure 2-10: Existing access from Merivale Road to 1083 Merivale Road, looking north..... 6

Figure 2-11: Existing parking area at the rear of the site behind 1095 Merivale, looking east. 6

Figure 2-12: Existing access to 1095 Merivale from Panet Street / Emperor Drive, looking south..... 6

Figure 2-13: Development Activity in Vicinity of the Site..... 7

Figure 2-14: Schedule C5 - Downtown Core Road Network (Excerpt), (City of Ottawa New Official Plan (Adopted November 24, 2021 and approved by MMAH on November 4, 2022 with modifications)).. 8

Figure 2-15: OC Transpo Network Map (December 25, 2022) 8

Figure 3-1: 1083-1095 Merivale Road – Site Plan (Prepared by CSV, dated June 2, 2023).....	11
Figure 3-2: Proposed Building Elevations (East) – 1083-1095 Merivale Road (Prepared by CSV Architects, dated June 2, 2023)	11
Figure 3-3: Proposed Building Elevations (North) – 1083-1095 Merivale Road (Prepared by CSV Architects, dated June 2, 2023)	13
Figure 3-4: Proposed Building Elevations (West) – 1083-1095 Merivale Road (Prepared by CSV Architects, dated June 2, 2023)	14
Figure 3-5: Proposed Building Elevations (South) – 1083-1095 Merivale Road (Prepared by CSV Architects, dated June 2, 2023)	15
Figure 3-6: Preliminary Rendering – 1083-1095 Merivale Road – Looking Southeast on Merivale Road (Prepared by CSV Architects, June 2, 2023)	16
Figure 3-7: Preliminary Rendering – 1083-1095 Merivale Road – Looking Southeast on Merivale Road (Prepared by CSV Architects, June 2, 2023)	17
Figure 5-1: Schedule B2 – Inner Urban Transect (Excerpt) (City of Ottawa New Official Plan (Adopted November 24, 2021 and approved by MMAH on November 4, 2022 with modifications)).....	20
Figure 5-2: Schedule A - Transect Policy Areas (Excerpt), New City of Ottawa Official Plan (Adopted November 24, 2021)	23
Figure 5-3: Schedule C7-A- Design Priority Areas – Urban (Excerpt) (City of Ottawa New Official Plan (Adopted November 24, 2021 and approved by MMAH on November 4, 2022 with modifications)	26
Figure 5-4: Figure 1 - CDP Area (Extract) (Merivale Road North Community Design Plan (June 2018))	33
Figure 5-5: Figure 11 - Character Areas (Excerpt) (Merivale Road North CDP (June 2018))	34
Figure 5-6: Site Zoning (geoOttawa, 2023)	38



Tables

Figure 5-7: Schedule 1A – Areas for Minimum Parking Space Requirements, City of Ottawa Zoning By-law (via GeoOttawa, 2022)..... 46

Table 2-1 Development Activity in Vicinity of the Site 7

Table 5-1: Urban Design Guidelines for Development along Traditional Mainstreets – Analysis for 1083-1095 Merivale Road..... 28

Table 5-2: Zoning Compliance for the Traditional Mainstreet, Subzone 12 (TM12) 40

Table 5-3: Provisions for Permitted Projections Above Height Limit / into Required Yards..... 43

Table 5-4: Parking and Loading Space Provisions..... 46

Table 5-5: Provisions for Amenity Areas 55

Appendices

Appendix A Site Plan

Appendix B Landscape Plan

Appendix C Building Elevations

1 Introduction

WSP was retained by CSV Architects on behalf of the Shepherds of Good Hope (“SGH”), to prepare a Planning Rationale (the “Report”) in support of a Site Plan Control application for the lands municipally known as 1083-1095 Merivale Road in the City of Ottawa. The requested Site Plan Control application (the “Application”) would permit the development of a six-storey residential care facility by the Shepherds of Good Hope.

The proposed development consists of a new six-storey residential care facility building at 1083 Merivale Road, with a ground floor area of 732 m² (7,879 ft²), containing the following uses:

- Residential dwelling units (70 units) on the second to sixth storeys;
- Programs / multi-purpose rooms for residents, facility management space, and garbage and recycling room on the ground level; and
- Facility and storage space for use by SGH.

There is an existing three-storey building on site at 1095 Merivale Road, which is owned and operated by SGH, and will be maintained. The building is currently used as a residential care facility with 57 units, kitchen, and shared dining room for residents. The existing building recently underwent interior renovations which did not require Site Plan approval. These renovations were completed in Spring 2023 and the building is now in full operation.

A total of 70 dwelling units, including 36 barrier-free units, are proposed for the new building at 1083 Merivale Road. Facility management space to be utilized by SGH will be located in the basement and ground floor with a gross floor area of approximately 4,244 m² (45,682 ft²).

This Report is set up as follows:

- **Section 2** provides a description of the site location and community context;
- **Section 3** provides an explanation of the proposed development;
- **Section 4** outlines our consultation strategy;
- **Section 5** outlines the policy and regulatory framework applicable to the site, and provides a planning rationale and scoped urban design brief for the proposed development;
- **Section 6** summarizes the planning opinion;
- **Appendix A** contains the Site Plan;
- **Appendix B** contains the Landscape Plan; and
- **Appendix C** contains the Building Elevations.

A number of technical studies have been prepared in support of the application and submitted to the City; the supporting studies are available under separate cover.

2 Site Location and Community Context

2.1 Site Location

The subject property (the “Site”) is municipally known as 1083-1095 Merivale Road in the City of Ottawa. It is legally described as ‘Part of Lots 15 and 93, Registered Plan 294 and Part of Lot “L” Concession ‘A’ (Rideau Front), City of Ottawa’ as per the Topographic Survey, prepared by Fairhall, Moffatt & Woodland Limited, dated August 9, 2021.

The site is located in Ottawa’s Carlington neighbourhood and is part of Ward 16 (River). It is located on the east side of Merivale Road, between Emperor Avenue and Dorchester Avenue. The registered owner of the site is the Shepherds of Good Hope (SGH). The site is irregular in shape and has a frontage of approximately 55.83 m (183.17 ft) along Merivale Road and an area of approximately 0.52 hectares (1.28 acres). **Figure 2-1** illustrates the site’s location and approximate boundaries.

Figure 2-1: Site Location (GeoOttawa, 2023)



The site is currently occupied by a one-storey detached dwelling at 1083 Merivale Road, which is currently being used by SGH and site contractors for facility storage and informal office space respectively. As part of the redevelopment of 1083 Merivale Road, the existing dwelling will be demolished. There is also an existing three-storey residential care facility building at 1095 Merivale Road, which has a gross floor area of approximately 2,379 m² (25,607 ft²). The existing residential care facility at 1095 Merivale recently completed interior renovations in Spring 2023. A rendering of the final building at 1095 Merivale Road is shown in **Figure 2-2**.

Figure 2-2: Rendering of 1095 Merivale Road (CSV Architects, 2022)



The site is accessed from Panet Street / Emperor Avenue and Merivale Road. There is a surface parking lot located at the rear of the building at 1095 Merivale Road, which is currently utilized by staff and visitors of 1095 Merivale Road. There is also an informal surface parking area south of the existing detached dwelling at 1083 Merivale Road.

A site visit was undertaken by WSP on October 20, 2022. All photos in this Planning Rationale were taken by WSP unless otherwise noted.

2.2 Community Context

The site is located within the Carlington neighbourhood, which is primarily comprised of low to mid-rise residential, commercial, and institutional uses. Specifically along Merivale Road, there are a range of commercial uses including restaurants, personal services, convenience stores, and a gas station. It is also located north of the Central Experimental Farm, which is owned by Agriculture and Agri-Food Canada and is also a National Historic Site of Canada. Alexander Park is located north of the site, and includes the Alexander Community Centre, sports fields, outdoor rink, and wading pool.

The site is located approximately 60 metres north of the Oaks Residence at 1057 Merivale Road, a four-storey residential building also owned and operated by SGH, containing supportive housing, as well as program space for residents.

Nearby amenities and services are shown in **Figure 2-3**. The surrounding community context to the site is shown in **Figure 2-4** (Site Photos – Pinpoint Map), and **Figures 2-5 through 2-12**.

Land uses adjacent to the site are as follows:

- **North:** A four-storey apartment building at 1077 Merivale Road. North of 1095 Merivale Road is a four-storey apartment building at 1030 Panet Street. There is a surface parking lot for the two apartment dwellings located at the rear of the buildings. Further north there is a three-storey commercial plaza at 1061 Merivale Road containing a pawn shop, driving school, personal care salon, and a retail and repair store.
- **South:** A four-storey apartment building at 1900 Merivale Road and a mix of one-, 1.5-, and two-storey detached dwellings along Dorchester Avenue.
- **East:** A two-storey commercial building at 1076-1078 Merivale Road and surface parking lot in the front yard. There is also a 3.5-storey apartment building at 1030 Merivale Road, southeast of Merivale Road and Kirkwood Avenue.
- **West:** A mix of two- and 2.5-storey semi-detached and townhouse dwellings along the north side of Emperor Avenue. On the south side of Emperor Avenue are 2.5-storey apartment buildings, with a gradual eastward transition to 2.5-storey townhouse dwellings.

Figure 2-3: Surrounding Community Context (WSP, 2023)



Figure 2-4: Site Photos - Existing Site Conditions and Context - Pinpoint Map (WSP, 2023)

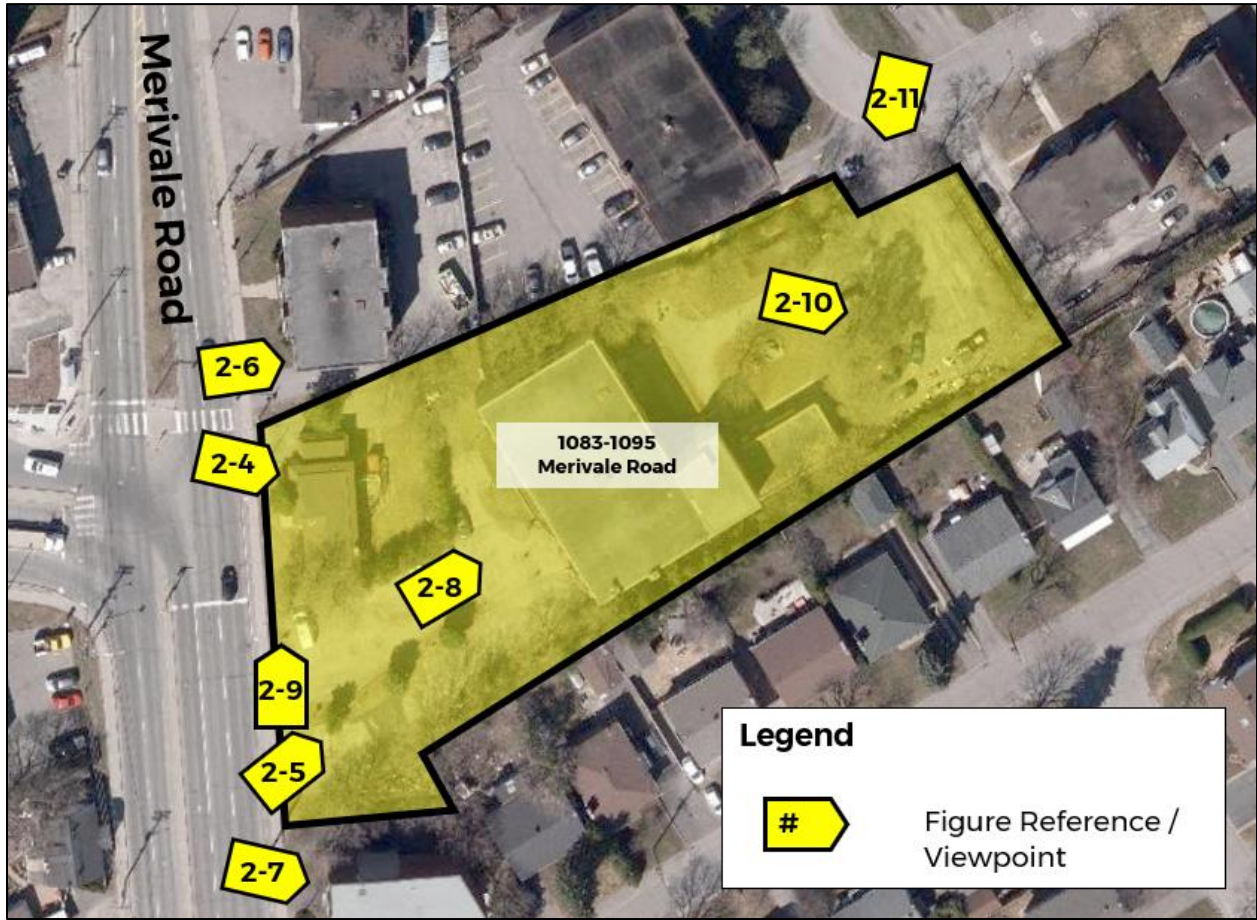


Figure 2-5: Site, looking east - Existing single-detached dwelling utilized as an informal office.



Figure 2-6: Site, looking northeast along Merivale Road.



Figure 2-7: Four-storey apartment building (1077 Merivale, north of the site.



Figure 2-8: Four-storey apartment building (1900 Merivale), south of the site.



Figure 2-9: Existing three-storey SGH building at 1095 Merivale Road, looking east.



Figure 2-10: Existing access from Merivale Road to 1083 Merivale Road, looking north.



Figure 2-11: Existing parking area at the rear of the site behind 1095 Merivale, looking east.



Figure 2-12: Existing access to 1095 Merivale from Panet Street / Emperor Drive, looking south.



2.2.1 Surrounding Development Activity

Figure 2-13 identifies active and recently completed development applications in the vicinity of the site, and **Table 2-1** provides a summary of these development applications.

Figure 2-13: Development Activity in Vicinity of the Site



Table 2-1 Development Activity in Vicinity of the Site

No.	Address	Land Use	Max. Height (Storeys)	No. of Units or GFA	Development Application Status
1	1435 Morisset Avenue	Residential	4	31 units	ZBLA* approved in June 2022
2	1305 Summerville Avenue	Residential	3	18 units	Under construction; SPA** approved in 2020

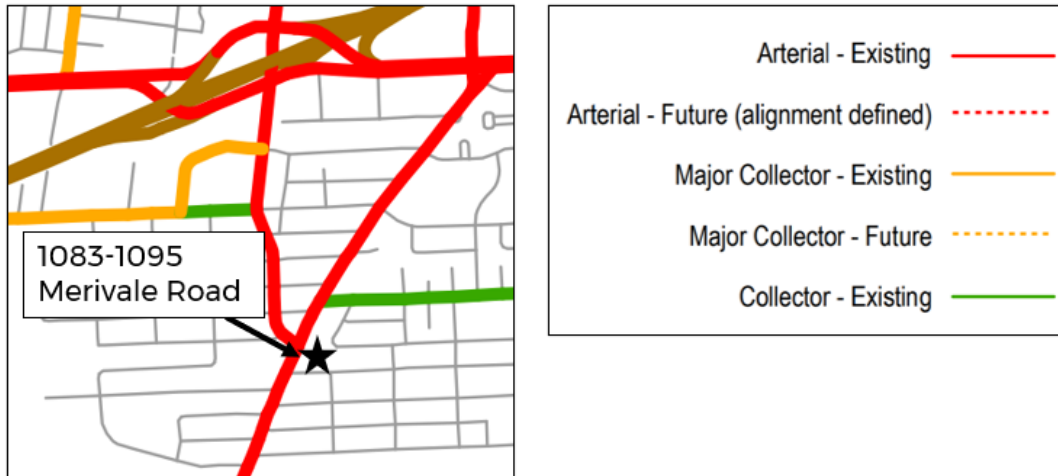
*Zoning By-law Amendment Application (ZBLA)

**Site Plan Control Application (SPA)

2.3 Transportation Network

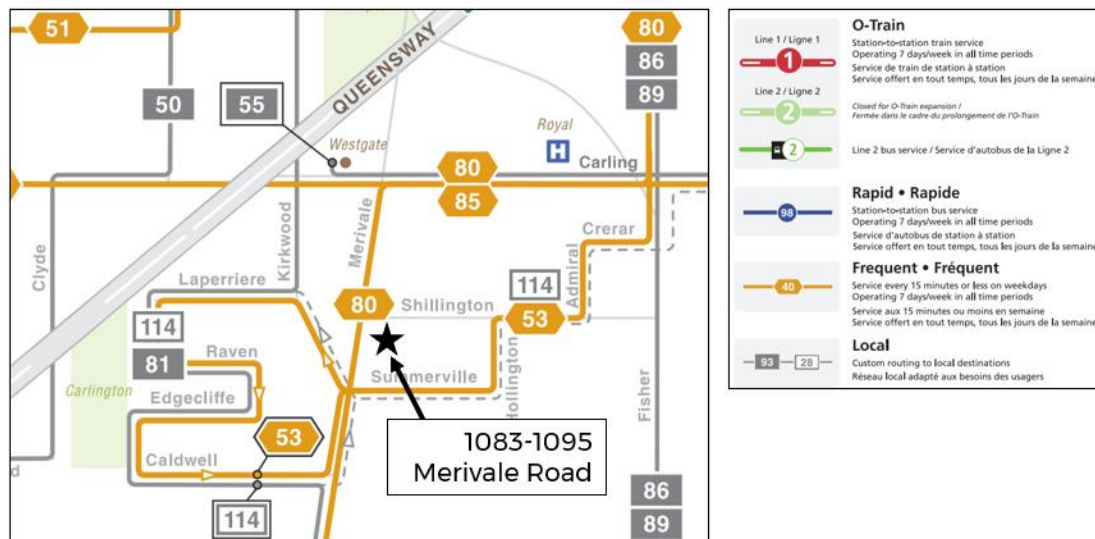
The road network surrounding the site as per Schedule C5 – Downtown Core Road Network to the City of Ottawa New Official Plan (Adopted November 24, 2021 and approved by MMAH on November 4, 2022 with modifications) is illustrated in **Figure 2-14**. Merivale Road is identified as an Existing Arterial Road and provides vehicular and transit access to the greater Ottawa area. Panet Street / Emperor Avenue are local roads.

Figure 2-14: Schedule C5 - Downtown Core Road Network (Excerpt), (City of Ottawa New Official Plan (Adopted November 24, 2021 and approved by MMAH on November 4, 2022 with modifications))



The site is also serviced by existing public transit, as shown in **Figure 2-15**. Merivale Road is serviced by Route 80: Barrhaven Centre, which runs between Barrhaven Centre and Tunney's Pasture LRT station, which is along the Confederation O-Train Line 1 (running east-west).

