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## 47 Beechwood Avenue Planning Rationale



Prepared for: 2317916 Ontario Inc.

Engineering excellence.

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**47 Beechwood Avenue**

**Ottawa, Ontario**

**Planning Rationale**

**in support of**

**Minor Zoning By-law Amendment and**

**Site Plan Control Applications**

Prepared For:

**2317916 Ontario Inc.**

Prepared By:

**NOVATECH**

Suite 200, 240 Michael Cowpland Drive

Ottawa, Ontario

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June / 13 / 2023

Novatech File: 122186

Ref: R-2023-091

June 13, 2023

City of Ottawa  
Planning, Real Estate and Economic Development Department  
110 Laurier Avenue West, 4<sup>th</sup> Floor  
Ottawa, ON, K1P 1J1

**Attention: Colette Gorni, Planner II, Development Review, Central**

**Reference: Minor Zoning By-law Amendment and Site Plan Control Applications  
47 Beechwood Avenue  
Our File No.: 122186**

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The following Planning Rationale has been prepared in support of Minor Zoning By-law Amendment and Site Plan Control applications to facilitate the development of the property at 47 Beechwood Avenue (the "Subject Property").

The Subject Property is within the Inner Urban Transect and is designated Mainstreet Corridor on Schedule B2 of the City of Ottawa's Official Plan. The Subject Property is located within the Evolving Neighbourhood Overlay. The property is zoned Traditional Mainstreet, Subzone 8 (TM8) and is located within the Mature Neighbourhoods Overlay in the City of Ottawa's Zoning By-law 2008-250.

It is proposed to construct an eight-storey, mixed-use building on the Subject Property. The building will include four commercial units on the ground floor and 121 residential dwelling units. A minor rezoning application is required to provide relief from zoning provisions on the Subject Property to decrease the required building setback in the front yard above the third storey, decrease the required building setback in the rear yard above the third storey, increase the maximum permitted building height, alter the required angular plane for the proposed building from the rear lot line, reduce the minimum required landscaped area abutting the rear lot line reduce the minimum communal amenity area requirement and to reduce the minimum parking space requirement for residential units.

A one-level underground parking garage will provide 30 secure parking spaces for residents and 17 parking spaces for retail and visitors. The parking spaces will be accessible via a driveway and ramp off Springfield Road. A drop-off space and loading area under a cantilevered portion of the building will be accessed via a driveway off Douglas Avenue. Walkways from Springfield Road, Beechwood Avenue and Douglas Avenue are proposed along the west, south and east property lines. The proposed walkways will facilitate pedestrian and bicycle access off Springfield Road and Douglas Avenue to building entrances and bicycle parking located near building entrances and within the building. Two indoor amenity areas with common access for residents are provided on the second floor of the building. A communal outdoor terrace on level two with barrier free access for residents is provided off the common corridor. No impacts to traffic or existing land uses surrounding the Subject Property are anticipated as a result of the proposed development. A concurrent site plan control application is being filed as part of the submission package.



This Planning Rationale examines the location and context of the Subject Property, the planning policy and regulatory framework applicable to the site and makes recommendations on the Minor Zoning By-law Amendment and Site Plan Control applications required to facilitate the proposed development on the Subject Property.

Should you have any questions regarding any aspect of these applications please feel free to contact me at your earliest convenience.

Yours truly,

**NOVATECH**

Jeffrey Kelly, MCIP RPP  
Project Planner

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## 1.0 INTRODUCTION

Novatech has prepared this Planning Rationale in support of Minor Zoning By-law Amendment and Site Plan Control applications for the property at 47 Beechwood Avenue (the “Subject Property”). The proposed development consists of an eight-storey, mixed-use building with four commercial units located on the ground floor and 121 residential units. A one-level underground parking garage will provide 30 secure parking spaces for residents and 17 parking spaces for retail and visitors. A drop-off space and loading area under a cantilevered portion of the building will be accessed via a driveway off Douglas Avenue. Walkways from Springfield Road, Beechwood Avenue and Douglas Avenue are proposed along the west, south and east property lines. The proposed walkways will facilitate pedestrian and bicycle access off Springfield Road and Douglas Avenue to building entrances and bicycle parking located along near building entrances and within the building. The proposed development will require demolition of existing buildings, modifications to site grading, drainage and landscaping on the property.

The Subject Property is within the Inner Urban Transect and is designated Mainstreet Corridor on Schedule B2 of the City of Ottawa’s Official Plan. The Subject Property is located within the Evolving Neighbourhood Overlay. The property is zoned Traditional Mainstreet, Subzone 8 (TM8) and is located within the Mature Neighbourhoods Overlay in the City of Ottawa’s Zoning By-law 2008-250.

This Planning Rationale will demonstrate that the proposed Minor Zoning By-law Amendment and Site Plan Control applications will:

- be consistent with the policies of the Provincial Policy Statement (2020);
- conform to the policies of the City of Ottawa Official Plan (2021);
- adhere to relevant Urban Design Guidelines applicable to the Subject Property;
- establish appropriate Zoning standards for the Subject Property; and
- maintain compatibility with the surrounding uses and community.

1.1 Description of Subject Property

Figure 1. Aerial Photo of Subject Property

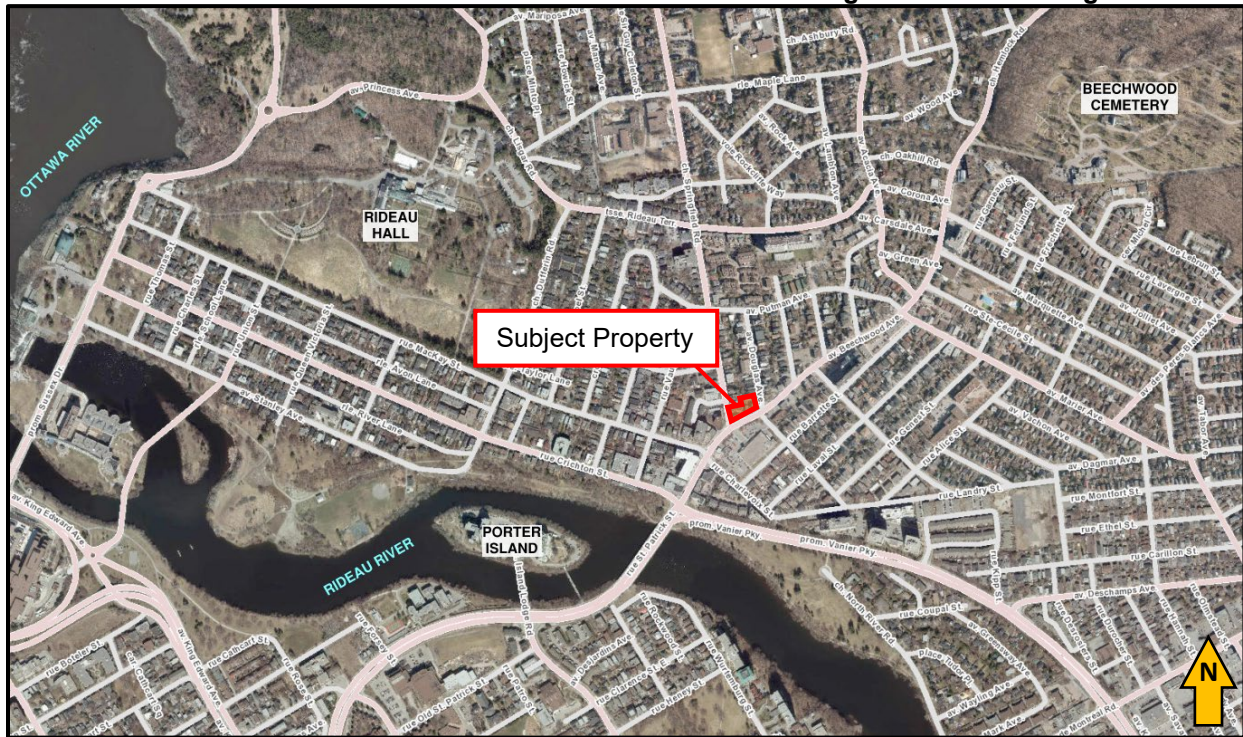


The Subject Property is located in Ward 13 (Rideau-Rockcliffe) in the City of Ottawa. The Subject Property is located in an area bounded by Putman Avenue to the north, Beechwood Avenue to the south, Douglas Avenue to the east, and Springfield Road to the west (see Figure 1). The Subject Property is currently occupied by low-rise commercial use buildings with residential units above and surface parking. The Subject Property is a corner through lot with an area of 1,930.5 square metres, approximately 62 metres of frontage on Beechwood Avenue, approximately 31 metres of frontage on Springfield Road and approximately 31 metres of frontage on Douglas Avenue.



1.2 Site Location and Community Context

Figure 2: Surrounding Context



The Subject Property is located within the Lindenlea neighbourhood. Directly to the south of the Subject Property across Beechwood Avenue is a grocery store with a surface parking lot. The Vanier Parkway and Rideau River are located further to the south. To the north of the Subject Property is predominantly a mix of low-rise residential uses. West of the Subject Property along Beechwood Avenue is predominantly characterized by a mix of low to mid-rise buildings containing offices, retail uses at grade and residential uses above. Further to the west is Rideau Hall and the Ottawa River. East of the Subject Property along Beechwood Avenue is predominantly characterized by a mix of low rise buildings containing retail uses at grade and residential uses above. Further to the east of the Subject Property is characterized by a mix of low to mid-rise buildings and Beechwood Cemetery (see Figure 2).

Additional details are provided in Figures 3 to 7 and in the descriptions below.

**Figure 3: Land uses to the north of the Subject Property (Springfield Road)**



**North:** To the north of the Subject Property along Springfield Road are low-rise retail and residential uses.

**Figure 4: Land uses to the north of the Subject Property (Douglas Avenue)**



**North:** To the north of the Subject Property along Douglas Avenue are low-rise residential uses.

**Figure 5: Land uses to the south of the Subject Property**



**South:** To the south of the Subject Property is a grocery store with surface parking.

**Figure 6: Land uses to the west of the Subject Property**



**West:** To the west of the Subject Property along Beechwood Avenue is a mix of low to mid-rise buildings containing offices, retail uses at grade and residential uses above.

Figure 7: Land uses to the east of the Subject Property



**East:** To the east of the Subject Property is a mix of low rise buildings containing retail uses at grade and some with residential uses above.

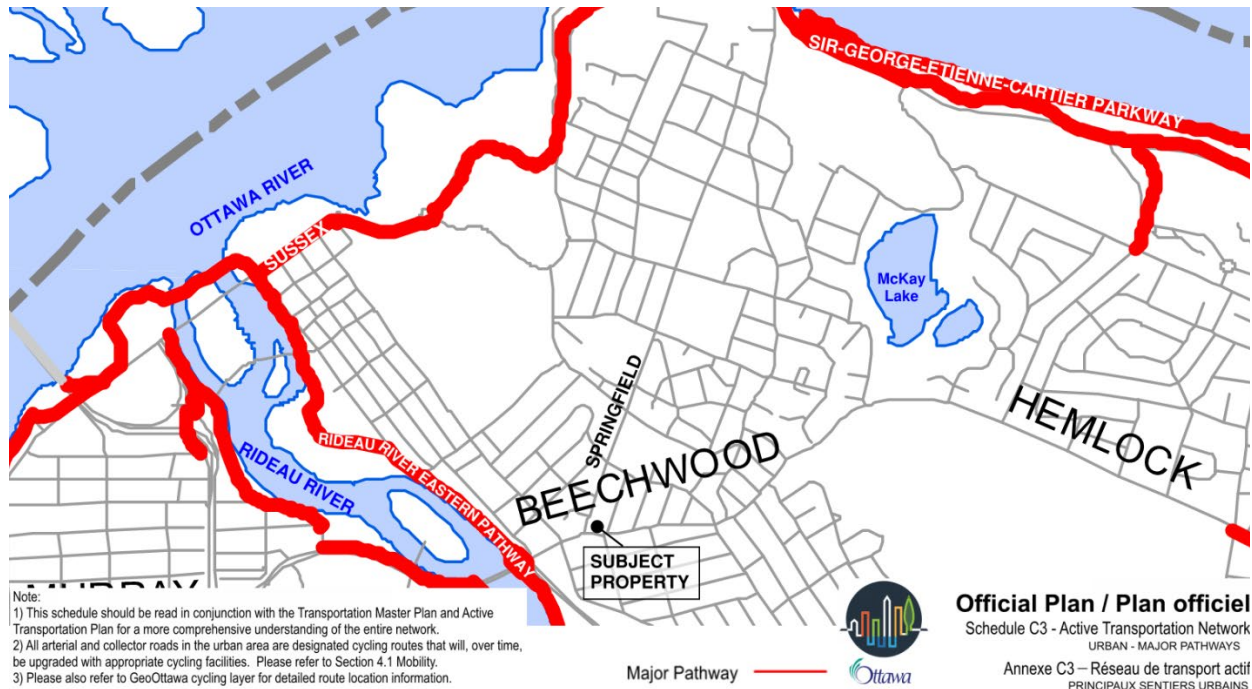
### 1.3 Linkages and Transportation Framework

