



788 March Road

Urban Design Brief
June 24, 2024

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Project Description

Proposed Development

Project Description

Fotenn Consultants Inc., acting as agents for SINA, is pleased to submit the enclosed Site Plan Control Revision Application for the lands municipally known as 788 March Road (“the subject property”) in the Kanata North community of the City of Ottawa. The intent of this Design Brief is to illustrate how the proposed development represents a high-quality and context sensitive design that implements policies of the Official Plan, and Council approved plans and guidelines.

Proposed Development

The proposed development consists of two (2) six (6) storey mid-rise residential apartment buildings consisting of a total of 196 units (Figure 1). Both buildings share a common underground parking garage and will be developed concurrently. The proposed development is designed in a “U” shape, with minimal setbacks from the subject property’s frontages along March and Klondike Roads allowing it to maintain an active street frontage where the subject property abuts public rights-of-way. The proposed development is also sited to provide an adequate separation from Shirley’s Brook and its surrounding riparian corridor.

275 vehicle parking spaces are proposed onsite, 252 of which are proposed to be located in a two (2) storey underground parking garage. The remaining 23 spaces will be located at the rear of the buildings, largely hidden from the public right-of-way.

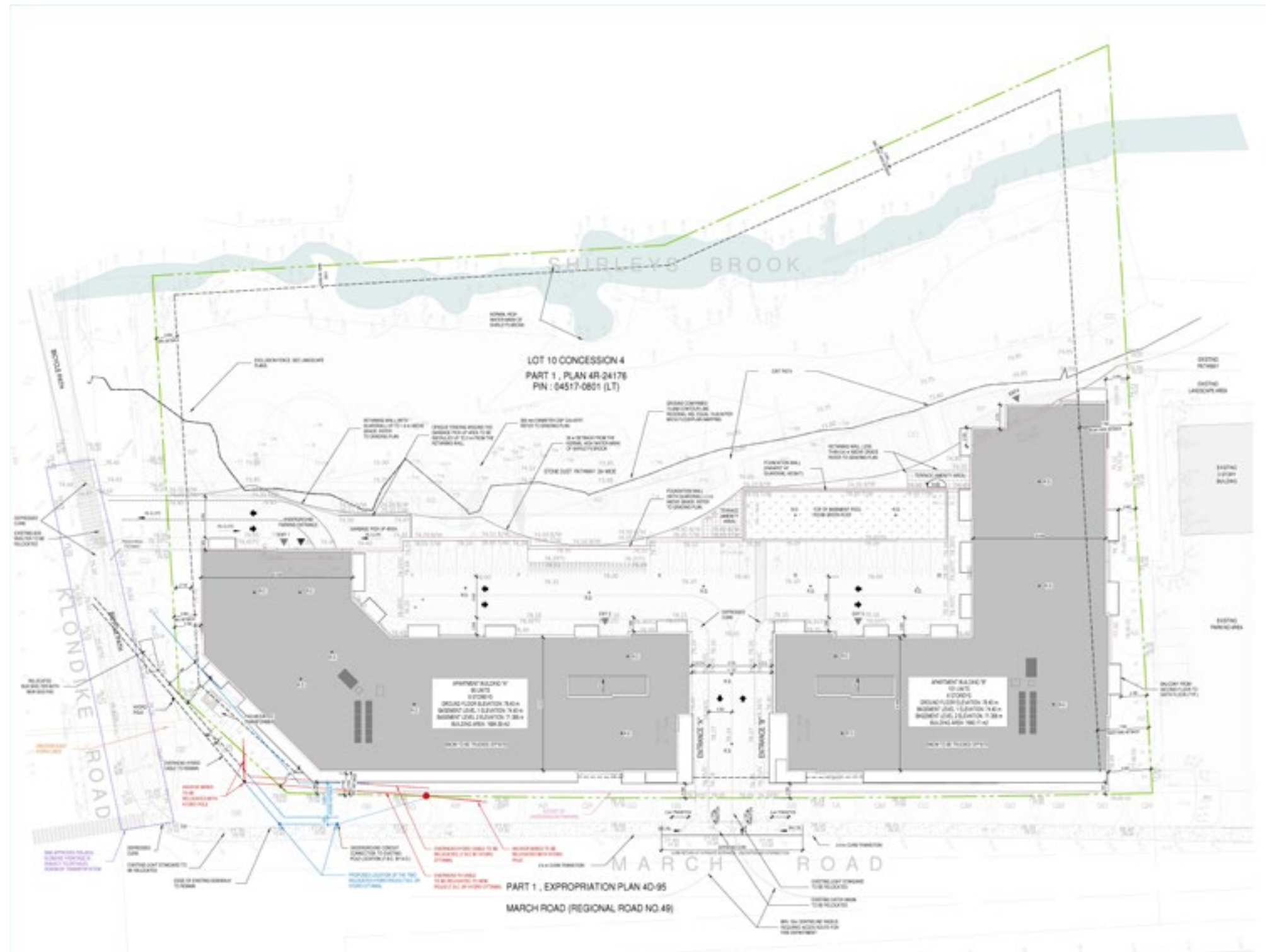


Figure 1: Excerpt from the Site Plan of the proposed development

Proposed Development

Project Statistics

Statistic	Proposed	
Lot Area	12,210.01 m ²	
Building Footprint	3,235 m ²	
Gross Floor Area	16,451 m ²	
Floor Area of Uses	Residential	16,451 m ²
Dwelling Units	196 units	
Dwelling Unit Breakdown	1 bedroom:	48 (25%)
	1+den:	49 (25%)
	2 bedroom:	65 (33%)
	2+den	22 (11%)
	3 bedroom	12 (6%)
Lot Coverage	26.5%	
Maximum Building Height	21.2 m	
Minimum Front Yard Setback (Klondike Road)	3.71 m	
Minimum Corner Side Yard Setback (March Road)	3.17 m	
Minimum Interior Side Yard Setback	38.91 m	
Minimum Rear Yard Setback	3.06 m	
Floor Space Index	1.3	
Amenity Area	Total	1,823 m ²
	Communal	600 m ²

Statistic	Proposed	
Vehicle Parking	275 parking spaces	
	Residential	236
	Visitor	39
Vehicle Parking Location	At-grade	23
	Underground	213
Driveway Width	6.0 m	
Drive Aisle Width	6.0 m	
Bicycle Parking	194 bicycle parking spaces	
Bicycle Parking Type	Horizontal	194
Bicycle Parking Location	Indoor	165
	Outdoor	29
Bicycle Parking Aisle Width	Aisle width ≥ 1.5 m	95
	Aisle width < 1.5 m	99

Design Directives

Design Directives

City of Ottawa Official Plan (2021)

The subject property is located in the Suburban Transect and is designated Mainstreet Corridor. The Corridor also generates an Evolving Neighbourhood overlay, which applies to the adjacent lands.

Mainstreet Corridor

The Mainstreet Corridor designation applies to bands of land along specified streets whose planned function combines a higher density of development, a greater degree of mixed uses and a higher level of street transit service than abutting Neighbourhoods, but lower density than nearby Hubs. The Mainstreet Corridor designation applies to the subject property and adjacent lands.

Corridor policies that are relevant to the subject property and proposed development include:

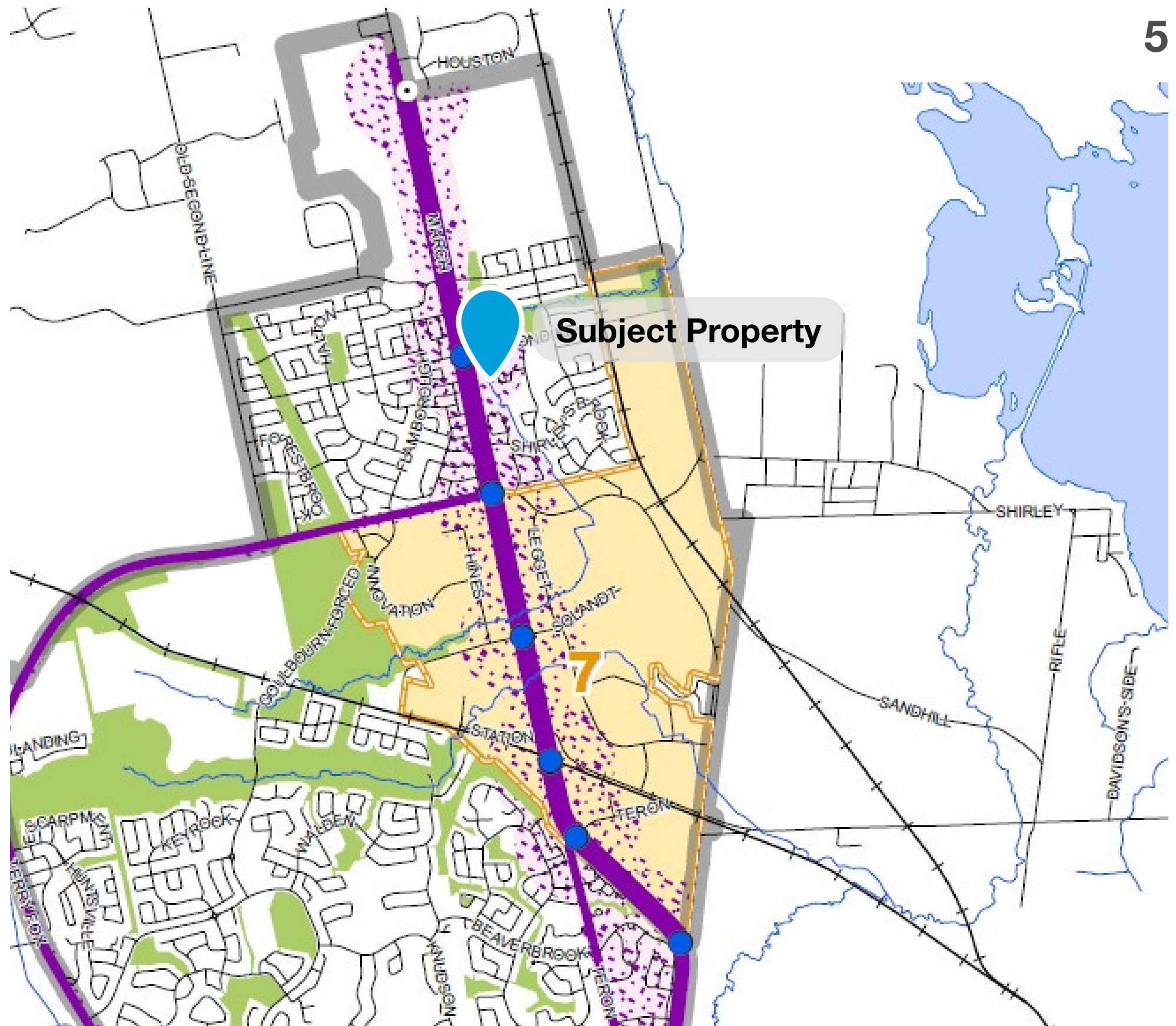
- / Locate the maximum permitted building heights and highest densities close to the Corridor, subject to building setbacks where appropriate;
- / Ensure appropriate transitions in height, use of land, site design and development character through the site, to where the Corridor designation meets abutting designations;
- / When land fronts on both a Corridor and a parallel street or side street:
 - Address the Corridor; and
 - Provide vehicle access from the side street.

Growth Management Framework

The Density Targets for Mainstreet Designation are as follows:

- / Minimum Area-wide Density Requirement, People and Jobs per Gross Hectare: 120
- / Minimum Residential Density Requirement for Intensification, Dwellings per Net Hectare: 120
- / Minimum Proportion of Large-household Dwellings within Intensification: minimum 5%, target 10%

17% of proposed units are considered Large-household dwellings and include 2+den and 3-bedroom units.



Schedule B5 - Suburban (West) Transect (City of Ottawa Official Plan, 2022)

Design Directives

City of Ottawa Official Plan (2021)

Urban Design

The site is subject to Urban Design policies located in section 4.6 of the Official Plan. The subject property is located in a Design Priority Area by virtue of its designation as a Mainstreet Corridor. Table 5 of the Official Plan identifies three (3) Design Priority Area tiers, which include:

- Tier 1 - International
- Tier 2 - National & Regional
- Tier 3 - Local (Major)

Design Priority Area

Tier 3 – Local (Major)
Mainstreet Corridors and Hubs outside of the Downtown Core; Village Cores; and Kanata North Economic District
Tier 3 areas define the image of the city at the local level. Characterized by neighbourhood commercial streets and village mainstreets, these areas provide a high-quality pedestrian environment. The areas within Hubs around existing rapid transit stations are locations for higher densities and intensification.
Tier 3 areas also represent emerging areas that may contribute to defining Ottawa’s local image in the future and areas that represent hubs of significant economic activity. These include commercial streets reflecting a suburban built form that may transition into a more walkable environment.

Responses to Urban Design Policies are summarized in the below table:

Responses to Urban Design Policies

Policy	Response
4.6.1 Promote design excellence in Design Priority Areas	
5) Development and capital projects within DPAs shall consider four season comfort, enjoyment, pedestrian amenities, beauty and interest through the appropriate use of the following elements: <ul style="list-style-type: none"> a) The provision of colour in building materials, coordinated street furniture, fixtures and surface treatments, greening and public art, and other enhanced pedestrian amenities to offset seasonal darkness, promote sustainability and provide visual interest; b) Lighting that is context appropriate and in accordance with applicable standards and guidelines; and c) Mitigating micro-climate impacts, including in the winter and during extreme heat conditions in the summer, on public and private amenity spaces through such measures as strategic tree planting, shade structures, setbacks, and providing south facing exposure where feasible. 	<ul style="list-style-type: none"> / A variety of materials are used to distinguish different parts of the massing and complement articulation and setbacks. / Lighting will be appropriately incorporated into the proposed development, with a future Site Lighting Plan ensuring minimal light spillover on adjacent sensitive areas; and / The main building facades are south-facing.

Policy	Response
4.6.2 Protect views and enhance Scenic Routes including those associated with national symbols	
4) Development abutting Scenic Routes, as identified on Schedule C13, shall contribute to conserving or creating a desirable context by such means as: <ul style="list-style-type: none"> a) Protecting the opportunity to view natural and cultural heritage features; b) Preserving and restoring landscaping, including but not limited to distinctive trees and vegetation along the right of way; c) Orienting buildings towards the Scenic Route and providing direct pedestrian access, where appropriate; 	<p>March Road is identified on Schedule C13 as a Scenic Route.</p> <ul style="list-style-type: none"> / A public multi-use path will be constructed along Shirley’s Brook to protect the opportunity for the public to view natural heritage features; / The building’s materiality complements March House, a heritage building across the street; / Trees and landscaping are proposed in the public right-of-way along March Road; / Buildings are oriented to front onto and face March Road. Direct pedestrian access to both buildings is provided from the sidewalk on March Road; and
<ul style="list-style-type: none"> d) Providing screening by way of opaque fencing or landscape buffers to hide surface parking lots or outside storage; and e) Managing the intensity and spill-over of lighting on adjacent parcels. 	<ul style="list-style-type: none"> / Due to the grade change between the site and Shirley’s Brook, the at-grade parking area is at a higher elevation than the creek; the wall of the parking garage will also function as a visual barrier. No exterior storage is proposed; and / The buildings fronting onto March Road provide a visual barrier between the public realm and the small surface lot at the rear of the subject property.

