

ÉCOLE ÉLÉMENTAIRE PUBLIQUE ORLÉANS SUD

URBAN DESIGN BRIEF

PREPARED BY

EDWARD J. CUHACI AND
ASSOCIATES ARCHITECTS INC.

FOR

CONSEIL DES ÉCOLES PUBLIQUES DE
L'EST DE L'ONTARIO

September 2023



1.0 Introduction

Edward J. Cuhaci and Associates Architects Inc. (EJC) has been retained by Conseil des écoles publiques de l'Est de l'Ontario (CEPEO) to provide professional services to design a new Elementary School project, including preparation of the Site Plan Control Application at the following property:

Municipal Address: 2405 Mer Bleue Rd, Ottawa, Ontario

Legal Description: Part of Lot 4, Concession 11, Geographic Township of Cumberland, City of Ottawa

The subject lands are located within Suburban Transect of the Official Plan, west end of Mer Bleue Urban Expansion Area 10. The Owner, Conseil des écoles publiques de l'Est de l'Ontario (CEPEO), wishes to construct a 2 storey Elementary School of approximately 3,915 square metres (42,125 square feet) of gross floor area, located at the east part of the property, complete with associated site development.

This Urban Design Brief has been prepared in support of the Site Plan Control Application. Based on the review and analysis contained herein, we concluded that the proposal complies with the various planning and design policies of the City of Ottawa Official Plan

Our Approach

Edward J. Cuhaci and Associates Architects Inc., representing the Owner, has created this Urban Design Brief in alignment with the design vision. It demonstrates how the proposed development complies with the relevant policy guidelines for the Subject Lands.

Should you have any questions or would like to discuss the brief in further detail, please do not hesitate to contact us.

Yours truly,

Edward J. Cuhaci and Associates Architects Inc.



Zofia Jurewicz, OAA, AIA
President

A handwritten signature in blue ink, appearing to read "Xu Feng".