Figure 8: Official Plan Schedule C4 Excerpt



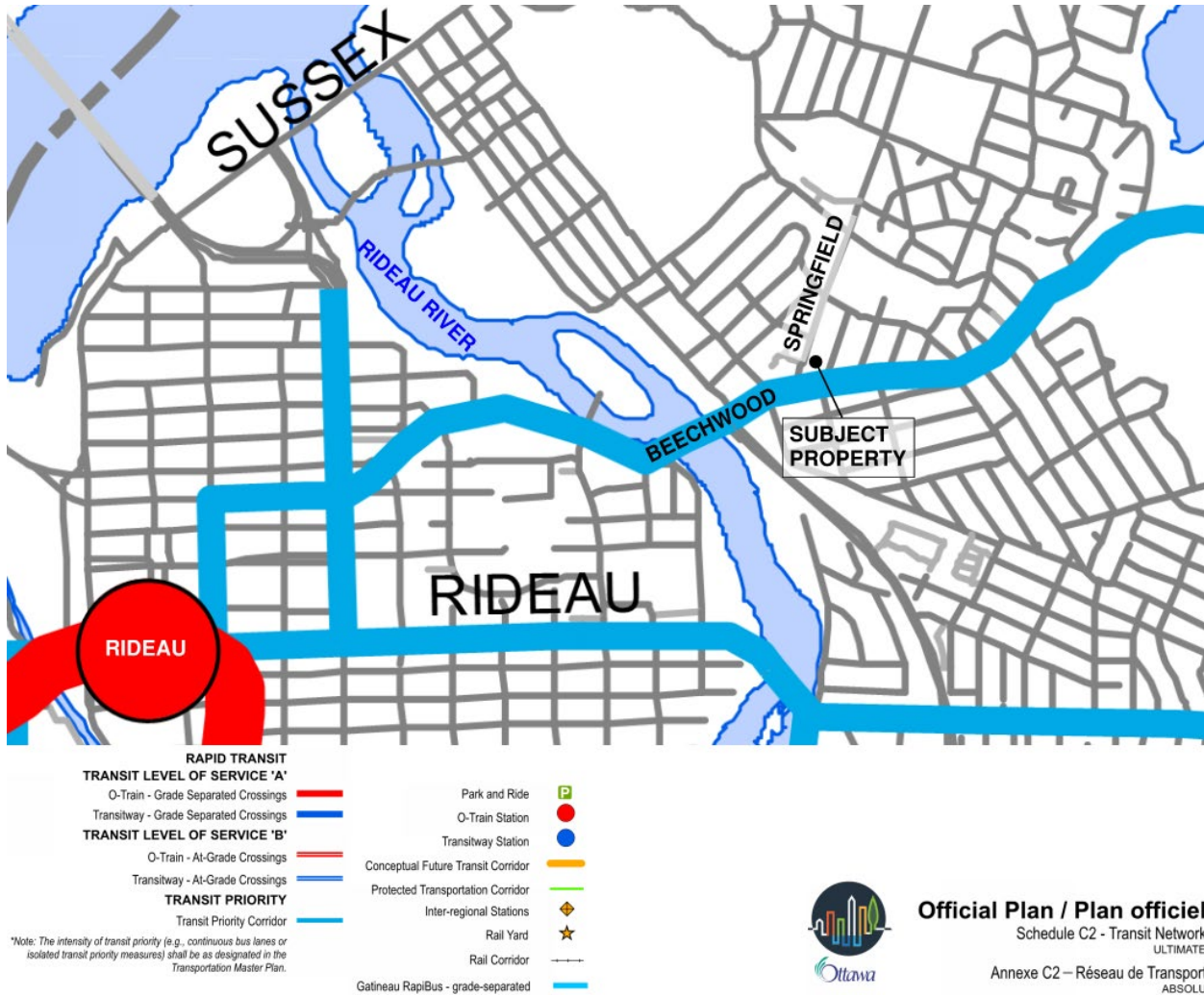
The Subject Property has frontage on Beechwood Avenue and Springfield Road. Beechwood Avenue is designated as an arterial road and Springfield Road is designated as a collector road on Schedule C4 of the Official Plan (see Figure 8).

Figure 9: Official Plan Schedule C3 Excerpt



The Subject Property is located north of the Rideau River Eastern Pathway which is designated as a Major Pathway on Schedule C3 of the Official Plan (see Figure 9). All arterial and collector roads in the urban area are designated cycling routes. There are public sidewalks located along Springfield Road, Douglas Avenue and the north and south sides of Beechwood Avenue. Pedestrian and bicycle access is proposed off Springfield Road and Douglas Avenue to facilitate access to commercial units at grade and to bicycle parking located within the building.

Figure 10: Official Plan Schedule C2 Excerpt



Beechwood Avenue is designated as a Transit Priority Corridor on Schedule C2 of the Official Plan. The Rideau O-Train station is located to the west of the Subject Property across the Rideau River (see Figure 10).

Figure 11. Walking Distance to Transit Network



The Subject Property is located along bus transit route options with connections to rapid transit options nearby (see Figure 11). Bus routes 6 and 7 service the Subject Property directly with a bus stop provided on the west side of the property along Springfield Road and on the south side of Beechwood Avenue across from the Subject Property. Bus routes 6 and 7 are designated as frequent routes offering high frequency bus service along major roads on the OC Transpo network.

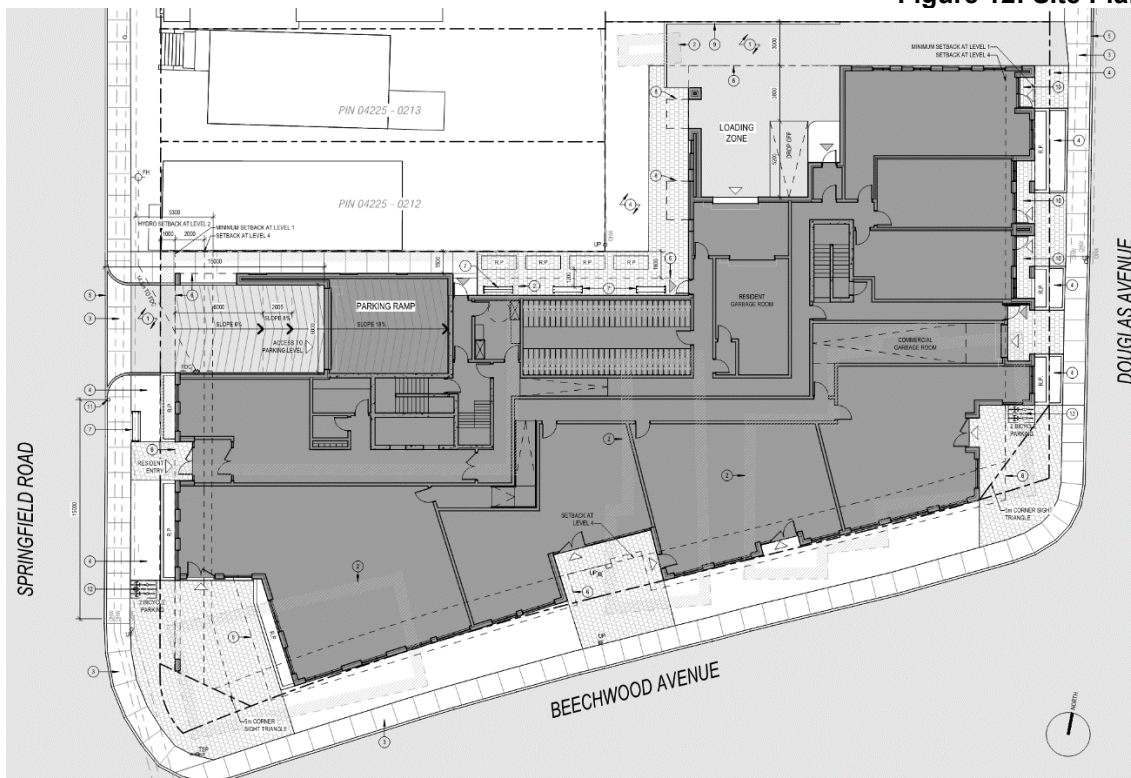
## 2.0 DEVELOPMENT PROPOSAL

It is proposed to develop an eight-storey, mixed-use building on the Subject Property. The proposed building will have four commercial units on the ground floor. 121 residential units will be provided ranging in size from studio to 3-bedroom apartments. The principal access to the residential units will be provided off Springfield Road and Douglas Avenue. Access to commercial units is provided off Beechwood Avenue. 30 secure underground motor vehicle parking spaces are provided for residents, 18 retail and visitor parking spaces are provided with access off Springfield Road. Bicycle parking spaces are provided at building entrances along Beechwood Avenue and within the building on the ground level.

A Minor Zoning By-law Amendment application is required to facilitate the proposed development on the Subject Property. Site-specific amendments for the Subject Property are required to incorporate the required relief from the zoning provisions to decrease the required building setback in the front yard above the third storey, decrease the required building setback in the rear yard above the third storey, increase the maximum permitted building height, alter the required angular plane for the proposed building from the rear lot line, reduce the minimum required landscaped area abutting the rear lot line, reduce the minimum communal amenity area requirement and to reduce the minimum number of required parking spaces for residential units.

A detailed Site Plan Control application is required to facilitate development on the Subject Property and is being filed as part of this submission. The proposed development will require demolition of the existing buildings, modifications to site grading, drainage and landscaping. The proposed site plan is attached as Appendix A (see Figure 12).

Figure 12: Site Plan





### 3.0 PLANNING POLICY AND REGULATORY FRAMEWORK

#### 3.1 Provincial Policy Statement

The Provincial Policy Statement (2020) provides policy direction on land use planning and development matters of provincial interest. The PPS was issued under the authority of Section 3 of the Planning Act and came into effect on May 1, 2020. All decisions affecting planning matters “shall be consistent with” policies issued under Section 3 of the Planning Act.

Section 1.1 of the PPS provides policies to manage and direct land use to achieve efficient and resilient development. Policy 1.1.1 states:

*“1.1.1 Healthy, liveable and safe communities are sustained by:*

- a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*
- b) accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
- c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
- d) avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;*
- e) promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;*
- f) improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;*
- g) ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs;*
- h) promoting development and land use patterns that conserve biodiversity; and*
- i) preparing for the regional and local impacts of a changing climate”*

The PPS defines “intensification” as:

*“the development of a property, site or area at a higher density than currently exists through:*

- a) redevelopment, including the reuse of brownfield sites;*
- b) the development of vacant and/or underutilized lots within previously developed areas;*
- c) infill development; and*
- d) the expansion or conversion of existing buildings”*

The proposed mixed-use development on the Subject Property will promote the efficient use of land through redevelopment of an underutilized site within the City's urban area. The proposed infill development is an example of intensification. The mixed-use building will provide 121 dwelling units on a site that is comprised of low-rise commercial and retail uses with residential units above. The proposed development will provide a ground-floor retail and commercial component fronting on Beechwood Avenue to enhance and activate the Beechwood Avenue corridor. The location of the development along a Mainstreet is well suited to intensification with bus stops on frequent transit routes located immediately adjacent to the proposed building along Springfield Road and on the south side of Beechwood Avenue. Residents of the building will have access to a grocery store, several restaurants, retail stores, and other amenities along Beechwood Avenue. The proposed building has direct access at grade to the sidewalks along Springfield Road, Douglas Avenue and Beechwood Avenue. The proposed building design removes potential barriers to accessibility for pedestrians.

Section 1.1.3 of the PPS defines and lays out policies for settlement areas. The Subject Property is considered part of a settlement area, as it is within the Urban Area for the City of Ottawa. Policy 1.1.3.1 states: "*Settlement areas shall be the focus of growth and development.*" Policy 1.1.3.2 states:

*"1.1.3.2 Land use patterns within settlement areas shall be based on densities and a mix of land uses which:*

- a) efficiently use land and resources;*
- b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
- c) minimize negative impacts to air quality and climate change, and promote energy efficiency;*
- d) prepare for the impacts of a changing climate;*
- e) support active transportation;*
- f) are transit-supportive, where transit is planned, exists or may be developed; and*
- g) are freight-supportive."*

The Minor Zoning By-law Amendment and Site Plan Control applications will facilitate the development of an eight-storey, mixed-use building on the Subject Property. The proposed development is consistent with Policy 1.1.3.1 and 1.1.3.2 of the PPS as it represents an efficient use of an existing fully serviced property and existing municipal infrastructure. The property is within proximity of designated cycling routes along Beechwood Avenue and promotes active transportation opportunities for residents, commercial tenants and customers. The Subject Property is within immediate proximity of transit stops along Beechwood Avenue located along routes 6 and 7 on the frequent transit network. The proposed development is transit supportive.

Policy 1.1.3.3 states:

*"1.1.3.3 Planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated taking into account existing building stock or*

*areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.”*

The Subject Property is underutilized and can accommodate intensification and redevelopment. The proposed development is situated in an appropriate location to establish transit-supportive development. The proposed eight-storey, mixed-use building on the Subject Property will increase the City’s housing supply and provide a range of housing options within proximity to the existing active transportation routes and transit stops located along the frequent transit network.

Policy 1.1.3.4 states:

*“1.1.3.4 Appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety.”*

The proposed development is subject to the City of Ottawa’s Site Plan Control process as a more intensive mixed-use development is proposed on the Subject Property.

Section 1.3 of the PPS provides policy direction for employment areas and includes the following policy:

*“1.3.1 Planning authorities shall promote economic development and competitiveness by:*

- d) encouraging compact, mixed-use development that incorporates compatible employment uses to support liveable and resilient communities, with consideration of housing policy 1.4;”*

The proposed mixed-use building is consistent with policy 1.3.1 of the PPS as it will provide a mix of uses, including employment uses along the Beechwood Avenue corridor. The proposed development allows residents access to shopping and commercial uses, as well as potential employment opportunities.