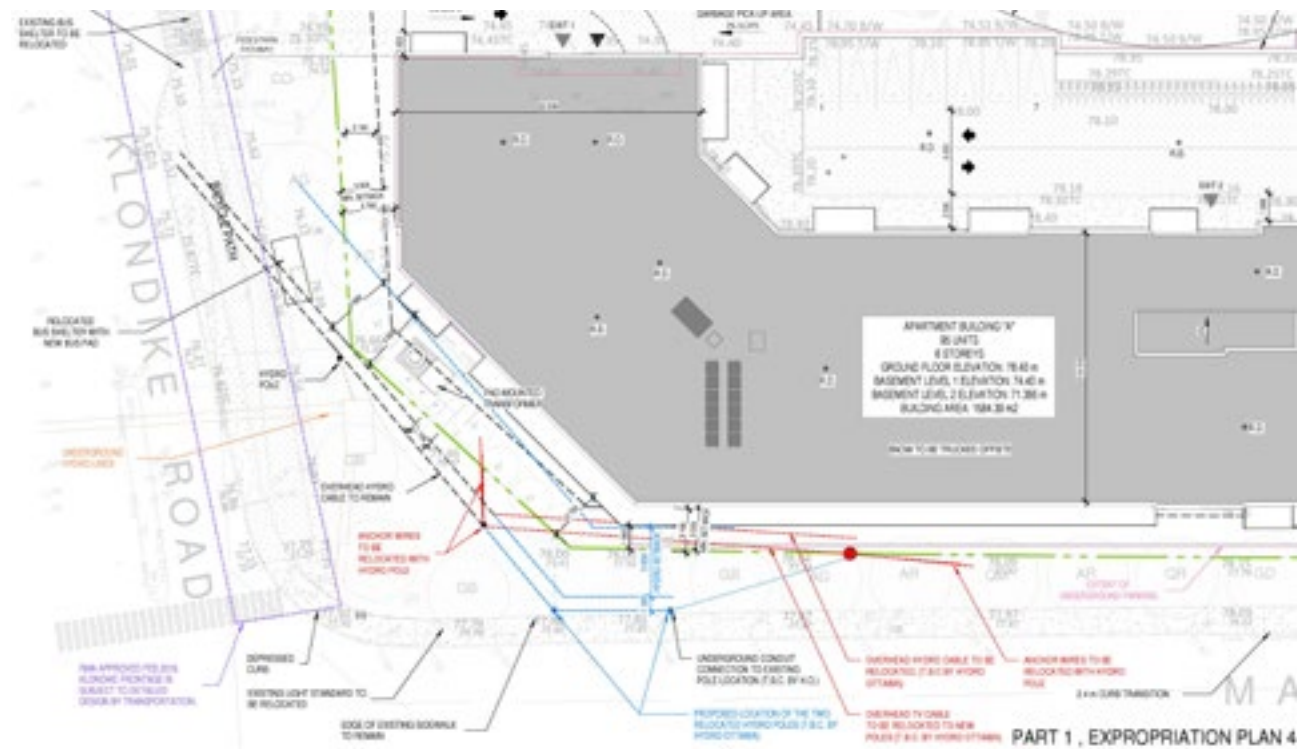
Design Directives

City of Ottawa Official Plan (2021)

Policy	Response
4.6.3 Ensure capital investments enhance the City's streets, sidewalks, and other public spaces supporting a healthy lifestyle	
1) Development and capital projects shall enhance the public realm where appropriate by using methods such as: curb extensions, curbside boulevards that accommodate wider pedestrian walkways, trees, landscaping, and street furniture. These enhancements will make streets safer and more enjoyable by dedicating more space to pedestrians, creating opportunities for relaxation and social interaction, and where necessary, buffering pedestrians from traffic.	<ul style="list-style-type: none"> / Street trees are proposed along both March Road and Klondike Road; / A minimum 2-metre sidewalk is proposed and additional space is provided at the intersection of March Road and Klondike Road; / The bi-directional cycle track on Klondike Road will be upgraded to an off-street, grade-separated cycle track. The bus shelter will be relocated so that the bus no longer pulls into the cycle track to pick up passengers.

Policy	Response
4.6.5 Ensure effective site planning that supports the objectives of Corridors, Hubs, Neighbourhoods and the character of our villages and rural landscapes	
2) Development in Hubs and along Corridors shall respond to context, transect area and overlay policies. The development should generally be located to frame the adjacent street, park or greenspace, and should provide an appropriate setback within the street context, with clearly visible main entrances from public sidewalks. Visual impacts associated with above grade utilities should be mitigated.	<ul style="list-style-type: none"> / The proposed development is generally located to frame the adjacent streets, and locates parking at the rear and underground; and / The proposed development's reduced setback from its rear property line will be mitigated through landscaping.

Policy	Response
3) Development shall minimize conflict between vehicles and pedestrians and improve the attractiveness of the public realm by internalizing all servicing, loading areas, mechanical equipment and utilities into the design of the building, and by accommodating space on the site for trees, where possible. Shared service areas, and accesses should be used to limit interruptions along sidewalks. Where underground parking is not viable, surface parking must be visually screened from the public realm.	<ul style="list-style-type: none"> / At-grade parking is located to the rear of the site to reinforce the streetwall character that is emerging on March Road and to provide a visual screen; / Access to underground parking, where the majority of the parking is located, is provided via a side street: Klondike Road; and / Conflict between vehicles and pedestrians has been considered by carefully designing the location of pathways and cycle tracks.
4) Development shall demonstrate universal accessibility, in accordance with the City's Accessibility Design Standards. Designing universally accessible places ensures that the built environment addresses the needs of diverse users and provides a healthy, equitable and inclusive environment.	<ul style="list-style-type: none"> / Along the site's frontage on Klondike Road, the on-road barrier-separated cycle track has been upgraded to move it to the curb, which is generally a safer location for people cycling and using mobility devices; / The bus stop shelter has been located behind the cycle track to minimize conflicts between using the sidewalk and cycle track; / A two (2) metre wide stone dust path is proposed behind the property running parallel to Shirley's Brook; and / Pathways provide direct access from the rear of the site to the proposed path running parallel to Shirley's Brook



Extract of the Site Plan showing the re-oriented cycle track on Klondike Road

Design Directives

City of Ottawa Official Plan (2021)

Policy	Response
4.6.6 Enable the sensitive integration of new development of Low-rise, Mid-rise and High-rise buildings to ensure Ottawa meets its intensification targets while considering liveability for all	
<p>1) To minimize impacts on neighbouring properties and on the public realm, transition in building heights shall be designed in accordance with applicable design guidelines. In addition, the Zoning By-law shall include transition requirements for Mid-rise and High-rise buildings, as follows:</p> <ul style="list-style-type: none"> a) Between existing buildings of different heights; b) Where the planned context anticipates the adjacency of buildings of different heights; c) Within a designation that is the target for intensification, specifically: <ul style="list-style-type: none"> ii) Built form transition between a Corridor and a surrounding Low-rise area should occur within the Corridor. 	<p>/ The proposed development's setback from Shirley's Brook ensures an effective transition to adjacent properties with a different land use designation.</p>

Policy	Response
<p>2) Transitions between Mid-rise and High-rise buildings, and adjacent properties designated as Neighbourhood on the B-series of schedules, will be achieved by providing a gradual change in height and massing, through the stepping down of buildings, and setbacks from the Low-rise properties, generally guided by the application of an angular plane as may be set in the Zoning Bylaw or by other means in accordance with Council-approved Plans and design guidelines.</p>	<ul style="list-style-type: none"> / The proposed development's setback from Shirley's Brook ensures an effective transition to adjacent properties with a different land use designation; and / The proposed development is separated into two (2) buildings and characterized by a variety of materials, which will help break up the buildings' massing
<p>4) Amenity areas shall be provided in residential development in accordance with the Zoning By-law and applicable design guidelines. These areas should serve the needs of all age groups, and consider all four seasons, taking into account future climate conditions. The following amenity area requirements apply for mid-rise and high-rise residential</p> <ul style="list-style-type: none"> a) Provide protection from heat, wind, extreme weather, noise and air pollution; and b) With respect to indoor amenity areas, be multi-functional spaces, including some with access to natural light and also designed to support residents during extreme heat events, power outages or other emergencies. 	<p>/ Quality indoor and outdoor communal amenity space is provided.</p>

Policy	Response
<p>7) Mid-rise buildings shall be designed to respond to context, and transect area policies, and should:</p> <ul style="list-style-type: none"> a) Frame the street block and provide mid-block connections to break up large blocks; b) Include a base with active frontages, and a middle portion that relates to the scale and character of the surrounding buildings, or, planned context; c) Be generally proportionate in height to the width of the right of way as illustrated in the Figure below, with additional height permitted in the Downtown Core Transect; and d) Provide sufficient setbacks and step backs to: <ul style="list-style-type: none"> i) Provide landscaping and adequate space for tree planting; ii) Avoid a street canyon effect; and iii) Minimize microclimate impacts on the public realm and private amenity areas. 	<ul style="list-style-type: none"> / The proposed six (6) storey building height is appropriate for its context along March Road, a very wide right-of-way, and will not result in a canyon-like effect. / The proposed development is separated into two (2) buildings and characterized by a variety of materials, which will help break up the buildings' massing

Design Directives

City of Ottawa Zoning By-law (2008-250)

The subject property is designated General Mixed Use (“GM”).

The General Mixed Use zone permits a range of residential, commercial and institutional uses in the General Urban Area. The proposed development is an apartment dwelling, mid-rise, which is a permitted use in the GM zone.

The proposed development complies with applicable zoning by-law provisions except for two:



- 1. Minimum Rear Yard Setback**- 3.06 metres provided whereas 7.5 metres is required

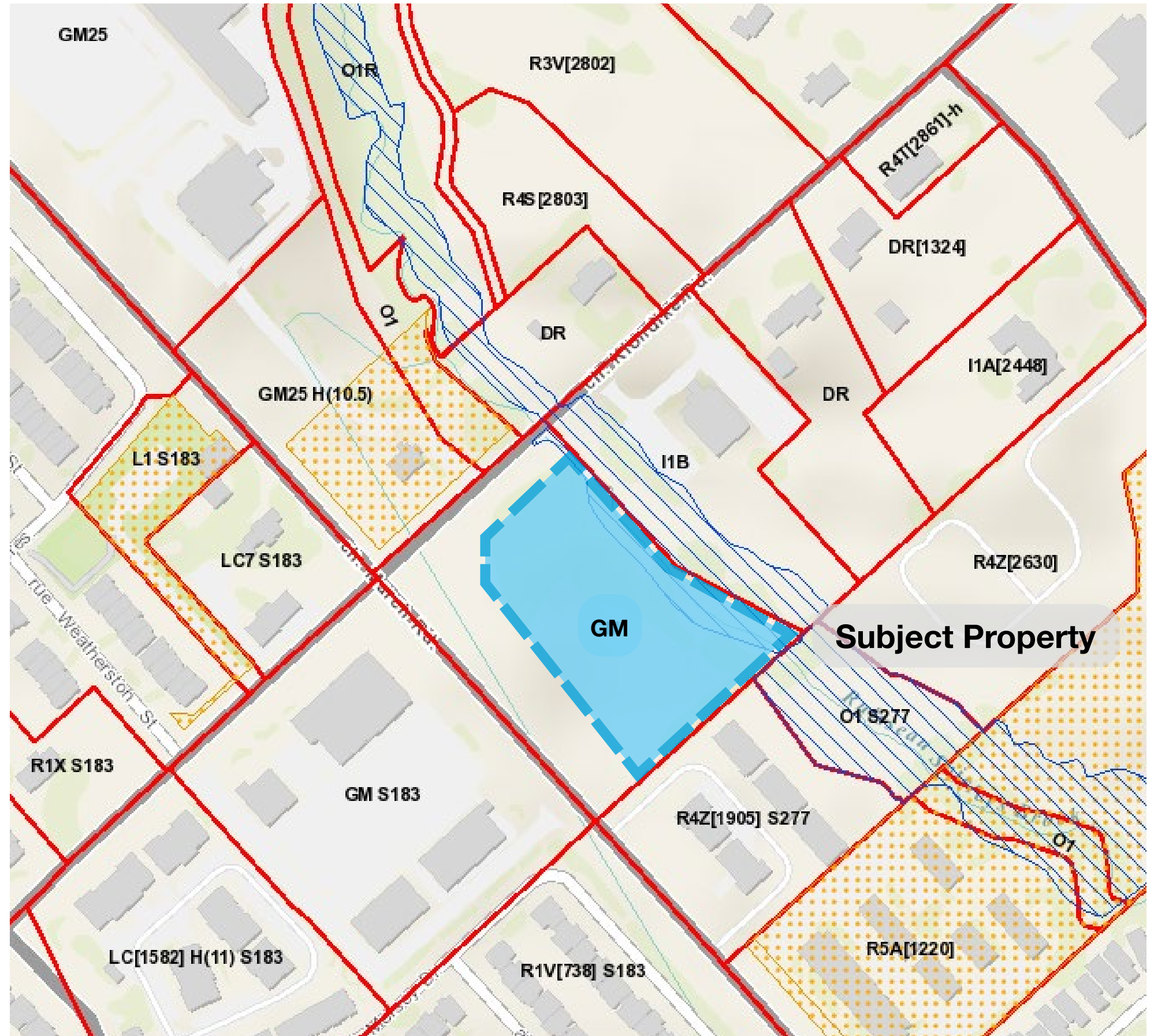
Per the zoning by-law definition, the lot line adjacent to Klondike Road is defined as the front lot line, whereas the Official Plan requires the development to address the Mainstreet Corridor (March Road). The proposed development has been designed to effectively orient its main building walls toward March Road and provides a shorter rear yard setback of three (3) metres which will help provide a more continuous street wall along the public right-of-way. The 3.06-metre setback is consistent with the required Interior Side Yard Setback in the GM zone.

- 2. Maximum Building Height** - 21.2 metres proposed whereas 18 metres is required.

The proposed development is seeking an increase to the maximum permitted building height in order to account for a significant change in grade on the subject property and allow for comfortable floor-to-ceiling heights.

Since the subject property slopes down significantly from March Road to Shirley’s Brook, the average finished grade of the proposed development is lowered significantly by the northeast (rear) building wall, where a significant portion of the P1 basement level wall is exposed. This condition thereby “increases” the calculated building height to 21.2 metres.