Figure 2-15: OC Transpo Network Map (December 25, 2022)



3 The Proposed Development

The Shepherds of Good Hope (SGH) is proposing to develop a six-storey (20 m in height), 70-unit, residential care facility containing supportive housing on the site. The proposed building has a gross floor area (GFA) of approximately 4,244 m² (45,682 ft²) and a proposed building envelope of 732 m² (7,879 ft²).

The proposed building would consist of the following:

- Residential dwelling units (70 units) on the second to sixth storeys;
- Programs / multi-purpose rooms for residents, facility management space, and garbage and recycling room on the ground level; and
- Facility and storage space for use by SGH.

Staff will have a 24-hour presence at the new building at 1083 Merivale Road for supervision and support. A minimum of ten (10) new jobs will be created with the development of the new residential care facility at the site.

Mid-Rise Residential Care Facility

The proposed mid-rise residential care facility would provide 70 units of supportive housing. The building will feature a high-performance building envelope intended to reduce energy and carbon consumption. The façade is designed to complement the existing neighbourhood and the existing SGH facility on the site. The main public entrance is located off Merivale Road. A secondary entrance faces the courtyard at the rear of the building serving both buildings on the site.

Residential units will have independent kitchens and bathrooms to support autonomous living. The existing building at 1095 Merivale Road contains a kitchen and shared dining area that will be available for use by residents of 1083 Merivale Road.

Amenity Area

The proposed development includes approximately 600 m² of interior amenity space consisting of a ground floor multi-purpose room, and approximately 500 m² of exterior amenity space for residents. An outdoor courtyard is proposed central to the site between the buildings at 1083 and 1095 Merivale Road. The outdoor courtyard will include outdoor seating, a gazebo, and raised planters. Additionally, a community garden is proposed at the rear of the site, adjacent to the existing shed at the northeast corner of the site. It is intended that these outdoor spaces will be used by residents, visitors, staff, and volunteers.

Parking, Vehicular Access, Circulation, and Loading

The existing vehicular access to the site from Panet Street / Emperor Avenue will be maintained at the rear of the site. It is proposed that the two-way entrance will lead into the surface parking lot at the rear of the building at 1095 Merivale Road. There are 18 regular parking spaces and two (2) barrier-free parking spaces proposed. Of the regular parking spaces, six (6) of these are proposed to be “Electric Vehicle (EV) Ready”, i.e. equipped with electric vehicle charging stations. Additionally, the rear vehicular parking access will also provide fire route access to the building at 1095 Merivale Road and a loading area for supply trucks and Para Transpo.

The existing fire route serving 1095 Merivale Road will serve as a shared access route for 1083 Merivale Road as well. The prior access for 1083 Merivale Road will be removed. This secondary access will also lead to a drop-off area between the two buildings. A loading area is also proposed at the south side of the new building at 1083 Merivale Road and will be accessed from the vehicular access from Merivale Road.

Bicycle parking racks are proposed in five (5) locations on the site and will be located by building entrances, including main building entrances. Eighteen (18) bicycle parking spaces are proposed across the site.

The proposed Site Plan, prepared by CSV Architects (June 2, 2023) is shown in **Figure 3-1** and is also available in a larger format in **Appendix A**. Elevations of the proposed development are shown in **Figures 3-2 to 3-5** and are also included in **Appendix C**. Preliminary renderings of the proposed development are shown in **Figures 3-6 and 3-7**.

Municipal Services

As outlined in the Servicing and Stormwater Management Report, prepared by McIntosh Perry (May 24, 2023), there are existing sanitary and storm sewers and watermain stubs that are available on site.

Figure 3-1: 1083-1095 Merivale Road – Site Plan (Prepared by CSV, dated June 2, 2023)

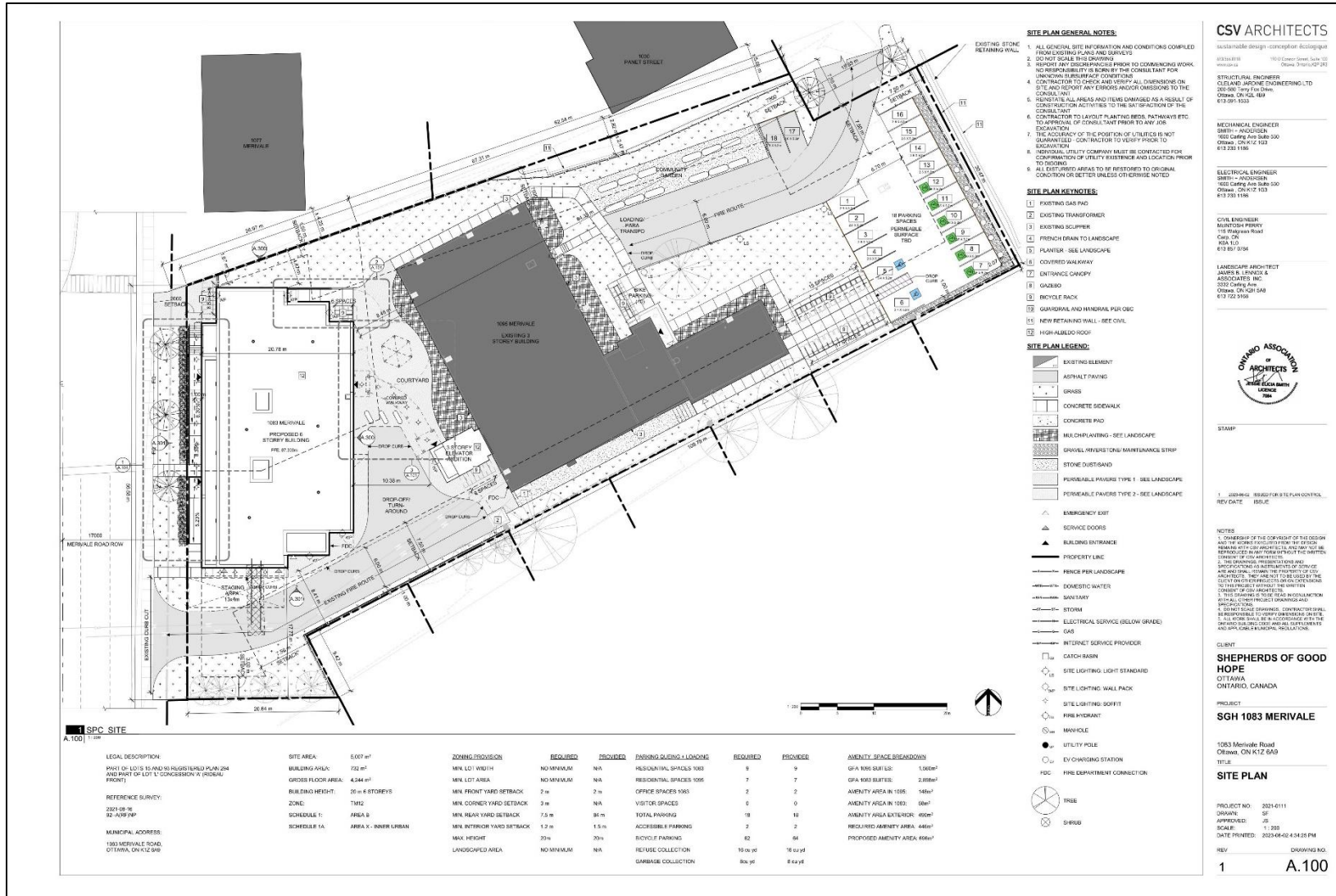


Figure 3-2: Proposed Building Elevations (East) - 1083-1095 Merivale Road (Prepared by CSV Architects, dated June 2, 2023)



Figure 3-3: Proposed Building Elevations (North) - 1083-1095 Merivale Road (Prepared by CSV Architects, dated June 2, 2023)



Figure 3-4: Proposed Building Elevations (West) – 1083-1095 Merivale Road (Prepared by CSV Architects, dated June 2, 2023)



1 SPC WEST ELEVATION
A.301 | 1:100

Figure 3-5: Proposed Building Elevations (South) - 1083-1095 Merivale Road (Prepared by CSV Architects, dated June 2, 2023)



Figure 3-6: Preliminary Rendering - 1083-1095 Merivale Road - Looking Southeast on Merivale Road (Prepared by CSV Architects, June 2, 2023)



Figure 3-7: Preliminary Rendering - 1083-1095 Merivale Road - Looking Southeast on Merivale Road (Prepared by CSV Architects, June 2, 2023)



4 Community Engagement and Outreach (Public Consultation Strategy)

The public engagement for the Site Plan Control application will follow the City of Ottawa public consultation process and practices pursuant to the City's [website](#).

The Shepherds of Good Hope (SGH) has undertaken community engagement in support of the proposed development. To date, SGH has met with local Councillor Riley Brockington on several occasions, including to discuss the retrofit of the existing building at 1095 Merivale Road and a site tour of 1095 Merivale Road was held with Councillor Brockington on April 17, 2023. Additionally, SGH staff have attended meetings with the Carlington Community Association to provide regular updates on the proposed development at 1083-1095 Merivale Road.

5 Policy and Regulatory Framework

This section describes the provincial and local policy framework that is relevant or applicable to the proposed development of the site including: the Provincial Policy Statement, 2020, City of Ottawa Official Plan (Adopted November 24, 2021 and approved by MMAH on November 4, 2022 with modifications), and the City of Ottawa Zoning By-law 2008-250 (September 8, 2021 Consolidation).

5.1 Provincial Policy Statement, 2020

The Provincial Policy Statement, 2020 (PPS) was issued by the Province of Ontario under Section 3 of the Planning Act in May 2020, replacing the previous 2014 PPS. The PPS provides policy direction on matters of provincial interest including quality of the natural and built environment and public health and safety. All land use planning decisions shall be consistent with the policies of the PPS.

Part IV: Vision for Ontario's Land Use Planning System identifies that land use must be carefully managed to accommodate appropriate development to meet the full range of current and future needs. Planning authorities are encouraged to permit and facilitate a range of housing options, including new development as well as residential intensification, while promoting efficient development patterns that promote a mix of housing, including affordable housing. Growth should also be focused within settlement areas and away from significant or sensitive resources and areas which may pose a risk to public health and safety.

Section 1.1 Managing and Directing Land Use to Achieve Efficient and Resilient Development and Land Use Patterns includes policies to sustain healthy, liveable, and safe communities. Policy 1.1.1 (b) states that healthy, liveable and safe communities are sustained by

accommodating an appropriate affordable and market-based range and mix of residential types, including multi-unit housing and affordable housing.

Policy 1.3.1 directs that planning authorities promote economic development by providing an appropriate range and mix of jobs to meet long-term needs.

Policy 1.1.3.2 (a) states that land use patterns within settlement areas shall be based on densities and a mix of land uses which:

“efficiently use land and resources;
are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;
minimize negative impacts to air quality and climate change;
support active transportation; and
are transit supportive, where transit is planned, exists or may be developed.”

Policy 1.1.3.4 states that appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety.

Section 1.4 Housing includes policies on the provision of an appropriate range and mix of housing options and densities. Specifically, Policy 1.4.3 directs planning authorities to provide for an appropriate and mix of housing types and densities to meet projected requirements of current and future residents of the regional market area by:

b) permitting and facilitating:

1. all forms of housing required to meet the social, health and well-being requirements of current and future residents, including special needs requirements and needs arising from demographic changes and employment opportunities; and
2. all forms of residential intensification, including second units, and redevelopment in accordance with policy 1.1.3.3;”

c) directing the development of new housing towards locations where appropriate levels of infrastructure and public service; and promoting densities for new housing which efficiently use land, resources, infrastructure and public facilities...”

Section 6.0 Definitions defines “housing options” as follows:

“a range of housing types such as, but not limited to single-detached, semi-detached, rowhouses, townhouses, stacked townhouses, multiplexes, additional residential units, tiny homes, multi-residential buildings. The term can also refer to a variety of housing arrangements and forms such as, but not limited to life leasing housing, co-ownership housing, co-operative housing, community land trusts, land lease community homes, affordable housing, **housing for people with special needs**, and housing related to employment, institutional or educational uses.”

Section 6.0 defines “Special needs” as follows:

“any housing, including dedicated facilities, in whole or in part, that is used by people who have specific needs beyond economic needs, including but not limited to, **needs such as mobility requirements or support functions required for daily living** [...].

The proposed development is consistent with the PPS as it represents development within the settlement area, which efficiently uses existing infrastructure and facilities. The proposed development is transit-supportive as it is a compact, mixed-use development in proximity to transit with no more parking being proposed than what is required. Barrier-free parking spaces and electric vehicle spaces will also be provided on-site. The mid-rise residential care facility use, in the form of supportive housing, contributes to the range of local housing options and provides housing for seniors and for people with special needs by offering social, health and well-being services on-site.

5.2 City of Ottawa Official Plan (Adopted November 24, 2021 and Approved by MMAH on November 4, 2022 with Modifications)

The City of Ottawa Official Plan (the “OP”) was approved by Ottawa City Council on October 27, 2021 and adopted on November 24, 2021. It subsequently was approved by the Ontario Ministry of Municipal Affairs and Housing (MMAH) with modifications on November 4, 2022. The OP sets the vision for how the city will grow and develop to 2046. The OP introduces significant policy changes, moving from traditional land use planning with a focus on permitted land uses to context and transect-based planning with an emphasis on form and function.

In the OP, the site is located within the Inner Urban Transect as per Schedule A – Transect Areas (**Figure 5-2**). It is designated as Corridor – Mainstreet and is subject to the Evolving Neighbourhood Overlay as per Schedule B2 – Inner Urban Transect (**Figure 5-1**).

Figure 5-1: Schedule B2 – Inner Urban Transect (Excerpt) (City of Ottawa New Official Plan (Adopted November 24, 2021 and approved by MMAH on November 4, 2022 with modifications))



As per Section 11.1(1), the entire City of Ottawa is designated as a Site Plan Control Area to be implemented by the Site Plan Control By-law. Site Plan Control may be applied to all commercial, institutional, industrial and multiple residential developments in the city. The site is therefore subject to Site Plan Control.

Strategic Directions

Section 2 of the OP, Strategic Directions, outlines the broad policies that will govern the growth and development of Ottawa over the next 25 years.

Section 2.1 – The Big Policy Moves outlines five broad policy directions to shape Ottawa as a liveable, mid-sized city.

1. Achieve, by the end of the planning period, more growth by intensification than by greenfield development.
2. By 2046, the majority of trips in the city will be made by sustainable transportation.
3. Improve our sophistication in urban and community design and put this knowledge to the service of good urbanism at all scales, from the largest to the very small.
4. Embed environmental, climate and health resiliency and energy into the framework of our planning policies.
5. Embed economic development into the framework of our planning policies.

Section 2.2 – Cross Cutting Issues identifies six (6) policy direction categories that span a range of topics related to achieving the City of Ottawa’s goal of becoming a livable City. The relevant cross-cutting issues are described as follows.

Section 2.2.1 – Intensifying and Diversifying Housing Options directs residential growth within the built-up urban area towards 15-minute neighbourhoods. This direction supports the creation of 15-minute neighbourhoods by locating daily and weekly needs in proximity to Hubs, Corridors and surrounding Neighbourhoods. Promoting intensification is a key part of achieving this goal. The OP’s definition of intensification includes developing a property at a higher density than currently exists through the expansion or conversion of existing buildings. To support the City’s intensification goal of 60 per cent by 2046, the City will direct residential intensification to Hubs, Corridors and residential neighbourhoods within a short walking distance of those Hubs and Corridors.

Section 2.2.3 – Energy and Climate Change promotes the development of a compact urban form with a mix of land uses and housing options to ensure both energy efficient and sustainable patterns of development are created. The City also supports sustainable site and building design as part of development. Sustainable and resilient design measures, such as supporting the adaptive reuse and retrofit of existing buildings, are to be applied.

Section 2.2.4 – Healthy and Inclusive Communities is intended to support healthy and inclusive communities through the development of walkable 15-minute neighbourhoods that feature a range of housing options, supporting services and amenities. 15-minute

neighbourhoods support cultural expression and community identity as well as ensuring access to goods and services within peoples' communities using active transportation modes.

The proposed development supports the Strategic Directions of the OP by providing residential intensification within the urban area. The proposed development contributes to the development of healthy, inclusive 15-minute neighbourhoods by adding housing options, including affordable, supportive housing options, to the neighbourhood that are within walking distance of amenities such as community facilities, convenience stores, services, and transit.

These broad policy directions inform and generally capture the intent of the OP policies, which are described in further detail in the following sections.

Growth Management Framework

Section 3 – Growth Management Framework of the OP contains the City's strategy for managing different types of growth in Ottawa, which ensures that there are sufficient development opportunities and a range of choices of where to locate growth and how to design it. The Growth Management Framework supports growth that increases sustainable mode shares and uses existing infrastructure efficiently while reducing greenhouse gas emissions.

Growth is to be concentrated within the urban area with a majority of residential growth to occur in the built-up area through intensification. The intent of the Growth Management Framework policies is:

- a) "To provide an appropriate range and mix of housing that considers the geographic distribution of new dwelling types and/or sizes to 2046;
- b) To provide a transportation network that prioritizes sustainable modes over private vehicles, based on the opportunities for mode shifts presented by each transect area context;
- c) To prioritize the location of residential growth to areas with existing municipal infrastructure, including piped services, rapid transit, neighbourhood facilities and a diversity of commercial services;
- d) To reduce greenhouse gas emissions in the development and building sectors and in the transportation network; and
- e) To establish a growth management framework that maintains a greater amount of population and employment inside the Greenbelt than outside the Greenbelt".

Table 3a in Section 3.2 contains a target residential density range for Mainstreet Corridors. Mainstreet Corridors have a residential density target of 120 dwellings per net hectare. The site, once fully redeveloped, would have a residential density of approximately 244 dwelling units per net hectare (127 units / 0.52 ha), which accounts for the two (2) buildings at 1083-1095 Merivale Road, once fully built out. **The proposed residential density exceeds the target residential density and therefore conforms with the density targets of the OP.**

Table 3b also contains minimum requirements for large-household dwellings in the Neighbourhood designation, however, the intention of the proposed development is to serve as supportive housing space for individuals and not large households.