Xu Feng, OAA
Principal

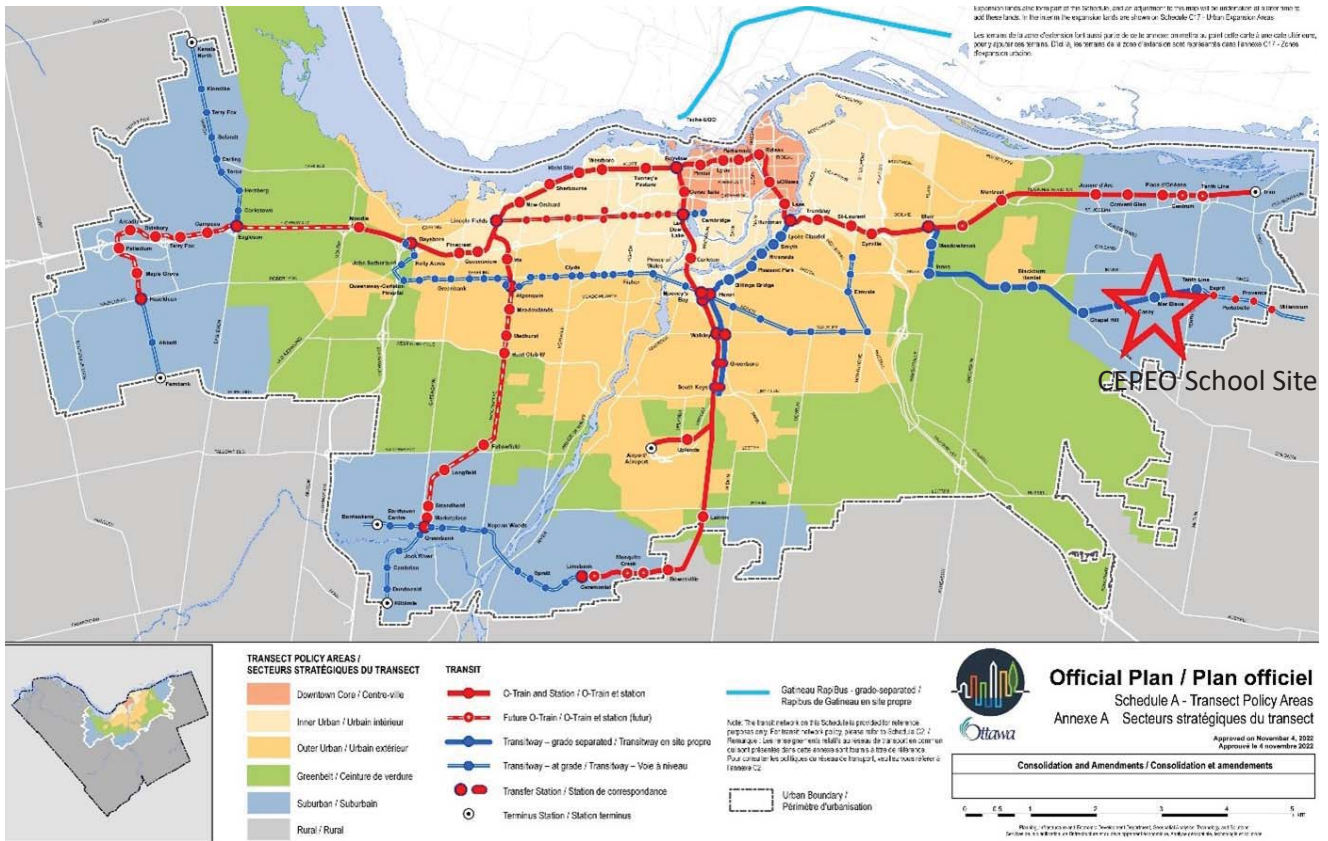


Figure 1.1: City of Ottawa New Official Plan Schedule

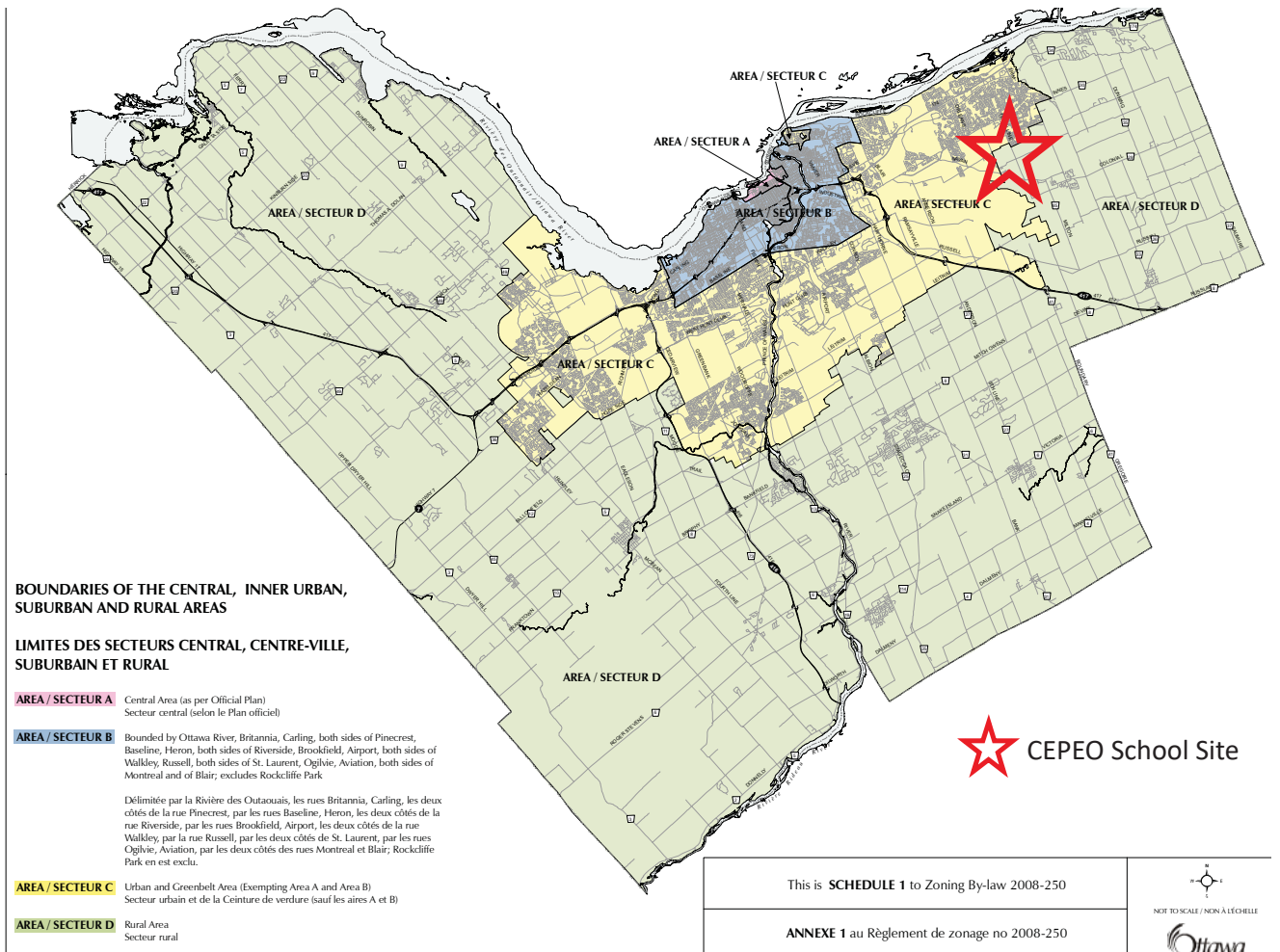


Figure 1.2: City of Ottawa Schedule 1 Zoning By-Law

2.0 Site & Context Analysis

2.1 Existing Context

The proposed school is located to the west of Jerome Jodoin Drive and to the south of Monardia Way. The lot area is 40,484.55 square meters, with approximately 116 meters of frontage along Jerrome Jodoin Drive and about 108 meters of frontage along Monardia Way.

Zoning Designation of the site is Minor Institutional Zone I1A(2530)/Residential R3Z. There is an existing single family detached home located at the southwest corner of the property that accessed from Mer Bleue Road. The balance of the site was used for agricultural purposes and currently is vacant. A summary of the uses that surround the subject lands include the following:

EAST low to medium density residential subdivision known as Summerside West by Mattamy

NORTH low to medium residential subdivision known as Summerside West development by Mattamy. Georges Dassylva Park, a neighbourhood park is located right across Monardia Way.

SOUTH vacant land held for future residential development.

WEST Mer Bleue Road, which is an arterial road, subject to future Mer Bleue Road Realignment along school property line. Lands west of Mer Bleue Road include single family detached homes and vacant land held for future residential

Photographs of the context:

- 1 Looking southwest from the subject land, there is a property featuring a 1-storey single detached residential dwelling that faces Mer-Bleue Road
- 2 When viewing the site from Mer-Bleue Road, there is a row of townhouses located to the north of the subject property
- 3 View to the south of the site from Jerome Jodoin Drive is a vacant land held for future residential development
- 4 Site perspective from Jerome Jodoin Drive towards the subject property
- 5 View from Monardia Way, with single detached dwellings located to the east of the property
- 6 Single detached dwellings are visible along Jerome Jodoin Drive when viewed from Monardia Way
- 7 To the east of Mer-Bleue Road, there is a series of 3-storey townhouses on the property
- 8 View of the crossing of Willow Aster Cir and Mer-Bleue Road, there are a series of 2-storey and 3-storey townhouses