-  Heritage (s. 60)
-  Flood Plain (s. 58)



Excerpt of the Zoning for the Subject Property (taken from geoOttawa)

Response to Urban Design Directions

Response to Comments dated January 19, 2024

The City of Ottawa provided comments in a Feedback Form dated January 19, 2024. Responses to these comments are as follows:

Comment	Response
<p>7(c) Minimum setback for any wall of a residential Use building to a Private Way reduction from to 1.3 metres from 1.8 metres as required by Section 131 of the Zoning By-law:</p> <p>Please rationalize why this is needed.</p> <p>The relief required can be obtained either through a Minor Variance application to the Committee of Adjustment or a Minor Zoning By-Law Amendment application, it is up to the applicant. If the Committee of Adjustment route is sought, please reach out to a Committee planner to discuss. In either scenario, please provide a planning rationale for the relief being sought and how the relief will result in a better proposal.</p> <p>The proposal will need to be zoning compliant with all avenues of appeal exhausted prior to submission of the formal site plan application, and a Zoning Confirmation Report is required to be submitted.</p>	<p>Relief from this provision is no longer required; the distance from the building wall to a Private Way has been increased.</p> <p>A Zoning Confirmation Report has been prepared under a separate cover.</p>
<p>13. Clearly delineate snow storage area on the plan, or include a note if it is to be trucked offsite. Snow removal shall not interfere with the servicing of the subject lands. The Grading and Drainage Plan shall not be compromised by the storage of snow. Snow storage areas shall be setback from property lines, foundations, fencing or landscaping a minimum of 1.5 metres, shall not occupy required parking spaces, and shall not occur within fifteen (15 m) metre setback from the top bank of the watercourse, per the Zoning By-law.</p>	<p>Snow will be trucked offsite.</p>

Comment	Response
<p>14. Thank you for providing bicycle parking beyond the minimum required by the Zoning By-law! Please provide a detail of the bicycle parking. Another consideration could be to secure or cover the parking located in the exterior parking lot for usability.</p>	<p>Architectural materials have been updated to show the bike parking details.</p> <p>Note that outdoor bike parking is intended for visitors, whereas bike parking intended for residents is indoors and therefore sheltered.</p>
<p>15. Public realm interaction and engagement with the frontage of March Road should be improved by locating building entrances on the road frontage. This is supported by OP policy 4.6.5.2, which guides development on Mainstreet Corridors towards having clearly visible main entrances from public sidewalks.</p> <p>Similarly, consider a pedestrian entry from the building to the proposed Klondike bus stop area. Considering the density permissions for the site, based on the proximity to transit, there should another building entrance close to the bus stop to encourage and support convenient transit use.</p>	<p>Although not facing March Road, the main entrances to the buildings are located in close proximity to the March Road frontage and are easily visible from the public realm. The entrance areas are characterized by heavy glazing, which distinguishes them from the rest of the buildings' facades and make the entrances more easily identifiable. As well, the two entrances will feature signage displaying the address and logo of the project facing March Road.</p> <p>A walkway has been provided from the garage in the updated site plan; grading is otherwise very challenging here and a direct entrance from the Klondike bus stop was not possible to be implemented.</p>
<p>16. The pathway along Shirley's Brook at the rear of the site is identified on Official Plan Schedule C-3 as a Major Pathway as part of the City's Active Transportation Network. As such, please consider updating from a stone-dust pathway into a 3-metre-wide asphalt mixed-use pathway adjacent to Shirley's Brook.</p>	<p>A stone-dust pathway is proposed adjacent to Shirley's Brook, which is consistent with direction previously provided by the City and allows for a lesser impact on the riparian corridor.</p>
<p>17. Please tie in the rear pathway to the existing cycle track along March Road with signage or pavement marking, ideally to connect westward to corner of March and Klondike where there would be pedestrian crossing at traffic lights. The rendering makes it very clear that a pathway user may continue across into the path of a right-turning vehicle coming at a high-speed off March Road.</p>	<p>This has been provided. Please see revised rendering showing the stone-dust pathway's connection to the cycling connection along Klondike Road. Existing concrete barriers provide a visual and physical deterrent to crossing Klondike Road at this juncture.</p>
<p>18. Please increase opportunities for pedestrian movement on the site.</p>	<p>A pedestrian pathway is provided from the parking garage entrance to the subject property's frontage along Klondike Road, with convenient access to the bus stop.</p>

Site, Context, & Analysis

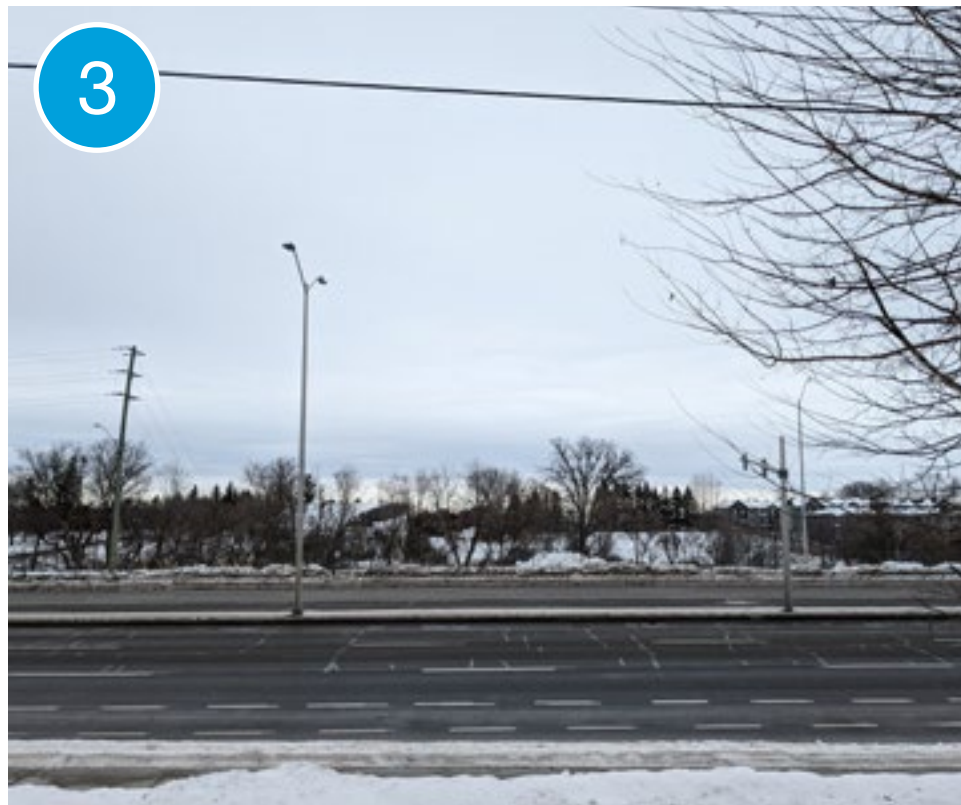
Site Photos

Subject Property and Surrounding Area



Site Photos

Subject Property and Surrounding Area



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Subject Property and Surrounding Context



Perspective Images



June 2024

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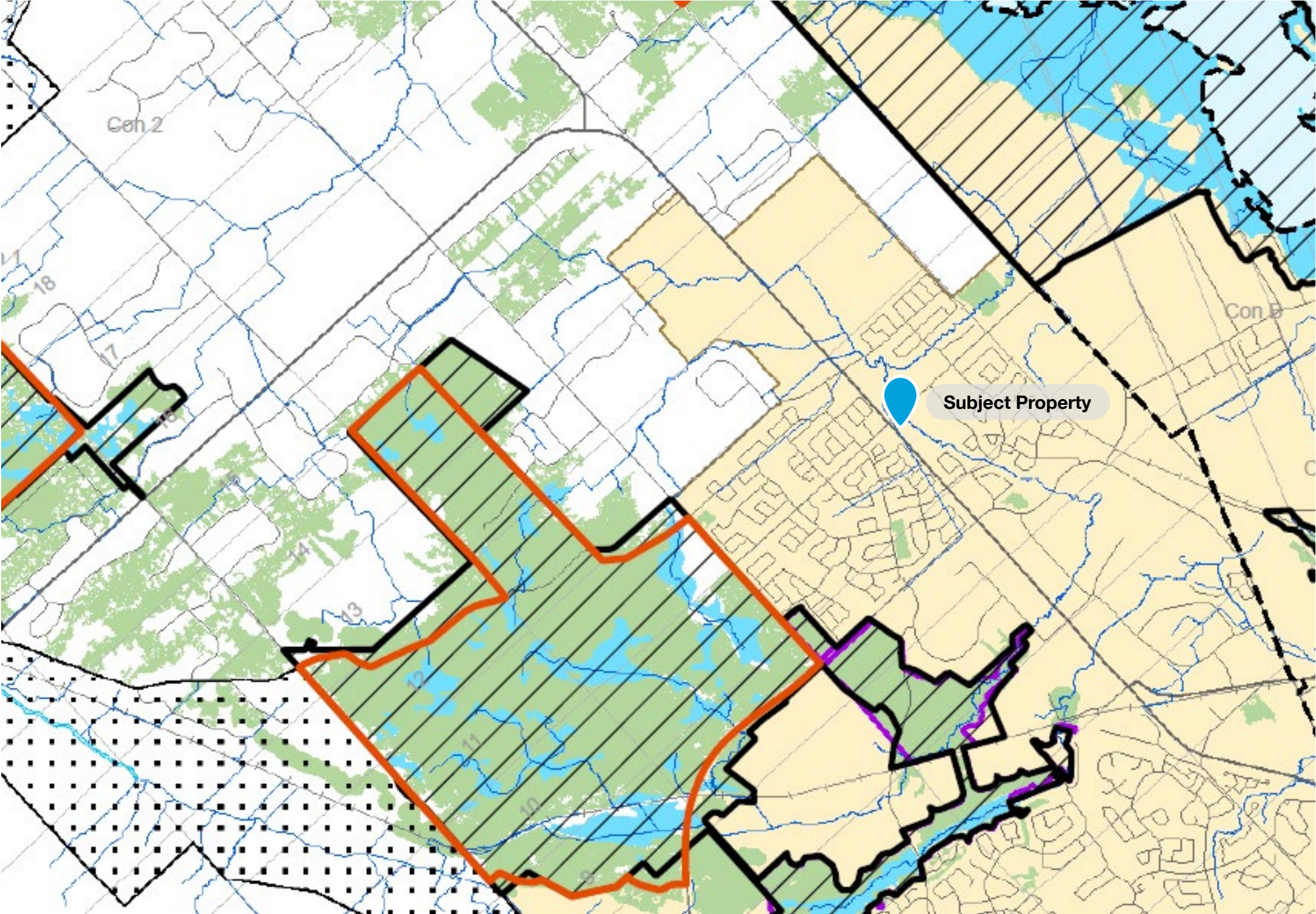
Perspective Images



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Built and Natural Heritage Context



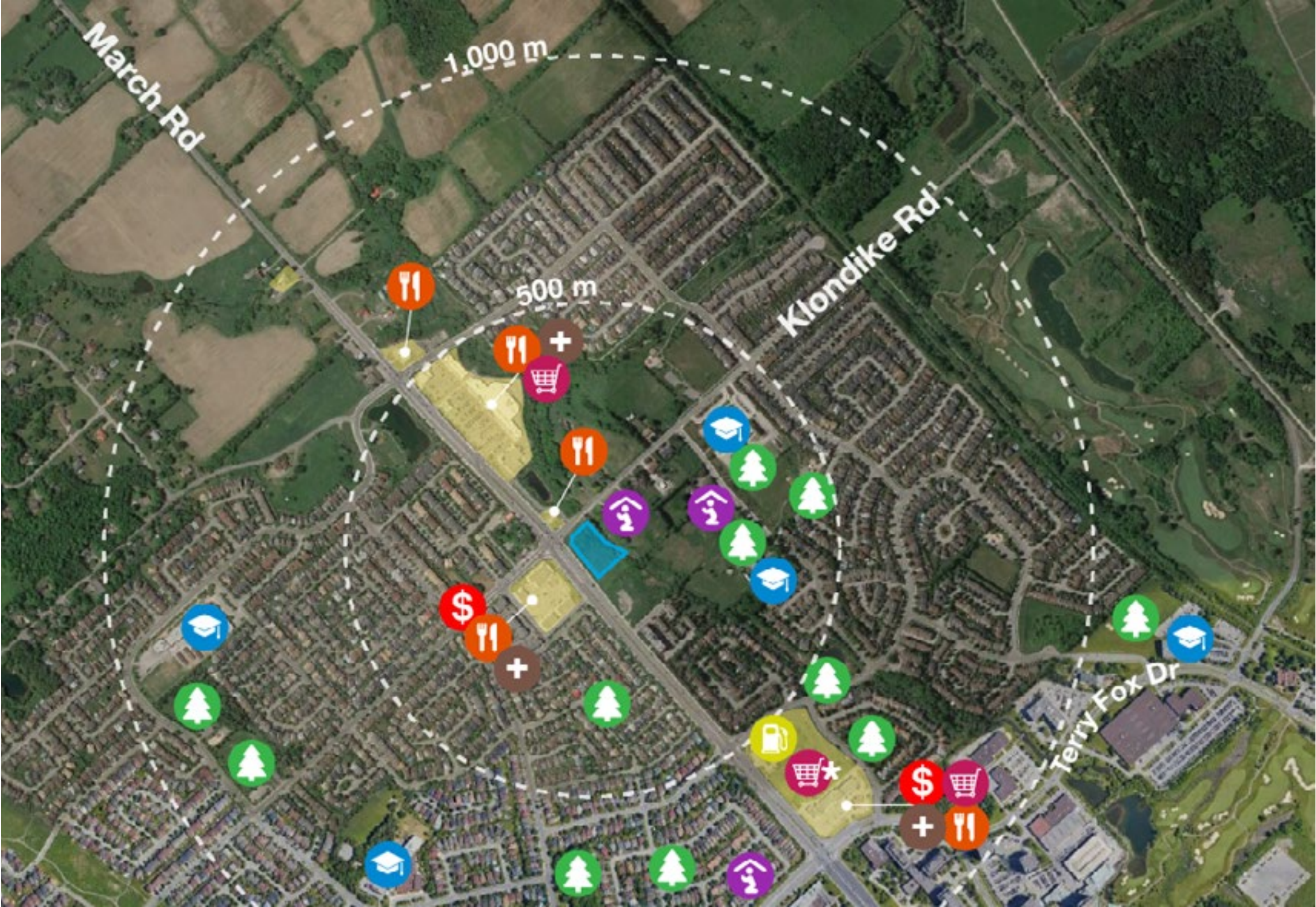
The proposed development is located in the Urban Area adjacent to Shirely’s Brook, a watercourse identified in Schedule C-11 of the Official Plan. Responding to the natural context was an important consideration in the design of the building, the majority of which is set back a minimum of 30 metres from the normal high water mark of Shirely’s Brook. Canilevered balconies project 3.5 metres into the 30-metre setback area. In the EIS Addendum letter prepared by Gemtec dated March 10, 2024, it noted that the balconies project into a Category 2 habitat for Blanding’s turtle, which is less restrictive than Category 1 habitat.

A turtle exclusion barrier is provided where the retaining wall adjacent to the underground parking garage entrance is less than 0.6 metres high. A stone dust path is proposed to run parallel to Shirley’s Brook for public enjoyment. Finally, lands associated with Shirley’s Brook, including the asphalt pathway, will be conveyed to the city to ensure their ongoing protection.