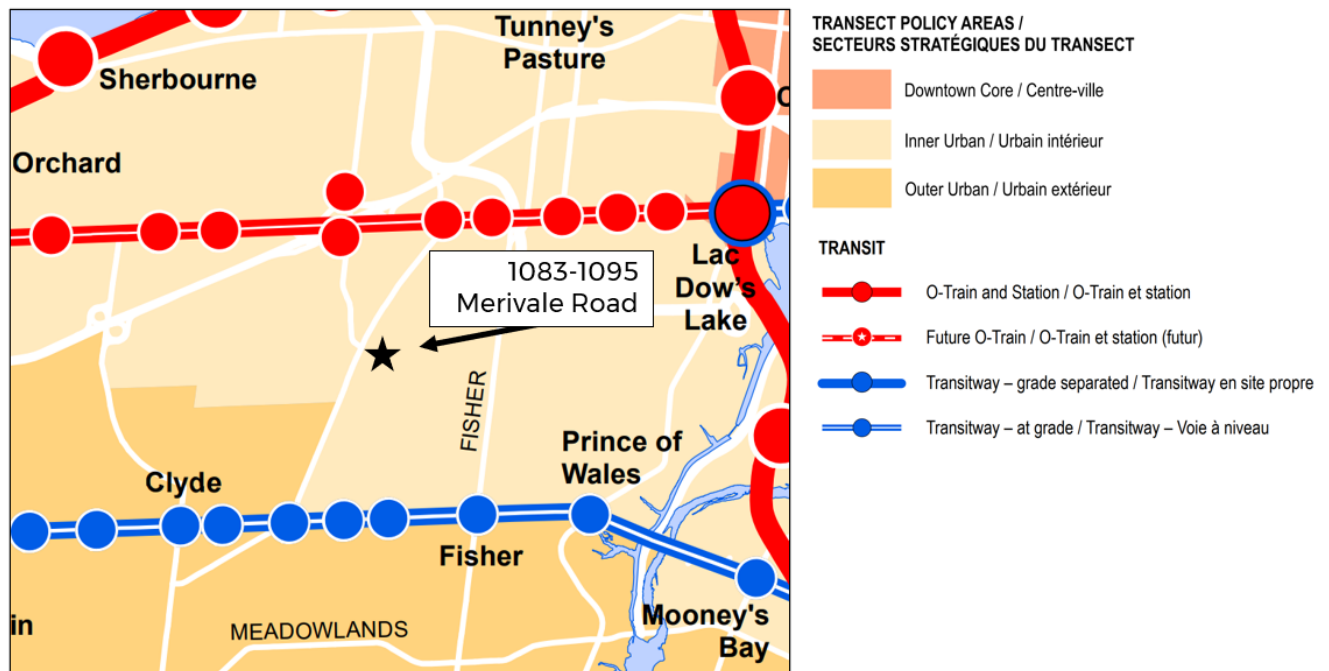
The proposed development conforms with the Growth Management Framework policies by serving as residential intensification within the urban area where municipal services already exist. The target residential density target for Mainstreet Corridors will be met.

Transect Policy Area

The site is located within the urban boundary and is part of the Inner Urban Transect Policy Area, as illustrated in **Figure 5-2**, which provides an excerpt from Schedule A – Transect Policy Areas. The OP states that the Inner Urban Transect includes the pre-World War II neighbourhoods that immediately surround the Downtown Core, and the earliest post-World War II areas directly adjacent to them. Generally, the older neighbourhoods reflect the urban built form characteristics described in the OP, while the post-war neighbourhoods reflect suburban characteristics.

The goals of the Inner Urban Transect designation include establishing an urban pattern of built form, site design and mix of uses. Prioritizing walking, cycling and transit within, and to and from, the Inner Urban Transect.

Figure 5-2: Schedule A - Transect Policy Areas (Excerpt), New City of Ottawa Official Plan (Adopted November 24, 2021)



Section 4.4.1(3) states that for Site Plan Control applications in the Inner Urban Transect where the development site exceeds 4,000 m², the City shall place a priority on acquisition of land for park(s) as per the Planning Act and the Parkland Dedication By-law.

As confirmed by the minutes from the pre-application consultation meeting held on August 17, 2022, the addition to the existing building at 1095 Merivale Road is exempt from parkland dedication under subsection 11(1) of Parkland Dedication By-law No. 2022-280 as it does not result in an increase in density providing a net dwelling unit gain. The proposed building at 1083 Merivale Road may be exempted from parkland dedication as SGH is considered to be a non-profit housing organization.

Section 5.2 of the OP contains policies related to the Inner Urban Transect. Section 5.2.1.2 states that the development of large parcels and superblocks into fully urban districts shall be supporting, including the intensification or redevelopment of old shopping centres and infill on underutilized lands. Limits on building height and massing will be established through secondary plans, area-specific policy, and the urban design policies in Subsection 4.6 of the OP (Section 5.2.1.3).

Section 5.2.1 further states that mid- to high-density development is generally planned for this transect where no on-site parking is not required or where parking is provided in a common area or garage accessed by a common driveway.

As per Policy 5.2.3(2)(a), lands designated as Corridor – Mainstreet in the Inner Urban Transect that front on streets with a ROW 30 metres or greater as per Schedule C16, have a minimum building height of two (2) storeys and a maximum building height of 40 storeys (high-rise).

The proposed development conforms to the Inner Urban Transect policies as its built form positively contributes to the existing mature neighbourhood. The provision of bicycle parking and EV ready parking spaces will encourage alternative forms of transportation to and from the site. The proposed building height of six (6) storeys conforms with the minimum and maximum building height requirements, and the proposed institutional use consisting of supportive housing contributes positively to the Mainstreet Corridor permitted uses.

Land Designation

In the OP, the site is designated as Mainstreet Corridor as per Schedule B2 (**Figure 5-1**). The Corridor Mainstreet designation permits a range of residential and non-residential built forms with a higher density than the adjacent Neighbourhoods designation.

Low to high rise building heights are permitted in the Corridor Mainstreet designation. Section 13 of the OP defines low-rise buildings as up to and including 4 storeys, and high-rise buildings 10 to 40 storeys. The proposed building would constitute a “mid-rise building” as defined by the OP.

As per Policy 6.2.1(2)), the OP supports development of buildings that locate the maximum permitted building heights and highest densities close to the Corridor.

Furthermore, Policy 6.2.1(3) states that the Mainstreet Corridor designation supports mixed-uses including residential, office and commercial uses.

Lastly, the Evolving Overlay policies in Section 5.6.1.1 of the OP relate to development standards that may guide gradual change in character, allow for new building forms and provide direction for the evaluation of development. Development standards applicable to lands within the Evolving Overlay would be created through the Zoning By-law. The City of Ottawa is currently in the process of updating its Zoning By-law to create such development standards.

The proposed institutional building conforms to Corridor designation policies that permit residential and non-residential uses that integrate with a dense, mixed-use environment. The proposed six storey residential care facility building will not exceed the maximum permitted building height of 40 storeys and is adjacent to the Corridor Mainstreet (Merivale Road). The proposed development conforms to the policies of the Official Plan.

The proposed development conforms to the policies of the Official Plan policies including Strategic Directions, Growth Management, Transect Areas, Compatibility, and Land Designation policies.

5.2.1 Road Classification and Rights-of-Way

As per Section 4.1.7 of the OP, the City may acquire land for rights-of-way or the widening of rights-of-way through conditions of approval for a plan of subdivision, severance (severed and retained parcels), site plan or a plan of condominium.

Table 1 – Road Right-of-Way Protection of the OP sets forth the right-of-way (ROW) widths that the City may acquire for roads. Table 1 identifies Merivale Road as an Arterial Urban Road, with a ROW protection of 34 m. There are no ROW protection requirements for Emperor Avenue or Panet Street.

The OP has an existing right-of-way protection requirement of 34 m for Merivale Road. The proposed development has accounted for the Right-of-Way (ROW) protection as shown on the Site Plan, prepared by CSV Architects (dated June 2, 2023).

5.3 Scoped Design Brief

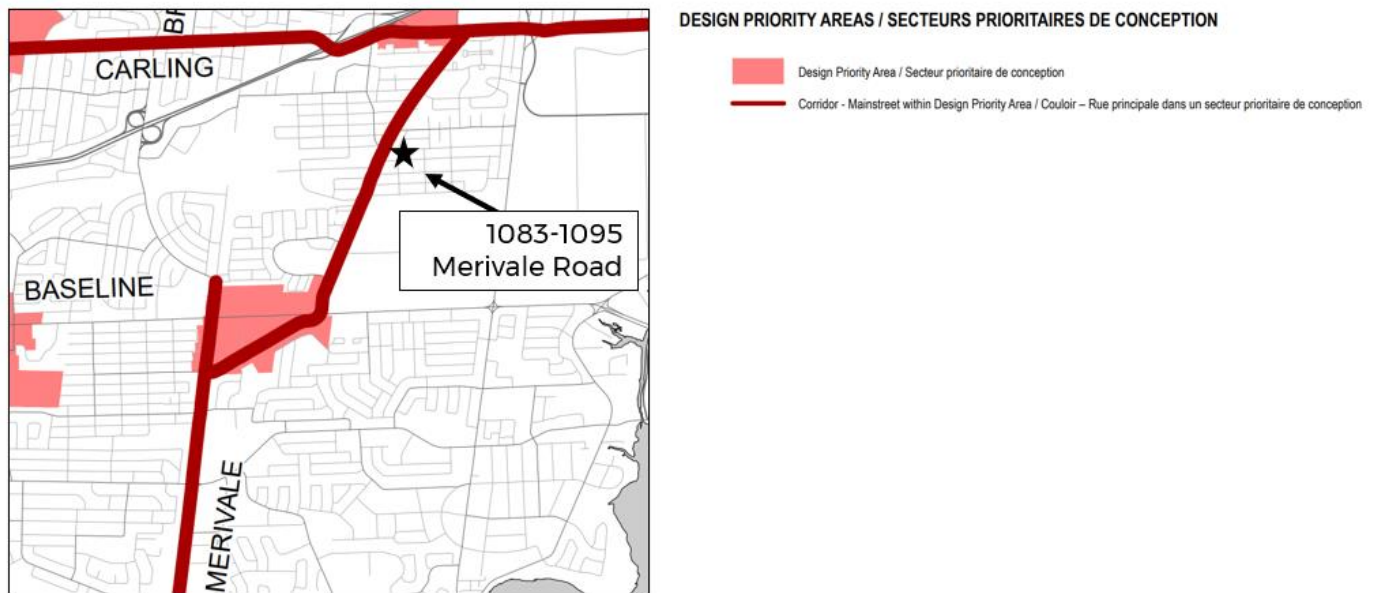
The Planning Act gives municipalities the authority to require an Urban Design Brief to be prepared. Under Section 34(10.2) and Section 41(4) of the Planning Act, Council has the authority to request other information or material that the authority needs to evaluate and decide on an application. Section 11.1 of the OP sets out the information and/or reports that may be required in support of development applications, which includes an Urban Design Brief. As a part of the Site Plan Control application, the City has requested a scoped Urban Design Brief be included. As such, a review of the proposed development against the policies of Section 4.6 of the OP is summarized in this section, as well as an overview of how the proposed development will respond to the City of Ottawa’s Urban Design Guidelines for Development along Traditional Mainstreets, the Merivale Road North Community Design Plan (June 2018), and comments provided by Urban Design staff at the pre-application consultation meeting held on August 17, 2022.

5.3.1 Design Priority Areas

Urban design and compatibility considerations ensure that the design of a new development contributes and enhances an area’s sense of community and identity. Section 4.6 of the OP includes Urban Design policies that are to be applied within all land use designations.

Policy 4.6.1 of the OP states that Design Priority Areas (DPAs) are identified to promote design excellence through the development review process. As shown in **Figure 5-3**, the site is within a Corridor – Mainstreet with Design Priority Area.

Figure 5-3: Schedule C7-A- Design Priority Areas – Urban (Excerpt) (City of Ottawa New Official Plan (Adopted November 24, 2021 and approved by MMAH on November 4, 2022 with modifications))



As per Section 4.6, Table 5 – Design Priority Areas, the site is categorized as a “Tier 3 – Local (Major)” Design Priority Area (DPA). Tier 3 areas include lands that are designated as Mainstreet Corridors that are located outside of the Downtown Core Transect and can be characterized by neighbourhood commercial streets that provide a high-quality pedestrian environment. Private and public development projects located within DPAs are subject to review by the City’s Urban Design Panel (UDRP). **As confirmed at the pre-application consultation meeting on August 17, 2022, a Formal Review with the UDRP is required. Formal Review with the UDRP will be held following the initial submission of the Site Plan Control application.**

5.3.2 Section 4.6 – Urban Design

Policy 4.6.5(2) states that development along Corridors shall respond to context, transect area, and overlay policies. Development should be generally located to frame the street and provide appropriate setbacks within the street context. Main entrances are to be visible from public sidewalks and above grade utilities should be screened from the street.

Policy 4.6.5(3) states that development shall minimize conflict between vehicles and pedestrians and that servicing, loading areas, mechanical equipment, and utilities shall be internal to the building in order to improve the public realm.

Section 4.6.6 of the OP provides policy direction for mid-rise buildings:

- 1) “Mid-rise buildings shall be designed to respond to context, and transect area policies, and should:
 - a. Frame the street block and provide mid-block connections to break up large blocks;
 - b. Include a base with active frontages, and a middle portion that relates to the scale and
 - c. character of the surrounding buildings, or, planned context;
 - d. Be generally proportionate in height to the width of the right of way as illustrated in the
 - e. Figure below, with additional height permitted in the Downtown Core Transect; and
 - f. Provide sufficient setbacks and step backs to:
 - i. Provide landscaping and adequate space for tree planting;
 - ii. Avoid a street canyon effect; and
 - iii. iii) Minimize microclimate impacts on the public realm and private amenity areas”.

5.3.3 Urban Design Guidelines for Development along Traditional Mainstreets (May 2006)

The Urban Design Guidelines for Development along Traditional Mainstreets were approved by Council in May 2006 and apply to all streets designated Traditional Mainstreets in the City's previous Official Plan (2003, Website Consolidation).

A detailed outline of how the proposed design responds to each of the relevant guidelines is provided in **Table 5-1**.

Table 5-1: Urban Design Guidelines for Development along Traditional Mainstreets - Analysis for 1083-1095 Merivale Road

Guideline No.	Guideline	Response
Streetscape		
1	Align streetwall buildings with the existing built form or with the average setback of the adjacent buildings in order to create a visually continuous streetscape.	<ul style="list-style-type: none"> The proposed building at 1083 Merivale Road is placed close to the front lot line along Merivale Road, which will contribute to a continuous streetscape. The building is aligned with the setbacks of the adjacent buildings.
2	Plant clusters of trees on the flanking residential streets, where they meet the mainstreet, for additional greenspace.	<ul style="list-style-type: none"> Additional trees are intended to be planted along Merivale per the landscape plan. The existing cluster of trees at the south west corner of the site will be maintained. Additional trees are intended to be planted at the site entrance off Panet Street.
3	Provide or restore a minimum 2.0 metre wide concrete sidewalk and locate to match approved streetscape design plans for the area. Where there is no approved streetscape plan, match the existing context. Provide a boulevard for street furniture, trees, and utilities; next to the sidewalk where possible. Provide an area adjacent to storefronts for canopies, outdoor patios or special merchant displays (the frontage zone). Create wider	<ul style="list-style-type: none"> The existing 2m sidewalk will be maintained/ restored. Space for street furniture, utilities and planting will be provided. Wider sidewalks will be provided at entrances.

Guideline No.	Guideline	Response
	sidewalks for locations with high pedestrian volumes such as along traditional mainstreets in core urban areas.	
4	Use periodic breaks in the street wall or minor variations in building setback and alignment to add interest to the streetscape and to provide space for activities adjacent to the sidewalk.	<ul style="list-style-type: none"> Variations in setback and materiality provide interest to the streetscape. At main building entrances, space is provided for activities.
6	Create attractive public and semi-public outdoor amenity spaces such as green spaces with trees, pocket parks, courtyards, outdoor cafés, seating and decorative pools or fountains.	<ul style="list-style-type: none"> Planting and space for seating will be provided along Merivale Road. A shared semi-public courtyard is provided between 1083 and 1095 Merivale.
Built Form		
8	Design quality buildings that are rich in architectural detail and respect the rhythm and pattern of the existing or planned buildings on the street, through the alignment of elements such as windows, front doors, cornice lines, and fascias.	<ul style="list-style-type: none"> Setback recommendations from the City of Ottawa have been integrated into the design. The proposed materials compliment existing adjacent buildings and the existing building on site.
9	Ensure sufficient light and privacy for residential and institutional properties to the rear by ensuring that new development is compatible and sensitive with adjacent uses with regard to maximizing light and minimizing overlook.	<ul style="list-style-type: none"> Ample exterior lighting throughout the site is achieved with via lamp standards in parking areas, bollard lighting along walkways and exterior wall lighting along the buildings. Fixtures will be dark-sky friendly to focus the light on the site and avoid overlook
11	Use clear windows and doors, to make the pedestrian level façade of walls facing the street highly transparent, and locate active pedestrian-oriented uses at-grade.	<ul style="list-style-type: none"> Extensive glazing is proposed at level one to maximise transparency at the street level.

Guideline No.	Guideline	Response
12	Set back the upper floors of taller buildings to help achieve a human scale and more light on the sidewalks.	<ul style="list-style-type: none"> A front yard setback is provided at levels 5 and 6.
17	Provide pedestrian weather protection such as colonnades, individual canopies, awnings and balconies	<ul style="list-style-type: none"> Canopies at entrances extend to cover adjacent bike parking where suitable.
Pedestrians and Cyclists		
18	Provide sheltered bicycle parking in visible locations near building entrances and pedestrian walkways. Ensure that these locations minimize conflict with pedestrians.	<ul style="list-style-type: none"> Exterior bike parking is available throughout the site, with some areas protected.
19	Locate front doors to face the mainstreet and be directly accessible from the public sidewalk.	<ul style="list-style-type: none"> The main entrance is off Merivale Road, and is emphasized in the facade design with walkway and canopy
20	Design pedestrian walkways of materials such as concrete or unit pavers that are easily maintained for safety.	<ul style="list-style-type: none"> Accessible pedestrian pathways will be designed with solid-surface, durable materials that are easily maintained for safety and accessibility.
21	Create inviting, well-lit pedestrian walkways to link rear parking areas to the public sidewalk/street	<ul style="list-style-type: none"> Pedestrian pathways will be well lit, with connections throughout the site.
Vehicles and Parking		
22	Share vehicular access to parking areas between adjacent properties in order to reduce the extent of interruption along the sidewalk and the streetscape.	<ul style="list-style-type: none"> Additional parking beyond by-law requirements may be coordinated by the client at their near-by property.
23	Locate surface parking in the rear yard with vehicular access off side streets and laneways.	<ul style="list-style-type: none"> Surface parking is located in the rear yard with access from the side street.