Section 1.4 of the PPS sets out policies for housing. Policy 1.4.3 states:

*“1.4.3 Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs of current and future residents of the regional market area by:*

- b) permitting and facilitating:
  - a. all housing options required to meet the social, health, economic and well-being requirements of current and future residents, including special needs requirements and needs arising from demographic changes and employment opportunities; and*
  - b. all types of residential intensification, including additional residential units, and redevelopment in accordance with policy 1.1.3.3;**
- c) directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;*

- d) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed;
- e) requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations; and

The proposed eight-storey, mixed-use building on the Subject Property is an example of residential intensification directed to an area where appropriate levels of infrastructure and public service facilities exist to support the needs of the community. The proposed development will make efficient use of underutilized land and be supportive of active transportation and transit in an area where suitable infrastructure is in place. The Subject Property is located in an area where intensification is appropriate and supports the use of transit given proximity to the existing active transportation corridors and direct access to bus stops along the frequent transit network. Commercial uses located on the ground floor fronting on Beechwood Avenue will encourage walking and cycling in the area.

Section 1.6.6 of the PPS sets out policies for Sewer, Water and Stormwater. Policy 1.6.6.2 of the PPS states: “Within settlement areas with existing municipal sewage services and municipal water services, intensification and redevelopment shall be promoted wherever feasible to optimize the use of the services.” The proposed development on the Subject Property represents intensification from the current use. The proposed development of a mid-rise, mixed-use building has been designed to align with and optimize existing municipal infrastructure capacity and avoid the requirement for servicing upgrades on the Subject Property.

Section 1.6.7 of the PPS sets out policies for transportation systems. Policy 1.6.7.4 states: “A land use pattern, density and mix of uses should be promoted that minimize the length and number of vehicle trips and support current and future use of transit and active transportation.” The proposed development of an eight-storey, mixed-use building with limited off-street motor vehicle parking provided on the Subject Property will introduce a level of density that promotes the viability of active transportation and transit use and contributes to minimizing the required length and number of vehicle trips for residents and visitors.

Section 1.6.8 of the PPS sets out policies for transportation and infrastructure corridors. Policy 1.6.8.3 states: “New development proposed on adjacent lands to existing or planned corridors and transportation facilities should be compatible with, and supportive of, the long-term purposes of the corridor and should be designed to avoid, mitigate or minimize negative impacts on and from the corridor and transportation facilities.” The Subject Property abuts Beechwood Avenue and Springfield Road. Beechwood Avenue and Springfield Road are each designated as cycling routes on Schedule C3 and Schedule C4 of the Official Plan. Transit stops along routes 6 and 7 of the frequent transit network are located immediately abutting the Subject Property. The proposed development of an eight-storey, mixed-use building on the Subject Property will be compatible with the existing use of the Beechwood Avenue corridor, will be transit-supportive and designed to create no negative impacts on the existing or planned function of transportation corridors in the area.

Section 1.8 of the PPS provides policy direction related to energy conservation, air quality, and climate change. Policy 1.8.1 states:

*“1.8.1 Planning authorities shall support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and preparing for the impacts of a changing climate through land use and development patterns which:*

- e) *encourage transit-supportive development and intensification to improve the mix of employment and housing uses to shorten commute journeys and decrease transportation congestion;”*

The proposed development meets the climate change objectives and policies of the PPS by providing intensification in proximity to active transportation routes and rapid transit. The introduction of ground-oriented commercial uses and proximity to bus stops on frequent transit routes increases opportunities for residents to fulfill most of their daily needs by walking, cycling, or taking transit.

**The proposed Minor Zoning By-law Amendment and Site Plan Control applications are consistent with the policies of the Provincial Policy Statement.**

### 3.2 City of Ottawa Official Plan

The City of Ottawa Official Plan (the “Official Plan”) received final approval, as modified by the Minister of Municipal Affairs and Housing on November 4<sup>th</sup>, 2022. For the purposes of this Planning Rationale, the Official Plan as modified by the Minister of Municipal Affairs and Housing was used for reference.

**Figure 13. Official Plan Schedule B2 Excerpt**



### 3.2.1 Land Use Designation

The Subject Property is designated “Mainstreet Corridor” and “Evolving Neighbourhood Overlay” in the Inner Urban Transect on Schedule B2 of the Official Plan (see Figure 13).

Section 5.2 of the Official Plan sets out general policies and guidance for proposed development within the Inner Urban Transect.

Section 5.2.1, Policy 4 of the Official Plan states that:

- “4) The Inner Urban Transect shall continue to develop as a mixed-use environment, where:*
- a) Hubs and a network of Mainstreets and Minor Corridors provide residents with a full range of services within a walking distance from home, in order to support the growth of 15-minute neighbourhoods;*
  - b) Small, locally oriented services may be appropriately located within Neighbourhoods;*
  - c) Existing and new cultural assets are supported, including those that support music and nightlife;*
  - d) Larger employment uses are directed to Hubs and Corridors; and*
  - e) Increases in existing residential densities are supported to sustain the full range of services noted in Policy a).”*

Policies of the Official Plan direct areas designated “Mainstreets” to accommodate increased residential growth and mixed-use development that support a wide variety of housing types and services for residents in proximity to home. The proposed development of a mixed-use building on the Subject Property is consistent with policies of the Official Plan for Mainstreet Corridors within the Inner Urban Transect.

Section 5.2.3, Policy 2 of the Official Plan states that:

- “2) Along Mainstreets, permitted building heights are as follows, subject to appropriate height transitions, setbacks, and angular planes:*
- a) On sites that front on segments of streets whose right-of-way (after widening requirements have been exercised) is 30 metres or greater as identified in Schedule C16 for the planned street context, and where the parcel is of sufficient size to allow for a transition in built form massing, not less than 2 storeys and up to High-rise;*
  - b) On sites that front on segments of streets whose right-of-way is narrower than 30 metres, generally up to 9 storeys except where a secondary plan or area-specific policy specifies different heights; and*
  - c) In all cases:*
    - i) The wall heights directly adjacent to a street, and the heights of the podiums of High-rise buildings, where permitted, shall be proportionate to the width of the abutting right of way, and consistent with the objectives in the urban design section on Mid-rise and High-rise built form in Subsection 4.6.6, Policies 7), 8) and 9); and*
    - ii) The height of such buildings may be limited further on lots too small to accommodate an appropriate height transition.”*

The Subject Property is designated as Mainstreet on Schedule B2 of the Official Plan. The Beechwood Avenue right-of-way is less than 30 metres in width abutting the Subject Property. A building height of up to 9 storeys on the Subject Property would be supported by this policy of the Official Plan.

Section 5.6.1 of the Official Plan provides policy direction for Built Form Overlays, including the Evolving Neighbourhoods Overlay.

Section 5.6.1.1, Policy 1 of the Official Plan states that:

*“1) The Evolving Overlay will apply to areas that are in a location or at stage of evolution that create the opportunity to achieve an urban form in terms of use, density, built form and site design. These areas are proximate to the boundaries of Hubs and Corridors as shown in the B-series of schedules of this Plan. The Evolving Overlay will be applied generally to the properties that have a lot line along a Minor Corridor; lands 150 metres from the boundary of a Hub or Mainstreet designation; and to lands within a 400-metre radius of a rapid transit station. The Overlay is intended to provide opportunities that allow the City to reach the goals of its Growth Management Framework for intensification through the Zoning By-law, by providing:*

- a) Guidance for a gradual change in character based on proximity to Hubs and Corridors.*
- b) Allowance for new building forms and typologies, such as missing middle housing;*
- c) Direction to built form and site design that support an evolution towards more urban built form patterns and applicable transportation mode share goals; and*
- d) Direction to govern the evaluation of development.*

The Subject Property is suitable for intensification being located within the Evolving Overlay. The proposed development of an eight-storey, mixed-use building on the Subject Property is consistent with City objectives to establish a more urban community character and to create opportunities for infill and intensification within walking distance of bus stops along frequent transit routes in the Inner Urban Transect.

Section 5.6.1.1, Policy 2 of the Official Plan states that:

*“2) Where an Evolving overlay is applied:*

- a) The Zoning By-law shall provide development standards for the built form and buildable envelope consistent with the planned characteristics of the overlay area, which may differ from the existing characteristics of the area to which the overlay applies; and*
- b) The Zoning By-law shall include minimum-density requirements as identified in Table 3a, and permissions to meet or exceed the density targets of Table 3b.”*

For Mainstreet Corridors, the required minimum density is 120 dwelling units per net hectare. The proposed development meets the required minimum density, as it provides approximately 785 dwelling units per net hectare.

Section 5.6.1.1, Policy 6 of the Official Plan states that:

*“6) Zoning By-law development standards and development on lands with an Evolving Overlay should generally include built form and site design attributes that meet most of the urban characteristics described in Table 6 in Section 5, and where suburban attributes are retained, that these do not structurally impede the achievement of a fully urban site design over time.”*

The urban characteristics outlined in Table 6 of Section 5 of the Official Plan are as follows:

- *Shallow front yard setbacks and in some contexts zero front yards with an emphasis on built-form relationship with the public realm*
- *Principal entrances at grade with direct relationship to public realm*
- *Range of lot sizes that will include smaller lots, and higher lot coverage and floor area ratios*
- *Minimum of two functional storeys*
- *Buildings attached or with minimal functional side yard setbacks*
- *Small areas of formal landscape that should include space for soft landscape, trees and hard surfacing*
- *No automobile parking, or limited parking that is concealed from the street and not forming an integral part of a building, such as in a front facing garage*

The proposed development has a zero metre front yard setback after road widening requirements and three metre corner side yard setbacks to establish a strong interface with the public realm along Springfield Road, Douglas Avenue and Beechwood Avenue. To create an active street frontage, principal entrances to the commercial portions of the building are located facing Beechwood Avenue. Access to the residential lobby is provided off both Springfield Road for residents and visitors. The principal entrances are at grade with the street level to provide direct access and reduce accessibility concerns for residents and visitors. The proposed development has limited off-street parking located entirely underground. The entrance ramp to the parking garage is located to the west extent of the building along Springfield Road. A separate drop-off space, service and loading area is provided via a driveway off Douglas Avenue. All vehicular access to the Subject Property is separated from principal building entrances and active frontages along Springfield Road, Douglas Avenue and Beechwood Avenue.

Section 6.2 of the Official Plan sets out functional policies related to Corridor designations that are to be applied to each Transect on a site-specific basis.

Section 6.2.1, Policy 1 of the Official Plan states that:

*“The Corridor designation applies to any lot abutting the Corridor, subject to:*

- a) *Generally, a maximum depth of:*
  - i) *In the case of Mainstreet Corridors, a maximum depth of 220 metres from the centreline of the street identified as a Mainstreet Corridor;*

The Subject Property has frontage on Beechwood Avenue. The Subject Property is located entirely within 220 metres of the centreline of Beechwood Avenue, designated as Mainstreet



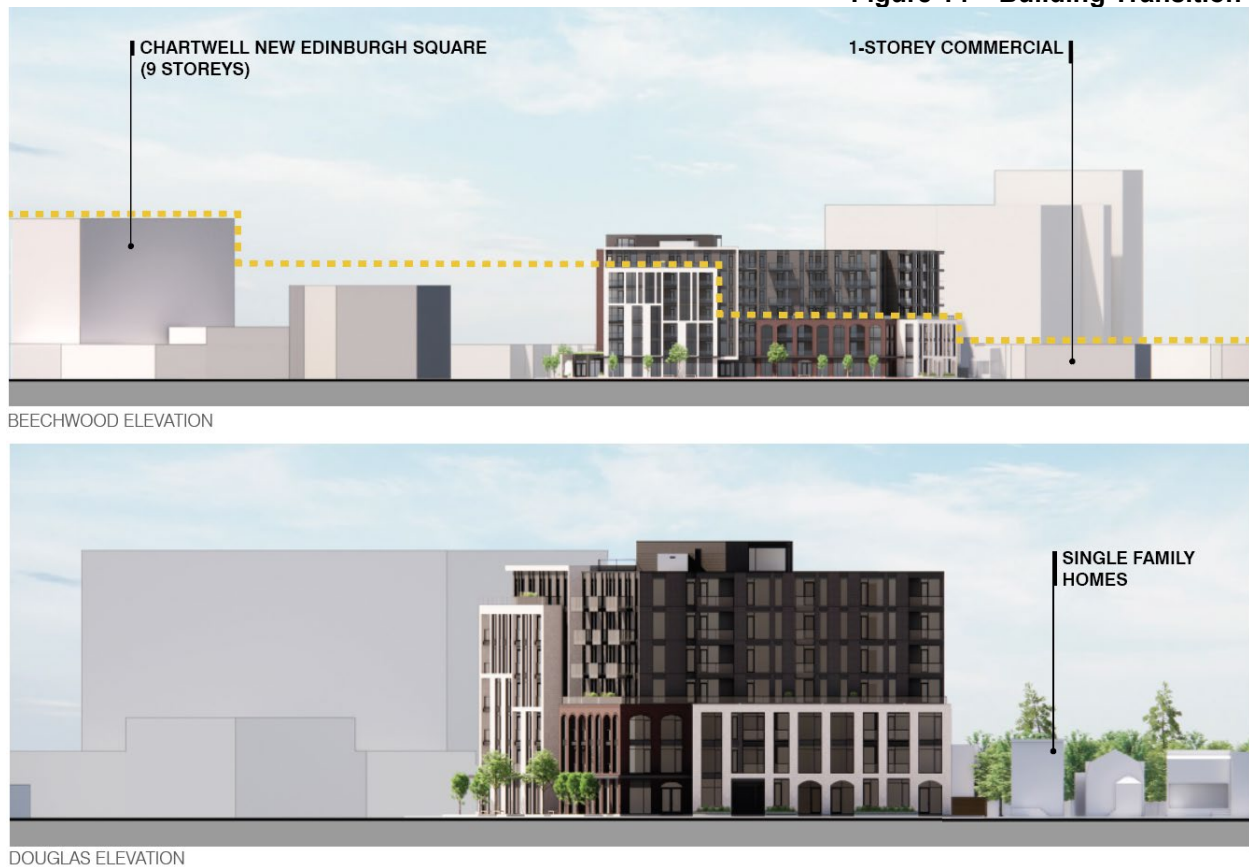
Corridor in the Official Plan. The Mainstreet Corridor designation applies to the entire area of the Subject Property.