- Overlays**
- Natural Heritage System Core Area
 - Natural Heritage System Linkage Area
 - Natural Heritage Features Overlay
- Sub-Designations**
- Natural Environment Area
 - Significant Wetlands
- Other**
- Watercourse
 - Urban Area
 - Greenbelt

Schedule C11-A - Natural Heritage System (West), City of Ottawa Official Plan, 2021

Key Uses, Destinations, and Spatial Elements



Nearby amenities for future residents include commercial uses primarily located on March Road as well as parks and schools distributed throughout the nearby neighbourhoods.

Nearby retail and commercial plazas include:

- / RioCentre Kanata (to the northwest on March Rd), which includes a grocery store, pharmacy, liquor store, medical services, and fast food store;
- / Klondike Crossing (southeast corner of March Rd and Klondike Rd), which includes a pharmacy, bank, fast food restaurants, medical services, and dental clinic; and
- / March Road Mall (northwest corner of March Rd and Terry Fox Dr), which includes a specialty grocery store, restaurant, fast food restaurants, pub, bank, dentist, medical services, convenience store, and other commercial businesses.

	Subject Property		Commercial lands
	School		Bank
	Healthcare		Gas Station
	Grocery Store		Future
	Place of Worship		
	Park or Open Space		
	Restaurant, Fast Food		

Adjacent Streets and Public Realm



The proposed development has frontage on two roads: March Road, an Arterial Road, and Klondike Road, a Collector. The surrounding neighbourhood is built out with a variety of road typologies, ranging from Arterial to Collector and Minor Collector Roads, as well as Local Roads, which provide direct access to predominantly low-rise residential areas.

The public realm is defined by wide, auto-oriented roads and associated infrastructure. March Road has a protected right-of-way (ROW) of 48 metres west of Klondike Road and 44.5 metres east of Klondike Road until Campeau Drive. Near the subject property, March Road has six (6) vehicular lanes, two (2) turning lanes, and painted on-road unidirectional bike lanes. The vehicle lanes are separated by a raised curb. A curb-separated sidewalk runs along the road adjacent to the bike lanes.

Klondike Road is a predominantly two (2) lane road with a lefthand turn pocket for vehicles traveling south and turning left to travel eastward on March Road. Klondike Road has a curb-separated bi-directional cycle track on the east side of the street. On the west side of the road, there is a paved shoulder. Klondike Road has a maximum speed limit of 40 km/h. Between March Road and Sandhill Road, there are no sidewalks; north of Sandhill Road, there are sidewalks on either side of the street and the cycle track ends. Sidewalks are present in front of South March Public School.

There are multiple shopping centres located off of March Road, with stores generally being separated from the road with a parking lot.



Public Transit

The subject property is located at the intersection of March Road and Klondike Road and approximately 1.3 kilometres from Innovation Station, a bus rapid transit (BRT) station and park and ride. March Road is identified as a future at-grade Transitway per Schedule C2 - Transit Network, Ultimate. Bus service is provided by multiple bus routes, including:

- / 165 Innovation-Morgan's Grant a Local bus route and the closest bus route, which runs along Klondike Road providing access to Kanata via Terry Fox Road and terminating at Terry Fox Station; and
- / 63 Briarbrok, a Rapid bus route, located approximately 510 metres to the north on Marconi Avenue, which provides access to Kanata North, terminating at Teron Station via March Road.

Other bus routes operating in proximity to the subject property include:

- / 64 Morgan's Grant, a Local route
- / 66 Kanata-Solandt, a Local route
- / 110 Innovation, a Local route
- / 166 Innovation, a Local route

OC Transpo defines Rapid routes as being routes with station-to-station bus service operating seven (7) days per week in all time periods. Local routes are defined as routes with custom routing to local destinations.

- Subject Property
- Local Bus Route
- Frequent Bus Route
- Transitway At-Grade (Future)
- Transitway Station (Future)



Road Network

The subject lands is connected to major roadways including:

- / March Road, an Arterial Road, runs along the site's front property line;
- / Klondike Road, a Collector Road, runs along the site's corner side property line;
- / Marconi Avenue, a Collector Road; and
- / Maxwell Bridge Road, a Collector Road.

Additionally, there are multiple Collector Roads planned to the northwest of the subject property.



Cycling Network

Existing Network

The existing network is retrieved from GeoOttawa and includes multiple types of cycling routes, ranging from cycle tracks, bike lanes, and pathways, to suggested connector routes and paved shoulders. March Road has an on-road bike lane, which transitions to a paved shoulder northwest of the intersection with Maxwell Bridge Road. A bi-directional cycle track is located on the east side of Klondike Road between March Road and Sandhill Road.

Future Network

A pathway is proposed to run parallel to Shirley's Brook to the rear of the site per Schedule C3 of the Official Plan. Policy 1 of s. 4.1.1 of the Official Plan, in the Urban area and Villages, people who walk, cycle and use transit shall, by default, be given priority for safety and movement. Per policy 11, during the review of development and as part of new road construction and road reconstruction projects Arterials, Major Collectors, and Collectors in the Urban area shall include sidewalks on both sides and unidirectional cycling facilities. Finally, policy 12 states that all urban area collectors, major collectors and arterials are cycling routes that, over time, are to include cycling facilities.

- Subject Property
- Urban - Major Pathways
- Cycle Track
- Path
- Bike Lane
- Paved Shoulder
- - - Suggested Connector Route

GeoOttawa Cycling layer and Schedule C3 - Active Transportation Network - Urban - Major Pathways (City of Ottawa Official Plan, 2021)

Development Proposals



- Future Development Context**
 Existing development applications were retrieved from devapps.ottawa.ca. Applications are summarized below:
- 706 March Road** · Approved
 ZBLA
 / ZBLA to permit a retail food store, commercial building, and drive-through
 - 762 March Road** · Constructed
 Plan of Condominium, ZBLA, SPC
 / 60 stacked dwelling units in five (5) buildings
 - 927 March Road** · Proposed
 OPA, ZBLA, SPC, Plan of Subdivision
 / 28 single-detached homes, 33 town-homes, 2 low-rise apartment buildings
 - 910 March Road** · Proposed
 SPC
 / Four (4) buildings, commercial development including two (2) restaurants, a gas bar, and retail store
 - 1104 Halton Terrace** · Proposed
 ZBLA, SPC
 / 4-storey low-rise apartment building with 86 units
 - 1050 Klondike Road** · Proposed
 ZBLA, SPC
 / Seven (7) three (3) storey townhomes and two (2) storey stacked dwelling with nine (9) units
 - 1055 Klondike Road** · Proposed
 ZBLA, SPC
 / 53-unit low-rise apartment building
 - 100 Atwell Private** · Proposed
 Plan of Condominium
 / 14 blocks of townhouses, 60 units total

Design Research

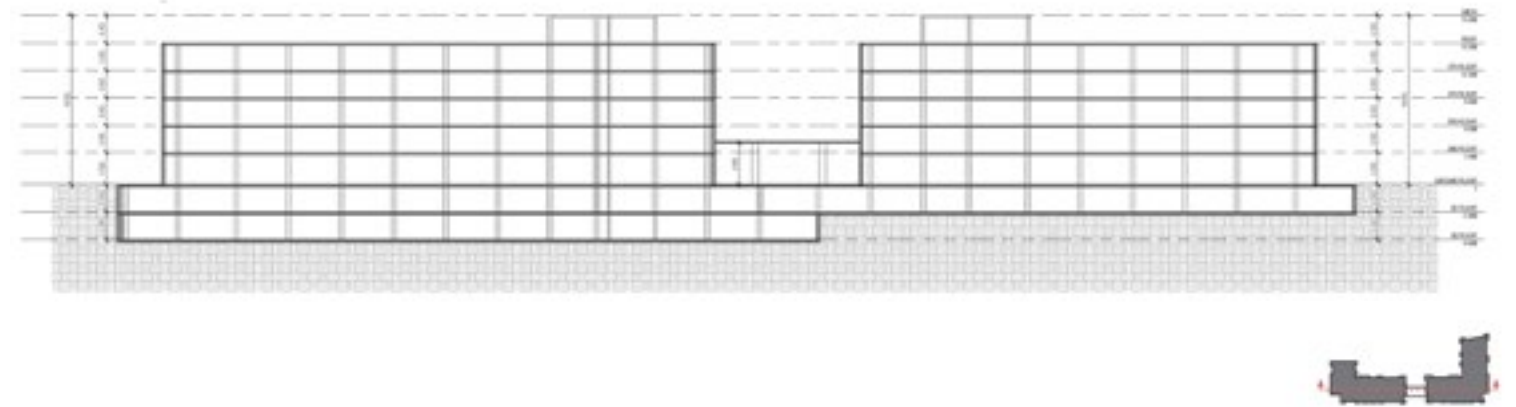
Design Research

Design Evolution



Previous Concept Design

A six (6) storey concept was previously prepared by Neuf prior to the sale of the property. This concept featured two (2) 'L'-shaped buildings and a canopy over the drive aisle to access the rear yard parking area.



June 2024

788 March Road
Design Brief

Design Research

Design Evolution



Previous Concept Design

The previous concept featured a greater amount of charcoal bricks and beige stone to reflect the materiality of the March House.



Ground floor site plan

Design Research

Materiality



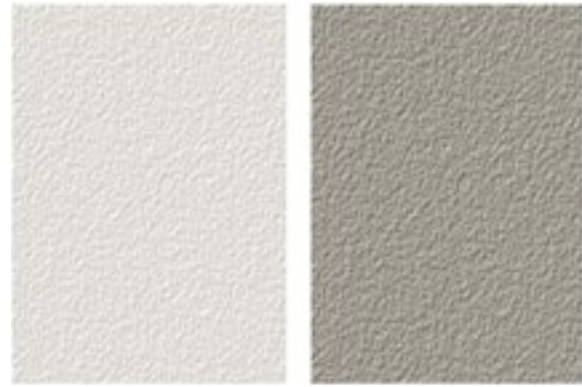
1
CLAY BRICK TAN
SIMILAR COLOR AND DIMENSIONS



2
CLAY BRICK LIGHT
SIMILAR COLOR AND DIMENSIONS



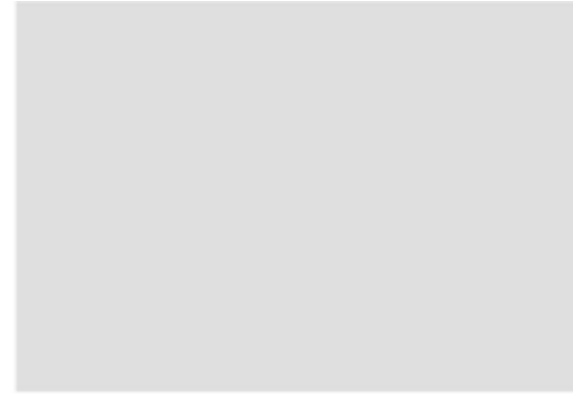
3
STEEL SIDING DARK, BLACK WOOD IMITATION
SIMILAR COLOR AND DIMENSIONS



4
ACRYLIC
COLOR LIGHT GREY AND CHARCOAL



6
EXPOSED CONCRETE



7
METAL CLADDING PANNEL
COLOR "ALUMINIUM"

The façade design was carefully planned to match the nearby buildings in size. Mixing tan and off-white brick, along with black metal that looks like wood, helps create the feeling of smaller residential buildings within our project.

Firstly, the tan brick was chosen to pay homage to the neighboring March House, seamlessly blending our development with its historic surroundings. This choice not only respects the area's architectural heritage but also adds a touch of warmth and character to the façade.

In addition, the white brick introduces a contemporary element to the design, creating visual interest and contrast against the beige brick.

Lastly, the black metal accents lend a touch of sophistication and durability to the design. They provide a sleek contrast to the brickwork while offering long-term durability and low maintenance.

In summary, our selection of materials was chosen to strike a balance between honouring the neighborhood's history and infusing the building with a fresh, modern aesthetic. These choices ensure that our development not only fits seamlessly into its surroundings but also stands out as a timeless and elegant addition to the urban landscape.

Design Research

Massing in Existing Context



Façade of closest adjacent building

The side wall of the closest adjacent building has very few windows and faces the eastern façade of the proposed development. Therefore, common anticipated adverse impacts, such as overlook from adjacent windows, have already been mitigated in the design.

Photographs from site visit, January 23, 2024

Design Research

Massing in Existing Context



Contribution to evolving streetwall

The proposed development matches the front yard setback of the existing development of stacked dwellings to the east, contributing to the evolution of a consistent streetwall. This reinforces the rhythm of an urbanizing character of March Road.

Contribution to soft landscaping

Street trees are proposed along both the March Road and Klondike Road frontages, which continues the landscaping along both roads.



Photograph from site visit, January 23, 2024



View of site from Google Streetview, September 2023

Design Research

Massing Shown in Policy Context



Mainstreet Corridor policies

The proposed development is consistent with Official Plan policies. Section 6.2.1 provides policies on how to define the Corridors and set the stage for their function and change over the life of [the] Plan. Policy 1(a)(i) confirms that the Corridor designation applies to any lot abutting the Corridor, subject to generally, a maximum depth of 220 metres from the centreline of the street identified as a Mainstreet Corridor. Policy 1(b) notes that where a side street intersects with a Corridor, the Corridor designation may include one or more lots on the side street so as to extend the Corridor designation along the side street to the average depth of the Corridor designation along the rest of the Corridor block.

The image to the left shows a the maximum 220-metre extent of the Mainstreet Corridor designation. Beyond this, the Evolving Neighbourhood Overlay is shown as a 150-metre band extending from the edge of the Mainstreet Corridor designation, beyond which are lands shown designated as Neighbourhood.

The proposed development falls fully within the Mainstreet Corridor designation band and further, the development addresses both the Mainstreet Corridor (March Road) and the intersecting side street (Klondike Road).

Design Research

Built Form Transition



Design Research

Building Cross Sections



BUILDING CROSS SECTIONS WITH NEIGHBOURS NORTH
1:20



BUILDING CROSS SECTIONS WITH NEIGHBOURS SOUTH
1:20

Design Research

Building Elevations



Design Research

Building Elevations



ENTRANCE BUILDING 'A' ELEVATION



ENTRANCE BUILDING 'B' ELEVATION



COURTYARD BUILDING 'B' ELEVATION

OVERALL COURTYARD ELEVATION

COURTYARD BUILDING 'A' CORNER ELEVATION

COURTYARD BUILDING 'A' ELEVATION

Design Research

Bird-Safe Design Guidelines

The Bird Safe Design Guidelines were implemented in 2020. The purpose of these guidelines is to inform building, landscape and lighting design at the planning stage of private or public development projects to minimize the threat of bird collisions. These guidelines apply to buildings and other structures that incorporate glass and glass-like panels (e.g., transit shelters, railings).