Guideline No.	Guideline	Response
25	Provide a minimum 3.0 metre wide landscape area along the edge of a site where parking areas, drive lanes or stacking lanes are adjacent to a public street. Use trees, shrubs and low walls to screen cars from view while allowing eye level visibility into the site.	<ul style="list-style-type: none"> Existing site conditions will be improved with the addition of opaque fencing where parking is less than 3.0m but greater than 1.2m from a parking area or a drive lane.
27	Provide only the minimum number of required car parking spaces . Consider parking on the mainstreet.	<ul style="list-style-type: none"> Parking has been provided on-site as per the minimum requirements of the Zoning By-law. Parking is located at the rear of the site.
Landscape and Environment		
28	Select trees, shrubs and other vegetation considering their tolerance to urban conditions such as road salt or heat. Give preference to native species of the region that are of equal suitability.	<ul style="list-style-type: none"> The landscape design will give consideration to plantings suitable for the location, including salt and heat tolerance. Native species will be prioritized in the design.
29	Protect and feature heritage, specimen and mature trees on site by minimizing grade changes.	<ul style="list-style-type: none"> The site design identifies specimen trees to remain, and the grading and site plan will take these trees into account.
30	Plant street trees between 6.0 and 8.0 metres apart along public streets and internal pedestrian walkways. Plant trees in permeable surfaces with approximately 10.0 square metres of soil area per tree.	<ul style="list-style-type: none"> Trees will be planted where safety concerns and ample growing conditions can be met.
31	Provide a minimum 3.0 metre wide landscape area, which may include a solid wall or fence in addition to planting, at the edges of sites adjacent to residential or institutional properties.	<ul style="list-style-type: none"> Existing site conditions will be improved with the addition of opaque fencing where parking is within the 3.0m setback from parking area or drive lane.

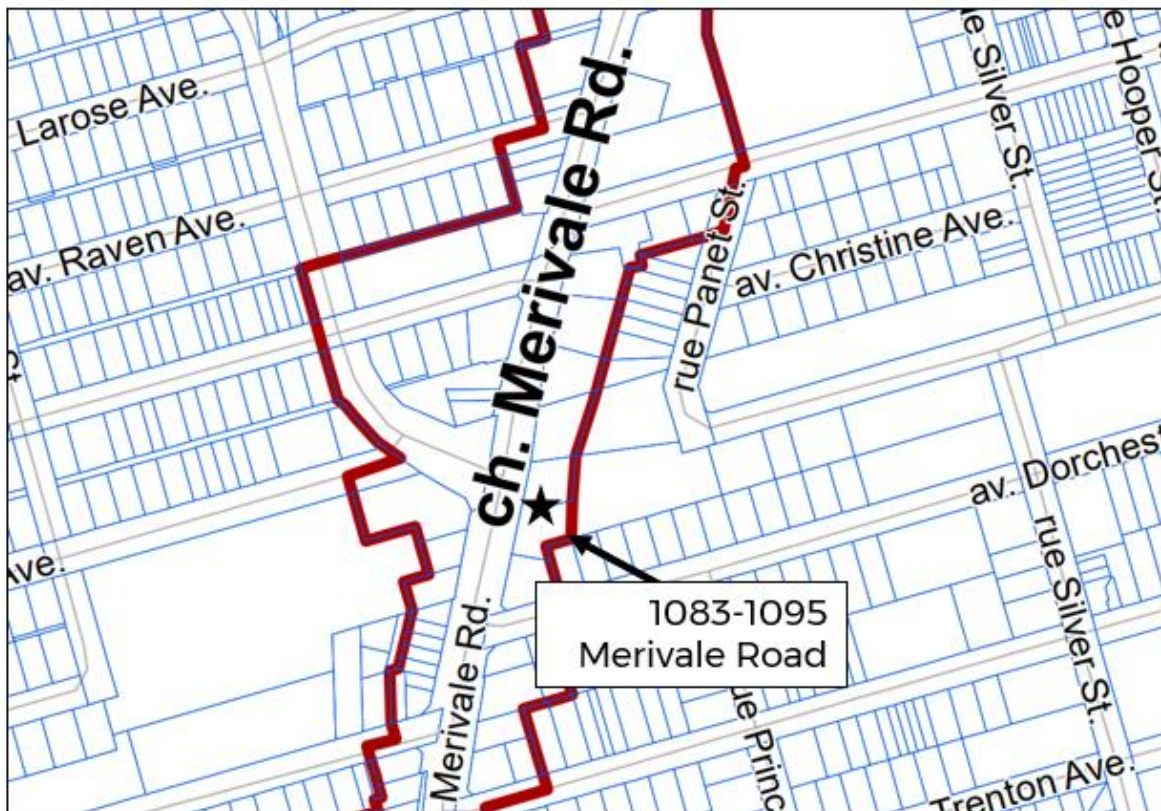
Guideline No.	Guideline	Response
32	Use green building technologies such as green roofs, drip irrigation, and other Leadership in Energy and Environmental Design (LEED) approaches.	<ul style="list-style-type: none"> Sustainable building practices will target Passive-House standards. Permeable pavers are intended to be used throughout the site where suitable.
Servicing and Utilities		
37	Share service and utility areas between different users within a single building or among different buildings.	<ul style="list-style-type: none"> Service and utility areas will be shared between 1085 and 1083 Merivale, including: garbage collection, road cuts, and service connections below the street.
38	Enclose all utility equipment within buildings or screen them from both the traditional mainstreet and private properties to the rear. These include utility boxes, garbage and recycling container storage, loading docks, ramps, air conditioner compressors, utility meters and transformers.	<ul style="list-style-type: none"> Utility equipment (transformers, gas pads) are located away from the mainstreet until collection time. Garbage containers will be stored inside the building. Mechanical equipment will be located below grade in a designated mechanical room or on the rooftop.
39	Plant street trees between 6.0 and 8.0 metres apart along public streets and internal pedestrian walkways. Plant trees in permeable surfaces with approximately 10.0 square metres of soil area per tree.	<ul style="list-style-type: none"> Trees will be planted where safety concerns and ample growing conditions can be provided.
40	Design lighting so that there is no glare or light spilling onto surrounding uses.	<ul style="list-style-type: none"> Fixtures will be dark-sky friendly and located to avoid glare and prevent light spill on to surrounding areas.
41	Provide lighting that is appropriate to the street character and mainstreet ground-floor use, with a focus on pedestrian areas.	<ul style="list-style-type: none"> Ample exterior lighting throughout the site is achieved with via lamp standards in parking areas, bollard lighting along walkways and exterior wall lighting along the buildings. Lighting will be with resident and pedestrian safety a priority.

5.3.4 Merivale Road North Community Design Plan (June 2018)

The Merivale Road North Community Design Plan (“CDP”) was approved by Council in July 2018 and is a guiding physical and economic growth strategy for shaping development along Merivale Road (North), with a 20-year planning horizon to 2038. As shown in **Figure 5-4**, the portion of the site abutting Merivale Road is located within the CDP Area.

Section 1.2 of the CDP states that a number of municipal documents, including the previous City of Ottawa Official Plan (2003, Website Consolidation) and the Urban Design Guidelines for Development along Traditional Mainstreets, informed the preparation of the CDP. Where there is discrepancy between the referenced documents and the CDP, the policies in the CDP shall take precedence. **As such, it is assumed that the CDP policy direction has precedence over the current OP.**

Figure 5-4: Figure 1 - CDP Area (Extract) (Merivale Road North Community Design Plan (June 2018))



 CDP Boundary / Limites du PCC

Section 1.4 CDP Goals and Objectives

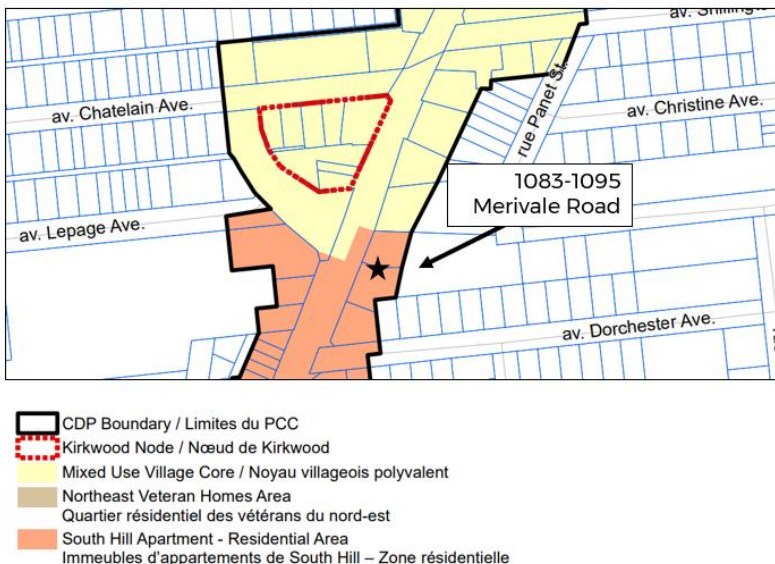
Section 1.4 sets out the objectives of the CDP as follows:

- Provide a design vision for the future development of Merivale Road into a complete street, supported by an assessment of transportation conditions for pedestrian and cycling facilities, public transit and motor vehicles;
- Establish an economic development strategy to assist in the prosperity of the CDP area and broader community over time;
- Provide an urban design strategy including a framework of urban form components to guide future development and redevelopment along the street;
- Provide a vision for gateway and public art opportunities that will enhance citywide recognition of the area;
- Support prosperity by providing opportunities for lot consolidation and redevelopment in appropriate locations along the corridor, and
- Assess servicing infrastructure capacities in relation to the ability to support forecasted growth along the corridor.

Section 3.1.1 Character Areas

Section 3.1.1 of the CDP introduces three (3) character areas for the lands within the CDP Area. Figure 11 – Character Areas in the CDP identifies a portion of the site as being located within the South Hill Apartment – Residential Area, as well as being southeast of the Kirkwood Node, an area identified as being potential to be the focal point of redevelopment in the CDP Area (**Figure 5-5**). There is no specific policy direction contained in the CDP for the South Hill Area, however it is noted that the site is located in a designation (Mainstreet Corridor) where redevelopment is intended to occur.

Figure 5-5: Figure 11 - Character Areas (Excerpt) (Merivale Road North CDP (June 2018))



Section 4.0 CDP Land Use Strategy

Section 4.0 of the CDP sets out a land use strategy for the Merivale Road North CDP Area. The CDP acknowledges that the Traditional Mainstreet policies in the previous OP support a range of commercial and medium density residential uses, with maximum building heights of six (6) storeys. The TM Zone in the City's Zoning By-law implements the Traditional Mainstreet policies and will support this area's evolution to a pedestrian-oriented mainstreet, with greater densities of development.

Section 4.1 of the CDP includes direction for infill development and lot consolidation in the CDP Area. The CDP states that the opportunity for growth and change to the CDP Area will be primarily through redevelopment, reuse/renovation, expansion of existing buildings, and lot consolidation. Larger properties (up to 0.25 ha) may be permissive to substantial redevelopment opportunities.

Section 4.1 notes that a number of existing lots on Merivale Road are at an angle to the road ROW. The CDP sets out the following direction for redevelopment lots on Merivale Road:

“In the future when these lots are redeveloped or existing buildings are substantially rebuilt, the front wall of the building should be built parallel to the Merivale Road right-of-way to create a more desirable street-oriented development frontage.

Whenever possible, where two or more lots are assembled to create a larger development parcel, existing driveway access points to Merivale Road should be consolidated to reduce vehicle friction points and increase opportunities for on-street parking”.

The proposed building at 1083 Merivale Road is positioned so that the front wall of the building is parallel to the Merivale Road ROW. It is noted that SGH intends to merge the two properties in the future. As part of the proposed development, one (1) driveway access point to Merivale Road is proposed.

5.3.5 Response to Preliminary City Urban Design Comments

Comments were provided by Urban Design staff at the pre-application consultation meeting held on August 17, 2022. These comments are summarized below and a response has been provided where the proposed development has addressed staff's comments.

- **Conceptually, the proposed courtyard space between the two buildings is strongly supported.**

Response: As shown on the Site Plan, prepared by CSV Architects (dated June 2, 2023), the proposed courtyard space has been maintained.

- **Relocate the loading zone fronting onto Merivale Road to the south side of the building. This will likely require reconfiguring the ground floor and relocating the garbage/recycling room. With the paved loading area eliminated, the building setback should be reduced, which may permit increasing the size of the courtyard space.**

Response: The loading zone has been relocated to the shared access driveway. Only one curb cut off Merivale is proposed.

- **As the design of the building at 1083 progresses, please ensure that the front of the façade facing Merivale does not read as the back of the building. This would help to achieve the vision of the Merivale Road North CDP to have the street mature as a vibrant urban mainstreet.**

Response: The entrance off Merivale has been redesigned to with more prominently as a main entrance.

- **Use clear windows and doors, to make the pedestrian level façade of walls facing the street highly transparent.**

Response: Extensive glazing has been added at the pedestrian level at resident amenities, staff offices and community spaces to increase transparency of the ground floor

- **Try to have animated ground-floor internal uses front towards the street (though it is understandable that it may be preferable to have some resident amenities front and/or open up onto the courtyard).**

Response: The ground floor has been redesigned since the pre-application consultation meeting, with more programming along the Merivale Road street edge which will help to animate the main facade.

- **The front entrance currently looks very small. It should read as the primary entrance to the building and be prominently designed with a direct pedestrian connection to the street. Consider a through-lobby, rather than a hallway connecting to the lobby in the rear.**

Response: The front entrance on Merivale has been redesigned to feature more prominently.

- **It is recommended that the bike storage, currently shown in the basement, be relocated to the ground floor or to an exterior location. If located outside, please ensure it is located on a concrete pad and sheltered.**

Bicycle parking is provided throughout the site in five (5) locations, including by main building entrances and the proposed parking area at the rear of the existing building at 1095 Merivale Road.

- **There are a number of relevant policy and guideline documents that should help guide the design of this development. Please reference the following documents and any others that may apply:**

The Urban Design Guidelines for Development along Traditional Mainstreets
The Merivale Road North Community Design Plan

Response: Please refer to Subsections 5.3.3 and 5.3.4 of this Report which discuss the Urban Design Guidelines for Development along Traditional Mainstreets and the Merivale Road North Community Design Plan respectively, in relation to the proposed development.

- **Please note that the proposed property is located in a Design Priority Area and as such, the Official Plan sets an expectation for an elevated level of attention to be paid to design of the building, its materiality, and the treatment of the public realm as a part of development applications.**

Response: Noted.

- **An Urban Design Brief is required as a part of your submission. This may be combined with your Planning Rationale report. Please refer to the attached Urban Design Brief Terms of Reference to inform the content of the brief.**

Response: Please refer to Section 6 of this Report for the Urban Design Brief, as well as the overview of the proposed development, Site Plan, exterior elevations, and architectural renderings in Section 3.

- **A Formal Review with the City's Urban Design Review Panel (UDRP) is required. Please contact the Panel Coordinator to schedule the meeting. Providing the coordinator with an early "heads-up" as to which meeting is being targeted, once it is known, is recommended. A full list of upcoming panel meeting dates, submission deadlines and other information can be found on the UDRP website.**

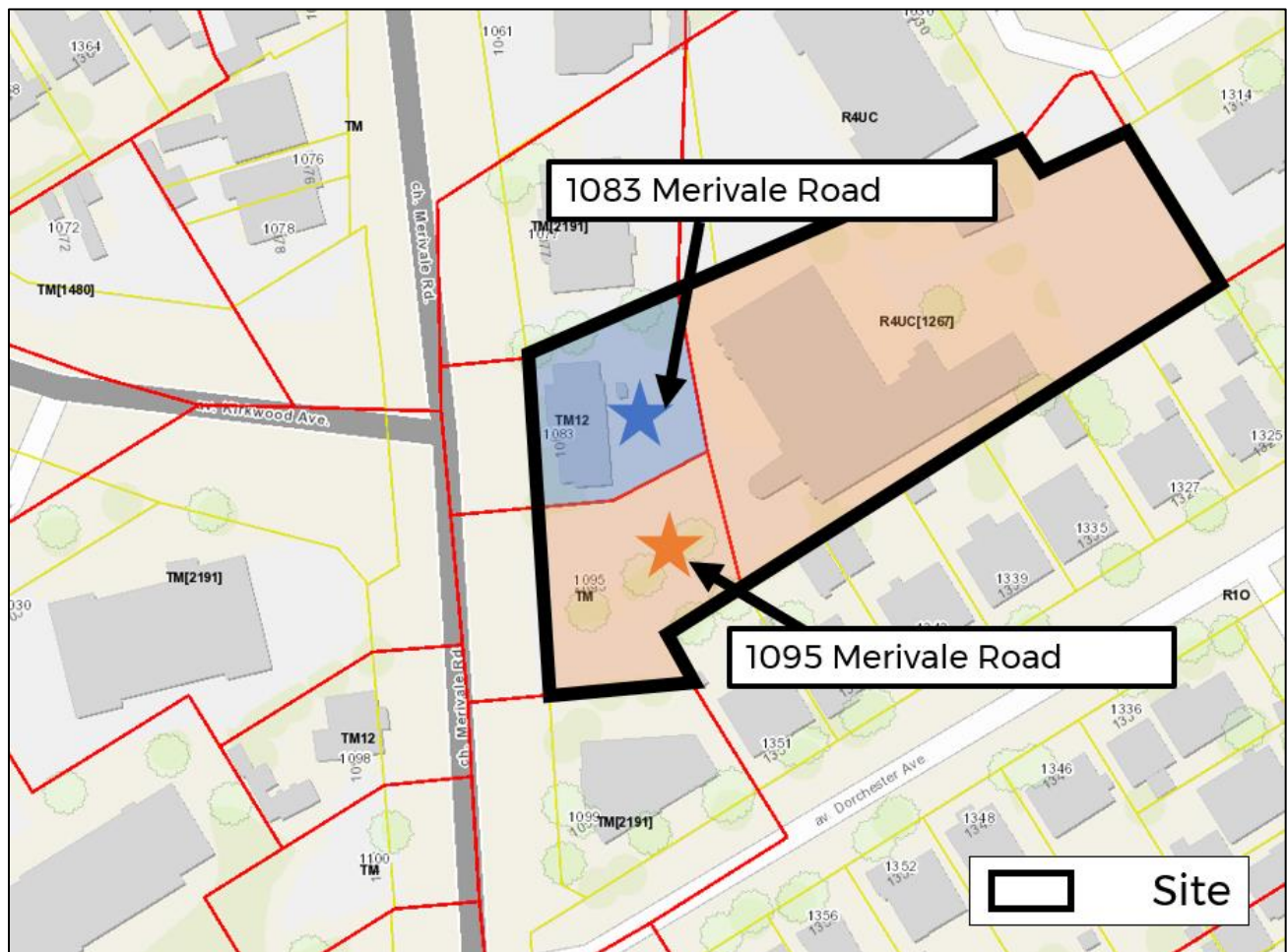
Response: It is the proponent's intent that Formal Review with the UDRP will be undertaken following the initial Site Plan Control application submission.