Section 6.2.1, Policy 2 of the Official Plan states that:

“2) Development within the Corridor designation shall establish buildings that locate the maximum permitted building heights and highest densities close to the Corridor, subject to building setbacks where appropriate. Further, development:

- a) Shall ensure appropriate transitions in height, use of land, site design and development character through the site, to where the Corridor designation meets abutting designations;
- b) *May be required to provide public mid-block pedestrian connections to nearby streets or abutting designations;”*

**Figure 14 – Building Transition**



Policies of the Official Plan direct new development to locate the maximum permitted building heights and greatest densities close to the corridor along Beechwood Avenue. Policies of the Official Plan direct new development to ensure compatibility with surrounding land use designations through appropriate design and transitions in height and land use. The proposed building is located within a Traditional Main Street environment, bordered by taller structures to the west and smaller commercial buildings to the east. To transition between these

contrasting building forms, the proposed development is designed to appear as a collection of smaller structures, rather than a single monolithic building.

Strategic use of diverse materials and colors creates a visually appealing variation both horizontally and vertically. A key design feature is the six-storey white stone block at the southwest corner, contrasting with the three-storey podium on the east side. The ground level fronting along Beechwood Avenue features commercial patio spaces that break up the building mass visually and enhance the walkability of the area, fostering a harmonious relationship with the surrounding environment.

The proposed development provides a two-metre setback above the third storey along Beechwood Avenue and Douglas Avenue, providing a comfortable transition from taller buildings located to the west to low-rise commercial buildings located to the east and low to mid-rise buildings located to the north (See Figure 14). An additional stepback above the sixth floor where the building meets Springfield Road mirrors the height of existing mid-rise buildings located to the west along Beechwood Road.

The proposed eight-storey building height is designed to complement the permitted building heights on neighbouring properties and provides an appropriate transition to surrounding land uses and built forms, creating a comfortable pedestrian environment along the Beechwood Avenue corridor.

Section 6.2.1, Policy 3 of the Official Plan states that:

*“3) Corridors will generally permit residential uses and such non-residential uses that integrate with a dense, mixed-use urban environment. The City may require through the Zoning By-law and/or development applications to amend the Zoning By-law:*

- a) Commercial and service uses on the ground floor of otherwise residential, office and institutional buildings with a strong emphasis on uses needed to contribute to 15-minute neighbourhoods;*
- b) Residential and/or office uses on the upper floors of otherwise commercial buildings; and/or*
- c) Minimum building heights in terms of number of storeys to ensure multi-storey structures where uses can be mixed vertically within the building.”*

The proposed development provides a mix of uses within the building. The Subject Property is well located within walking distance from a number of shopping opportunities as well as commercial, employment, and recreational amenities. The proposed mixed-use development will help contribute to the establishment of 15-minute neighbourhoods by providing the residential density needed to support commercial uses on the ground floor and within the surrounding area.

### **3.2.2 Growth Management Framework**

Section 3 of the Official Plan sets out policies to direct growth toward target areas for intensification. The majority of projected growth between 2018 and 2046 is directed within the urban boundary, representing 93% of all new development. 47% of the growth allocation is directed to take place within existing built-up areas inside the urban boundary where

services are available or can be easily provided for new development to accommodate the creation of jobs, housing and increased transit use.

Section 3.2, Policy 3 of the Official Plan identifies “Corridors” as one of the target areas designated for residential and employment intensification in the City of Ottawa.

The Subject Property is designated as Mainstreet Corridor and is located within the Inner Urban Transect on Schedule B2 of the Official Plan. The Subject Property is located within a land use designation targeted for intensification. The proposed development of an eight-storey, mixed use building on the Subject Property is consistent with the growth objectives of the Official Plan.

Section 3.2, Policy 10 of the Official Plan sets out residential density targets that apply to Corridors and states: “The residential density and proportion of large household dwelling targets as shown on Schedules B1 through B8 are established in Table 3a for Hubs and Mainstreet Corridors and Table 3b for Neighbourhoods and Minor Corridors.”

The Subject Property is designated as Mainstreet Corridor in the Official Plan. Policies of the Official Plan establish a minimum area-wide density requirement of 120 people and jobs per gross hectare and a minimum a residential density requirement of 120 dwellings per net hectare for proposed developments in the Inner Urban Transect.

The Subject Property has a gross area of 1,930.5 square metres. A mixed-use development is proposed with commercial uses on the ground level and residential units located above. At a rate of 1 resident per unit, the Subject Property would accommodate 121 residents. Additional residents are anticipated, and jobs would be created through the commercial components of the proposed development. The minimum density of the proposed development is approximately 627 people and jobs per gross hectare. The proposed development would exceed the required minimum density targets for people and jobs per gross hectare in the Official Plan.

121 dwelling units are proposed on the Subject Property, which has a net buildable area of approximately 1,489 square metres. The density of the proposed development is approximately 813 dwelling units per net hectare, which exceeds the minimum requirement and is consistent with residential density targets for Mainstreets in the Official Plan.

### **3.2.3 City-wide Policies**

Section 4 of the Official Plan sets out city-wide policies to be considered where all new development is proposed.

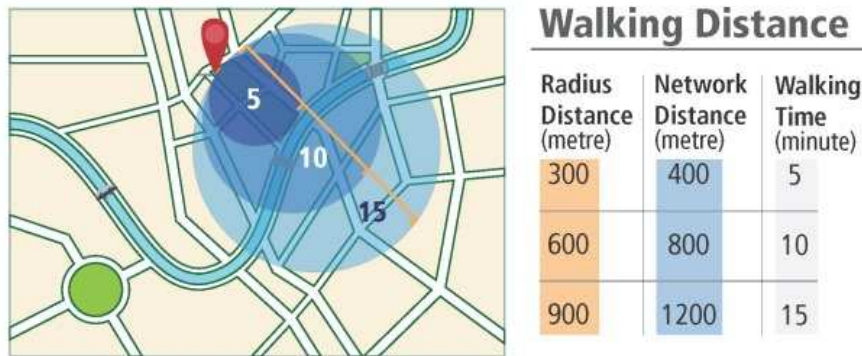
Section 4.1.2, Policy 1 of the Official Plan sets out the framework for establishing 15-minute neighbourhoods across the City and states that:

*“1) In general, this Plan equates a walking time of:*

- a) 5 minutes to be equivalent to a radius of 300 metres, or 400 metres on the pedestrian network;*
- b) 10 minutes to be equivalent to a radius of 600 metres, or 800 metres on the pedestrian network; and*

- c) 15 minutes to be equivalent to a radius of 900 metres or 1,200 metres on the pedestrian network.”

Figure 15 – Official Plan Figure 11



The Subject Property is located fully within 300 metres radius and within 400 metres network distance of transit stops along frequent bus routes 6 and 7, located south of the Subject Property along Beechwood Avenue and west of the property along Springfield Road. Frequent bus routes 6 and 7 each provide connections to the Rideau O-Train station. The proximity of the proposed development to a range of transit options in the immediate area represents a walking distance of 5 minutes or less for residents to access frequent street transit options from their home (see Figure 15).

### 3.2.4 Urban Design

Section 4.6 of the Official Plan sets out policies related to Urban Design and is intended to promote design excellence in Design Priority areas, encourage innovation in site planning and building design, support the objectives of Corridors, Hubs and Neighbourhoods and enable integration of new development with existing communities.

Section 4.6.2, Policy 2 of the Official Plan states that:

*“Views of the Parliament Buildings shall be protected from two locations in the Beechwood National Cemetery: The Tommy Douglas Memorial and Poet’s Hill, as identified on Schedule C6A. New buildings or structures should be located to complement or enhance the view of these national symbols and shall not visually block the foreground view or visually change the background silhouette of the Parliament Buildings when viewed from the identified locations. For each property in the viewshed, planning applications shall not be supported that would permit a proposed building to obstruct the view unless it is demonstrated that the view is already impacted and would not be further impacted by the proposal. This includes fences, signs and trees and other elements that could obstruct views.”*

A portion of the Subject Property fronting along Beechwood Avenue is located within the protected viewshed of Parliament Hill from Beechwood Cemetery as identified on Schedule C6A of the Official Plan. An eight-storey portion of the building along the west side facing Springfield Road is located outside of the protected viewshed. The majority of the proposed building has been designed at a maximum height of seven storeys to respect the protected viewshed and maintain views to the roofline of the Parliament Buildings from Beechwood Cemetery.

**Figure 16: View of Building Looking West (Douglas Avenue and Beechwood Avenue)**

Section 4.6.5, Policy 2 of the Official Plan states that:

*“Development in Hubs and along Corridors shall respond to context, transect area and overlay policies. The development should generally be located to frame the adjacent street, park or greenspace, and should provide an appropriate setback within the street context, with clearly visible main entrances from public sidewalks. Visual impacts associated with above grade utilities should be mitigated.”*

The proposed development has been designed to frame Beechwood Avenue, Springfield Road and Douglas Avenue with an appropriate building height, setback, and building materials for the area. The active frontage on Beechwood Avenue provides direct pedestrian connections to the Beechwood Avenue Corridor. The variation of building façade materials along the Springfield, Beechwood and Douglas frontages and building setbacks above the third storey along Beechwood Avenue, Douglas Avenue and above the sixth storey along the west side of the façade along Beechwood Avenue provide a visual transition, complements neighbouring low to mid-rise buildings and provides a human-scaled environment for pedestrians (see Figure 16). A continuous canopy frames the ground floor commercial spaces while the entrances to the building and retail areas are marked by a common architectural theme featuring subtly recessed archways. The use of a variety of colours and textures for façade materials, providing a significant proportion of glazing, and hard landscape treatments surrounding the building at grade level creates a welcoming pedestrian condition at the building base along Springfield Road, Beechwood Avenue and Douglas Avenue and contributes to the commercial streetscape along Beechwood Avenue.

**Figure 17: View of Building Looking East (Springfield Road and Beechwood Avenue)**

Section 4.6.6, Policy 7 of the Official Plan states that:

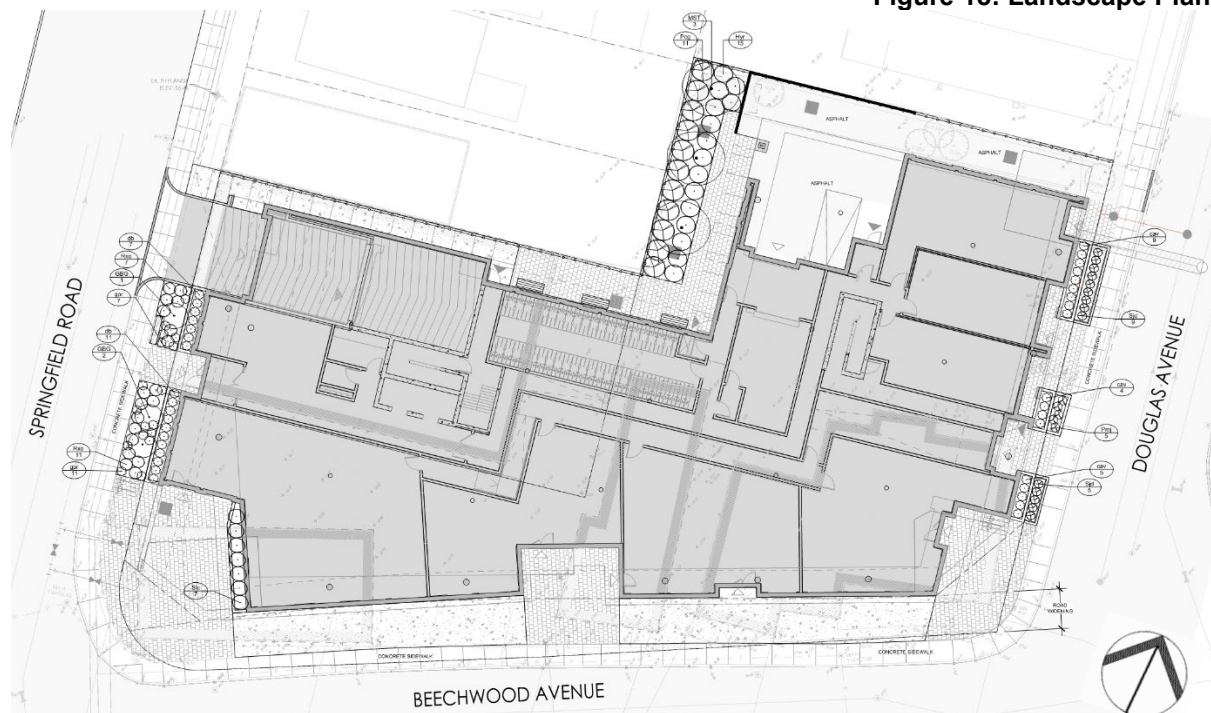
*“7) Mid-rise buildings shall be designed to respond to context, and transect area policies, and should:*

- a) Frame the street block and provide mid-block connections to break up large blocks;*
- b) Include a base with active frontages, and a middle portion that relates to the scale and character of the surrounding buildings, or, planned context;*
- c) Be generally proportionate in height to the width of the right of way as illustrated in the Figure below, with additional height permitted in the Downtown Core Transect; and*
- d) Provide sufficient setbacks and step backs to:*
  - i) Provide landscaping and adequate space for tree planting;*
  - ii) Avoid a street canyon effect; and*
  - iii) Minimize microclimate impacts on the public realm and private amenity areas.”*

The proposed development of a mid-rise, mixed-use building on the Subject Property has been designed to frame Beechwood Avenue (see Figure 17). Principal building entrances to access the residential lobby from the sidewalk are provided off Springfield Road. Entrances to ground floor commercial and retail units are provided directly off Beechwood Avenue to establish an active street frontage. The building has been designed with a distinct base comprised of generous glazing and hard landscape elements where feasible. The building's base predominantly features light-toned architectural stone cladding, with contrasting accents of dark brick masonry and physical recesses used to define building entrances. The Beechwood Avenue elevation features a notable shift at its midpoint, moving from red brick masonry to a light-toned architectural stone. This transition is emphasized by a vertical void

and a commercial patio space at grade. Along with the horizontal banding comprised of stone, the design elements used serve to successfully modulate the building's scale and character. Above the third storey, a vertical shift to dark brick masonry imparts a sense of a distinct structure when viewed from the street looking towards Douglas Avenue. The shift in materials, along with suitable building setbacks and an articulated façade design preserves the overall scale and unique character of the surrounding community.