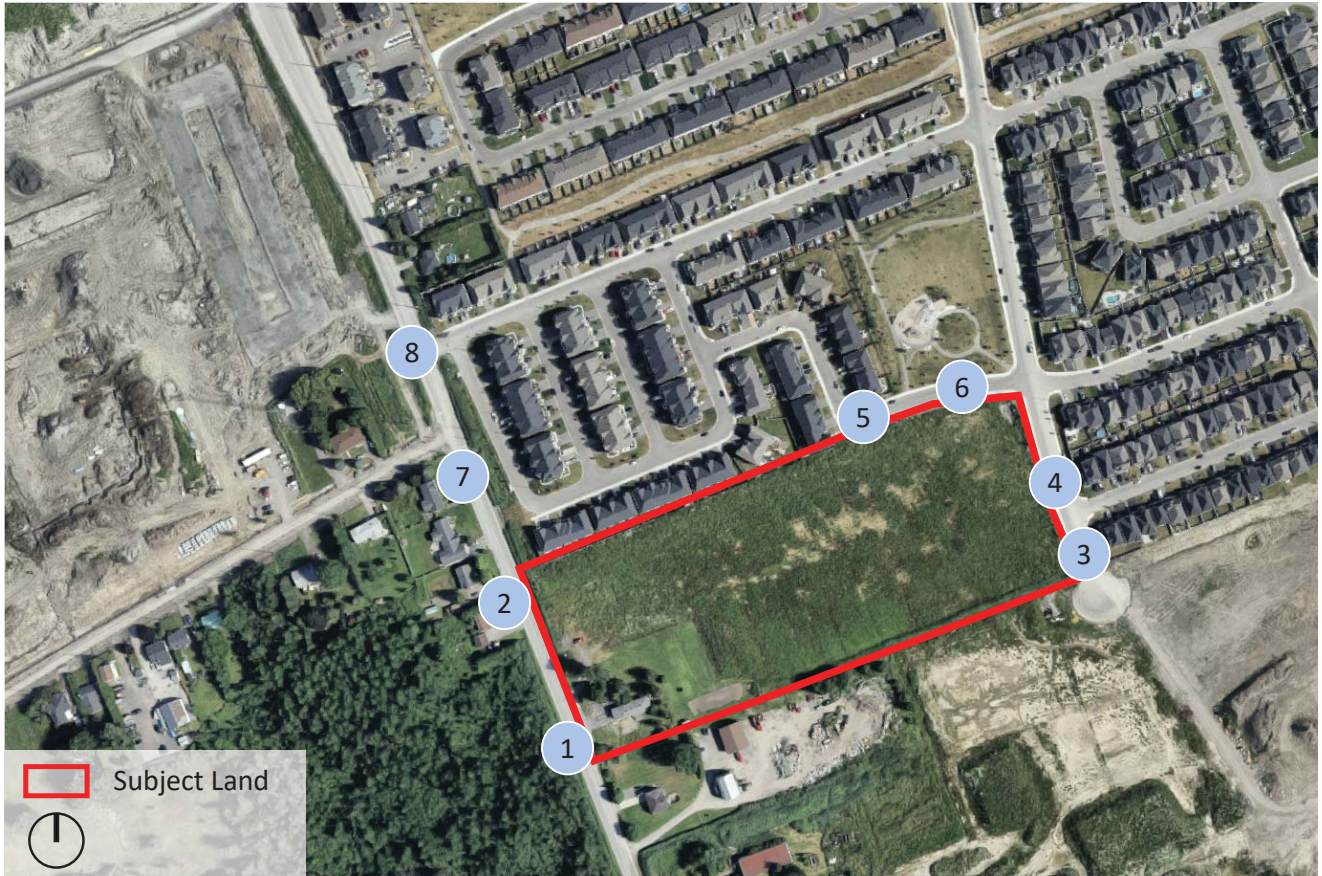


Figure 2.1: Aerial view of the Subject Land within the context

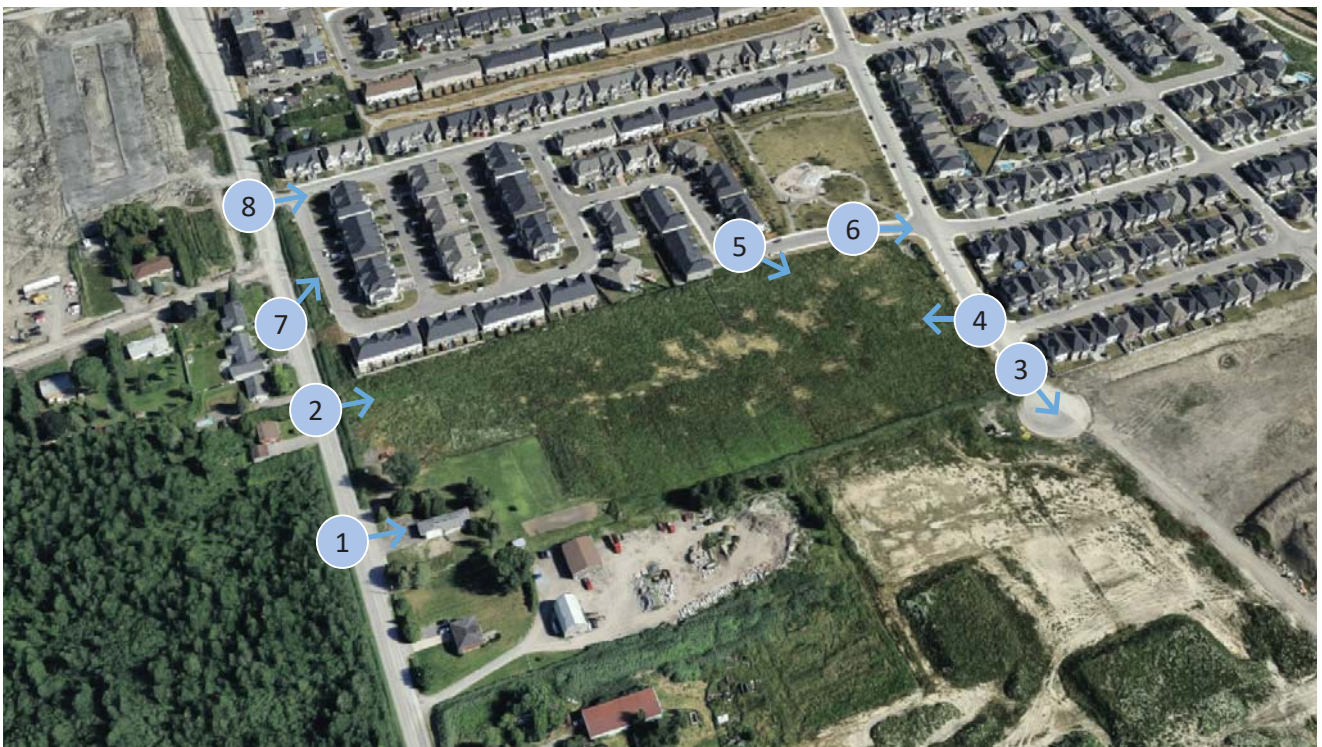


Figure 2.2: Aerial 3D view of the context



Figure 2.3: Photographs of the context



Georges Dassyva Park

Georges Dassyva Park spans 2.0 acres and offers opportunities for walking, gatherings, family activities, and includes amenities such as a playground and picnic furniture.

- 1 Park perspective from the Monardia Way and Jerome Jodoin Drive intersection
- 2 View from the playground
- 3 The Playground and the picnic area in the park
- 4 View of the park from Monardia Way



Figure 2.4: Photographs of Georges Dassyva Park

2.2 Surrounding Roads and Transit Network

The school site is located between Mer Bleue Road and Jerome Jodoin Drive. Mer Bleue Road is identified as an “Arterial Road” on Transportation Master Plan Map 6, Road Network – Urban. It serves through the travel between points not directly served by the road itself and only limited direct access is provided to major parcels of adjacent lands. Mer Bleue Road is subject to future road re-alignment along school west property line. Jerome Jodoin, which is a Collector Road, serves neighbourhood travel to and from major collector or arterial roads and provides direct access to adjacent lands. It currently terminates at the southeast corner of school property and will be extended to the south in the future.