5.4 City of Ottawa Comprehensive Zoning By-law 2008-250 (Consolidation September 8, 2021)

Under the City of Ottawa Comprehensive Zoning By-law 2008-250 (Website Consolidation September 8, 2021), three (3) Zones apply to the site. As shown in **Figure 5-6**, 1083 Merivale Road is zoned Traditional Mainstreet, Subzone 12 (TM12) and 1095 Merivale Road is zoned Traditional Mainstreet (TM) and Residential Fourth Density, Subzone UC, Urban Exception 1267 (R4UC[1267]).

All properties which are immediately adjacent to the site are zoned Traditional Mainstreet (TM), Traditional Mainstreet, Subzone 12 (TM12), Residential First Density, Subzone O (R1O), and Residential Fourth Density, Subzone UC, Urban Exception 1267 (R4UC[1267]).

Figure 5-6: Site Zoning (geoOttawa, 2023)



5.4.1 Zoning Provisions

The general purpose of the TM Zone is to:

- accommodate a broad range of uses including retail, service commercial, office, residential and institutional uses, including mixed-use buildings but excluding auto-related uses, in areas designated Traditional Mainstreet in the Official Plan;
- foster and promote compact, mixed-use, pedestrian-oriented development that provide for access by foot, cycle, transit and automobile;
- recognize the function of Business Improvement Areas as primary business or shopping areas; and
- impose development standards that will ensure that street continuity, scale and character is maintained, and that the uses are compatible and complement surrounding land uses.

The TM Zone permits a broad range of residential uses and non-residential uses, including residential care facilities. A residential care facility is a permitted use in the TM and TM12 Zones. **As confirmed by email correspondence from City staff on November 3, 2022, for the purpose of the zoning review, the proposed use for the site is a residential care facility.**

Section 54 – Definitions, defines Residential Care Facility as an “establishment providing supervised or supportive in-house care for those who need assistance with daily living, that may also provide on-going medical or nursing care or counselling and social support services and which may include services such as medical, counselling, and personal services”.

Subzone 12 permits additional uses including detached dwellings, duplex dwellings, semi-detached dwellings, three-unit dwellings and townhouse dwellings.

Section 33(1) of the Zoning By-law states that, “Where a lot is divided into more than one zone, each portion of the lot must be used in accordance with the provisions of the applicable zone; however, the zone boundary is not to be treated as a lot line. Where two or more regulations are equally applicable to a lot, Section 18 applies”.

Section 18 states, “[...] where a situation arises that is not covered by a specific regulation, or where two or more regulations are equally applicable, all provisions must be complied with or, where it is not possible to comply with all the provisions applicable, the most restrictive provisions must be complied with”.

Section 93 of the Zoning By-law contains provisions which permit the application of ‘one lot for zoning purposes’ in certain circumstances. Section 93 provides that a group of occupancies in an AM, GM, LC, MC, MD, IG, IH, IL, IP, or RC Zone shall be considered as one lot for the purposes of applying zoning provisions and regulations provided that they:

- are designed, developed, and managed, including site access and infrastructure servicing, as a unit by a single owner, group of owners, or tenants acting in collaboration;
- are made up entirely of permitted or lawfully non-conforming uses on the site, and has either:

- a common parking lot or parking garage or combination thereof; or
- a group of parking lots or parking garages or a combination thereof which are managed as a unit by the same owner, owners or tenants of the occupancies required in clause (1) above, and are on the same lot or lots as the occupancies required in clause above.

It is noted that per email correspondence with City Planning staff on December 19, 2022, ‘one lot for zoning purposes’ will apply to 1083-1095 Merivale Road. Staff noted the definition for ‘Lot’ as per Section 54 – Definitions: “Lot means all contiguous land under one ownership [...]”. Therefore, the site is considered as one lot for zoning purposes, although it is split zoned between TM, TM12, and R4UC[1267]. Additionally, at a meeting held with City Planning staff on February 22, 2023, it was further confirmed that ‘one lot for zoning purposes’ will apply to the site with respect to off-street parking and landscape requirements and that the proposed development will not operate as a planned unit development.

As confirmed at the pre-application consultation meeting held on August 17, 2022, and through follow-up correspondence by email with City staff, for the purposes of the zoning review, the provisions of the TM12 Zone are to be reviewed for the entire site. However, at a meeting held with City Planning staff on February 22, 2023, the City confirmed that the provisions of each applicable Zone are to apply the portion of the lot as illustrated City’s Zoning Schedule. As the only additional provisions under the TM12 Subzone include prohibited uses, **Table 5-2** provides a detailed compliance analysis of how the proposed development at 1083 Merivale Road meets the Zoning By-law provisions for the TM Zone, based on the Site Plan prepared by CSV Architects (dated June 2 2023).

Further, as stated in email correspondence from City Planning staff on December 19, 2022, the rear yard setback for the TM Zone does not apply to the proposed building at 1083 Merivale Road for the purposes of reviewing the proposed development for zoning compliance.

Table 5-2: Zoning Compliance for the Traditional Mainstreet, Subzone 12 (TM12) – 1083 Merivale Road

Zoning Provisions - Residential Care Facility	Requirement	Calculation (if applicable)	Compliance (Yes or No)
Minimum lot area - Sec. 1197, Table 197(a)	No minimum	N/A	N/A - No requirement.
Minimum lot width - Sec. 197, Table 197(b)	No minimum	N/A	N/A - No requirement.

Zoning Provisions - Residential Care Facility	Requirement	Calculation (if applicable)	Compliance (Yes or No)
Maximum front yard setback - Sec. 197, Table 197(c)	2 m, subject to provisions of subsection 197(4)	N/A	Yes - 2 m
Maximum interior side yard setback - Sec. 197, Table 197(d)(i)	3 m between a non-residential use building or a mixed-use building and another non-residential use building or mixed-use building, except where a driveway is provided, in which case a setback must be a max of 6 m where the driveway leads to a parking area of 20 or more spaces	N/A	N/A - Maximum interior side yard setback not applicable as per Table 197(d)(i). The proposed building at 1083 Merivale will be a non-residential use, however the buildings on abutting properties at 1077 Merivale and 1099 Merivale are residential. The proposed driveway leads does not lead to a parking area.
Minimum interior side yard setback - Sec. 197, Table 197(d)(ii)	<p>The max setback provisions of row (d)(i) above do not apply to the following cases and the following min setbacks apply:</p> <p>(1) 3 m for a non-residential use building or a mixed-use building abutting a residential zone,</p> <p>(2) 1.2 metres for a residential use building</p> <p>all other cases - no minimum (maximum setback provisions of row (d)(i) apply in these cases)</p>	N/A	<p>Proposed building at 1083 Merivale is a non-residential use and abuts a TM (mixed use) zone.</p> <p>Table 197(d)(ii)(2):</p> <p>Where the proposed building at 1083 Merivale abuts a residential zone (R10):</p> <p>Southern setback: 8.41 m</p> <p>Northern interior setback: 1.67 m (No minimum applies here)</p>
Minimum corner side yard setback - Sec. 197, Table 197(e)	3 m, except for any part of a building above 15 m for which an additional 2 m setback must be provided	N/A	N/A - Site is an interior lot.

Zoning Provisions - Residential Care Facility	Requirement	Calculation (if applicable)	Compliance (Yes or No)
Minimum rear yard setback - Sec. 197, Table 197(f)	(i) rear lot line abutting a residential zone - 7.5 m (iii) for residential use building - 7.5 m	N/A	N/A - rear yard setback for 1083 Merivale not required - one lot for zoning purposes. The building at 1095 Merivale Road is an existing condition.
Minimum building height - Sec. 197, Table 197(g)(i)	6.7 m for a distance of 20 m from the front lot line as set out under subsection 197(5)	N/A	Yes - 20 m
Maximum building height - Sec. 197, Table 197(g)(ii)	(1) 20 metres but not more than 6 storeys, except where otherwise shown on the zoning maps	N/A	Yes - 6 storeys (20 m)
Maximum floor space index - Sec. 197, Table 197(h)	No maximum	N/A	N/A - No maximum
Minimum width of landscaped area - Sec. 197, Table 197(i)	(i) abutting a residential zone - 3 m; may be reduced to 1 m where a min 1.4 m high opaque fence is provided (ii) in all other cases - No min, except that where a yard is provided and not used for required driveways, aisles, parking or loading spaces, the whole yard must be landscaped	N/A	Portion of the site abuts a Residential Zone, Subzone 10 (R10) to the south. A 1.8 m wood board fence is proposed along the southern and eastern limits of the site, accordingly a 1 m landscape area is required and this is provided.

5.4.2 Permitted Projections Above the Height Limit and Permitted Projections into Required Yards Provisions

The Zoning By-law contains provisions for permitted projections above the height limit and permitted projections into required yards, as set out in **Table 5-3**.

Table 5-3: Provisions for Permitted Projections Above Height Limit / into Required Yards - 1083 Merivale Road

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
Permitted projections above the height limit - Sec. 64	<p>Except in the case of buildings or structures located within the area shown on Schedules 11 to 88 (Central Area Height Schedules), the maximum height limits do not apply to the structures listed below or to any other similar structures that may require a height in excess of maximum height limits in order to serve their intended purpose, unless otherwise specified in the by-law and provided these structures are erected only to such height or area as is necessary to accomplish the purpose they are to serve and that is necessary to operate effectively and safely:</p> <p>6. Mechanical and service equipment penthouse, elevator or stairway penthouse</p>	N/A	Yes - a mechanical penthouse with elevator and stair is proposed for the building at 1083 Merivale Road, which are permitted height projections as per Section 64.
Chimney, chimney box and fireplace box, Sec. 65, Table 65(1)	1 m, but not closer than 0.6 m to a lot line	N/A	N/A
Eaves, eaves-troughs and	1 m, but not closer than 0.3 m to a lot line	N/A	N/A

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
gutters, Sec. 65, Table 65(2)			
Ornamental elements such as sills, belt courses, cornices, parapets and pilasters, Sec. 65, Table 65(3)	0.6 m, but not closer than 0.6 m to a lot line	N/A	N/A
Canopies and awnings, Sec. 65, Table 65(4)	(b) All other buildings including a low-rise apartment and mid-high-rise apartment dwelling: i) a distance equal to ½ the depth of a front, rear, or corner side yard but not closer than 0.6 m to a lot line	N/A	Yes - a canopy at the front of the building is proposed for the building at 1083 Merivale Road. This canopy will project 1 m, however a maximum front yard setback of 2 m applies.
Fire escapes, open stairways, stoop, landing, steps and ramps - Sec. 65, Table 65(5)	(b) Other features: i) where at or below the floor level of the first floor: 1. in the case of the interior side yard or rear yard: no limit, and ii) other cases: 1. In the case of any yard: 1.5m, but not closer than 1 m to a lot line	N/A	Yes - a stair at floor level is proposed at the front of the building at 1083 Merivale and is located 0 m from the front lot line. A maximum front yard setback of 2 m applies.
Covered or uncovered balcony, porch, deck, platform and verandah, with a maximum of two enclosed sides, excluding those covered by	a) uncovered, unenclosed features such as decks or platforms where the walking surface is not higher than 0.6 m above adjacent grade: (i) in the interior side yard and rear yard: no limit (ii) in the front yard and corner side yard - the greater	N/A	N/A

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
canopies and awnings, Sec. 65, Table 65(6)	of 2m or 50% of the required front yard or corner side yard, but no closer than 1m to a property line		
	c) In all other cases: 2 metres, but no closer than 1 metre from any lot line.		
Bay window where window faces a lot line, Sec. 65, Table 65(7)	1 m, but not closer than 1.2 m from a lot line	N/A	N/A
Air conditioner condenser, heat pump or similar equipment, Sec. 65, Table 65(8)	1 m, but not closer to a lot line than 0.3 m, and may not be located in a front yard or a corner side yard	N/A	N/A
Exit stairs – Sec. 161(13)(m)	Exit stairs providing required egress under the Building Code may project a maximum of 2.2 metres into the required rear yard.	N/A	N/A

5.4.3 Parking Provisions

The Zoning By-law contains provisions for parking, as set out in **Table 5-4**. The site is within Area X: Inner Urban on Zoning By-law Schedule 1A, as illustrated in **Figure 5-7**.

Figure 5-7: Schedule 1A – Areas for Minimum Parking Space Requirements, City of Ottawa Zoning By-law (via GeoOttawa, 2022)

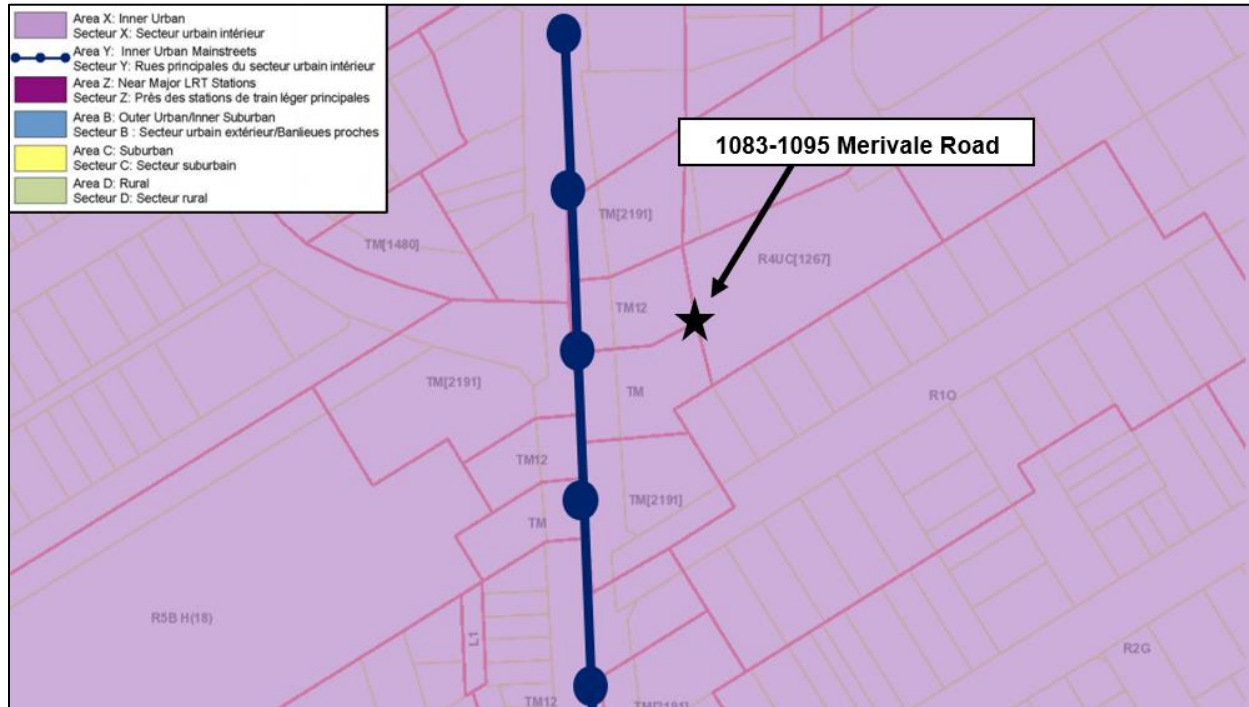


Table 5-4: Parking and Loading Space Provisions

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
General Provisions for Motor Vehicle Parking, Queuing and Loading – Sec. 100	<p>1) Parking, queuing and loading spaces and all driveways and aisles leading to those spaces must be provided for each land use in accordance with the provisions of Part 4 of this by-law, and:</p> <p>a) must be set aside for and used exclusively for that purpose;</p>	N/A	N/A

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
	<p>b) must not be obstructed; and</p> <p>c) except for driveways, must be located on the same lot as the use or building for which they are provided, except where otherwise permitted. (By-law 2015-190)</p> <p>2) All motor vehicle parking spaces and queuing and loading spaces must have unobstructed access directly to a public street by:</p> <p>a) A driveway or private way</p> <p>b) A public lane</p> <p>c) An aisle leading to a driveway, private way or public lane</p> <p>d) Some combination of (a), (b) and (c)</p>		
Minimum Parking Space Rates – Sec. 101	<p>Within the areas shown as Areas B, C, D, X and Y on Schedule 1A, off-street motor vehicle parking must be provided for any land use at the rate set out in Table 101 below.</p> <p>3) Despite Subsection (1), within the area shown as Area X on Schedule 1A</p> <p>a) In the case of a building containing residential uses, no off-street motor vehicle parking is</p>	N/A	N/A

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
	required to be provided under this section for the first twelve dwelling units and the parking requirements under Table 101 apply only to dwelling units and rooming units in excess of 12.		
Minimum Parking Space Rates – Sec. 101, Table 101	<p>Residential care facility (Area X, Schedule 1A):</p> <p>0.125 per dwelling unit or rooming unit plus 0.5/100 m² of GFA used for medical, health or personal services</p> <p>Office (Area X, Schedule 1A):</p> <p>1 per 100 m² of gross floor area</p>	<p>1083 Merivale Road:</p> <p>Residential:</p> <p>70 units – 12 units (as per Section 101(1)(3)(a))</p> <p>= 58 units</p> <p>0.125 x 58 units</p> <p>= 7.25</p> <p>= 7 spaces (rounded)</p> <p>Office:</p> <p>215 m² / 100</p> <p>= 2.15</p> <p>= 2 spaces (rounded)</p> <p>1095 Merivale Road:</p> <p>57 units – 12 units (as per Section 101(1)(3)(a))</p> <p>= 45 units</p> <p>0.125 x 45 units</p> <p>= 5.625</p>	Yes – 18 parking spaces are provided across the site.

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
		= 6 spaces (rounded) Total (Required): 15 spaces	
Parking Space Provisions – Sec. 106	1) Any motor vehicle parking space must be <ul style="list-style-type: none"> a) At least 2.6 m wide b) Not more than 3.1 m wide c) At least 5.2 m long 	N/A	Yes – Proposed regular motor parking spaces are 2.6 m x 5.2 m.
Location of Parking Abutting a Street in a Residential Zone – Sec. 109(1)(a)	In the TM Zones, no person may park a motor vehicle: <ul style="list-style-type: none"> i. in a required and provided front yard; ii. in a required and provided corner side yard; iii. in the extension of a required corner side yard into a rear yard 	N/A	N/A – Parking spaces are located in the rear yard.
Minimum visitor parking space rates, Sec. 102, Table 102	None required for the proposed use.	N/A	N/A
Minimum number accessible parking spaces – City of Ottawa Accessibility	Total number of parking spaces: One Type A space is required.	N/A	Yes – 2 Type A accessible parking spaces are proposed.