Figure 18: Landscape Plan



Section 4.6.5, Policy 3 of the Official Plan states that:

*“Development shall minimize conflict between vehicles and pedestrians and improve the attractiveness of the public realm by internalizing all servicing, loading areas, mechanical equipment and utilities into the design of the building, and by accommodating space on the site for trees, where possible. Shared service areas, and accesses should be used to limit interruptions along sidewalks. Where underground parking is not viable, surface parking must be visually screened from the public realm.”*

Direct pedestrian access to the Subject Property is provided from the principal building entrances to the public sidewalk along Springfield Road and Beechwood Avenue. Entrances to grade level commercial patio spaces are provided along Beechwood Avenue and at the corners of Springfield Road and Douglas Avenue. Landscaping treatments are provided at grade along Springfield Road and Douglas Avenue where possible (see Figure 18). A soft landscape area with ornamental grasses and shrubs in a raised planter is provided on the second level terrace along the west edge of the Subject Property.

Motor vehicle access to an underground garage is provided off Springfield Road for the proposed development. A separate drop-off, service and loading area is accessed off Douglas Avenue. Providing motor vehicle access to the Subject Property that is separated from

Beechwood Avenue reduces the potential for vehicle conflicts and improves the safety, accessibility, and movement of pedestrians and bicycles to and from the Subject Property and is consistent with policies of the Official Plan.

### 3.2.5 Review of Development Applications

The Official Plan requires a range of studies to be included as part of a complete development application to meet the objectives of the Official Plan. The appropriate policies, related studies, and plans were identified through a pre-application consultation meeting with City staff at the beginning of the application review process.

Required studies and plans identified as relevant have been prepared in support of the proposed development. Detailed and technical information can be obtained by reviewing the respective documents.

#### Relating to Section 4.7.1 – Stormwater Management and Site Servicing

Policy 6 of Section 4.7.1 requires redevelopment applications to implement site, grading, building, and servicing design measures. As part of this application, a Site Servicing Plan, Site Grading Plan, and Erosion Control Plan have been completed by Stantec, dated May 24, 2023.

Policy 8 of Section 4.7.1 requires proof of sufficient stormwater management and drainage system as a condition of Site Plan Control approval. A Servicing and Stormwater Management Report was completed by Stantec dated May 24, 2023. The Servicing and Stormwater Management Report examined the potential water quality and quantity impacts of the proposed development and how each will be addressed through the proposed development. The proposed stormwater design and recommendations are consistent with the policies of Section 4.7.1 of the Official Plan.

Policy 12 of Section 4.7.1 requires an approved master servicing study, an approved environmental management plan, and a subwatershed study to be included as part of a complete application for a new development in a future neighbourhood. The Subject Property is not located in a future neighbourhood and these studies are not required. City staff requested that a Site Servicing study and plan be included as part of the complete application package. A Servicing and Stormwater Management Report was completed by Stantec dated May 24, 2023. The recommendations of the Site Servicing Study conclude that:

*“the existing 305 mm diameter watermain on Springfield Road can provide adequate fire and domestic flows for the subject site.” and that “there are no concerns with respect to adding the proposed peak flows to the existing sanitary sewers in Douglas Street and Springfield Road”.*

The recommendations of the Site Servicing Study are consistent with Section 4.7.1 of the Official Plan.

#### Relating to Section 10.1.6 – Contaminated Sites

Policy 1 of Section 10.1.6 requires environmental site assessments and remedial or risk assessment/risk management activities reports to be completed as part of a development



application. City staff identified the need for a Phase I Environmental Assessment to be completed as part of the application. A Phase I ESA was completed by Pinchin Ltd. dated March 15, 2023.

The recommendations of the Phase I ESA state that:

*“Pinchin concludes that one or more contaminants originating from PCAs located on the Phase One Property and within the Phase One Study Area outside of the Phase One Property may have affected land or water on, in, or under the Phase One Property. Therefore, Pinchin recommends that a Phase Two ESA be conducted prior to filing an RSC for the Phase One Property.”*

Based on the recommendations of the Phase I ESA, a subsequent Phase II ESA was recommended to be completed.

The recommendations are consistent with Section 10.1.6. of the Official Plan.

#### Relating to Section 10.2.1 – Noise

Policy 2 of Section 10.2.1 requires a Roadway Traffic Noise Study to be prepared as part of a complete application. An Environmental Noise Control Study was completed by Paterson Group dated March 20, 2023. The recommendations of the Environmental Noise Control Study state the following:

*“...noise levels at second-floor and fourth-floor balcony terraces cannot be reduced to 55 dBA without the application of an excessively tall barrier. Therefore, since noise levels cannot be economically reduced to 55 dBA, the exceedances in noise level are considered acceptable provided that the warning clause Type A is included on all deeds of sale.”* And that, *“...noise levels on the southern, western, northern, and eastern elevations of proposed building are expected to exceed the 55 dBA threshold specified by the ENCG. Also, the noise levels on the southern and western elevations of proposed building are expected to exceed the 65 dBA threshold value. Therefore, the installation of a central air conditioning unit, along with a warning clause Type D, will be required for all units of the proposed building.”*

The recommendations are consistent with Section 10.2.1 of the Official Plan.

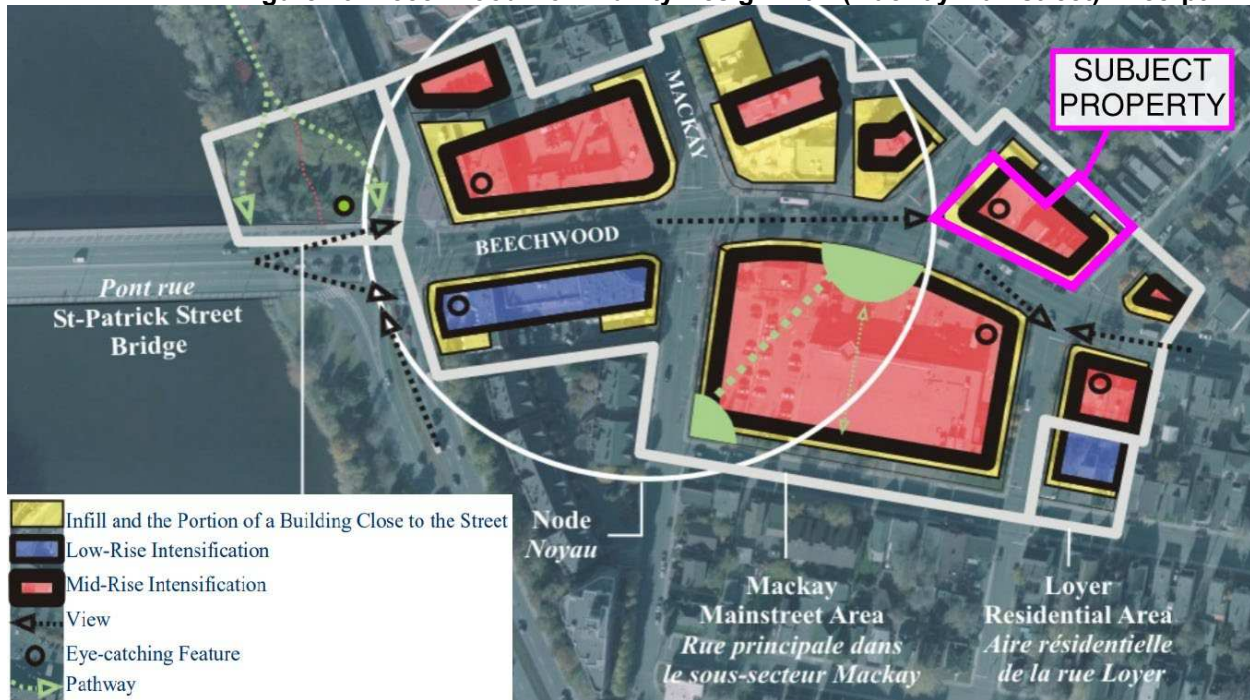
**The proposed Minor Zoning By-law Amendment and Site Plan Control applications conform to the policies of the City of Ottawa Official Plan.**

#### 4.0 BEECHWOOD COMMUNITY DESIGN PLAN

The Subject Property is located within the Mackay Mainstreet Area of the Beechwood Community Design Plan (the “Beechwood CDP”) (see Figure 19).

The Beechwood CDP establishes a vision and general design guidelines for future development of the Subject Property and surrounding Beechwood Avenue area. Although the Beechwood CDP is not a statutory policy document, it may be used by City staff and Council when considering development applications for the Subject Property. The Beechwood CDP was used as a general reference while preparing the proposed development.

Figure 19. Beechwood Community Design Plan (MacKay Mainstreet) Excerpt



Section 2.2.1 of the Beechwood CDP establishes a design vision for the Mackay Sector and states that:

*“In the future, this sector should intensify in such a way that it retains the positive qualities it presently has. New buildings should be high quality, mid-rise, mixed use buildings with small stores at grade. The provision for one large-format grocery store as an anchor should continue. New buildings and additions should be built close to the street and anything over 3-storeys should be setback further from the street. Low-rise infill buildings should not be single storey and larger buildings should appear as a group of smaller buildings that have been developed at different times. As a guide, the larger buildings should be divided vertically in sections and designed to replicate different buildings or shops.”*

The proposed building has been designed with high quality materials with commercial uses at grade and residential uses located above. The building is designed to read as several

distinct buildings from Beechwood Avenue through use of building stepbacks, vertical and horizontal breaks and transitions in the texture, colour and arrangement of selected building materials to fit well with the surrounding area. The proposed development of a mid-rise, mixed use building on the Subject Property is consistent with the design vision of the Beechwood CDP.

## 5.0 BEECHWOOD AVENUE TRANSPORTATION CORRIDOR

In 2017, the City of Ottawa prepared a functional design study for the Beechwood Avenue Transportation corridor. The functional design study sets out the planned reconfiguration of the road in line with the city's Cycling Master Plan and the Beechwood Community Design Plan, which calls for Beechwood to be reconfigured to two vehicular travel lanes east of Springfield Road.

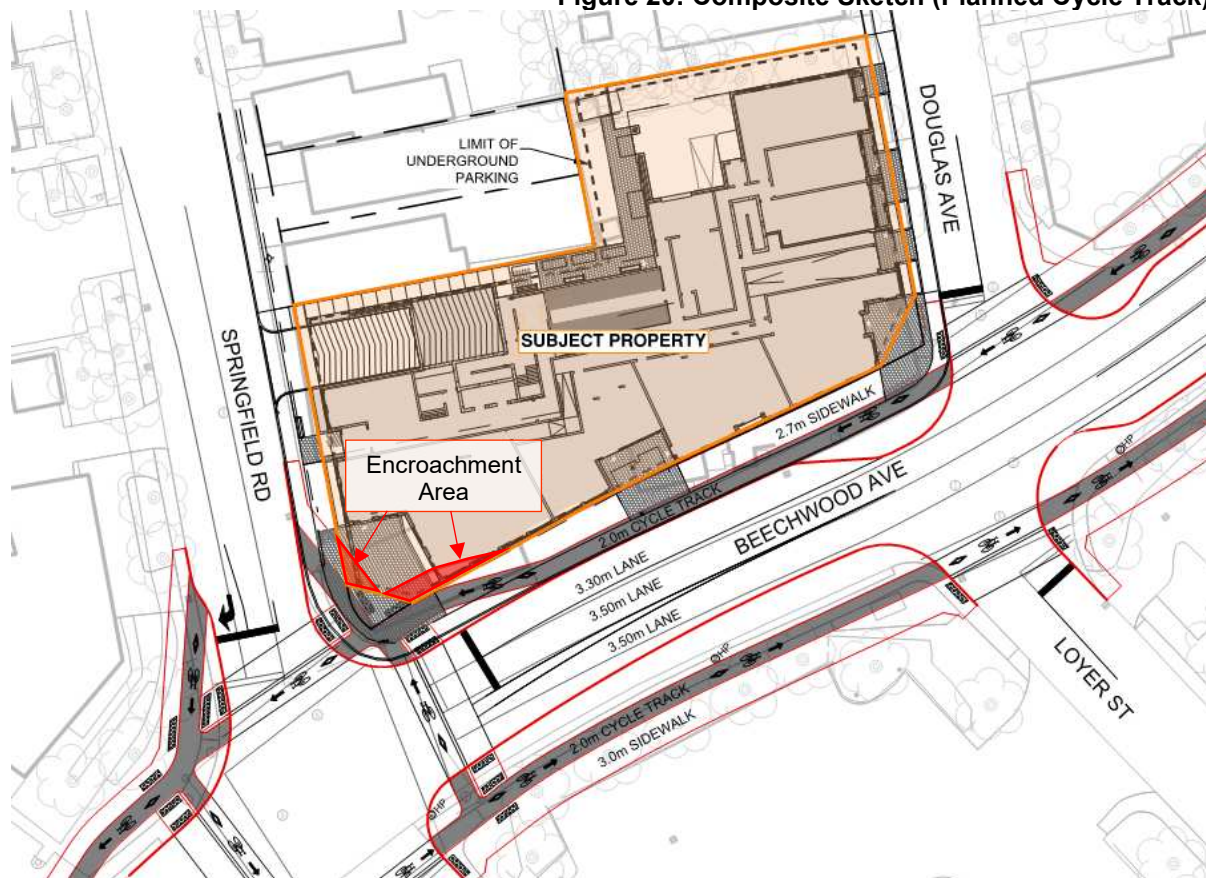
Section 11.6, Policy 5 (c) of the Official Plan states that:

*“The City may acquire, hold, or secure land for the purpose of implementing any policy of this Plan, including:*

*...c) Acquiring land through the development review process for mobility and or infrastructure needs, including: road corridor widening, corner sight triangles, grade triangle for future overpasses or elevation of grade crossings for rail lines, mid-block pedestrian circulation, multi-use paths, stormwater management, expansion of transit services and underground infrastructure”*

The protected right of way for Beechwood Avenue between Vanier Parkway and Marier Street is 11.5 metres on the north side and 13 metres on the south side of the Beechwood Avenue measured from the existing centreline, as identified on Schedule C16 of the Official Plan. Policies of the Official Plan provide authority to request a road corridor widening of up to 11.5 metres from the centreline of Beechwood Avenue abutting the Subject Property, and corner sight triangles at the intersections of Springfield Road and Beechwood Avenue.