The school site is currently served by 32 Blair bus route. There are 2 bus stops within approximately 250m range from the proposed school. Bus 302 on Mer Bleue runs only on Tuesdays, and there is no bus stop in the vicinity of the site.



Figure 2.5: City of Ottawa Schedule E - Urban Road Network

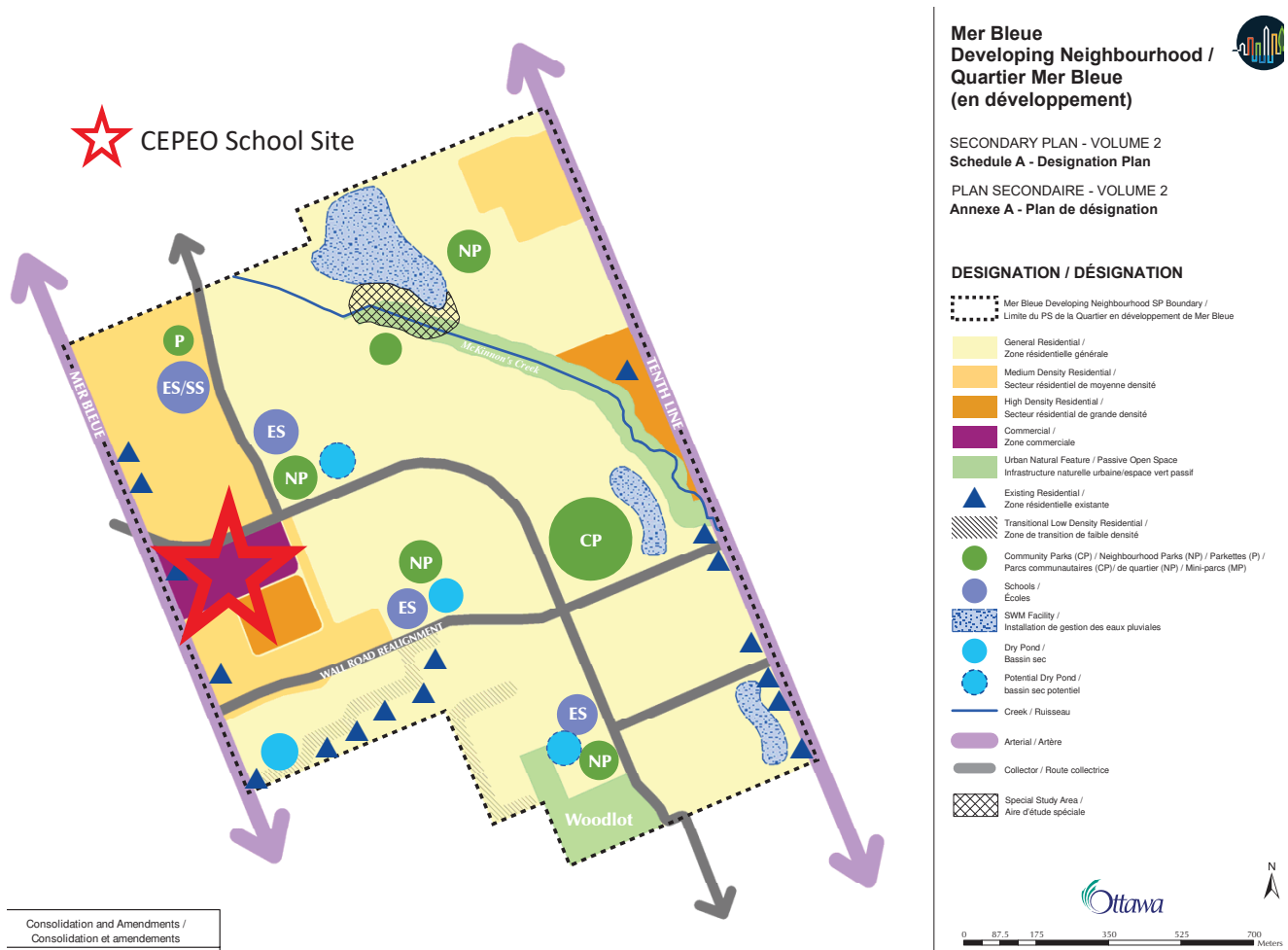


Figure 2.6: Mer Bleue Urban Expansion Area Community Design Plan - Land Use Plan



Legend

- Transit Stops
- 302 Bus Route
- 32 Bus Route
- Subject Lands

Figure 2.7: Transit Map

3.0 Conformance to Policies and Design Directions

3.1 Conformance to the new City of Ottawa Official Plan

The new Official Plan includes a specific set of planning and design policies in section 4.10 School Facilities. The following provides a commentary of how the proposal complies with these policies.

Section 4.10.1 entitled ‘Make it safe and easy to walk, bike or take a bus to school through supportive site and neighbourhood design’ includes the following policies: School buildings shall be sited in a compact and land-efficient manner. School sites shall prioritize safe, sustainable, active transportation mobility choices including walking and cycling as the primary means of travel to and from school.

Comment:

The subject property is surrounded by existing and future residential neighbourhoods, well served by roads and sidewalks. The two wings of the ‘L’ shaped school will be arranged along Jerome Jodoin drive (east) and Monardia Way (north), with the main entrance of the school placed close to the intersection of two streets. The school will be conveniently accessible for pedestrians from the neighbourhood on the sidewalks along both streets.

Public transportation will be provided by school buses utilizing lay-by parking lanes that to be located in the right-of-way along Jerome Jodoin Drive and Monardia Way to ensure the safe transportation of school children. Bulb-outs will also be incorporated on Jerome Jodoin Dr. to reduce vehicle speed as a traffic calming measure. Vehicular traffic into school property will be limited from Jerome Jodoin Drive only located to the south of the school. The parking lot is to be buffered with landscaping from Jerome Jodoin Drive. A separate drop-off lane will also be provided in the school parking lot for parents to use.

Section 4.10.2 Locate schools and other neighbourhood uses close together to provide convenient access to residents.

Comment:

the proposed school is to be 2 stories in height and will be sited adjacent to 2 public streets. The location of the building will meet setback requirements of the Institutional Zone being applied for. While the school property is adjacent to an arterial road - Mer Bleue Road to the west, the elementary school site will be located in the east portion of the property. The west part of the property will be reserved for future development. The outdoor children’s play areas for the elementary school will be far away from the arterial road. The potential health risks associated with air pollution cause by traffic will be mitigated. The new elementary school will include a licensed child-care centre to serve the local community. CEPEO also acknowledges that where the opportunity permits, the school board and the City may enter into an agreement for shared recreation amenities in parks, where appropriate.