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
Design Standards (2015), Sec. 3.1.2, Table 3			
Parking space dimensions - City of Ottawa Accessibility Design Standards (2015), Sec. 3.1.3(g)	(i) width: 3.4 m each		Yes - Type A spaces are 3.4 m in width and extend the full length of the space.
Required access aisle - City of Ottawa Accessibility Design Standards (2015), Sec. 3.1.3(h)	(ii) 1.5 m in width (ii) extend full length of the space (iii) indicated by high colour contrast diagonal pavement markings	N/A	Yes - Access aisles adjacent to accessible parking spaces are 1.5 m in width.
Minimum width of driveway providing access to a parking lot - Sec. 107(1)(a)(ii)	6 m for a double traffic lane	N/A	Yes - 6 m (Merivale Road).
Private Approach - By-law No. 2003-447, Sec. 11(1)	A private approach shall have a minimum width of 2.4 metres and a maximum width of 9.0 metres, and in no case shall the width exceed 50% of the frontage on which the approach or approaches are located.	N/A	N/A - No new private approach proposed. The existing access from 1083 Merivale will be removed and combined with the access for

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
			1095 Merivale. Proposed driveways utilizes existing accesses from Merivale Road and Emperor Avenue.
<p>Landscaping Provisions for Parking Lots - Sec. 110(1), Table 110</p>	<p>(2) [...] in the TM Zone, the 15% landscaped area requirement does not apply, but</p> <p>(a) Table 110 does apply;</p> <p>(b) any area not used for parking or buildings must be landscaped; and</p> <p>(c) the perimeter of the parking lot where it abuts a residential zone must be screened from view by an opaque screen with a minimum height of 1.5 m.</p> <p>Table 110:</p> <p>III. For a parking lot containing more than 10 but fewer than 100 spaces:</p> <p>(b) Not abutting a street - 1.5 m landscaped buffer required.</p>		<p>Yes - A 1 m buffer is provided along with a 1.8 opaque screen along the eastern and southern limits of the parking lot where it abuts a residential zone. All areas not used for parking or buildings are landscaped.</p>
<p>Outdoor loading and refuse collection areas within a parking lot - Sec. 110(3)</p>	<p>Requirements:</p> <ul style="list-style-type: none"> - 9 m from a lot line abutting a public street - 3 m from any other lot line - Screened from view by an opaque screen with a min. height of 2 m 	N/A	<p>N/A- A garbage room will be located in the interior of the building at 1083 Merivale and will be sized to serve both 1083 and 1095 Merivale.</p>

Zoning Provision	Requirement		Calculation (if applicable)	Compliance (Yes or No)
Minimum aisle width - Sec. 107, Table 107	6.7 m for angle of parking that is 90 degrees		N/A	Yes - 7 m
Minimum bicycle parking space rates - mid-rise apartment dwelling - Sec. 111, Table 111A(i)	0.5 per dwelling unit		<p>1083 Merivale Road: 70 units x 0.5 bicycle spaces per dwelling unit = 35 spaces</p> <p>1095 Merivale Road: 57 units x 0.5 bicycle spaces per dwelling unit = 28.5 spaces = 29 spaces (rounded)</p> <p>Total: 64 spaces required</p>	Yes - 64 bicycle parking spaces are proposed.
Minimum bicycle parking space dimensions - Sec. 111, Table 111B	(a) Horizontal	Width: 0.6 m Length: 1.8 m	N/A	Yes - bicycle spaces will be 0.6 m x 1.8 m
Minimum bicycle parking aisle width - Sec. 111 (9)	1.5 m		N/A	Yes - 1.5 m bike parking access aisle is provided.

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
Location of Bicycle Parking - Sec. 111	(3) Bicycle parking must be located on the same lot as the use or building for which it is provide	N/A	Yes - Bicycle parking is located on the same lot.
	(4) Bicycle parking spaces must be located in order to provide convenient access to main entrances or well-used areas.	N/A	Yes - Bicycle parking is located adjacent to building entrances.
	(5) A bicycle parking space may be located in any yard.	N/A	Yes - Bicycle parking is located central to the site.
	(6) A maximum of 50% of the required bicycle parking spaces or 15 spaces, whichever is greater, may be located in a landscaped area.	N/A	N/A - Bicycle parking is only proposed in landscaped areas.
Loading space rates and provisions - Sec. 113(4)	in the TM Zone, a vehicle loading space is only required for uses that have a gross floor area of 1,000 m ² or more, except for an office or research and development centre use for which a vehicle loading space is only required if the use has a gross floor area of 4,000 m ² or more.	N/A	
Minimum number of vehicle loading spaces	(d) All other non-residential uses, except in the TM Zone (e) Residential uses: None required	N/A	Yes - a loading space is not required, but one (1) loading area (13 m x 4 m) is

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
required – Sec. 113, Table 113A			proposed at the side of the proposed building at 1083 Merivale Road.
Minimum width of driveway accessing loading space – Sec. 113, Table 113B(a)	(i) – Single traffic lane – 3.5 m (ii) Double traffic lane – 6 m	N/A	Yes – 6 m for a double traffic lane
Minimum width of aisle accessing loading space, by angle of loading Space – Sec. 113, Table 113B(b)	For a standard space: (i) 45 degrees or less – 5 m (ii) Between 45 and 60 degrees – 6.3 m (iii) 60 to 90 degrees – 9 m	N/A	Yes – South loading space will be 13 m x 4 m.
Minimum width of loading space – Sec. 113, Table 113B(c)	For a standard space: 3.5 m	N/A	Yes – 4 m
Minimum length in metres of loading space – Sec. 113, Table 113B(d)	For a standard space: (i) Parallel – 9 m (ii) Other cases – 7 m	N/A	Yes – 13 m
Minimum vertical clearance for loading space – Sec. 113, Table 113B(e)	4.2 m	N/A	N/A
Permitted location of loading space –	Permitted in all locations other than in a required front yard or required corner side yard, or in	N/A	N/A

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
Sec. 113, Table 113B(f)	a required yard abutting a residential zone		

5.4.4 Amenity Area Provisions

Section 137 of the Zoning By-law contains provisions for amenity areas, as set out in **Table 5-5**.

Table 5-5: Provisions for Amenity Areas

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
Amenity Area Location, Sec. 137(2)	(9) Residential care facility Total amenity area: 10% of the gross floor area of each room unit Communal amenity area: All of the total amenity area	Total GFA of proposed units (1083 & 1095 Merivale): 1083 Merivale - 1,560 m ² 1095 Merivale - 2,898 m ² Total = 4,458 m ² Required amenity area: 10% * (4,458 m ²) = 446 m ² (rounded)	Yes - Interior amenity area proposed: 1083 Merivale - 58 m ² 109 Merivale - 148 m ² Total interior amenity area = 206 m ² Exterior amenity area proposed - 490 m ² Total amenity area across site = 696 m ²
Outdoor Amenity Area Location, Sec. 137(3)	Must not be located in a required front or corner side yard	N/A	Yes - Outdoor amenity areas are proposed central to the site and are not located in the front or corner side yard.
Amenity Area Required, Sec. 137, Table 137(7)	Total Amenity Area: 6 m ² per dwelling unit, and 10% of the gross floor area of each rooming unit	Required: 10% x GFA of rooming units GFA of proposed units (1083 & 1095 Merivale): 10% * (2,597 m ²) = 259 m ²	Yes - a total of 696 m ² of amenity area is provided across the site. Interior amenity area proposed: 1083 Merivale - 58 m ² 109 Merivale - 148 m ²

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
			<p>Total interior amenity area = 206 m²</p> <p>Exterior amenity area proposed - 490 m²</p> <p>Total amenity area across site = 696 m²</p>
	<p>Communal Amenity Area: A minimum of 50% of the required total amenity area</p>		<p>Yes - Proposed communal amenity area is 100% of the total amenity area.</p>
	<p>Layout of Communal Amenity Area: Aggregated into areas up to 54 m², and where more than one aggregated area is provided, at least one must be a minimum of 54 m²</p>	<p>N/A</p>	<p>Yes - The proposed community amenity area is as follows:</p> <p>Interior amenity area proposed: 1083 Merivale - 58 m² 109 Merivale - 148 m² Total interior amenity area = 206 m²</p> <p>Exterior amenity area proposed - 490 m²</p> <p>Total amenity area across site = 696 m²</p>

6 Summary of Opinion

Based on our review of the applicable land use planning policy framework, area context, and the supporting application materials, it is the professional opinion of WSP that the proposed Shepherds of Good Hope development at 1083-1095 Merivale Road represents good land use planning, and is appropriate for the site for the following reasons:

- The proposed development is consistent with the 2020 Provincial Policy Statement;
- The proposed development is permitted in the applicable land use designations and conforms to the strategic directions and policies of the City's Official Plan;
- The proposed development is compatible with adjacent development and meets many of the guidelines, principles, and policies under the Merivale Road North Community Design Plan and Urban Design Guidelines for Development along Traditional Mainstreets; and
- The proposed development complies with the general intent and purpose of the Zoning By-law.

In conclusion, the proposed Site Plan Control application being sought to support the proposed development at 1083-1095 Merivale Road represents good planning, and the development is in the public interest.


Please feel free to contact Nadia De Santi at Nadia.De-Santi@wsp.com or at (613) 690-1114 or Jill MacDonald at Jill.MacDonald@wsp.com or (613) 690-3936 if you have any questions or require additional information.

Yours truly,

WSP



Nadia De Santi, MCIP, RPP
Practice Lead



Jill MacDonald, MCIP, RPP
Project Planner

A Site Plan



STAMP

1 2023-06-02 ISSUED FOR SITE PLAN CONTROL
REV DATE ISSUE

NOTES
1. OWNERSHIP OF THE COPYRIGHT OF THE DESIGN AND THE WORKS EXECUTED FROM THE DESIGN REMAINS WITH CSV ARCHITECTS, AND MAY NOT BE REPRODUCED IN ANY FORM WITHOUT THE WRITTEN CONSENT OF CSV ARCHITECTS.
2. THE DRAWINGS, PRESENTATIONS AND SPECIFICATIONS AS INSTRUMENTS OF SERVICE ARE AND SHALL REMAIN THE PROPERTY OF CSV ARCHITECTS. THEY ARE NOT TO BE USED BY THE CLIENT ON OTHER PROJECTS OR ON EXTENSIONS TO THIS PROJECT WITHOUT THE WRITTEN CONSENT OF CSV ARCHITECTS.
3. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER PROJECT DRAWINGS AND SPECIFICATIONS.
4. DO NOT SCALE DRAWINGS. CONTRACTOR SHALL BE RESPONSIBLE TO VERIFY DIMENSIONS ON SITE.
5. ALL WORK SHALL BE IN ACCORDANCE WITH THE ONTARIO BUILDING CODE AND ALL SUPPLEMENTS AND APPLICABLE MUNICIPAL REGULATIONS.

CLIENT
SHEPHERDS OF GOOD HOPE
OTTAWA
ONTARIO, CANADA

PROJECT
SGH 1083 MERIVALE

1083 Merivale Road
Ottawa, ON K1Z 6A9
TITLE

SITE PLAN

PROJECT NO: 2021-0111
DRAWN: SF
APPROVED: JS
SCALE: 1:200
DATE PRINTED: 2023-06-02 4:34:25 PM

REV DRAWING NO.
1 **A.100**

SITE PLAN GENERAL NOTES:

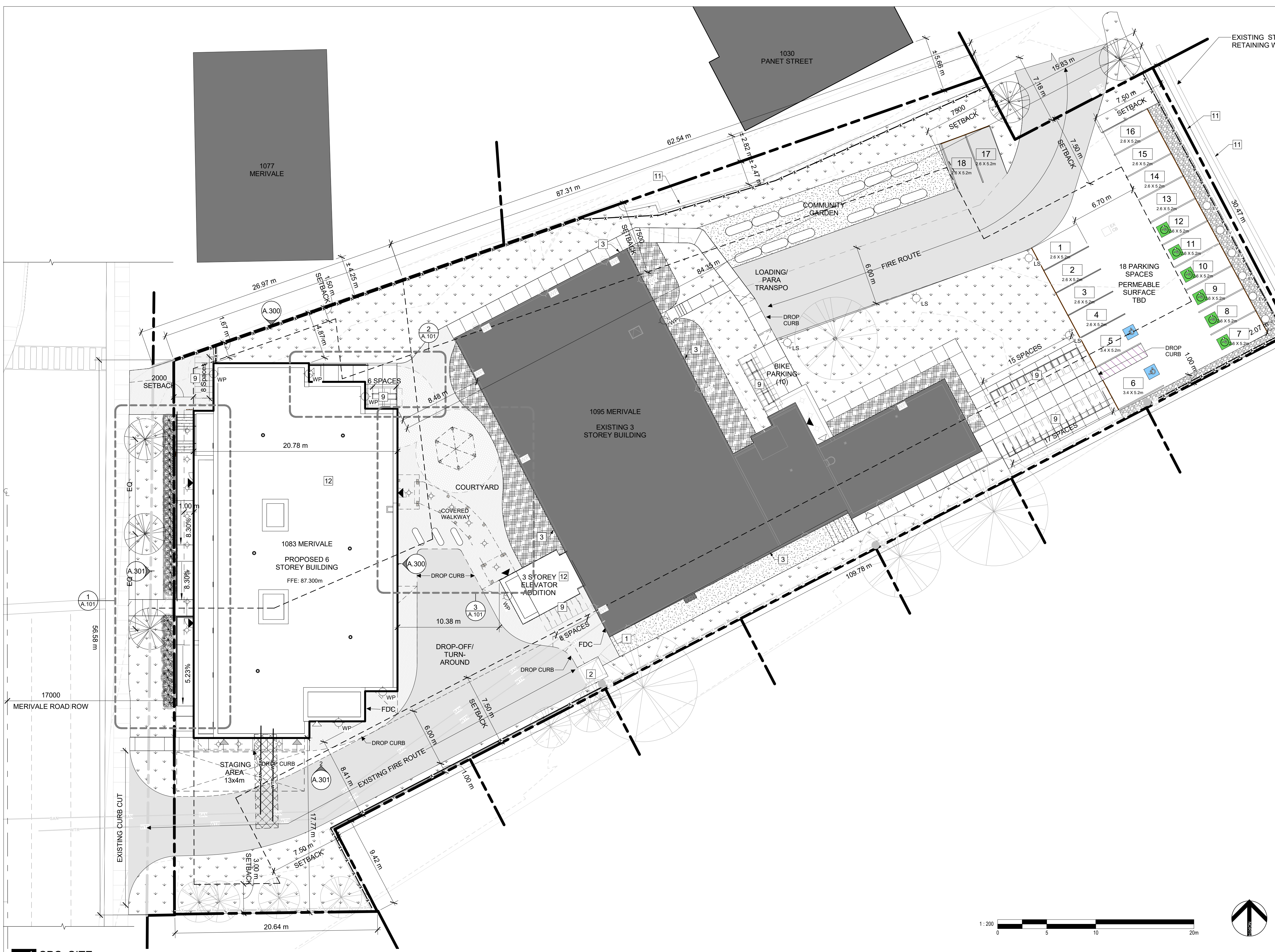
- ALL GENERAL SITE INFORMATION AND CONDITIONS COMPILED FROM EXISTING PLANS AND SURVEYS
- DO NOT SCALE THIS DRAWING
- REPORT ANY DISCREPANCIES PRIOR TO COMMENCING WORK. NO RESPONSIBILITY IS BORN BY THE CONSULTANT FOR UNKNOWN SUBSURFACE CONDITIONS
- CONTRACTOR TO CHECK AND VERIFY ALL DIMENSIONS ON SITE AND REPORT ANY ERRORS AND/OR OMISSIONS TO THE CONSULTANT
- REINSTATE ALL AREAS AND ITEMS DAMAGED AS A RESULT OF CONSTRUCTION ACTIVITIES TO THE SATISFACTION OF THE CONSULTANT
- CONTRACTOR TO LAYOUT PLANTING BEDS, PATHWAYS ETC. TO APPROVAL OF CONSULTANT PRIOR TO ANY JOB EXCAVATION
- THE ACCURACY OF THE POSITION OF UTILITIES IS NOT GUARANTEED - CONTRACTOR TO VERIFY PRIOR TO EXCAVATION
- INDIVIDUAL UTILITY COMPANY MUST BE CONTACTED FOR CONFIRMATION OF UTILITY EXISTENCE AND LOCATION PRIOR TO DIGGING
- ALL DISTURBED AREAS TO BE RESTORED TO ORIGINAL CONDITION OR BETTER UNLESS OTHERWISE NOTED

SITE PLAN KEYNOTES:

- EXISTING GAS PAD
- EXISTING TRANSFORMER
- EXISTING SCUPPER
- FRENCH DRAIN TO LANDSCAPE
- PLANTER - SEE LANDSCAPE
- COVERED WALKWAY
- ENTRANCE CANOPY
- GAZEBO
- BICYCLE RACK
- GUARDRAIL AND HANDRAIL PER OBC
- NEW RETAINING WALL - SEE CIVIL
- HIGH-ALBEDO ROOF

SITE PLAN LEGEND:

- EXISTING ELEMENT
- ASPHALT PAVING
- GRASS
- CONCRETE SIDEWALK
- CONCRETE PAD
- MULCH/PLANTING - SEE LANDSCAPE
- GRAVEL /RIVERSTONE/ MAINTENANCE STRIP
- STONE DUST/SAND
- PERMEABLE PAVERS TYPE 1 - SEE LANDSCAPE
- PERMEABLE PAVERS TYPE 2 - SEE LANDSCAPE
- EMERGENCY EXIT
- SERVICE DOORS
- BUILDING ENTRANCE
- PROPERTY LINE
- FENCE PER LANDSCAPE
- DOMESTIC WATER
- SANITARY
- STORM
- ELECTRICAL SERVICE (BELOW GRADE)
- GAS
- INTERNET SERVICE PROVIDER
- CATCH BASIN
- SITE LIGHTING: LIGHT STANDARD
- SITE LIGHTING: WALL PACK
- SITE LIGHTING: SOFFIT
- FIRE HYDRANT
- MANHOLE
- UTILITY POLE
- EV CHARGING STATION
- FIRE DEPARTMENT CONNECTION
- TREE
- SHRUB



1 SPC SITE
A.100 1:200

LEGAL DESCRIPTION:
PART OF LOTS 15 AND 93 REGISTERED PLAN 294 AND PART OF LOT 'L' CONCESSION 'A' (RIDEAU FRONT)