Figure 20: Composite Sketch (Planned Cycle Track)



Through applications for redevelopment, the City will require dedication of a road widening, measured 11.5 metres from the centreline of Beechwood Avenue to accommodate a two metre wide cycle track and 2.7 metre wide pedestrian sidewalk abutting the Subject Property. The planned cycle track design and sidewalk alignment eliminates any opportunity to provide soft landscaping, hard landscaping or street trees between the building and Beechwood Avenue.

The cycle track and sidewalk are shown to encroach into the Subject Property beyond the protected right of way for Beechwood Avenue and required corner sight triangle at the intersection of Springfield Road and Beechwood Avenue as identified on Schedule C16 of the Official Plan (see Figure 20). Policies of the Official Plan do not provide for the acquisition of this additional land as a condition of redevelopment.

Figure 21: Existing Cycle Track (MacKay / Beechwood)



To the west of the Subject Property, the road widening for redevelopment of 411 Beechwood Avenue resulted in a functional design which provides a cycle track, sidewalk and adequate space for street trees along Beechwood Avenue (see Figure 21). A similar approach should be implemented through the development of 47 Beechwood Avenue to accommodate the planned two metre wide cycle track, a 2.7 metre wide sidewalk within the required road widening and a corner sight triangle at the intersection of Springfield Road and Beechwood Avenue.

## 6.0 URBAN DESIGN GUIDELINES

The City of Ottawa's Urban Design Guidelines provide guidance in order to promote and achieve appropriate development within key growth areas throughout the City. Where the Urban Design guidelines apply, not all of the direction provided will apply to the proposed development. The Urban Design Guidelines are non-statutory documents and are intended to supplement the policies and regulations of the Official Plan and Zoning By-law. The Urban Design Guidelines for Development along Traditional Mainstreets have been reviewed for the proposed development.

### 6.1 Urban Design Guidelines for Development along Traditional Mainstreets

The Urban Design Guidelines for Development along Traditional Mainstreets were approved on May 24, 2006. These guidelines apply across the City to all streets identified as a Traditional Mainstreet in the Official Plan (2003) and provide urban design guidance for new developments.

The primary objectives of the guidelines are as follows:

- To promote development that will enhance and reinforce the recognized or planned scale and character of the street
- To promote development that is compatible with, and complements its surroundings
- To achieve high-quality built form and strengthen building continuity along Traditional Mainstreets
- To foster compact, pedestrian-oriented development linked to street level amenities
- To accommodate a broad range of uses including retail, services, commercial uses, offices, residential and institutional uses where one can live, shop and access amenities.

The proposed development adheres to the objectives of the Urban Design Guidelines for Development along Traditional Mainstreets. The proposed eight-storey, mixed-use development includes a setback above the third storey at the intersection of Beechwood Avenue and Douglas Avenue, a setback above the sixth storey at the intersection of Springfield Road and Beechwood Avenue and a setback along both Springfield Road and Douglas Avenue which contributes to maintaining the existing scale and character of the streetscape and improving pedestrian comfort along Beechwood Avenue. The proposed building design provides a transition between the street and adjacent low and mid-rise built forms. Ground floor commercial uses are intended to extend the existing retail character of the Beechwood Avenue Corridor along the site frontage. A significant proportion of glazed surfaces and landscape treatments at grade create active frontages designed to provide an open and transparent pedestrian experience and provide direct access to ground floor commercial uses from Beechwood Avenue. A separate principal entrance for residents is located along Springfield Road. The proposed residential density provided on the site will support the surrounding commercial uses, allowing residents to access their daily needs within walking distance of home. All parking on the site is separated from Beechwood Avenue with access off Springfield Road to an underground parking garage. Off-street parking is located below grade to minimize any potential impact on the street. Loading and drop-off parking for the proposed building are accessed via a driveway off Douglas Avenue to the rear of the building and is screened from view of the street.

## 7.0 CITY OF OTTAWA ZONING BY-LAW 2008-250

The Subject Property is zoned Traditional Mainstreet, Subzone 8 (TM8) in the City of Ottawa Zoning By-law 2008-250. The property is also subject to the Mature Neighbourhoods Overlay.

The purpose of the TM8 zone is to:

1. *accommodate a broad range of uses including retail, service commercial, office, residential and institutional uses, including mixed-use buildings but excluding auto-related uses, in areas designated **Traditional Mainstreet** in the Official Plan;*
2. *foster and promote compact, mixed-use, pedestrian-oriented development that provide for access by foot, cycle, transit and automobile;*

3. recognize the function of Business Improvement Areas as primary business or shopping areas; and
4. impose development standards that will ensure that street continuity, scale and character is maintained, and that the uses are compatible and complement surrounding land uses.

An eight-storey, mixed-use building is proposed for the Subject Property. The proposed development will include four ground-floor commercial units and 121 residential dwelling units. The incorporation of ground floor commercial uses, proximity to the Beechwood Avenue corridor and bus stops on the frequent transit network will promote a pedestrian-oriented public realm where residents can access daily needs by walking, cycling, or taking transit. The built form of the building is designed to fit well and maintain the scale and character of the area by introducing a façade that reads as a group of distinct buildings from Beechwood Avenue. To further reduce the scale and visual impact of the building from the street, a strategic use of building setbacks, vertical and horizontal breaks and transitions in the texture, colour and arrangement of selected building materials are provided facing Springfield Road, Beechwood Avenue and Douglas Avenue. The proposed development is consistent with the purpose of the TM zone.

Section 197 (1) of the Zoning By-law lists a number of permitted non-residential uses for the TM zone including retail store, personal service business, restaurant and office. The proposed commercial units are permitted to be occupied by a variety of retail and employment uses. No specific employment use has been identified for the proposed development. Section 197(2)(a) of the Zoning By-law lists “dwelling units” as a permitted use in the TM zone. The proposed building containing a mix of commercial units at grade with dwelling units above is permitted in the TM8 zone.

Table 1 below summarizes the applicable zoning provisions for the Subject Property.

**Table 1: Zoning Review Table**

Zoning Provision	Required	Provided
Minimum Lot Area (m <sup>2</sup> )	No minimum	1,930.5 m <sup>2</sup>
Minimum Lot Width (m)	No minimum	32 m
Front Yard Setback (m) (lots abutting Beechwood Avenue)	a minimum of 0 metres for the first three storeys and above the third storey, a minimum of 2 metres more than the setback of the first three storeys; despite Section 197(3)(c), no maximum above the third storey	0m (first three storeys)  <b>2m setback</b> (above sixth storey)
Corner Side Yard Setback (m) (lots abutting Beechwood Avenue)	for the first three storeys, a minimum of 1 metre and a maximum of 3 metres; above the third storey, a minimum of 2 metres more than the setback of the first three storeys;	1m (first three storeys)  Minimum 2m greater than the setback of storeys 1-3

Rear Yard Setback (m) (lots abutting Beechwood Avenue)	where abutting a residential zone,  - 5 metres for the first three storeys, <u>except in the case of corner or through lots 20 metres or greater in width, where setback is 3 metres for up to half the lot width measured from the corner side lot line and 7.5 metres for the remaining portion of the lot width</u>  - 7.5 metres above the third storey	<b>3m (first three storeys)</b>  <b>3.6m (above third storey)</b>
Max. Interior Yard Setback (m) (abutting residential zone – east side of property)	3 m 3 metres for a non-residential use building or a mixed-use building abutting a residential zone, and	3 m
Min. Interior Yard Setback (m) (abutting mixed use zone – west side of property)	No minimum	1.5 m
Minimum Building Height (m)	6.7 metres for a distance of 20 metres from the front lot line as set out under subsection 197(5)	24.4 m
Maximum Building Height (m)	TM – (20 m / 6 storeys)  Additional setback of 2 metres where building greater than 4 storeys	<b>24.4m / 8 Storeys</b>
		<b>Additional 2 m setback provided above 6<sup>th</sup> storey</b>
Angular Plane	Rear lot line abuts an R3 zone. A 45 degree angular plane is required measured at a height of 15 m from a point 7.5 m from the rear lot line, projecting upwards towards the front lot line.	A 45 degree angular plane is provided measured at a height of <b>21.7 m</b> from a point <b>3.6 m</b> from the rear lot line.
Minimum Width of Landscaped Area (m)	Abutting a Residential Zone: 3m	<b>0m (Rear Lot Line)</b>
	All other cases: No Minimum – lot abuts a TM zone	Min 1.5m abutting Interior Side Yard lot lines
Minimum Driveway Width (m)	197(8)(b) – 6m for parking lots with 20 or more parking spaces	6m
Required Total Amenity Area	726 m <sup>2</sup> (6 m <sup>2</sup> per dwelling unit)	Total = <b>763m<sup>2</sup></b>



Required Communal Amenity Area	363 m <sup>2</sup> (50% of required total amenity area)	Total = 296m <sup>2</sup>
<b>Parking Requirements - Residential</b>		
Minimum Parking Spaces  (0.5 spaces per dwelling unit after the first 12 units) + (10% reduction where located below grade)	49 spaces  (121 units – 12) = 109 x 0.5 (minus 10%) = 49.05 (49)	30 residential spaces  (0.28 spaces per dwelling unit after the first 12 units)
Minimum Visitor Parking Spaces	11 spaces (0.1 spaces per dwelling unit after the first 12 units)	11 visitor parking spaces
Maximum Visitor Parking Spaces	30 spaces	11 visitor parking spaces
Minimum Barrier Free Parking Spaces	1 Type A space (retail) (per Section 3.1.2, Table 3 of Accessibility Design Standards) 1 Accessible space (resident) (per Section 106(2)(b) of the City of Ottawa Zoning By-law; 112(1)(a)(i) of the City of Ottawa Traffic and Parking By-law)	1 Type A space (retail)  1 Accessible space (resident)
<b>Parking Requirements – Non-Residential</b>		
Area Y – Ground Floor/Basement	Retail GFA= 540m <sup>2</sup> 1.25 spaces/100m <sup>2</sup> GFA = 6.75 (7)	7 spaces
<b>Bicycle Parking Requirements</b>		
Minimum Bicycle Parking Spaces – Residential	61 (0.5 spaces per dwelling unit)	128 bicycle spaces total
Minimum Bicycle Parking Spaces – Retail / Commercial	2 (1 space per 250 m <sup>2</sup> gross floor area)	124 interior bicycle parking 4 exterior bicycle parking

The Subject Property is located in Area Y on Schedule 1A of the Zoning By-law. Section 101 of the Zoning By-law states that retail parking spaces are required at a rate of 1.25 spaces per 100 square metres of retail gross floor area. The combined gross floor area of the proposed ground floor commercial units is 540 square metres.

A total of seven off-street parking spaces are required for retail units as part of the proposed development. A total of seven off-street parking spaces are provided for retail units as part of the proposed development.

The proposed development fully conforms with the required front yard and corner side yard setback from levels one to three, required interior side yard setbacks, minimum required visitor parking, minimum required retail parking and minimum required bicycle parking for the residential and retail components on the Subject Property.

A minor rezoning application is required to establish site specific zoning provisions on the Subject Property to:

- decrease the required building setback in the front yard above the third storey;
- decrease the required building setback in the rear yard above the third storey;
- increase the maximum permitted building height;
- alter the required angular plane for the proposed building from the rear lot line;
- reduce the minimum required landscaped area abutting the rear lot line;
- reduce the minimum communal amenity area requirement; and,
- reduce the minimum required parking spaces for residential units.