Section 4.10.3 Make trees an important component of a school’s outdoor space

Comment:

New trees will be planted in the playground of the childcare facility and incorporated into other functional spaces such as exterior play area and the outdoor classroom of the new elementary school. There is an existing one storey single family detached home located at the southwest corner of the property. The existing house and trees around it will not be affected by the new development of the elementary school. The school site will be located in the east part of the subject land.

3.2 Conformance to the Mer Bleue Expansion Community Design Plan

The Mer Bleue Expansion Area Community Design Plan has a number of policies and guidelines intended to be evaluated for compliance.

The new elementary school will be located on the corner lot designated to CEPEO schools on Community Design Plan. The site is planned and sized to accommodate an elementary and a secondary school on the same parcel of land. This proposal of new elementary school is in response to immediate needs for many young families in the fast-growing community. The school will be located on the east part of the site, facing both Monardia Way (north) and Jerome Jodoin Drive (east) which is a collector street that are easily accessible for pedestrians, cyclists, school buses, etc. Soccer field, exterior classroom and landscaped children's playground will be located to the west of the school, next to the site of future high School. These facilities can be easily extended towards west and shared with future high school, creating a healthy and sustainable interface between the two. The new elementary school will also include a licensed childcare facility.

Georges Dassylva Park, an existing neighbourhood park is located right across the Monardia Way to the north of the proposed elementary school. There are play structures, sitting and picnic areas in the park allowing for joint use of recreation facilities and facilitate a more efficient use of the land.

The Mer Bleue Urban Expansion Area Community Design Plan also provides guidelines of school site design and built forms.

The proposed 2 storey school will have a welcoming main entrance facing Monardia Way with unique architectural characters. Building facades will consist of durable as well as aesthetically pleasing materials - a combination of bricks, prefinished masonry blocks, wood textured composite aluminum panels and glazed curtain wall system.

In addition to the school bus lay-bys along both Jerome Jodoin Drive and Monardia Way, bike racks will also be provided next to the sidewalks at two convenient locations for both students and the staff. Bulb-outs will be installed on Jerome Jodoin Drive as a traffic calming measure. School parking lot will be located to the south of the school, with landscape buffers from public road. Continuous sidewalks will provide safe access to various school entrances from bus lay-bys, parking lot and areas of bike parking. Lighting for school buildings and parking areas will be directed away from adjacent properties. A in-ground school signage will be located in the landscaped area close to the road intersection. Pad mount transformer will be screened from public view by landscaping, please refer to landscape drawings.

3.3 Conclusion

The proposed new elementary school development will respect the characteristics of the existing community. It will be a pedestrian-friendly, visually appealing, and harmonious addition to the neighborhood. The proposed site plan aligns with both the City of Ottawa Official Plan's School Facilities guidelines and the design policies outlined in the Mer Bleue Expansion Area Community Design Plan, as detailed in this report.

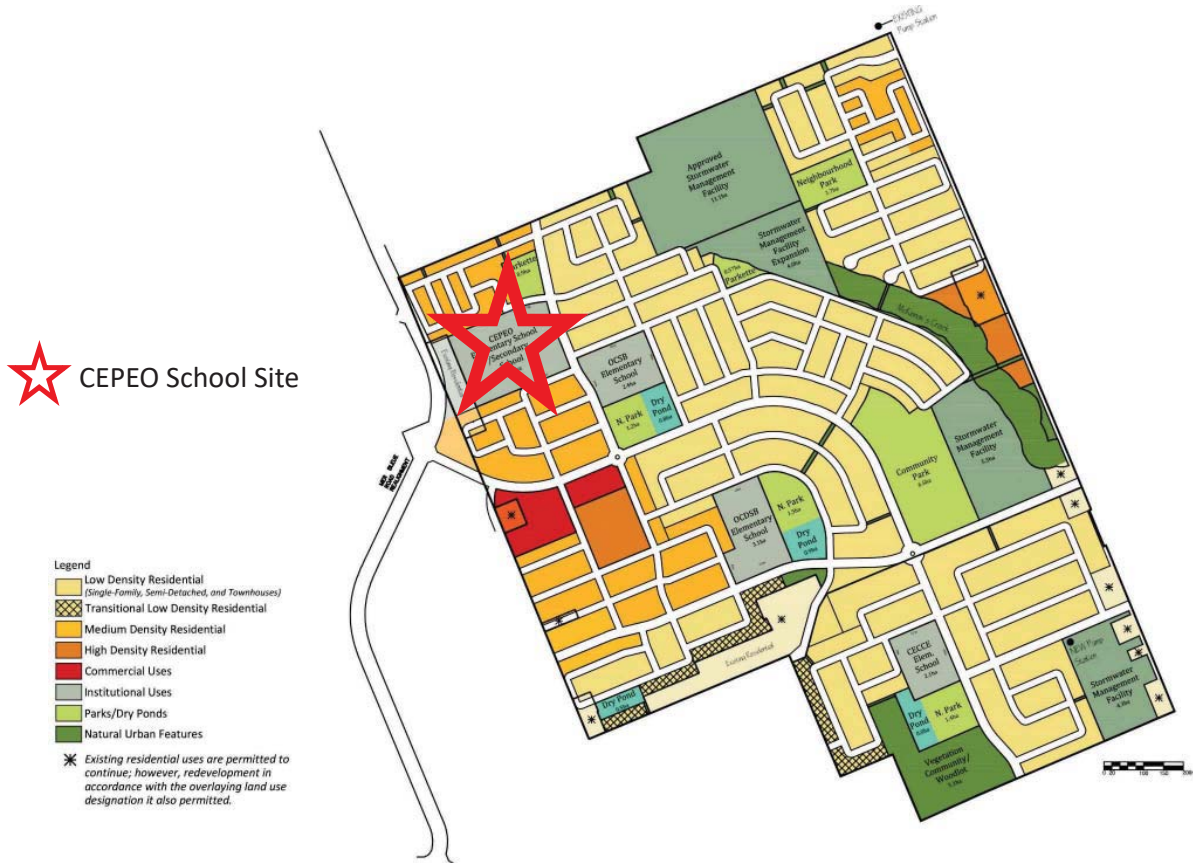


Figure 3.1: Demonstration Plan
 From: Mer Bleue Expansion Area - Community Design Plan [City of Ottawa 2017]



Figure 3.2: Pathway and Greenspace Plan
 From: Mer Bleue Expansion Area - Community Design Plan [City of Ottawa 2017]

4.0 Design Proposal

4.1 Overall Vision Statement & Goals for the Proposal

The proposed siting of the new elementary school building will help to create urban street fronts adjacent to the intersection of Jerome Jodoin Drive and Monardia Way. The urban design and building massing will strive to reflect the following principles:

- Compatible and fit planning patterns and network of the community
- Active 21st Century learning facility will be visible from both streets, creating a sense of community and a sense of identity
- Materiality of the building will reflect the permanence as well as energetic, progressive image appropriate for elementary school.
- The building and the site will be fully accessible.