REFERENCE SURVEY:
2021-08-16
92-A(RF)NP

MUNICIPAL ADDRESS:
1083 MERIVALE ROAD,
OTTAWA, ON K1Z 6A9

SITE AREA: 5,007 m²

BUILDING AREA: 732 m²

GROSS FLOOR AREA: 4,244 m²

BUILDING HEIGHT: 20 m 6 STOREYS

ZONE: TM12

SCHEDULE 1: AREA B

SCHEDULE 1A: AREA X - INNER URBAN

ZONING PROVISION	REQUIRED	PROVIDED	PARKING QUEING + LOADING	REQUIRED	PROVIDED	AMENITY SPACE BREAKDOWN
MIN. LOT WIDTH	NO MINIMUM	N/A	RESIDENTIAL SPACES 1083	9	9	GFA 1095 SUITES: 1,560m ²
MIN. LOT AREA	NO MINIMUM	N/A	RESIDENTIAL SPACES 1095	7	7	GFA 1083 SUITES: 2,898m ²
MIN. FRONT YARD SETBACK	2 m	2 m	OFFICE SPACES 1083	2	2	AMENITY AREA IN 1095: 148m ²
MIN. CORNER YARD SETBACK	3 m	N/A	VISITOR SPACES	0	0	AMENITY AREA IN 1083: 58m ²
MIN. REAR YARD SETBACK	7.5 m	84 m	TOTAL PARKING	18	18	AMENITY AREA EXTERIOR: 490m ²
MIN. INTERIOR YARD SETBACK	1.2 m	1.5 m	ACCESSIBLE PARKING	2	2	REQUIRED AMENITY AREA: 446m ²
MAX. HEIGHT	20m	20m	BICYCLE PARKING	62	64	PROPOSED AMENITY AREA: 696m ²
LANDSCAPED AREA	NO MINIMUM	N/A	REFUSE COLLECTION	16 cu yd	16 cu yd	
			GARBAGE COLLECTION	8cu yd	8 cu yd	

B Landscape Plan

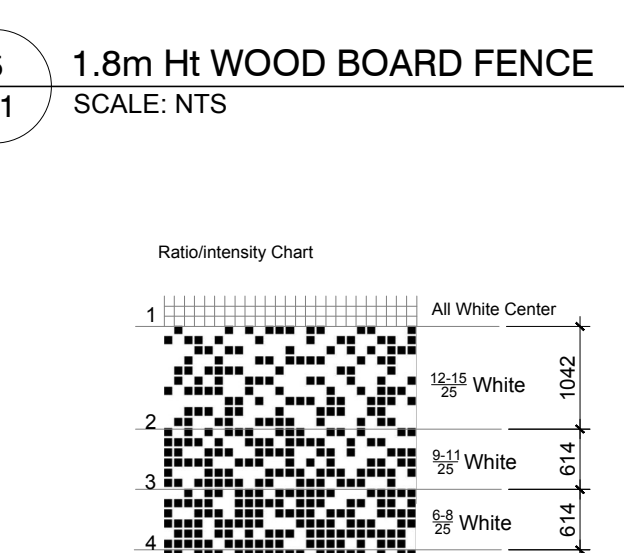
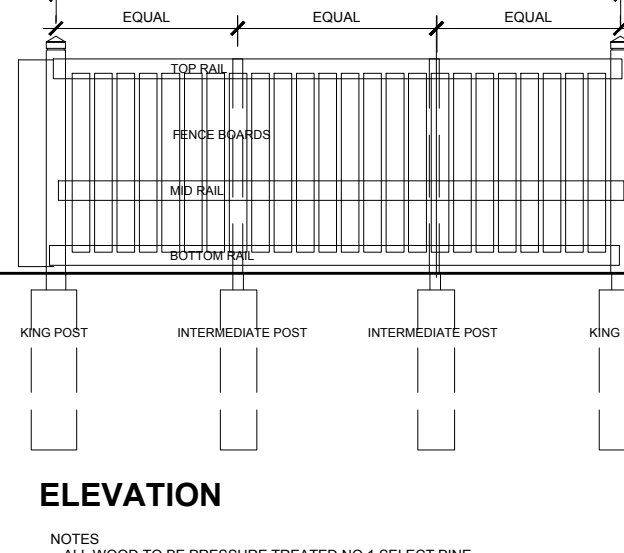
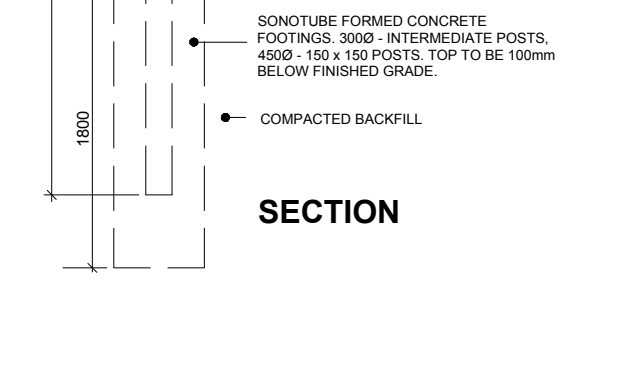
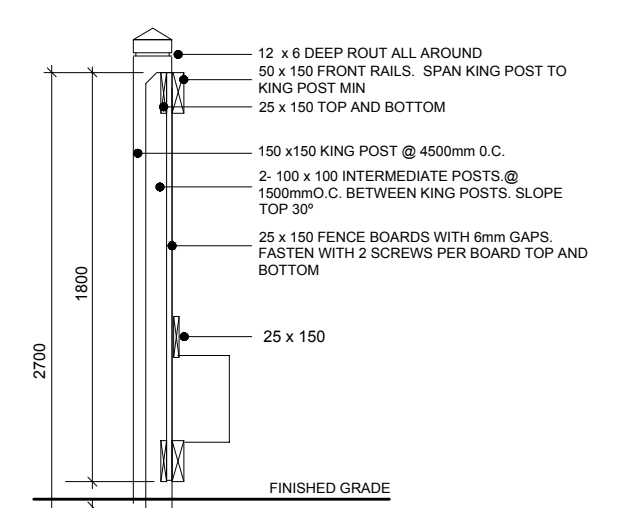
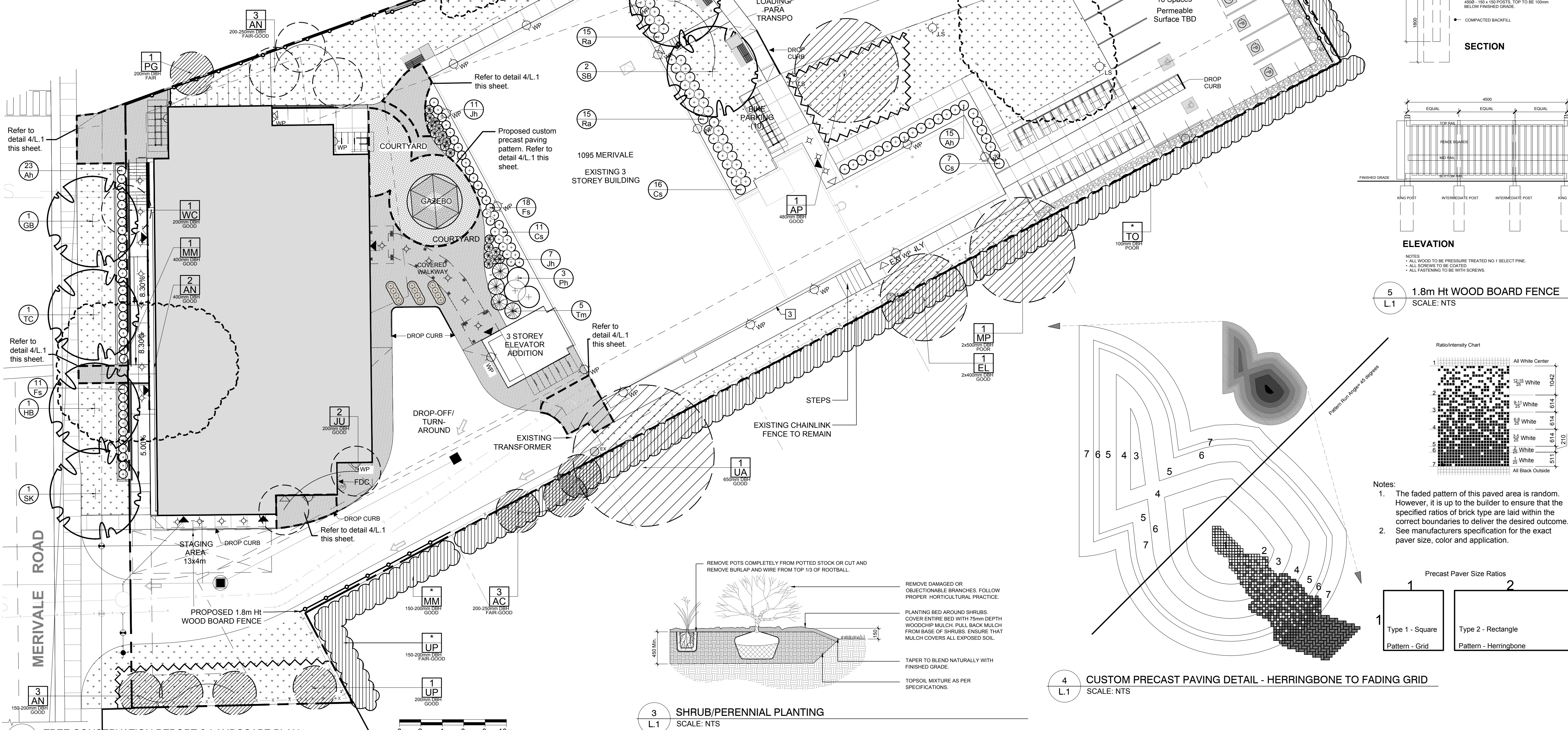


PRECEDENT IMAGE: STOCK TANK PLANTER

GALVANIZED STOCK TANKS ARE A DURABLE AND SIMPLE OPTION FOR PLANTER BEDS. THEY REQUIRE NO INSTALLATION AND COME IN VARIOUS SIZES. THEY CAN ALSO OFTEN BE COST-EFFECTIVE WHEN COMPARED TO LESS-DURABLE ALTERNATIVES THAT REQUIRE ASSEMBLY.

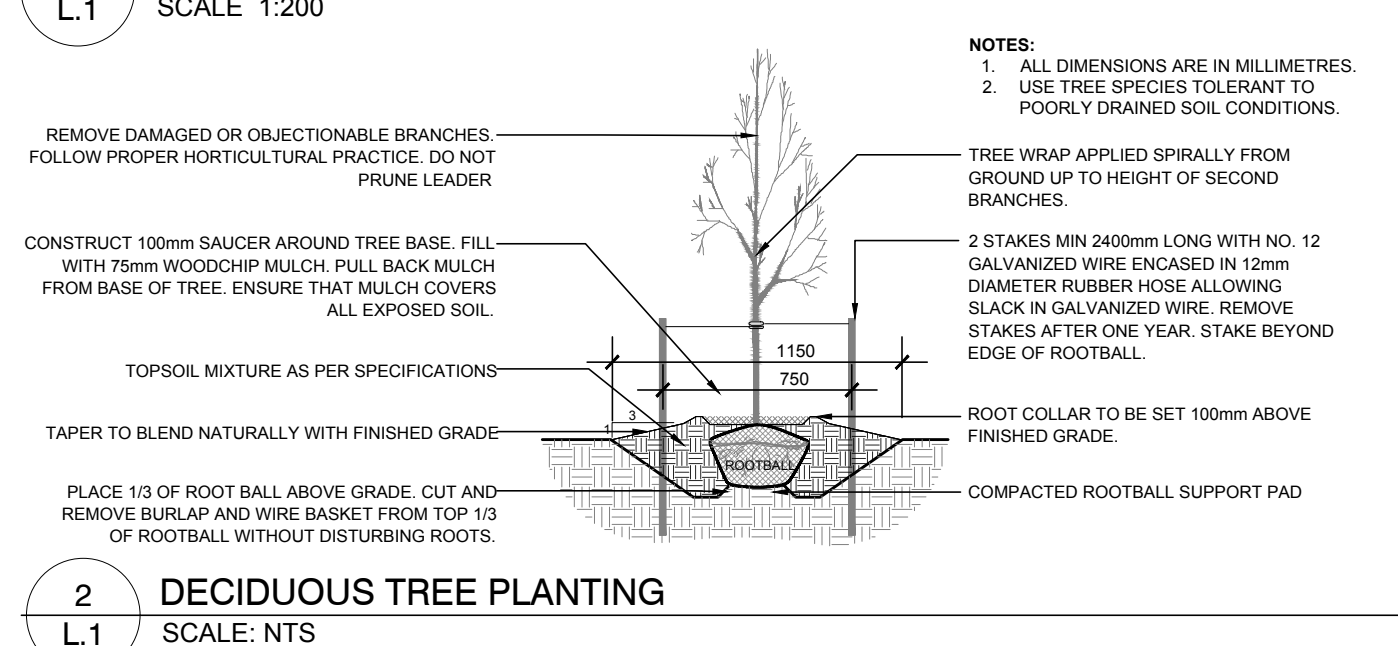
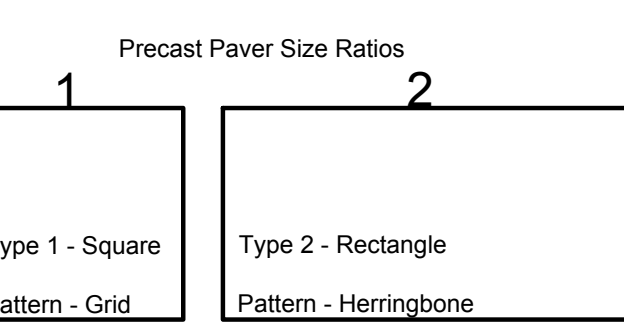
GENERAL NOTES:

- It is the responsibility of the appropriate contractor or official to report any errors, omissions or discrepancies on this plan with actual site conditions to the Landscape Architect before proceeding with construction.
- The contractor is to notify all utility companies and authorities prior to any excavation and ascertain locations of underground services.
- The contractor is to reinstate all areas and items damaged as a result of construction activity.
- The contractor is to comply with all pertinent codes and by-laws.
- The contractor is to maintain a positive surface run-off throughout the entire construction period.
- The Landscape Architect is not responsible for subsurface conditions.
- The contractor is to identify all existing trees to remain on site with the Landscape Architect prior to construction.
- The contractor is to stake the proposed location of all plant material in conjunction with the Landscape Architect prior to excavation.
- Minimum distances for selected deciduous trees are as follows:
 - Building Foundations 7.5m
 - Sidewalks 1.5m
 - Public Streets 2.5m
 - Underground Infrastructure 2.0m
- All trees within 1m of underground utility trenches are to be excavated by hand.
- Remove all protective wrapping from tree trunks after installation.
- Staking of trees shall only be performed if necessary.
- Ensure that mulch is pulled back a min. distance of 75mm from base of tree trunk.



- ### LEGEND
- EXISTING TREE TO REMAIN
 - GROUP OF EXISTING TREES TO REMAIN
 - EXISTING TREE TO BE REMOVED
 - GROUP OF EXISTING TREES TO BE REMOVED
 - PROPOSED DECIDUOUS TREE
 - PROPOSED SHRUBS / PERENNIALS
 - PROPOSED SODDED GRASS AREA
 - PROPOSED PRECAST TYPE 1 CONCRETE PAVERS
 - PROPOSED PRECAST TYPE 2 CONCRETE PAVERS
 - PROPOSED RIVERSTONE MULCH
 - PROPOSED STONEDUST
 - PROPOSED STOCK TANK PLANTER
 - PROPOSED TREE PROTECTION FENCING
 - PROPOSED 1.8m HT. WOOD BOARD FENCE
 - PROPOSED BENCH

Notes:
1. The faded pattern of this paved area is random. However, it is up to the builder to ensure that the specified ratios of brick type are laid within the correct boundaries to deliver the desired outcome.
2. See manufacturers specification for the exact paver size, color and application.



EXISTING TREE LIST

KEY	BOTANICAL NAME	COMMON NAME	SIZE	CRITICAL ROOT ZONE	OWNERSHIP	CONDITION	ACTION
DECIDUOUS TREES							
3-AC	<i>Acer negundo</i>	Manitoba Maple	200-250mm DBH	2000-3500mm	1347 Dorchester	FAIR-GOOD	TO REMAIN
*-AN	<i>Acer negundo</i>	Manitoba Maple	150-500mm DBH	1500-5000mm	1083 Merivale Ave	FAIR-GOOD	SEE PLAN
2-AP	<i>Acer platanoides</i>	Norway Maple	300-350mm DBH	3000-3500mm	1083 Merivale Ave	GOOD	SEE PLAN
1-EL	<i>Ulmus americana</i>	American Elm	2x400mm DBH	4000mm	1335 Dorchester	GOOD	SEE PLAN
3-JU	<i>Juniperus spp.</i>	Juniper	200mm DBH	2000mm	1083 Merivale Ave	GOOD	TO BE REMOVED
*-MA	<i>Malus spp.</i>	Crabapple	150-200mm DBH	1500-2000mm	1083 Merivale Ave	FAIR-GOOD	SEE PLAN
1-MP	<i>Acer negundo</i>	Manitoba Maple	500mm DBH	5000mm	1335 Dorchester	FAIR-GOOD	SEE PLAN
1-PG	<i>Picea glauca</i>	White Spruce	200mm DBH	2000mm	1077 Merivale Ave	FAIR	TO REMAIN
*-SE	<i>Ulmus pumila</i>	Siberian Elm	100-200mm DBH	2000-4000mm	1314 Emperor	GOOD	TO REMAIN
*-TO	<i>Thuja occidentalis</i>	White Cedar Hedge	1000-2000mm	1000-2000mm	Numerous	GOOD	TO REMAIN
1-UA	<i>Ulmus americana</i>	American Elm	650mm DBH	6500mm	1343 Dorchester	GOOD	SEE PLAN
*-UP	<i>Ulmus pumila</i>	Siberian Elm	300-350mm DBH	2000-4000mm	1083 Merivale Ave	FAIR-GOOD	SEE PLAN

PROPOSED PLANT LIST

KEY	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION	REMARKS
DECIDUOUS TREES						
FM	2	<i>Acer x freemanii</i>	Freeman's Maple	60mm dia.	B&B	
HB	1	<i>Celtis occidentalis</i>	Hackberry	60mm dia.	B&B	
GB	1	<i>Ginkgo biloba 'Princeton Sentry'</i>	Princeton Sentry Ginkgo	60mm dia.	B&B	Male tree
SB	2	<i>Amelanchier canadensis</i>	Serviceberry	60mm dia.	B&B	Single leader
SK	1	<i>Gleditsia triacanthos var. inermis 'Draves'</i>	Street Keeper Honeylocust	60mm dia.	B&B	
TC	1	<i>Tilia cordata</i>	Littleleaf Linden	60mm dia.	B&B	
SHRUBS						
An	38	<i>Hydrangea arborescens 'Annabelle'</i>	Annabelle Hydrangea	3 gal pot	Potted	Space 800mm o.c.
Cs	34	<i>Cornus sericea 'Farrow' Arctic Fire</i>	Red Twig Dogwood	600mm ht	Potted	Space 800mm o.c.
Fs	29	<i>Sorbaria sorbifolia 'Sem'</i>	Siam False Spirea	600mm ht	Potted	Space 800mm o.c.
Jh	18	<i>Juniperus horizontalis 'Sax Harbor'</i>	Sax Harbor Juniper	600mm spr	Potted	Space 800mm o.c.
Ph	3	<i>Hydrangea paniculata 'Peegee'</i>	Peegee Hydrangea	1200mm ht	Potted	Space 2000mm o.c.
Ra	30	<i>Ribes alpinum</i>	Alpine Currant	600mm ht	Potted	Space 800mm o.c.
Tm	5	<i>Taxus media 'Hicksii'</i>	Hick's Yew	1000mm ht	Potted	Space 1500mm o.c.