**The proposed Minor Zoning By-law Amendment and Site Plan Control application are consistent with the purpose of the Traditional Mainstreet zone and are consistent with many of the relevant provisions of the City of Ottawa Zoning By-law.**

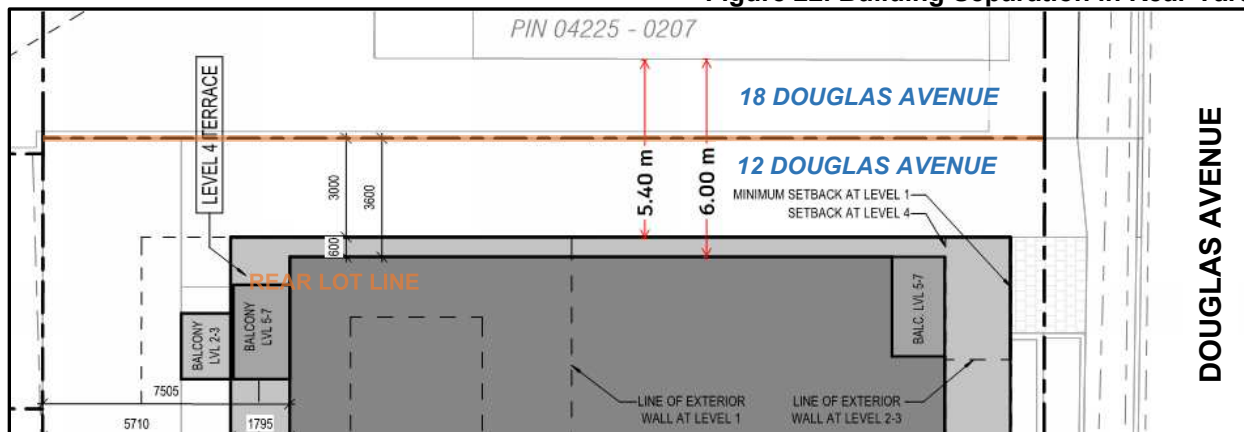
## **8.0 PROPOSED ZONING BY-LAW AMENDMENT**

### **Stepbacks:**

Provisions of the Traditional Mainstreet Subzone 8 zone (TM8) require a minimum setback of zero metres for the first three storeys and a minimum setback of two metres more than the setback of the first three storeys above the third level. Relief from the zoning provision is required to decrease the setback above level three by two metres to zero metres from level three to six to facilitate the proposed development. Fully conforming building setbacks are provided above the third storey along the east portion of the Beechwood frontage, along Springfield Road and Douglas Avenue. To break up the long building façade along Beechwood Avenue, a portion of the building has been designed to project into the required building setback above the third level to create a vertical separation and visual interest in the façade. A two metre setback is provided above the sixth storey along the west portion of the Beechwood frontage. The reduced building setback is designed to articulate the façade, establish the image of multiple buildings along Beechwood Avenue and provide a built form transition to existing properties to the west across Springfield Road and to the east across Douglas Avenue.

Where abutting a residential zone, provisions of the Traditional Mainstreet Subzone 8 zone require a minimum rear yard setback of three metres for up to half of the lot width and 7.5 metres for the remaining portion of the lot width. A rear yard setback of 7.5 metres is required above the third storey. Relief from the zoning provision is required to decrease the rear yard setback from levels one to three beyond the first half of the lot width by 4.5 metres to three metres and to decrease the required rear yard setback above level three by 3.9 metres to 3.6 metres from level three to seven to facilitate the proposed development.

Figure 22. Building Separation in Rear Yard



If the existing lot abutting the rear lot line (12 Douglas Avenue) were to develop as a separate project, the current rear lot line would be defined as the interior side yard. Provisions of Section 198(8)(c)(ii) state that the interior side yard setback required in this location would be three metres. No additional setback would be required above the third level. A three metre setback is provided from the rear lot line in addition to a 0.6 metre stepback above the third storey. The Subject Property abuts the interior side yard of a low-rise residential dwelling separated along the north property line by an existing driveway. A functional separation distance of approximately 5.4 metres for levels one to three and approximately six metres for levels four to seven will be provided between the existing dwelling to the north and the proposed development providing a visual separation from the adjacent property to the north (see Figure 22).

**Building Height:**

Provisions of the Traditional Mainstreet zone permit building heights of up to 20 metres or six storeys. Relief from the zoning provision is required to increase the maximum permitted building height by 4.4 metres to 24.4 metres and eight storeys to facilitate the proposed development. Policies of the Official Plan support intensification in areas designated Mainstreet Corridor and permit building heights of up to nine storeys where appropriate transitions can be provided to abutting lots. An eight-storey building is proposed on the Subject Property. The Subject Property is located with direct access to bus stops along the frequent transit network and the Beechwood Avenue corridor. The Subject Property is located within an appropriate area for increased building heights at a mid-rise scale that support the use of active transportation and public transit. The proposed development of a mid-rise, mixed-use building with a total building height greater than permitted by current height provisions on the Subject Property is appropriate for this location.

**Angular Plane:**

Provisions of the Traditional Mainstreet zone require a 45-degree angular plane be provided for any part of a building from the rear lot line at a distance of 7.5 metres and a height of 15 metres from average grade. Relief from the zoning provision is required to modify where the angular plane is applied by reducing the distance from the rear lot line from 7.5 metres to 3.6 metres and by increasing the height from which the angular plane is measured from 15 metres to 22 metres. Providing a 45-degree angular plane from a setback of 7.5 metres and a height of 15 metres would limit the development potential of levels four to seven. The request to

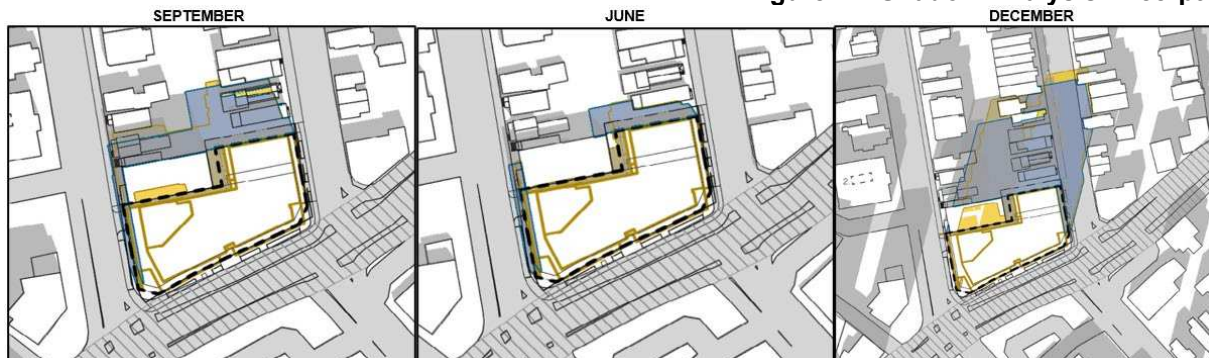
apply the angular plane from a reduced distance of 3.6 metres and an increased height of 22 metres is required to accommodate the proposed building envelope.

**Figure 23. Permitted Building Envelope (12 Douglas Avenue)**



The Subject Property abuts the interior side yard of a low-rise residential dwelling (18 Douglas Avenue) separated along the north property line by an existing driveway. If the existing lot abutting the rear lot line (12 Douglas Avenue) were to develop as a separate “as of right” project, the current rear lot line would be defined as the interior side yard. Provisions of the Traditional Mainstreet zone would not require an angular plane to be applied to the interior side yard in this scenario. A functional setback of 5.4 metres for levels one to three and six metres for levels four to seven will be provided between the existing dwelling to the north and the proposed development, providing a visual separation from the adjacent property to the north (see Figure 23).

**Figure 24. Shadow Analysis Excerpt**



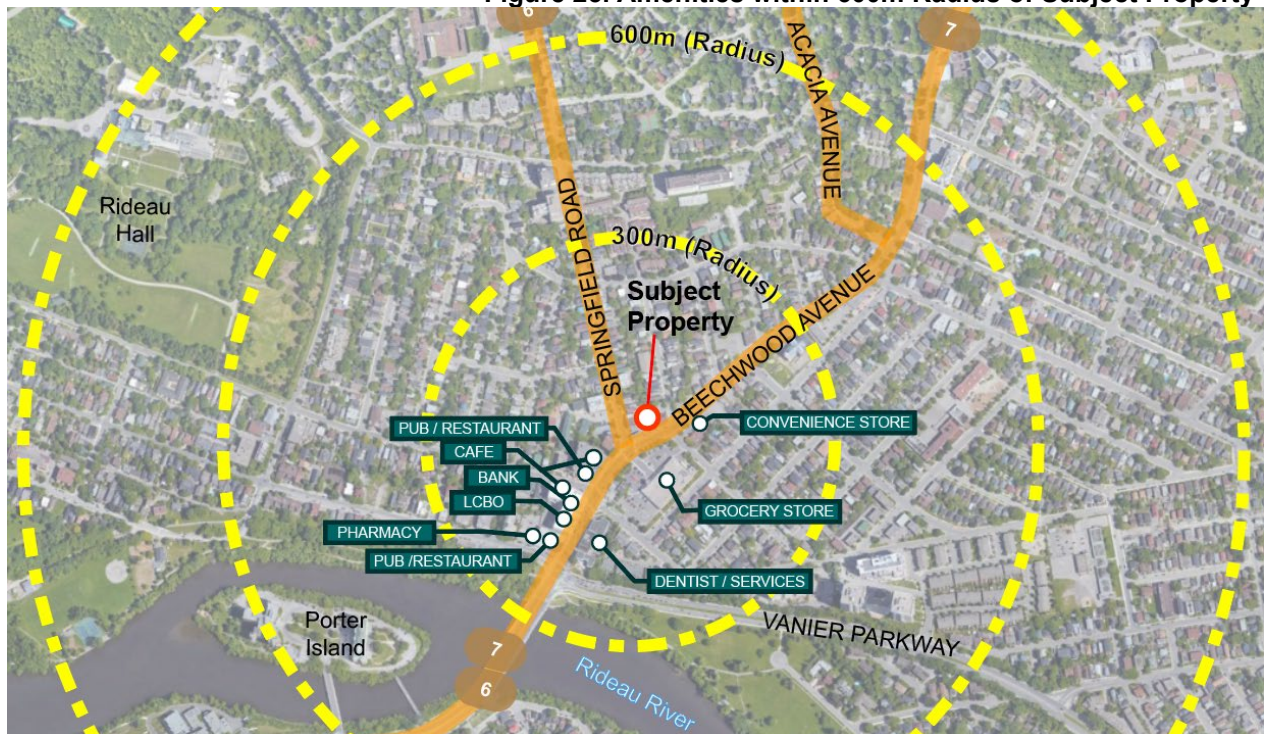
When compared to the “as of right” development on 12 Douglas (shown in blue) the shadow analysis for the proposed development (shown in yellow) shows a minimal amount of additional new net shadowing on the adjacent properties to the north, particularly in June (See Figure 24).

Establishing a modified 45-degree angular plane for the proposed building that is provided at a distance of 3.6 metres from the abutting residential lot to the north maintains the intent of the zoning by-law and is appropriate for this location.

**Landscaped Area:**

Where abutting a residential zone, provisions of the Traditional Mainstreet zone require a minimum landscaped strip three metres wide or one metre wide where a 1.4 metre high opaque fence is provided. The rear lot line abuts an R3 zone. Relief from the zoning provision is required to decrease the minimum width of landscaped area abutting the rear lot line to zero metres to facilitate service access to the proposed development off Douglas Avenue. The proposed service access abuts an existing driveway separated by a fence on the property to the north at 18 Douglas Avenue. A soft landscape area is provided at grade abutting the interior lot line at the central portion of the Subject Property. The proposed reduction to the required landscape buffer will have no impact on the existing condition of the abutting property. A reduction to the required landscape area abutting the rear lot line is appropriate for the proposed development on the Subject Property.

**Figure 25. Amenities within 600m Radius of Subject Property**



**Amenity Area:**

The combined total amenity area required for the proposed development is 726 square metres. The total required communal amenity area for the proposed development is 363 square metres. The combined total amenity area provided for building residents is 763 square metres. The total communal amenity area provided is 276 square metres located on the second floor of the proposed development. 83 of the 121 units proposed have direct access to a private balcony or terrace. The decision to provide a reduction to the total required communal amenity area for building residents is in response to the proximity of the proposed

development to a range of amenities located within a 15-minute walk of the Subject Property. Specifically, the Subject Property is located within walking distance of the Rideau River, walking paths, parks, retail stores, personal services, cafes, restaurants, banks and pharmacies along Beechwood Avenue and in the adjacent neighbourhood (see Figure 25). Based on publicly accessible amenities located in the immediate area, a reduction to the total required communal amenity area provided on the Subject Property is appropriate for the proposed development.

### **Parking:**

Section 101, Table 101, Row R15 of the Zoning By-law (Area “Y” – Schedule 1A) sets a minimum parking space rate of 0.5 spaces per dwelling unit for dwelling units in a mixed-use building after the first 12 units. Section 101 (6) (c) of the Zoning By-law states that “*where all parking spaces provided or required for a permitted land use are located below grade in the same building as that land use*” the parking requirement can be reduced by 10%. 49 parking spaces are required for the proposed residential uses. Section 102, Table 102, Column II of the Zoning By-law (Area “Y” – Schedule 1A) sets a minimum visitor parking space rate of 0.1 spaces per dwelling unit after the first 12 units. 11 parking spaces are required for visitor parking. Section 101, Table 101, Column II of the Zoning By-law (Area “Y” – Schedule 1A) sets a minimum parking space rate of 1.25 spaces per 100 square metres of retail gross floor area for retail uses. Section 101(4)(d)(iii) of the Zoning By-law states that in “Area Y” where a non-residential use with a gross floor area of 500 square metres or less is located partly or entirely on the ground floor or in the basement, no off-street motor vehicle parking is required to be provided. A total of 540 square metres of retail space is provided on the ground floor. Seven parking spaces are required for the proposed retail component.

The combined minimum number of parking spaces required for the proposed development is 67 spaces. A total of 30 off-street parking spaces for residential units are provided within an underground garage. A total of 11 visitor parking spaces are provided. A total of seven parking spaces are provided for the proposed ground floor retail and commercial uses.

The decision to provide a reduction to the number of resident parking spaces provided by 19 spaces from 49 spaces to 30 spaces is in response to the proximity of the Subject Property to transit stops along frequent transit routes along the Beechwood Avenue corridor. The location of the Subject Property is within 300 metres radius and a one minute walking distance of transit stops on bus routes 6 and 7 and is within a prime location to encourage the use of active transportation and transit options over use of private automobiles.