The design proposal is to show the building elevations, exterior details, transitions in form, treatment of the public realm and computability with adjacent buildings, using 3-D models, illustrations, plans and diagrams. Referencing new Official Plan, as determined at time of the pre-application consultation meeting, submissions will need to address the following in the forms of labeled graphics and written explanations.

4.2 Design Brief

The proposed 3,913 sqm two storey school building located on 40,484 sqm site is set in the east part of the property, close to the intersection of Jerome Jodoin Drive and Monardia Way. The architectural forms, elevations and the selection of building materials will animate the intersection and create its unique identity as well as dialogues with the community. The site development will include new 51 parking spaces, 2 of which are barrier-free spaces, 54 bicycle parking spaces, 1 loading space, utilities, sidewalks, play areas and landscaping.

The School will be designed as a “Building Block of the Community.” Exterior and interior social and activity facilities will be designed for a strong community presence. The new school will also be designed to support the 21st Century learning. Various spaces will be designed to create an active learning environment: project and problem based learning, multiple modes of learning, and leverages technology as a means to collaborate, communicate and learn.

- The Library and Classrooms will provide a variety of flexible furniture and furniture layout with opportunities for group learning and for the use by the community.
- Design will provide opportunities and encourage interaction and learning outside the classrooms
- Colours, natural light and combination of various quality building materials and finishes will enliven the space.

The new school facility will include the following programs:

- Educational Programs: Gymnasium, Change Rooms, Library, Multi-purpose Room, Classrooms and Kindergarten Classrooms
- Administration and Staff Offices: Administration Suite, Staff Lounge, Staff Workrooms, Supporting Spaces.
- Student Activities Spaces: Collaboration Spaces.
- Common Use Spaces: Restrooms, Storages
- Building Services: Mechanical Rooms, IT Rooms, Electrical Rooms, Janitor Rooms.
- Childcare Centre, which includes Playrooms, Washrooms, Office and Support Spaces.



- | | |
|---------------------------------|------------------------|
| 1. Proposed Elementary School | 8. Outdoor Classroom |
| 2. Soccer Field | 9. Daycare Entrance |
| 3. Main Entrance | 10. Daycare Playground |
| 4. Elementary School Playground | 11. Future Portables |
| 5. Bicycle Racks | 12. Parking Lot |
| 6. Bus Lay-Bys | 13. Loading Area |
| 7. Parent's Drop Off | |

Figure 4.1: Proposed site plan



Legend





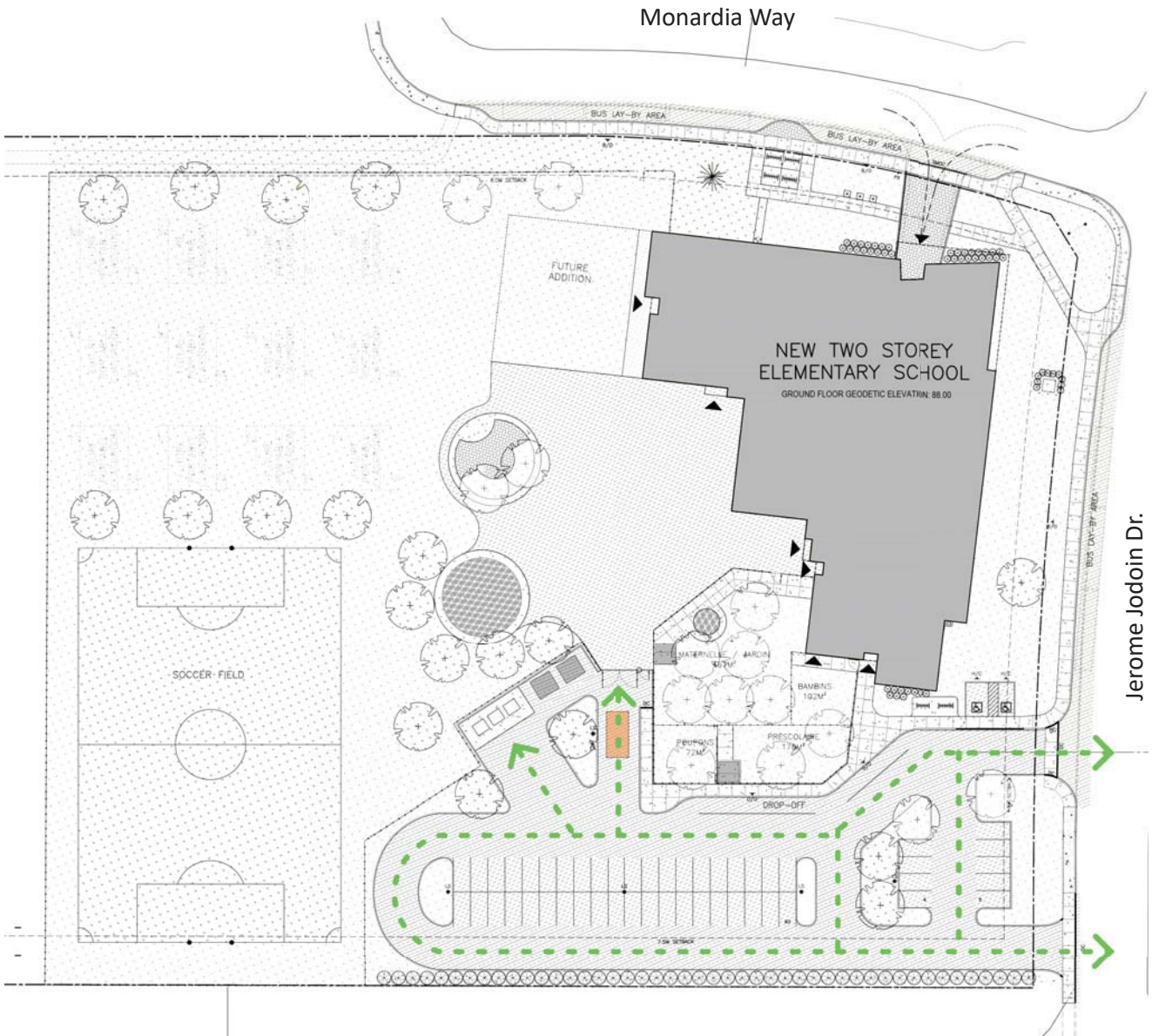
-  Vehicular Entrances
-  Bike Racks
-  Pedestrian Circulation
-  Landscape