There are seven (7) guidelines, with Guidelines 1-4 being related to building design, guideline 5 being related to landscaping, and guidelines 6 and 7 being related to lighting design:

1. Consider the environmental context
2. Minimize the transparency and reflectivity of glazing
3. Avoid or mitigate design traps
4. Consider other structural features
5. Create safe bird-friendly landscaping
6. Design exterior lighting to minimize light trespass at night
7. Avoid nighttime light trespass from the building's interior

The guidelines have been considered in the following ways:

- / The building is sited close to March Road and Klondike Road and almost fully set back a minimum of 30 metres from the normal high water mark of Shirley's Brook (Guideline 1);
- / The Environmental Impact Statement (EIS) prepared by Gemtec dated October 23, 2023, recommends avoiding vegetation removal outside the key breeding bird period (typically March 31 to August 31) to minimize the impact of the proposed development on common nighthawk, eastern wood-pewee, and wood thrush habitat (Guideline 1);
- / A variety of materials in a range of textures and colours are proposed to promote visual interest (Guideline 2);
- / Design traps are mitigated in the design; no glass corners are proposed (Guideline 3); and
- / Exterior structural features, including antennas, cellphone, television, and media structures, are not anticipated to be required on the proposed development (Guideline 4).



Conclusion

Conclusion

The proposed development responds to the direction outlined in the City of Ottawa Official Plan (2021) and further implements recommendations made by Urban Design staff. The proposed development is sensitively-designed and contextually-appropriate and advances several urban design goals.

Sincerely,



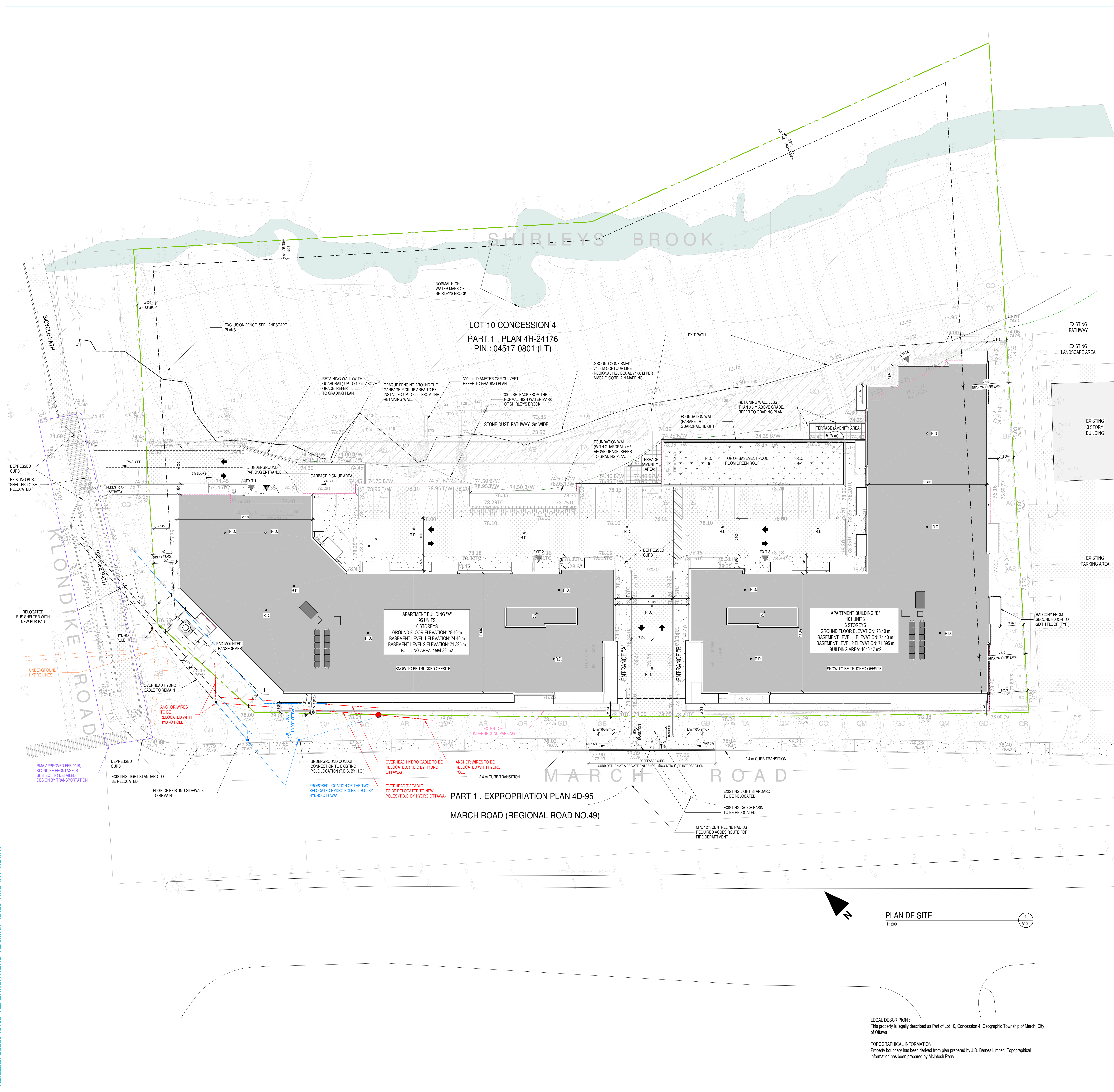
Nico Church, RPP MCIP
Senior Planner



Tamara Nahal, MPI
Planner

Appendices

Appendix A: Site Plan



Property Area City of Ottawa zoning By-law No. 2008-250
 Zoning: RES-10(1) sq. m. 131,429 sq. ft.

PROJECT STATISTICS

BUILDINGS	A	B
BUILDING HEIGHT (m)	21.2	21.2
TOTAL GFA AND RESIDENTIAL USE	10,651 m ²	10,651 m ²
LOT COVERAGE	3,236 m ²	3,236 m ²

UNIT STATISTICS

BUILDINGS	GF	2nd to 6th
1 Bedroom	24	4
2 Bedroom	13	13
3 Bedroom	35	5
4 Bedroom	17	1
5 Bedroom	6	1
TOTAL	95	24

APARTMENT BUILDING - 156 UNITS

REQUIRED	PROVIDED
1 Bedroom	24
2 Bedroom	36
3 Bedroom	50
4 Bedroom	6
5 Bedroom	6
TOTAL	118

LOCKERS

REQUIRED	PROVIDED
156	156

PROVIDING FOR PUD - SECTION 131

REQUIRED	PROVIDED
5m	5m
1.0m	1.73m
5.2m	32.09m
3m	11.73m

AMENITY AREA

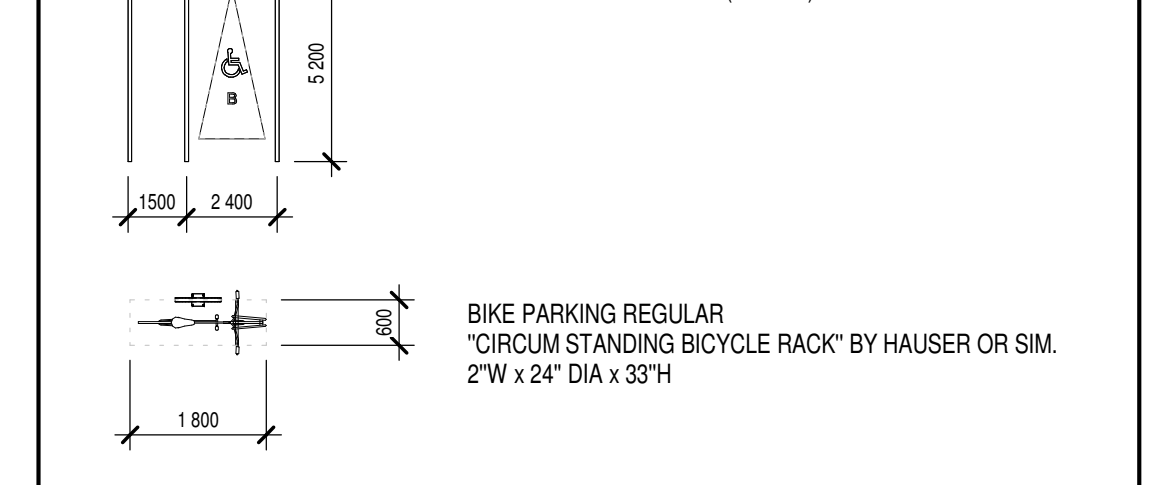
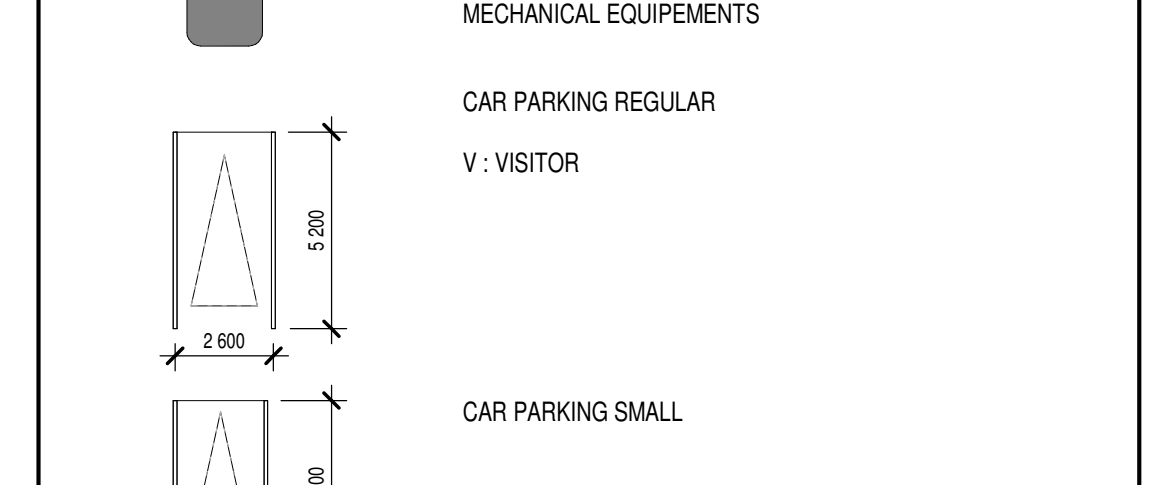
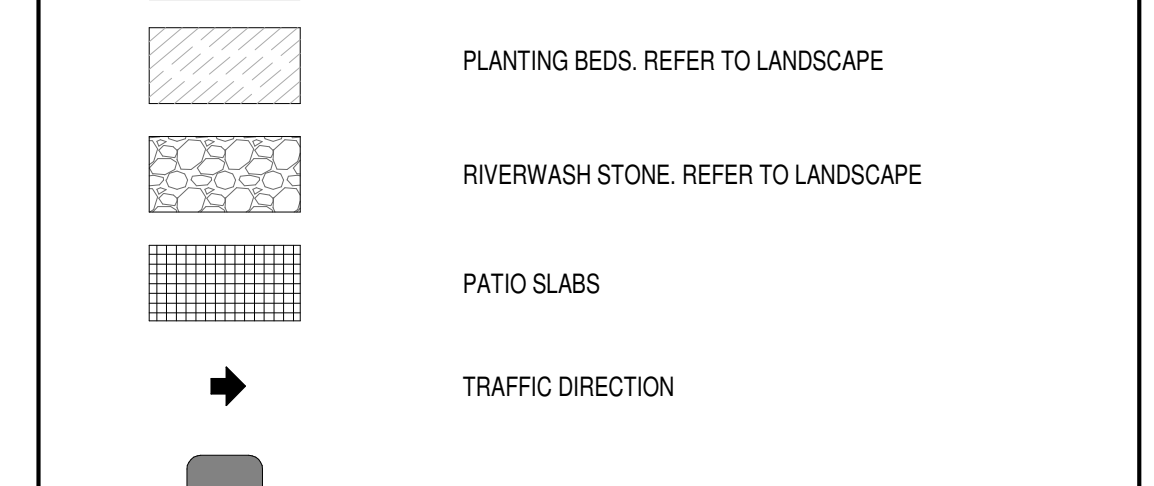
REQUIRED	PROVIDED
1176 m ²	1623 m ²
582 m ²	607 m ²
54 m m ²	62 m ²
54 m ²	233 m ²

WASTE MANAGEMENT

REQUIRED	PROVIDED
3.11 UNIT	12.42 COFF
0.018 UNIT	1.29 COFF
0.038 UNIT	3.54 COFF
249.50 UNIT	1.56

LEGEND

- PROPERTY LINE
- REQUIRED SETBACKS
- UNDERGROUND PARKING EXTEND
- ROAD IN ASPHALT
- PEDESTRIAN PATH
- SIDEWALK
- ADJACENT BUILDINGS
- BUILDING AREA
- SOFT LANDSCAPING TURF. REFER TO LANDSCAPE
- PLANTING BEDS. REFER TO LANDSCAPE
- RIVERWASH STONE. REFER TO LANDSCAPE
- PATIO SLABS
- TRAFFIC DIRECTION
- MECHANICAL EQUIPMENTS
- CAR PARKING REGULAR
- CAR PARKING SMALL
- BARRIER FREE PARKING (TYPE A)
- BARRIER FREE PARKING (TYPE B)
- BIKE PARKING REGULAR
- BIKE PARKING ELEVATION



NOTES GÉNÉRALES / General Notes

- Ces documents d'architecture sont la propriété exclusive de NEUF architectes et ne peuvent être utilisés, reproduits ou copiés sans autorisation écrite préalable. / These architectural documents are the exclusive property of NEUF architectes and cannot be used, copied or reproduced without written pre-authorization.
- Les dimensions apparentant aux documents doivent être vérifiées par l'entrepreneur avant le début des travaux. / All dimensions which appear on the documents must be verified by the contractor before to start the work.
- Vous devez aviser l'architecte de toute dimension erronée ou divergente entre ces documents et ceux des autres professionnels. / The architect must be notified of all errors, omissions and discrepancies between these documents and those of the other professionals.
- Les dimensions sur ces documents doivent être lues et non mesurées. / The dimensions on these documents must be read and not measured.