The Subject Property is within 600 metres radius of a broad range of land uses and amenities that contribute to development of 15-minute neighbourhoods and reduce the need for residents to use private automobiles to access daily needs. Specifically, the Subject Property is located within walking distance of retail stores, cafes, pubs, restaurant, banks, pharmacies and grocery stores along the Beechwood Avenue corridor. The Subject Property is also located near several walking paths, parks and the Rideau River (see Figure 23). A reduction to the total required residential parking spaces is appropriate for the proposed development on the Subject Property.

The proposed underground parking layout, retail parking spaces and visitor parking spaces fully conform with all other provisions of the zoning by-law.

Section 111, Table 111A, Row (b) of the Zoning By-law sets minimum bicycle parking space rates for the residential component of the proposed development. A rate of 0.5 bicycle parking spaces per dwelling unit is required for residential uses. The minimum number of bicycle parking spaces required for the proposed residential use is 61 spaces.

Section 111, Table 111A, Row (e) of the Zoning By-law sets minimum bicycle parking space rates for the commercial component of the proposed development. A rate of 1 bicycle parking space per 250 square metres of gross floor area is required for retail and commercial uses. A total of four commercial units are proposed. The combined gross floor area of the proposed commercial units is approximately 540 square metres in gross floor area. The minimum number of bicycle parking spaces required for the proposed retail and commercial use is two spaces.

90 bicycle parking spaces are provided within the proposed building and four bicycle parking spaces are provided at the exterior of the building at entrances along Springfield Road and Douglas Avenue. A total of 94 bicycle parking spaces are provided on the Subject Property, representing a supply of 31 bicycle parking spaces in excess of the minimum required by the Zoning By-law.

To facilitate the proposed development of a mid-rise, mixed-use building on the Subject Property, this application requests a site-specific amendment to incorporate the required relief from the zoning provisions to decrease the required building setback in the front yard above the third storey, decrease the required building setback in the rear yard above the third storey, increase the maximum permitted building height, alter the required angular plane for the proposed building from the rear lot line, reduce the minimum required landscaped area abutting the rear lot line, reduce the minimum communal amenity area requirement and to reduce the minimum required parking spaces for residential units as highlighted in orange on the Zoning Review Table (see Table 1). The site-specific amendment will result in development of an eight-storey, mixed-use building located with direct access and opportunities for residents to increase use of transit and active transportation infrastructure in the Lindenlea community. The proposed development is within proximity to restaurants, shops and outdoor amenity areas for residents. The requested relief from provisions of the TM8 zone is appropriate to facilitate the proposed development of an eight-storey, mixed-use building on the Subject Property.

## **9.0 PUBLIC CONSULTATION STRATEGY**

### **Prior to Submission**

A formal pre-application consultation meeting was held with City staff and members of the Lindenlea Community Association in October 2022 to solicit feedback on the proposed development.

### **Upon Submission**

The public will be consulted with regarding the proposed development through the legislated public consultation requirements. This includes a sign posted on the site and the posting of the application on the City's 'DevApps' website. At this time, neighbours will have the opportunity to comment on the proposal.

Immediately following the filing of the application, an information meeting will be coordinated with the Councillor's office. If necessary, a second public meeting will take place to discuss this development application with the community.

### **Virtual Open House**

- Who:** Residents of the community
- Where:** The Open House may be held electronically via Zoom
- When:** Soon after the City's circulation. This is to ensure that members of the public are aware of the project well in advance of any public meeting of Planning and Housing Committee.
- City rep.:** The File Lead may wish to attend, depending on the level of interest from the public. This meeting will be coordinated with the Ward Councillor.
- Follow up:** Attendees wishing to receive follow-up information may email Novatech's file lead or the City's file lead. The Project Team will do their best to keep interested citizens informed of significant changes and/or the final submission that will be heard at Planning and Housing Committee.

## **10.0 CONCLUSION**

This Planning Rationale has been prepared in support of Minor Zoning By-law Amendment and Site Plan Control applications to facilitate the development of an eight-storey, mixed-use building on the Subject Property. The Subject Property is within the Inner Urban Transect and is designated Mainstreet Corridor on Schedule B2 of the Official Plan. The Subject Property is within the Evolving Neighbourhood Overlay of the Official Plan. The property is zoned Traditional Mainstreet, Subzone 8 (TM8) and is located within the Mature Neighbourhoods Overlay in the City of Ottawa's Zoning By-law 2008-250.

The purpose of these applications is to facilitate the proposed development of an eight-storey, mixed-use building through establishing site-specific zoning provisions for the Subject Property to incorporate the required relief from the zoning provisions to decrease the required building setback in the front yard above the third storey, decrease the required building setback in the rear yard above the third storey, increase the maximum permitted building height, alter the required angular plane for the proposed building from the rear lot line, reduce the minimum required landscaped area abutting the rear lot line, reduce the minimum communal amenity area requirement and to reduce the minimum required parking spaces for residential units. Approval of the proposed development will permit a total of 121 residential dwelling units and four commercial units on the Subject Property. A total of 30 off-street resident parking spaces, 11 visitor parking spaces and seven retail parking spaces are provided in an underground garage. A total of 94 bicycle parking spaces are proposed on the Subject Property.

The proposed mid-rise, mixed-use development is appropriate to support growth objectives and residential intensification targets within the urban area of the City of Ottawa. The proposed Minor Zoning By-law Amendment and Site Plan Control applications will have no negative impacts to natural heritage and features, natural resources, or cultural heritage resources. The requested Minor Zoning By-law Amendment and Site Plan Control applications are consistent with the policies of the Provincial Policy Statement.



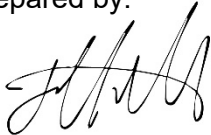
The Minor Zoning By-law Amendment and proposed development conform with the policies of the City of Ottawa Official Plan. The proposal is consistent with the Mainstreet Corridor designation and meets the City’s intensification goals while maintaining a form that is context-sensitive and adds to the existing characteristics of the Lindenlea community. The proposed ground-floor commercial uses are consistent with the retail context of the Beechwood Avenue corridor and contribute to the City’s objectives to establish 15-minute neighbourhoods. The requested Minor Zoning By-law Amendment establishes appropriate zoning provisions for the proposed mixed-use building and permits development that is compatible with surrounding land uses.

The Minor Zoning By-law Amendment and Site Plan Control applications are appropriate for the development of the Subject Property and represent good land use planning.

Yours Truly,

**NOVATECH**

Prepared by:



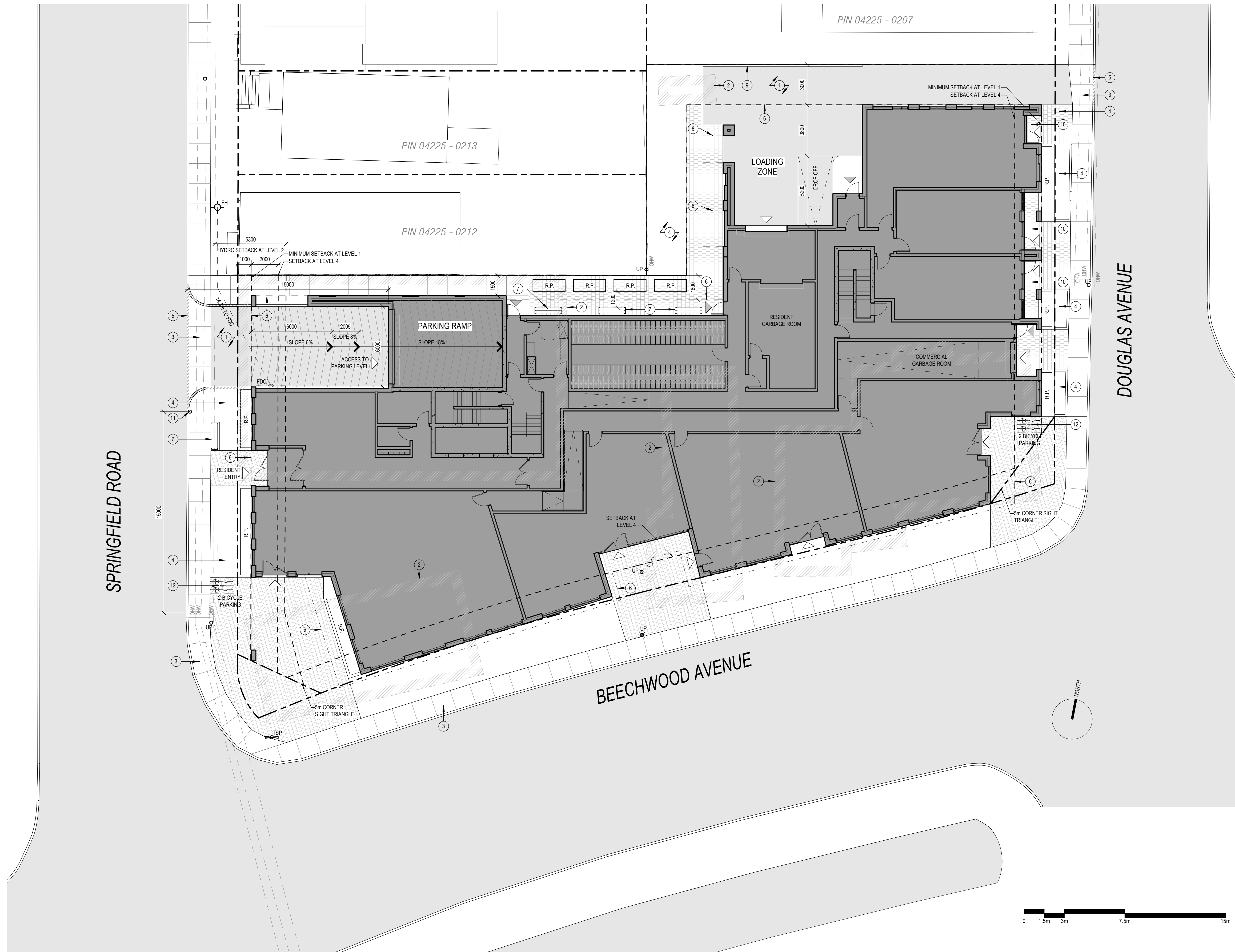
Jeffrey Kelly, MCIP, RPP  
Project Planner

Reviewed by:



Murray Chown, MCIP, RPP  
Director | Planning & Development

Appendix A:  
Site Plan



**SITE PLAN SYMBOLS LEGEND**

	BUILDING ENTRANCE		FIRE DEPARTMENT CONNECTION
	BUILDING EXIT		FIRE HYDRANT
	BICYCLE PARKING		NEW STREET LIGHT
	PROPERTY LINE		STREET LIGHT TO BE REMOVED
	SETBACK LINE		EXISTING STREET LIGHT TO REMAIN
	OVERHEAD WIRES		EXISTING UTILITY POLE TO REMAIN
	INTERLOCKING STONE PAVERS		UTILITY POLE TO BE REMOVED/RELOCATED
	EXISTING TRAFFIC SIGNAL POST		
	RAISED PLANTER		

- SITE PLAN NOTES**
- 1 ASPHALT
  - 2 EXISTING STRUCTURE TO BE DEMOLISHED
  - 3 CONCRETE SIDEWALK
  - 4 SOFT LANDSCAPING
  - 5 DEPRESSED CURB
  - 6 LINE OF CANOPY/BUILDING ABOVE
  - 7 BENCH
  - 8 BALCONY ABOVE
  - 9 CONCRETE CURB. SEE CIVIL
  - 10 RAISED TERRACE. SEE CIVIL
  - 11 BUS STOP SIGN
  - 12 BIKE RACK. REFER TO LANDSCAPE

**OWNER**  
 2317916 ONT INC.  
 2081 MERIVALE ROAD  
 OTTAWA, ON, K2G 1G9

**ARCHITECT**  
 PROJECT1 STUDIO  
 280 ST. PATRICK ST, SUITE 300  
 OTTAWA, ON, K1N 5K5

**PLANNER**  
 NOVATECH  
 240 MICHAEL COWPLAND DRIVE, SUITE 200  
 OTTAWA, ON, K2M 1P6

**LANDSCAPE ARCHITECT**  
 NOVATECH  
 240 MICHAEL COWPLAND DRIVE, SUITE 200  
 OTTAWA, ON, K2M 1P6

**CIVIL ENGINEER**  
 STANTEC  
 300 - 1331 CLYDE AVENUE  
 OTTAWA, ON, K2C 3G4

**SURVEYOR**  
 ANNIS O'SULLIVAN VOLLEBEKK LTD.  
 14 CONCOURSE GATE, SUITE 500  
 OTTAWA, ON, K2E 7S6

**GENERAL ARCHITECTURAL NOTES:**

1. This drawing is the property of the Architect and may not be reproduced or used without the expressed consent of the Architect.
2. Drawings are not to be scaled. The Contractor is responsible for checking and verifying all levels and dimensions and shall report all discrepancies to the Architect and obtain clarification prior to commencing work.
3. Upon notice in writing, the Architect will provide written clarification or supplementary information regarding the intent of the Contract Documents.
4. The Architectural drawings are to be read in conjunction with all other Contract Documents including Project Manuals and the Structural, Mechanical and Electrical Drawings.
5. Positions of exposed or finished Mechanical or Electrical devices, fittings and fixtures are indicated on the Architectural Drawings. Locations shown on the Architectural Drawings shall govern over Mechanical and Electrical Drawings. Mechanical and Electrical items not clearly located will be located as directed by the Architect.
6. These documents are not to be used for construction unless specifically noted for such purpose.

KEY PLAN

1 ISSUED FOR SPC 2023-06-08  
 ISSUE RECORD



**project1 studio**

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PROJ SCALE DRAWN REVIEWED  
 2218 NOTED JH RMK

SITE PLAN

**SP-01**

1 SITE PLAN  
 SP-01 SCALE: 1: 150