Figure 4.2: Landscape Design Plan showing principal elements



Legend



-  Loading Area
-  Vehicular Movement

Figure 4.3: Site Plan showing vehicular movement and loading areas

4.3 Building Form and Material Consideration

The school property is generally surrounded by low to medium density two storey residential and future residential dwellings to the north, the east and the south. Proposed two storey school will be compatible and fit the context of adjacent residential neighbourhood and will not overwhelm existing Georges Dassylva Park right across Monardia Way.

The new school will adopt a compact rectilinear geometry. Gymnasium will be centrally located with two classroom wings facing Monardia Way to the north and Jerome Jodoin Drive to the east. The Library, with large glazed windows, will be located at the northeast corner of the building, close to the intersection. It will provide visual connections and the library itself, as an important component of the school, will be visible from both streets. Main entrance of the building will be facing Monardia Way, next to the library. Wood colour / textured aluminum soffit and partial wall panels will create a friendly, warm, and welcoming entrance to the school. The “Y” shaped structural steel support for the canopy will also create a sense of identity of the building.

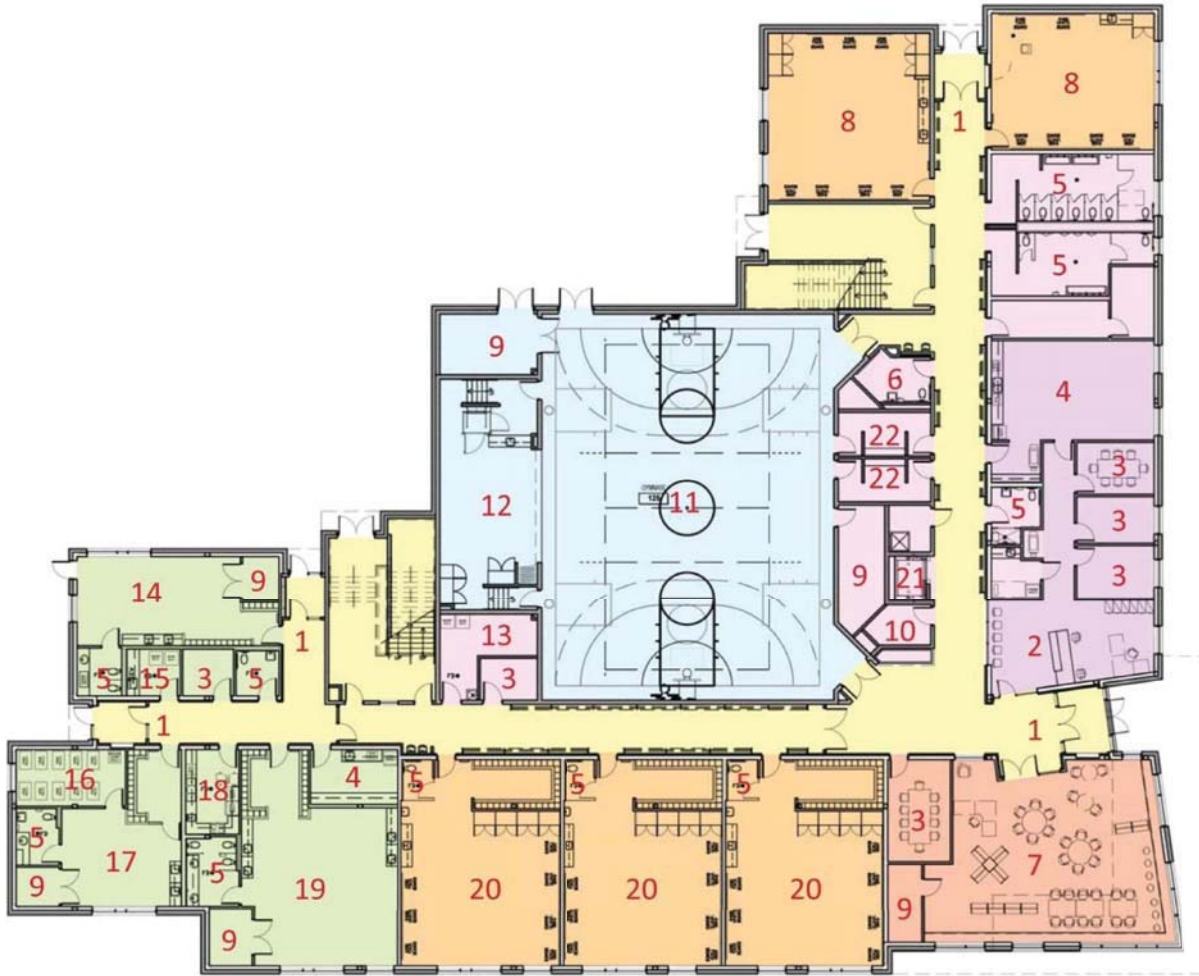
Exterior of the new school will consist of various quality and durable construction materials: bricks, pre-finished concrete blocks, composite aluminum panels, and insulated curtain wall glazing system. The roofing system will be a two-ply modified bituminous system by Soprema or approved equivalent.

The parking lot will be located to the south of the school, buffered from Jerome Jodoin Drive by landscape. It will have internal drop-off lanes which are completely separated from school bus lay-bys on the street. Exterior play areas and outdoor classroom will be located to the west of the school. Trees will be planted to provide shades in the summer in various locations.