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 398 Cooper Street, Suite 300, Ottawa, ON K2P 2H7
 T 613 730 5709 foten.com

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INGÉNIEUR TRANSPORT / Transportation Engineering
PARSONS
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CIVIL / Civil
McINTOSH PERRY
 115 Wakeham Road R3 C39, Ottawa, ON K0A 1L0
 T 613 836 2184 mcintoshperry.com

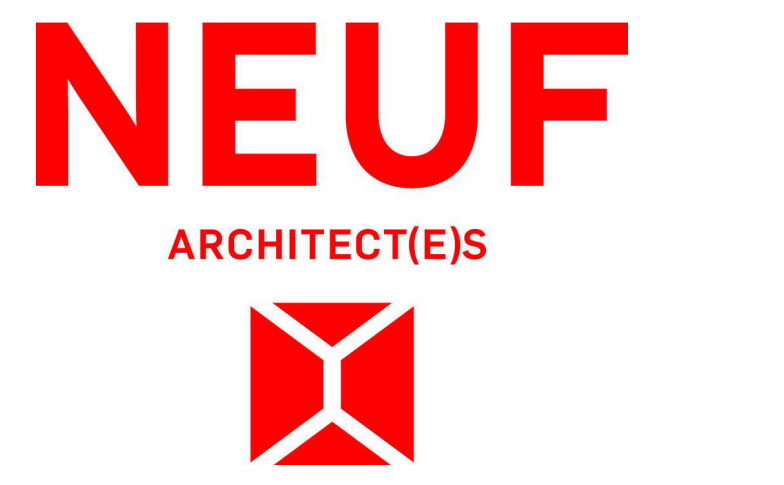
STRUCTURE / Structure
RJC Engineers
 1545 Carling Avenue, Suite 304, Ottawa, ON K1Z 6P9
 T 613 714 1000 rjc.ca

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NEUF architect(e)s
 650 boul. René-Lévesque O. 32e étage, Montréal, QC H3B 1S6
 T 514 847 1117 neuf.ca

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 T 514 718 6841 building-consultants.ca

INGÉNIEUR ÉLECTRIQUE / Electrical Engineer
Building Consultants
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 T 514 718 6841 building-consultants.ca

SCÉAU / Seal



CLIENT / Client
SINA
 5000 boul. La Carrière, Suite 1200, Laval, QC H7T 2P5
 T 514 567 7462 sina.ca

COUVREAGE / Project
788 MARCH ROAD RESIDENTIAL

EMPLACEMENT / Location
OTTAWA, ON 13109

NO. / REVISION **DATE (aa-mm-jj)**
 B FOR FOUNDATION PERMIT 2024-04-26

DESIGNÉ PAR / Drawn by **VÉRIFIÉ PAR / Checked by**
 N.S.H. B.R.

DATE (aa-mm-jj) **ÉCHELLE / Scale**
 2024-06-04 Comme indiqué

TITRE DU DESSIN / Drawing Title
SITE PLAN

REVISION / Revision **NO. / DESSIN / Drawing Number**
 B A100

Appendix B: Landscape Plan

TREE SCHEDULE 788 MARCH ROAD MULTI UNIT RESIDENTIAL						
CODE	QUANTITY	BOTANICAL NAME	COMMON NAME	B&B SPECIMEN	SPREAD	NATIVE
AC	0	Amelanchier canadensis	Tree Serviceberry	50mm cal	4M	YES
AR	0	Acer rubrum	Red Maple	60mm cal	12M	YES
AS	4	Acer saccharum	Sugar Maple	60mm cal	15M	YES
AG	1	Aesculus glabra	Ohio Buckeye	50mm cal	10M	YES
BP	6	Betula papyrifera	Paper Birch	50mm cal	10M	YES
CO	2	Celtis occidentalis	Common Hackberry	50mm cal	18M	YES
GB	0	Ginkgo biloba Princeton Sentry	Princeton Sentry Ginkgo	60mm cal	5M	N/A
GD	0	Gymnocladus dioica	Kentucky Coffee Tree	50mm cal	12M	YES
QM	0	Quercus macrocarpa	Bur Oak	50mm cal	20M	YES
QR	0	Quercus rubra	Red Oak	50mm cal	15M	YES
TA	2	Tilia americana	Basswood	50mm cal	13M	YES
TOTALS	15	23				
AB	1	Abies balsamea	Balsam Fir	1.5m Ht.	N/A	YES
JV	1	Juniperus virginiana	Eastern Red Cedar	1.5m Ht.	N/A	YES
LL	1	Larix laricina	Eastern Larch / Tamarack	1.5m Ht.	N/A	YES
PS	1	Pinus strobus	White Pine	1.5m Ht.	N/A	YES

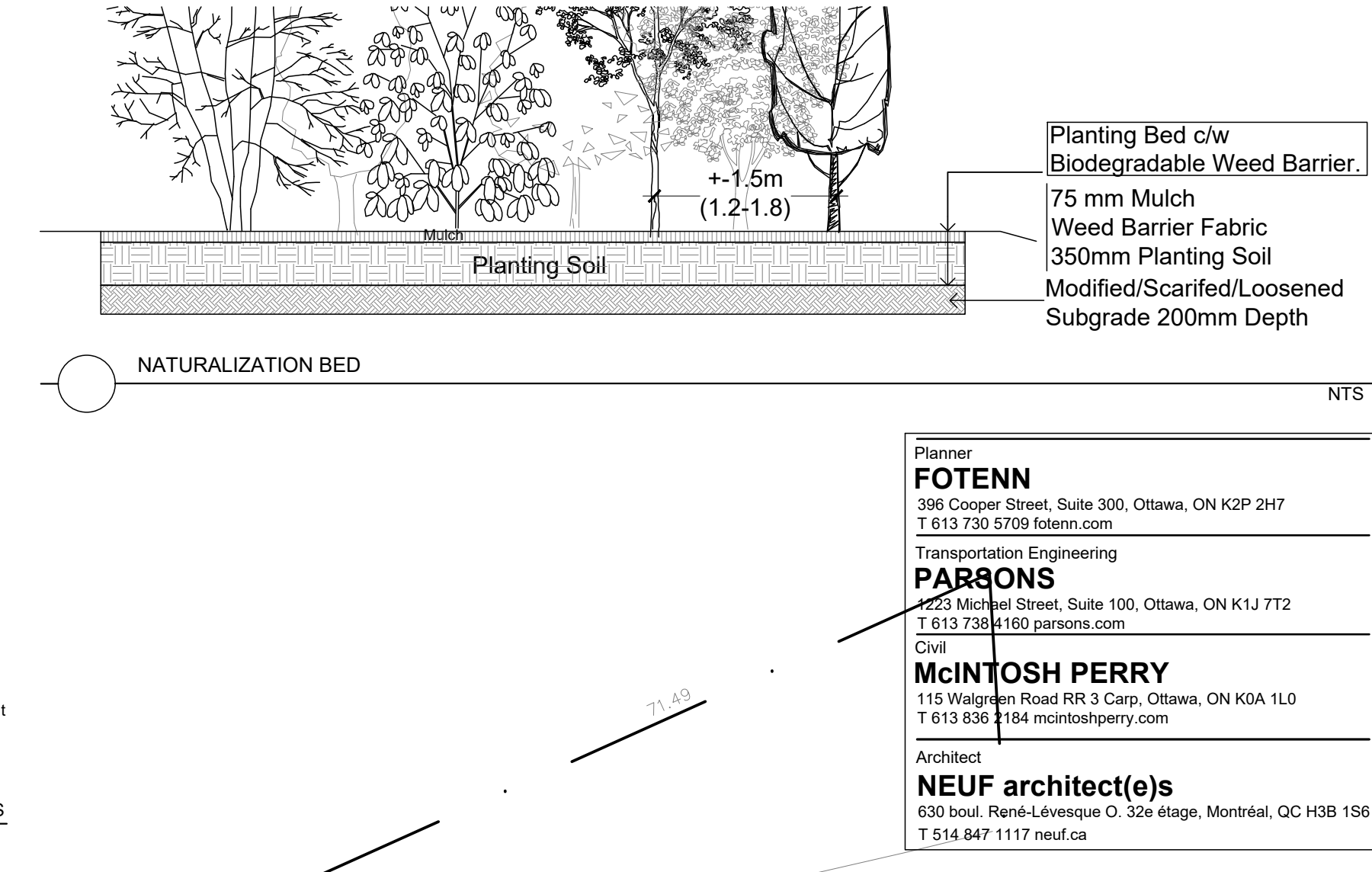
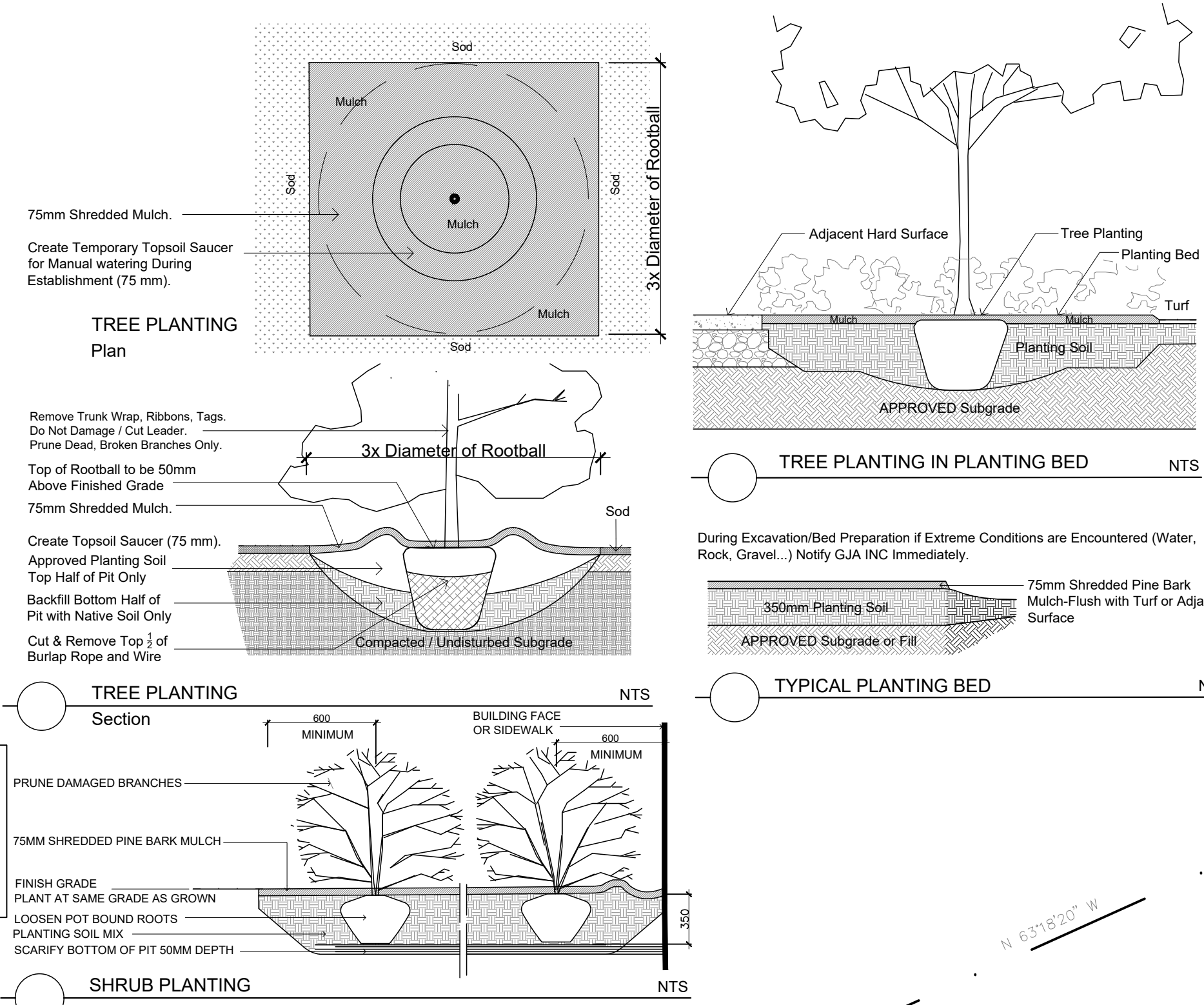
SHRUBS 788 MARCH ROAD KANATA - SINA - 750 SQ. M.						
Code	Qty.	Botanical Name	Common Name	Size	Condition	NATIVE
ac	TBD	Amelanchier canadensis	Serviceberry	60cm Ht.	Pot	YES
au	TBD	Arctostaphylos Uva Ursi	Bearberry	30cm spr	Pot	YES
cr	TBD	Cornus racemosa	Grey Dogwood	60cm Ht.	Pot	YES
hk	TBD	Hypericum kalmianum	Kalm St. John's Wort	30cm Ht.	Pot	YES
hk	TBD	Hypericum proflifum	Shrubby St. John's Wort	40cm Ht.	Pot	YES
pf	TBD	Potentilla fruticosa 'Abbotswood'	Abbotswood Potentilla	30cm Ht.	Pot	NO
rp	TBD	Rosa palustris	Swamp Rose	50cm Ht.	Pot	YES
rt	TBD	Rhus typhina	Staghorn Sumac	30cm Ht.	Pot	YES
sa	TBD	Symphoricarpos albus	Snowberry	50cm Ht.	Pot	YES
sa	TBD	Spiraea alba	Meadowsweet	50cm Ht.	Pot	YES
sb	TBD	Spiraea bumalda 'Dart's Red'	Dart's Red Spirea	40cm Ht.	Pot	NO
vl	TBD	Viburnum lentago	Nannyberry	60cm Ht.	Pot	YES
jo	TBD	Juniperus chinensis 'Ontario Green'	Ontario Green Juniper	30cm spr	Pot	NO
pm	TBD	Pinus mugo var. pumilio	Mugho Pine	30cm spr	Pot	NO
tc	TBD	Taxus cuspidata 'Monlo'	Emerald Spreader Yew	30cm spr	Pot	NO
sc	TBD	Sambucus canadensis	Elderberry	50cm Ht.	Pot	YES

TOTAL OF ABOVE SHRUBS 1250.				
Code	Qty.	Botanical Name	Common Name	Condition
GM	TBD	Geranium macrorrhizum	Bigroot Geranium	9cm Pot
GR	TBD	Geranium psilostemon 'Gerwat'	Rozanne Geranium	9cm Pot
PV	TBD	Panicum virgatum	Switch Grass	9cm Pot

*****ADDITIONAL LANDSCAPE PLAN NOTES*****
 LANDSCAPING ON CITY OF OTTAWA PROPERTY TO BE TO CITY OF OTTAWA DETAILS / STANDARDS. ADHERE TO TREE PLANTING DETAIL L3. TREES ON CITY OF OTTAWA PROPERTY TO CARRY A 2 YEAR WARRANTY.
 REFER TO SITE PLAN FOR LOCATIONS AND DIMENSIONS OF BUILDING, ROADS, PARKING, CURBS, SETBACKS, EASEMENTS, LEGAL DESCRIPTION ...
 EXISTING AND PROPOSED SITE SERVICING IS SHOWN ON THE LANDSCAPE PLAN TO PROVIDE AN AWARENESS TO REVIEWERS AND CONTRACTORS OF THEIR INTENDED LOCATION AND SEPARATION OF TREES/SERVICES/PROPOSED LANDSCAPING. DETERMINE AS BUILT LOCATIONS OF SERVICES PRIOR TO PLANTING AND ADJUST ACCORDINGLY TO RESPECT REQUIRED SETBACKS

NATURALIZATION BED 1				
475 sq. m.	Botanical Name	Common Name	Size	Condition
15	Amelanchier canadensis	Serviceberry	1.0m Ht	Pot
15	Acer pensylvanicum	Striped Maple	1.0m Ht	Pot
10	Acer rubrum	Red Maple	1.0m Ht	Pot
10	Acer saccharum	Sugar Maple	1.0m Ht	Pot
10	Betula papyrifera	Paper Birch	1.0m Ht	Pot
5	Carya ovata	Shagbark Hickory	1.0m Ht	Pot
5	Carya cordiformis	Bitternut Hickory	1.0m Ht	Pot
15	Celtis occidentalis	Common Hackberry	1.0m Ht	Pot
15	Cornus alternifolia	Pagoda Dogwood	1.0m Ht	Pot
25	Cornus racemosa	Grey Dogwood	1.0m Ht	Pot
15	Ostrya virginiana	Hophornbeam Ironwood	1.0m Ht	Pot
15	Prunus pensylvanica	Pin Cherry	1.0m Ht	Pot
15	Prunus serotina	Black Cherry	1.0m Ht	Pot
15	Quercus alba	White Oak	1.0m Ht	Pot
15	Quercus bicolor	Swamp White Oak	1.0m Ht	Pot
15	Quercus macrocarpa	Bur Oak	1.0m Ht	Pot
15	Quercus rubra	Red Oak	1.0m Ht	Pot
25	Viburnum lentago	Nannyberry	1.0m Ht	Pot
255				
15	Abies balsamea	Balsam Fir	50cm Ht.	Pot
5	Juniperus virginiana	Eastern Red Cedar	50cm Ht.	Pot
15	Larix laricina	Eastern Larch / Tamarack	50cm Ht.	Pot
5	Picea glauca	White Spruce	50cm Ht.	Pot
5	Picea mariana	Black Spruce	50cm Ht.	Pot
5	Pinus strobus	White Pine	50cm Ht.	Pot
15	Tsuga canadensis	Canadian Hemlock	50cm Ht.	Pot
50				
75	Rhus typhina	Staghorn Sumac	30cm Ht.	Pot

SITE AREA: 12 210 SQ. M.
 ROW AREA +- 1 700 SQ. M.
 CANOPY COVER:
 CURRENT APPROX FROM GEO OTTAWA 5 000 SQ. M.
 ADDITIONAL PROVIDED: 3800 SQ. M.