CLIENT
Shepherds of Good Hope
OTTAWA ONTARIO, CANADA

CONSULTANTS
ARCHITECTS:
CSV ARCHITECTS
sustainable design - conception écologique
613.564.8118 190 O Connor Street, Suite 100
www.csv.ca Ottawa, Ontario, K2P 2R3

SURVEYORS:
ANNIS, O'SULLIVAN, VOLLEBEKK LTD.
14 Concourse Gate, Suite 500
Nepean, Ont. K2E 7S8
Phone: (613) 727-0850 / Fax: (613) 727-1079
Email: nepean@osvltd.com

LEGEND (repeated)

1 ISSUED FOR SITE PLAN CONTROL 06/02/2023 ML JL
No. Issue Date ML/DR CK

JAMES B. LENNOX & ASSOCIATES INC.
LANDSCAPE ARCHITECTS
3332 CARLING AVE. OTTAWA, ONTARIO K2H 5A8
Tel. (613) 722-5168 Fax. (613) 343-3942

PROJECT
SGH 1083 MERIVALE
1083 MERIVALE ROAD, OTTAWA ON K1Z 6A9

DRAWING
TREE CONSERVATION REPORT & LANDSCAPE PLAN

STAMP
ASSOCIATION OF LANDSCAPE ARCHITECTS OF ONTARIO
MEMBER SINCE 2018

SCALE
AS SHOWN

START DATE
NOVEMBER 2022

PROJECT NO.

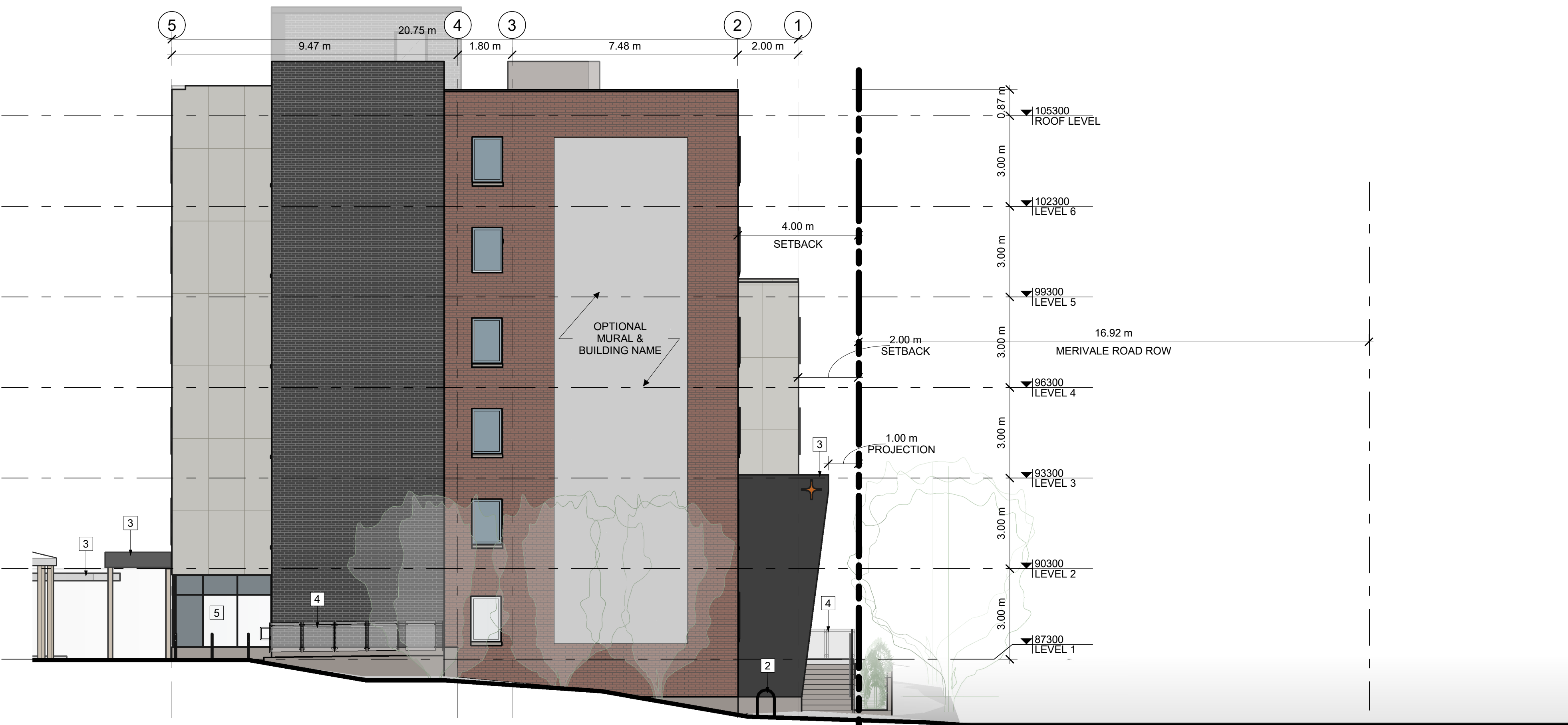
DRAWING NO.
L.1

PLOT SIZE ARCH-D

C Building Elevations



1 SPC EAST ELEVATION
A.300 | 1:100



2 SPC NORTH ELEVATION
A.300 | 1:100

ELEVATION NOTES:

CONTRACTOR TO COORDINATE WITH MANUFACTURER'S SPECIFICATIONS AND INSTALLATIONS GUIDELINES FOR ALL MATERIAL SEALING, TRANSITIONS, SEPARATIONS, FLASHING DETAILS, CONNECTIONS, ANCHORAGES, ETC. CONTRACTOR TO PROVIDE DETAIL DRAWINGS OR MOCK-UP FOR ARCHITECT'S REVIEW AND APPROVAL FOR ALL SPECIAL CONDITIONS NOT SHOWN IN THE CONTRACT DOCUMENTS.

ELEVATION KEYNOTES:

- 1 EXTERIOR LIGHTING
- 2 BICYCLE PARKING
- 3 CANOPY
- 4 GUARDRAIL
- 5 CURTAINWALL GLAZING - SPANDREL AND VISION

ELEVATION LEGEND:

HATCH PATTERNS SHOWN ARE FOR GRAPHIC PURPOSES ONLY AND SHOULD BE COORDINATED WITH ACTUAL SPECIFIED MATERIALS.

HATCH	SYMBOL	DESCRIPTION
		BRICK VENEER MANUFACTURER: TBD COLOUR(S): MEDIUM RED, DARK GREY
		METAL SIDING MANUFACTURER: TBD COLOUR: GRAPHITE
		FIBERCEMENT CLADDING MANUFACTURER: TBD COLOUR: OFF-WHITE
		ACCENT PANEL MANUFACTURER: TBS COLOUR(S): LIGHT BLUE, MEDIUM BLUE, NAVY BLUE



STAMP

REV	DATE	ISSUE
1	2023-06-02	ISSUED FOR SITE PLAN CONTROL

NOTES

1. OWNERSHIP OF THE COPYRIGHT OF THE DESIGN AND THE WORKS EXECUTED FROM THE DESIGN REMAINS WITH CSV ARCHITECTS, AND MAY NOT BE REPRODUCED IN ANY FORM WITHOUT THE WRITTEN CONSENT OF CSV ARCHITECTS.
2. THE DRAWINGS, PRESENTATIONS AND SPECIFICATIONS AS INSTRUMENTS OF SERVICE ARE AND SHALL REMAIN THE PROPERTY OF CSV ARCHITECTS. THEY ARE NOT TO BE USED BY THE CLIENT ON OTHER PROJECTS OR ON EXTENSIONS TO THIS PROJECT WITHOUT THE WRITTEN CONSENT OF CSV ARCHITECTS.
3. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER PROJECT DRAWINGS AND SPECIFICATIONS.
4. DO NOT SCALE DRAWINGS. CONTRACTOR SHALL BE RESPONSIBLE TO VERIFY DIMENSIONS ON SITE.
5. ALL WORK SHALL BE IN ACCORDANCE WITH THE ONTARIO BUILDING CODE AND ALL SUPPLEMENTS AND APPLICABLE MUNICIPAL REGULATIONS.

CLIENT

SHEPHERDS OF GOOD HOPE
OTTAWA
ONTARIO, CANADA

PROJECT

SGH 1083 MERIVALE

1083 Merivale Road
Ottawa, ON K1Z 6A9

TITLE

ELEVATIONS

PROJECT NO: 2021-0111
DRAWN: SF
APPROVED: JS
SCALE: As indicated
DATE PRINTED: 2023-06-02 4:34:40 PM

REV DRAWING NO.

1 A.300



ELEVATION NOTES:

CONTRACTOR TO COORDINATE WITH MANUFACTURER'S SPECIFICATIONS AND INSTALLATIONS GUIDELINES FOR ALL MATERIAL SEALING, TRANSITIONS, SEPARATIONS, FLASHING DETAILS, CONNECTIONS, ANCHORAGES, ETC. CONTRACTOR TO PROVIDE DETAIL DRAWINGS OR MOCK-UP FOR ARCHITECT'S REVIEW AND APPROVAL FOR ALL SPECIAL CONDITIONS NOT SHOWN IN THE CONTRACT DOCUMENTS.

ELEVATION KEYNOTES:

- 1 EXTERIOR LIGHTING
- 2 BICYCLE PARKING
- 3 CANOPY
- 4 GUARDRAIL
- 5 CURTAINWALL GLAZING - SPANDREL AND VISION

ELEVATION LEGEND:

HATCH PATTERNS SHOWN ARE FOR GRAPHIC PURPOSES ONLY AND SHOULD BE COORDINATED WITH ACTUAL SPECIFIED MATERIALS.

HATCH	SYMBOL	DESCRIPTION
		BRICK VENEER MANUFACTURER: TBD COLOUR(S): MEDIUM RED, DARK GREY
		METAL SIDING MANUFACTURER: TBD COLOUR: GRAPHITE
		FIBERCEMENT CLADDING MANUFACTURER: TBD COLOUR: OFF-WHITE
		ACCENT PANEL MANUFACTURER: TBS COLOUR(S): LIGHT BLUE, MEDIUM BLUE, NAVY BLUE

STAMP

1 2023-06-02 ISSUED FOR SITE PLAN CONTROL
REV DATE ISSUE

NOTES

1. OWNERSHIP OF THE COPYRIGHT OF THE DESIGN AND THE WORKS EXECUTED FROM THE DESIGN REMAINS WITH CSV ARCHITECTS, AND MAY NOT BE REPRODUCED IN ANY FORM WITHOUT THE WRITTEN CONSENT OF CSV ARCHITECTS.
2. THE DRAWINGS, PRESENTATIONS AND SPECIFICATIONS AS INSTRUMENTS OF SERVICE ARE AND SHALL REMAIN THE PROPERTY OF CSV ARCHITECTS. THEY ARE NOT TO BE USED BY THE CLIENT ON OTHER PROJECTS OR ON EXTENSIONS TO THIS PROJECT WITHOUT THE WRITTEN CONSENT OF CSV ARCHITECTS.
3. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER PROJECT DRAWINGS AND SPECIFICATIONS.
4. DO NOT SCALE DRAWINGS. CONTRACTOR SHALL BE RESPONSIBLE TO VERIFY DIMENSIONS ON SITE.
5. ALL WORK SHALL BE IN ACCORDANCE WITH THE ONTARIO BUILDING CODE AND ALL SUPPLEMENTS AND APPLICABLE MUNICIPAL REGULATIONS.

CLIENT

SHEPHERDS OF GOOD HOPE
OTTAWA
ONTARIO, CANADA

PROJECT

SGH 1083 MERIVALE

1083 Merivale Road
Ottawa, ON K1Z 6A9

TITLE

ELEVATIONS

PROJECT NO: 2021-0111
DRAWN: SF
APPROVED: JS
SCALE: As indicated
DATE PRINTED: 2023-06-02 4:34:51 PM

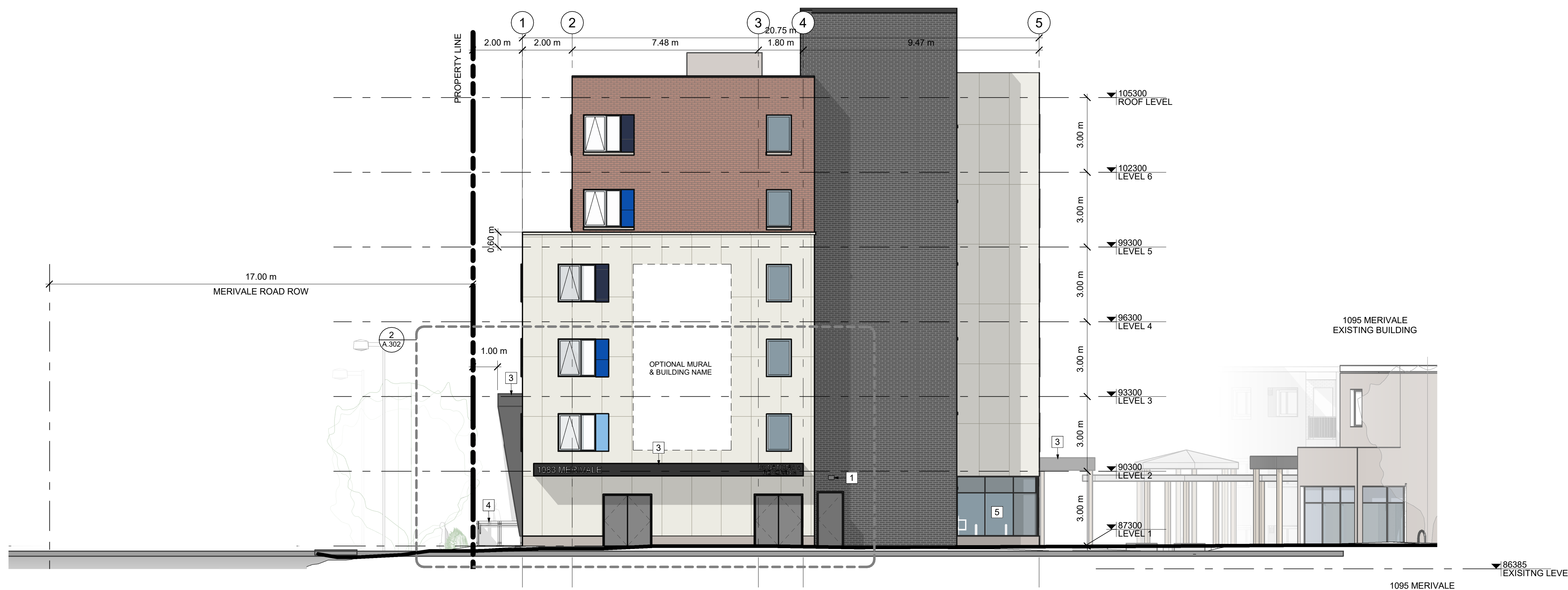
REV DRAWING NO.

1

A.301



1 SPC WEST ELEVATION
A.301 1:100



2 SPC SOUTH ELEVATION
A.301 1:100



STAMP

1 2023-06-02 ISSUED FOR SITE PLAN CONTROL
REV DATE ISSUE

NOTES

1. OWNERSHIP OF THE COPYRIGHT OF THE DESIGN AND THE WORKS EXECUTED FROM THE DESIGN REMAINS WITH CSV ARCHITECTS, AND MAY NOT BE REPRODUCED IN ANY FORM WITHOUT THE WRITTEN CONSENT OF CSV ARCHITECTS.
2. THE DRAWINGS, PRESENTATIONS AND SPECIFICATIONS AS INSTRUMENTS OF SERVICE ARE AND SHALL REMAIN THE PROPERTY OF CSV ARCHITECTS. THEY ARE NOT TO BE USED BY THE CLIENT ON OTHER PROJECTS OR ON EXTENSIONS TO THIS PROJECT WITHOUT THE WRITTEN CONSENT OF CSV ARCHITECTS.
3. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER PROJECT DRAWINGS AND SPECIFICATIONS.
4. DO NOT SCALE DRAWINGS. CONTRACTOR SHALL BE RESPONSIBLE TO VERIFY DIMENSIONS ON SITE.
5. ALL WORK SHALL BE IN ACCORDANCE WITH THE ONTARIO BUILDING CODE AND ALL SUPPLEMENTS AND APPLICABLE MUNICIPAL REGULATIONS.

CLIENT

SHEPHERDS OF GOOD HOPE
OTTAWA
ONTARIO, CANADA

PROJECT

SGH 1083 MERIVALE

1083 Merivale Road
Ottawa, ON K1Z 6A9

TITLE

STREET LEVEL ELEVATIONS

PROJECT NO: 2021-0111
DRAWN: SF
APPROVED: JS
SCALE: 1:50
DATE PRINTED: 2023-06-02 4:35:03 PM

REV DRAWING NO.

1

A.302



1 SPC WEST ELEVATION - MERIVALE ENTRANCE

A.302 1:50



2 SPC SOUTH ELEVATION - RECEIVING ENTRANCE

A.302 1:50

3 SPC EAST ELEVATION - COURTYARD ENTRANCE

A.302 1:50

ELEVATION NOTES:

CONTRACTOR TO COORDINATE WITH MANUFACTURER'S SPECIFICATIONS AND INSTALLATIONS GUIDELINES FOR ALL MATERIAL SEALING, TRANSITIONS, SEPARATIONS, FLASHING DETAILS, CONNECTIONS, ANCHORAGES, ETC. CONTRACTOR TO PROVIDE DETAIL DRAWINGS OR MOCK-UP FOR ARCHITECT'S REVIEW AND APPROVAL FOR ALL SPECIAL CONDITIONS NOT SHOWN IN THE CONTRACT DOCUMENTS.

HATCH PATTERNS SHOWN ARE FOR GRAPHIC PURPOSES ONLY AND SHOULD BE COORDINATED WITH ACTUAL SPECIFIED MATERIALS.