The following are a few graphics and plans of the proposed new Elementary School:

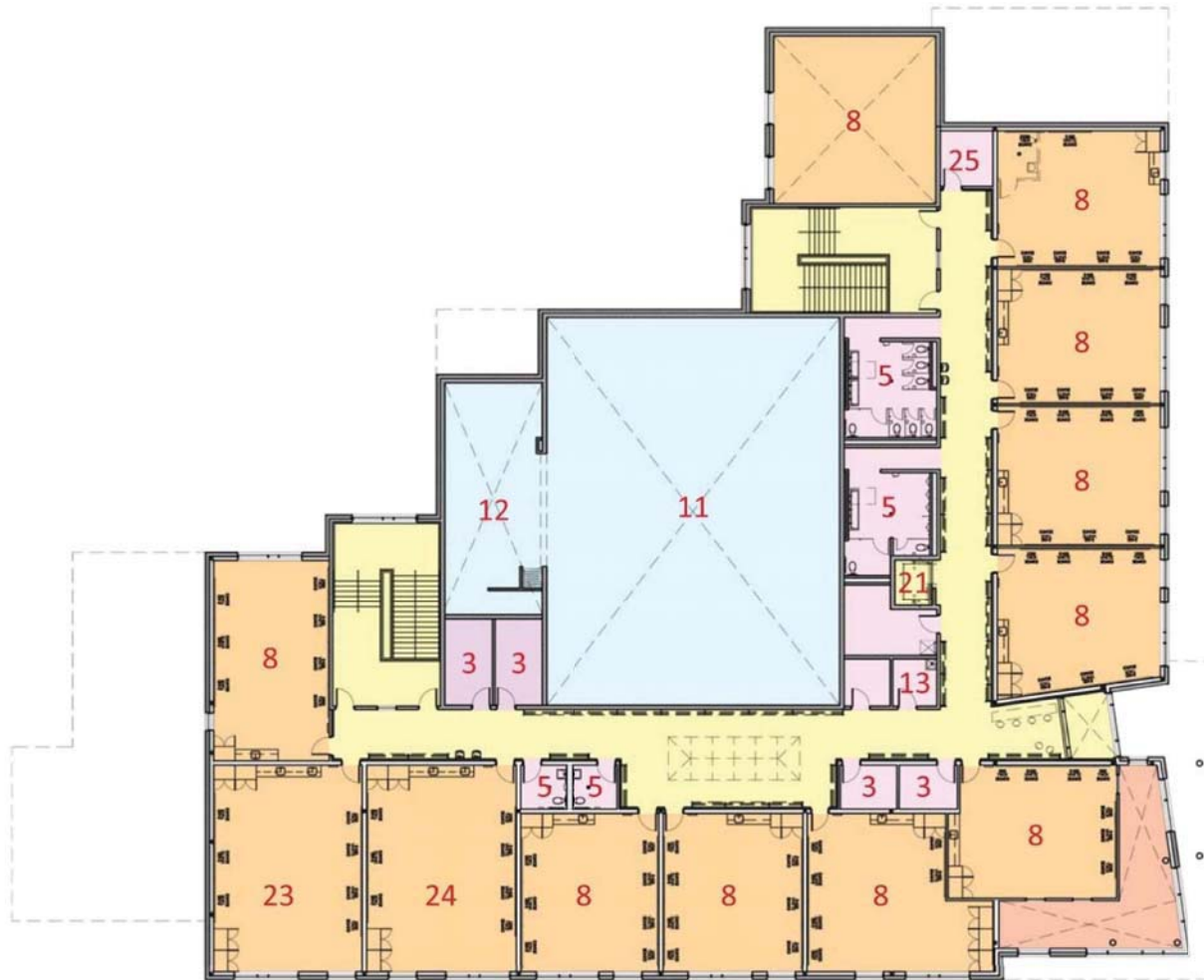
Figure 4.4: Perspectives of the proposed school





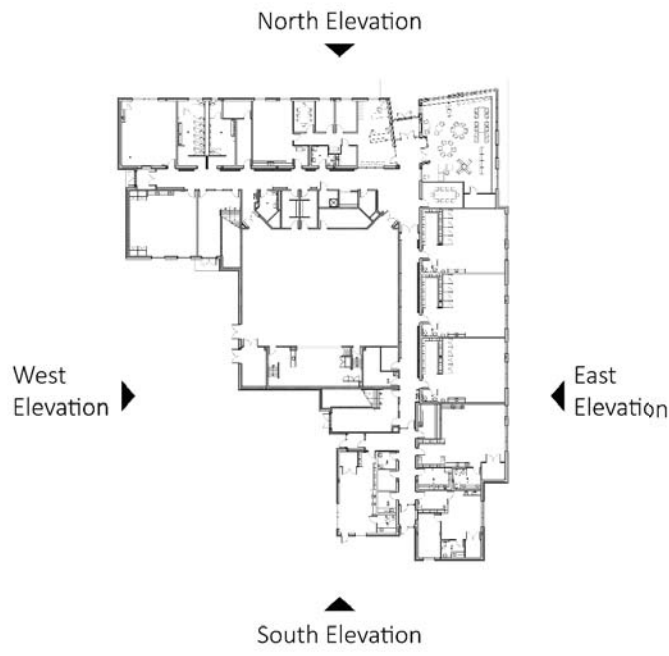
- | | |
|---------------------------|---------------------------------|
| 1. ENTRÉE | 14. BAMBINS |
| 2. ADMINISTRATION GÉNÉRAL | 15. BUANDRIE |
| 3. BUREAU | 16. SALLE DE REPOS |
| 4. SALON DU PERSONNEL | 17. POUPONS |
| 5. TOILETTE | 18. CUISINE |
| 6. TOILETTE UNIVERSEL | 19. SALLE D'ACTMTÉ PRÉSCOLAIRES |
| 7. BIBLIOTHÈQUE | 20. MATERNELLE / JARDIN |
| 8. CLASSE | 21. ASCENSEUR |
| 9. ENTERPÔT | 22. VESTIARE |
| 10. SERVEUS | 23. ARTS |
| 11. GYMNASSE | 24. SCIENCES |
| 12. SCÈNE | 25. SENSORIELLE |
| 13. CONCIERGE | |

Figure 4.5: Ground Floor Plan



- | | |
|---------------------------|---------------------------------|
| 1. ENTRÉE | 14. BAMBINS |
| 2. ADMINISTRATION GÉNÉRAL | 15. BUANDRIE |
| 3. BUREAU | 16. SALLE DE REPOS |
| 4. SALON DU PERSONNEL | 17. POUPONS |
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| 10. SERVEUS | 23. ARTS |
| 11. GYMNASSE | 24. SCIENCES |
| 12. SCÈNE | 25. SENSORIELLE |
| 13. CONCIERGE | |

Figure 4.6: Second Floor Plan








- LEGEND**
-  Brick Veneer
 -  SMU Block Veneer
 -  Alucabond Aluminum Panels
 -  Vertical Metal Siding
 -  Vision Insulated Glass Units in Aluminum Curtain Wall

Figure 4.7: Proposed elevations



Figure 4.8: Perspectives of the proposed school