Planting Bed c/w Biodegradable Weed Barrier.
 75mm Mulch
 Weed Barrier Fabric
 350mm Planting Soil
 Modified/Scarified/Loosened Subgrade 200mm Depth

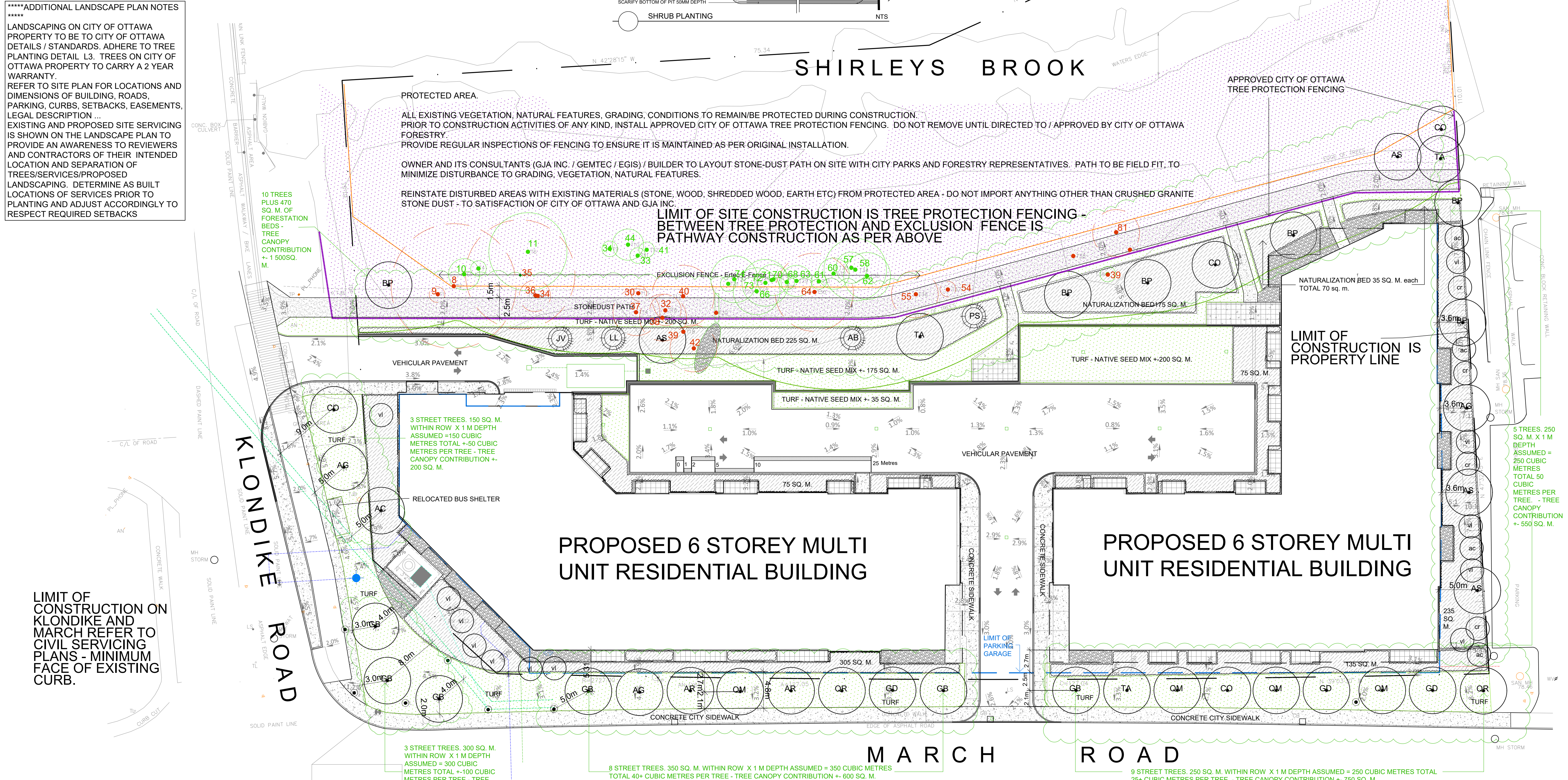
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 Civil

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Architect
NEUF architect(e)s
 630 boul. René-Lévesque O. 32e étage, Montréal, QC H3B 1S6
 T 514 847 1117 neuf.ca

Contractor is Responsible for Verification of Site Locations of all Utilities. Report any Discrepancies Between Site and Drawing Immediately to Gino J. Aiello. Do NOT Proceed Until Clarified.
 TURF AREAS TO BE SOD CW 150MM DEPTH TOPSOIL ON APPROVED SUBGRADE | SOD TO BE #1 GRADE TURF GRASS NURSERY SOD per NSGA - Fine Fescue/Kentucky Bluegrass Sod | PLANTING MIX TO BE APPROVED BY LANDSCAPE ARCHITECT | PLANT MATERIAL TO MEET CNLA STANDARDS/BE APPROVED BY LANDSCAPE ARCHITECT PRIOR TO PLANTING ALL MATERIALS & WORK TO BE MAINTAINED UNTIL FINAL ACCEPTANCE, (MOWING TURF, WEEDING BEDS, WATERING TURF & PLANTS), & INCLUDE 1 YEAR WARRANTY FROM DATE OF FINAL ACCEPTANCE | PROVIDE WATERING OF ALL PLANT MATERIAL AS REQUIRED FOR PROPER ESTABLISHMENT, UNTIL END OF WARRANTY PERIOD | REFER TO CIVIL ENGINEER'S DRAWING FOR ELEVATIONS / GRADING |
 The Landscape Plan is to be read in conjunction with the grading, servicing, site and survey plan | Bare root material to be stocked on site by the landscape contractor and checked by the landscape architect prior to planting | All trees to be preserved on or directly adjacent to the site will be protected | Plant material are to be installed a minimum of 3.0m away from any part of any hydro transformer, 3.0m corridor between a fire hydrant and the curb, a 2.5m radius beside or behind a fire hydrant, 2.0m from any servicing/utility line or structure |



REFER TO TCR BY GEMTEC

- Existing Tree to Remain
- Existing Tree to Remove
- Protection for Existing Trees

LANDSCAPE LEGEND

- Coniferous Tree
- Deciduous Tree
- Planting Bed
- Shrub Planting Bed
- Riverwash Stone On Filter Cloth
- Turf
- Concrete Monolithic Curb
- Concrete
- Stone Dust
- Patio Slabs
- Exclusion Fence
- E-Fence by Ertec
- Parking Garage
- Below
- Fire Hydrant
- FD Siamese Connection
- Light Standard
- Hydro/Utility Pole
- BELOW GRADE SERVICES REFER TO CIVIL/CUP
- WTR - U/G Water Service
- STM - U/G Storm Service
- SAN - U/G Sanitary Service
- GAS - U/G Gas Service
- BC - U/G TelCo Service
- H - U/G Hydro Service
- OHW - O/H Hydro Service

#	Revision	Date
3	3RD SUBMISSION SPC	2024 06 19
2	2ND SUBMISSION SPC	2024 03 19
1	1ST SUBMISSION SPC	2023 11 17
0	REVIEW/COORDINATION	2023 09 29

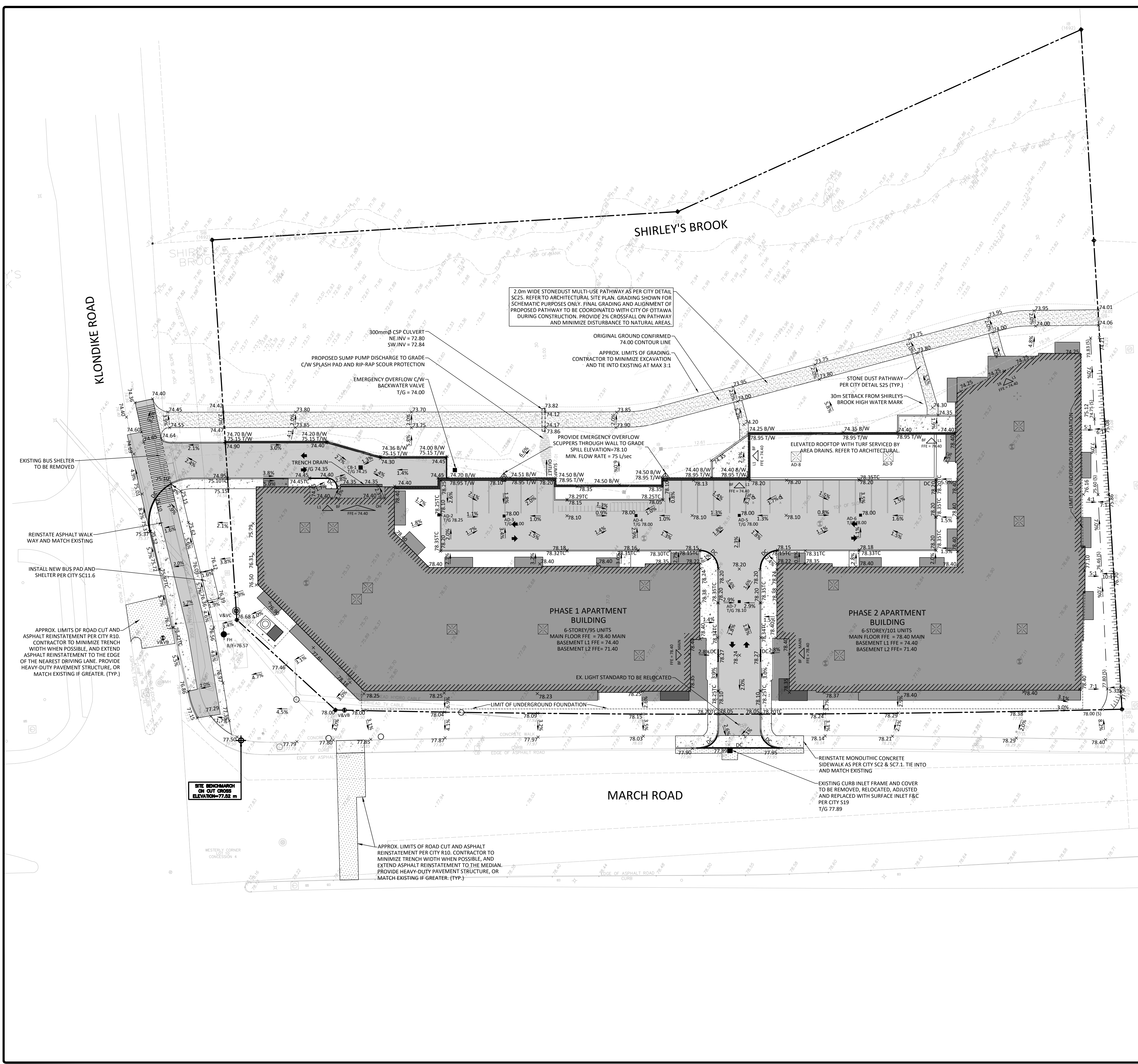
SINA
 3030 Carrefour Blvd, Office 1200, Laval, Quebec, H7T 2P5 514.565.7462

GJA INC.
 GINO J. AIELLO | LANDSCAPE ARCHITECT
 GJALA.COM | 613 298 5130 | GINO@GJALA.COM
 115 GERRARD ROAD UNIT #1 | OTTAWA, ONTARIO | K1P 6E2

PROPOSED 6 STOREY RESIDENTIAL BUILDING
 788 MARCH ROAD KANATA

Landscape Plan L1
 Scale: 1:250

Appendix C: Grading and Drainage Plan



GENERAL NOTES

1. THE ORIGINAL TOPOGRAPHY, GROUND ELEVATION AND SURVEY DATA SHOWN ARE SUPPLIED FOR INFORMATION PURPOSES ONLY, AND IMPLY NO GUARANTEE OF ACCURACY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY ALL INFORMATION SHOWN.
2. THIS PLAN IS NOT A CADASTRAL SURVEY SHOWING LEGAL PROPERTY BOUNDARIES AND EASEMENTS. THE PROPERTY BOUNDARIES SHOWN HEREON HAVE BEEN DERIVED FROM INFORMATION SUPPLIED BY OR SHOWN ON J.D. BARNES LTD. PLAN #8-2475 AND CANNOT BE RELIED UPON TO BE ACCURATE OR COMPLETE. THE PRECISE LOCATION OF THE CURRENT PROPERTY BOUNDARIES AND EASEMENTS CAN ONLY BE DETERMINED BY AN UP-TO-DATE LAND TITLE SEARCH AND A SUBSEQUENT CADASTRAL SURVEY PERFORMED AND CERTIFIED BY AN ONTARIO LAND SURVEYOR.
3. DO NOT TENDER USING DRAWINGS THAT ARE NOT MARKED "ISSUED FOR TENDER", AND DO NOT CONSTRUCT USING DRAWINGS THAT ARE NOT MARKED "ISSUED FOR CONSTRUCTION".
4. THE CONTRACTOR IS TO OBTAIN AND PAY FOR ALL NECESSARY PERMITS AND APPROVALS FROM THE TOWN BEFORE COMMENCING CONSTRUCTION.
5. THE CONTRACTOR IS RESPONSIBLE FOR ALL LAYOUT FOR CONSTRUCTION. THESE DRAWINGS SHALL NOT BE USED FOR ANY LAYOUT OF STRUCTURAL COMPONENTS AND OTHER UTILITIES INCLUDING, BUT NOT LIMITED TO, FOOTINGS, FOUNDATION WALLS, FOUNDATION DRAINAGE, SHOWING, ETC.
6. THE EXACT LOCATION OF ALL NEW SERVICES SHALL BE COORDINATED AND CONFIRMED ON SITE WITH THE ARCHITECTURAL AND STRUCTURAL PLANS. THE LOCATION OF ALL NEW SERVICES SHALL BE COORDINATED WITH THE LOCATION OF EXISTING UTILITIES, SERVICES, BUILDINGS AND ROADS, AS WELL AS THE FINAL BUILDING LAYOUT PRIOR TO INSTALLATION.
7. ABOVE GROUND APPURTENANCES INCLUDING, BUT NOT LIMITED TO, CURBS, SIDEWALKS, WALKWAYS, PLANTER BEDS, ETC. ARE SHOWN FOR SCHEMATIC PURPOSES ONLY. THE EXACT LAYOUT OF THE APPURTENANCES ARE TO BE COORDINATED WITH THE ARCHITECTURAL AND LANDSCAPE PLANS.
8. THE CONTRACTOR IS TO DETERMINE THE EXACT LOCATION, SIZE, MATERIAL AND ELEVATION OF ALL EXISTING UTILITIES PRIOR TO COMMENCING CONSTRUCTION. PROTECT AND ASSUME ALL RESPONSIBILITY FOR EXISTING UTILITIES WHETHER OR NOT SHOWN ON THESE DRAWINGS. IF THERE IS ANY DISCREPANCY THE CONTRACTOR IS TO NOTIFY THE ENGINEER PROMPTLY.
9. RESTORE ALL TRENCHES AND SURFACES OF PUBLIC ROAD ALLOWANCES TO CONDITION EQUAL OR BETTER THAN ORIGINAL CONDITION AND TO THE SATISFACTION OF CITY AUTHORITIES.
10. EXCAVATE AND DISPOSE OF ALL EXCESS EXCAVATED MATERIAL, SUCH AS ASPHALT, CURBING AND DEBRIS, OFF SITE AT THE CONTRACTOR'S EXPENSE.
11. TOPSOIL TO BE STRIPPED AND STOCKPILED FOR REHABILITATION. CLEAN FILL TO BE PLACED IN FILL AREAS AND COMPACTED TO 95% STANDARD PROCTOR DENSITY.
12. ALL DISTURBED AREAS TO BE RESTORED TO ORIGINAL CONDITION OR BETTER UNLESS OTHERWISE SPECIFIED.
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL AND SAFETY MEASURES DURING THE CONSTRUCTION PERIOD, INCLUDING THE SUPPLY, INSTALLATION, AND REMOVAL OF ALL NECESSARY SIGNAGE, DELINEATORS, MARKERS AND BARRIERS, MEETING OTM REQUIREMENTS.
14. DO NOT ALTER GRADING OF THE SITE WITHOUT PRIOR APPROVAL OF THE ENGINEER/CITY.
15. ALL ROADWAY, PARKING LOT, AND GRADING WORKS TO BE UNDERTAKEN IN ACCORDANCE WITH CITY OF OTTAWA AND OPS STANDARDS AND SPECIFICATIONS. THE CONTRACTOR IS TO PROVIDE POSITIVE DRAINAGE AWAY FROM THE BUILDING.
16. CONTACT THE CITY FOR INSPECTION OF ROUGH GRADING OF PARKING LOTS, ROADWAYS AND LANDSCAPED AREAS PRIOR TO PLACEMENT OF ASPHALT AND TOPSOIL. ALL DEFICIENCIES NOTED SHALL BE RECTIFIED TO THE CITY'S SATISFACTION PRIOR TO PLACEMENT OF ANY ASPHALT, TOPSOIL, SEED & MULCH AND/OR SOD.
17. ALL DIMENSIONS AND INVERTS MUST BE VERIFIED PRIOR TO CONSTRUCTION, IF THERE IS ANY DISCREPANCY THE CONTRACTOR IS TO NOTIFY THE ENGINEER PROMPTLY.
18. ELECTRICAL, GAS, TELEPHONE AND TELEVISION SERVICE UTILITIES ARE SUBJECT TO THE INDIVIDUAL AGENCY AND APPROVED AUTHORITIES. ALL WORK SHALL BE COORDINATED WITH THE RESPECTIVE AGENCIES HYDRO OTTAWA, BELL, ENBRIDGE, ETC. AND THE CITY.
19. CONTRACTOR TO ENSURE ALL APPLICABLE CITY AND OPS SPECIFICATIONS ARE FOLLOWED DURING CONSTRUCTION.
20. FOR GEOTECHNICAL AND HYDROGEOLOGICAL INFORMATION, REFER TO THE GEOTECHNICAL INVESTIGATION REPORT FOR 788 MARCH ROAD, KANATA PREPARED BY GEOFRMA ENGINEERING LTD. DATED DECEMBER 21, 2024 AND HYDROGEOLOGICAL STUDY DETERMINATION OF GROUNDWATER INFLOW TO THE PROPOSED 788 MARCH ROAD DEVELOPMENT, KANATA PREPARED BY GEOFRMA ENGINEERING LTD. DATED FEBRUARY 29, 2024.
21. CIVIL DRAWINGS ARE TO BE READ IN CONJUNCTION WITH ARCHITECTURAL, STRUCTURAL, LANDSCAPING, ELECTRICAL AND MECHANICAL PLANS.

GENERAL NOTES FOR ROADWAYS

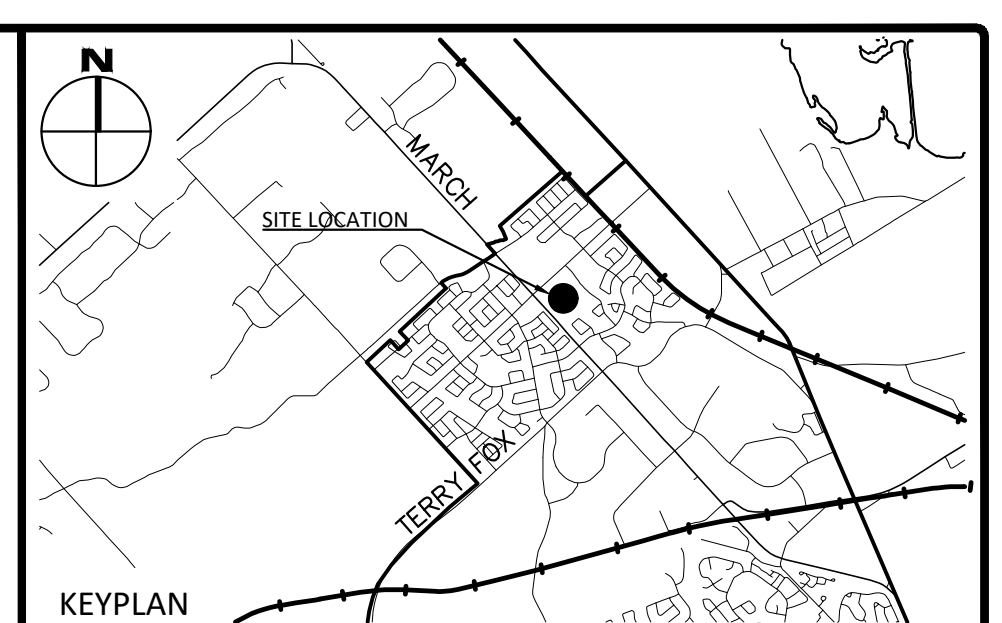
1. ALL TOPSOIL AND ORGANIC MATERIAL SHALL BE STRIPPED WITHIN THE ROADWAY PRIOR TO THE COMMENCEMENT OF CONSTRUCTION.
2. RESTORE ANY TRENCHES AND DISTURBED SURFACES OF PUBLIC ROAD ALLOWANCES TO CONDITION EQUAL OR BETTER THAN ORIGINAL CONDITION AND TO THE SATISFACTION OF TOWN AUTHORITIES.
3. CONCRETE CURB AND SIDEWALK SHALL BE IN ACCORDANCE WITH CITY OF OTTAWA STANDARD SC1.1 (BARRIER CURB), SC2 (MONOLITHIC SIDEWALK & CURB), AND SC4 (STANDARD SIDEWALK), UNLESS OTHERWISE SPECIFIED. CURB AND SIDEWALK SHALL BE MADE FOR CURB DEPRESSIONS AT SIDEWALKS, DRIVEWAYS AND RAMPS INCLUDING TACTILE WALKING SURFACE INDICATORS (TWSI) WHERE REQUIRED PER SC6 AND SC7.3.
4. PAVEMENT REINSTATEMENT FOR SERVICE AND UTILITY CUTS SHALL BE IN ACCORDANCE WITH CITY DETAIL R10, OPS5 509.010, OPS5 310.
5. ALL GRANULAR FOR ROADS SHALL BE PLACED TO A MINIMUM THICKNESS OF 300mm SPMD OR AS DIRECTED BY THE GEOTECHNICAL ENGINEER.
6. ASPHALT WEAR COURSE SHALL NOT BE PLACED UNTIL THE VIDEO INSPECTION OF SEWERS & NECESSARY REPAIRS HAVE BEEN CARRIED OUT TO THE SATISFACTION OF THE ENGINEER.
7. SUB-EXCAVATE SOFT AREAS AND FILL WITH GRANULAR 'B' COMPACTED IN MAXIMUM 300mm LIFTS AS DIRECTED BY A GEOTECHNICAL ENGINEER.
8. IN AREAS OF NEW PAVEMENT CONSTRUCTION, ALL SURFICIAL TOPSOIL AND ANY LOOSE/SOFT, WET, ORGANIC OR DELETERIOUS MATERIALS ARE TO BE REMOVED FROM THE PROPOSED SUBGRADE SURFACE. ANY SUB-EXCAVATED AREAS SHALL BE FILLED WITH COMPACTED EARTH BORROW APPROVED BY A GEOTECHNICAL ENGINEER. THE SUBGRADE SURFACE IS TO BE PRODUCED IN THE PRESENCE OF A GEOTECHNICAL ENGINEER. THE SUBGRADE SURFACE IS TO BE SHAPED AND CROWNED TO PROMOTE DRAINAGE OF THE GRANULAR MATERIALS.
9. ANY FILL REQUIRED TO MEET THE PROPOSED SUBGRADE ELEVATIONS FOR THE ACCESS ROADWAYS AND PARKING AREAS IS TO CONSIST OF MATERIAL WHICH MEETS OR EXCEEDS OPS5 SPECIFICATIONS FOR GRANULAR TYPE I OR C. GRANULAR SHALL BE PLACED IN MAXIMUM 300mm THICK LIFTS AND COMPACTED TO 98% SPMD MINIMUM USING VIBRATORY COMPACTION EQUIPMENT.
10. WHERE NEW PAVEMENT WILL ABUT EXISTING PAVEMENT, THE DEPTHS OF GRANULAR MATERIALS IS TO TAPER UP OR DOWN AT HORIZONTAL TO 1 VERTICAL OR FLATTER, TO MATCH THE DEPTHS OF THE GRANULAR MATERIALS EXPOSED IN THE EXISTING PAVEMENT.

PROPOSED PAVEMENT STRUCTURE

1. ASPHALT MATERIALS SHALL COMPLY WITH OPS5 310.
2. LIGHT-DUTY PAVEMENT STRUCTURE:
50mm HL-3 / SUPERPAVE 12.5mm TOP COURSE (PG58-34)
150mm GRANULAR 'A' BASE
300mm GRANULAR 'B' TYPE II SUB-BASE
3. HEAVY-DUTY PAVEMENT STRUCTURE:
40mm HL-3 / SUPERPAVE 12.5mm TOP COURSE (PG58-34)
60mm HL-8 / SUPERPAVE 19.0mm BINDER COURSE (PG58-34)
150mm GRANULAR 'A' BASE
450mm GRANULAR 'B' TYPE II SUB-BASE

GENERAL NOTES FOR GRADING

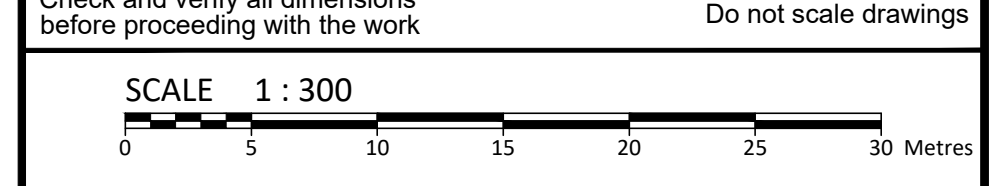
1. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT GRADING AROUND HYDRANTS, TRANSFORMERS, AND UTILITY PEDESTALS, ETC. MEET CURRENT HYDRO AND UTILITY COMPANY REQUIREMENTS.
2. ALL GROUND SURFACES SHALL BE EVENLY GRADED WITHOUT PONDING AREAS AND WITHOUT LOW POINTS EXCEPT WHERE APPROVED SWALE OR CATCH BASIN OUTLETS ARE PROVIDED.
3. CONTRACTOR TO ADJUST EXISTING CATCH BASINS, MANHOLES, FIRE HYDRANTS, VALVE CHAMBERS AND VALVE BOXES TO FINAL GRADE AS REQUIRED.
4. CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT EXISTING FOUNDATIONS OF ADJACENT BUILDINGS DURING EXCAVATION AND CONSTRUCTION PERIOD.
5. GRADING IN GRASSES AREAS WILL BE BETWEEN 2% TO 7%. GRADES IN EXCESS OF 7% WILL REQUIRE A MAXIMUM 3:1 TERRACING.
6. ALL PERVIOUS/LAWN AREAS TO BE TREATED WITH 100mm TOPSOIL & SEED OR SOD AS SOON AS FEASIBLE, EXCEPT WHERE NOTED.



LEGEND

LEGAL BOUNDARY	---
EXISTING FENCE	---
EXISTING STORM STRUCTURE	○
EXISTING CATCHBASIN	□
EXISTING SANITARY STRUCTURE	●
EXISTING FIRE HYDRANT	⊕
EXISTING VALVE & VALVE BOX	⊕
EXISTING HYDRO POLE	○
EXISTING HYDRO	○
EXISTING UTILITIES	---
EXISTING ELEVATION	99.00
PROPOSED STORM MANHOLE	○
PROPOSED STORM CATCHBASIN MH	○
PROPOSED SANITARY STRUCTURE	●
PROPOSED WATER VALVE/HYDRANT	⊕
PROPOSED FINISHED GROUND ELEVATION	99.00
PROPOSED TOP OF CURB ELEVATION	99.00TC
PROPOSED TOP OF WALL ELEVATION	99.00TW
PROPOSED BOTTOM OF WALL ELEVATION	99.00(B)
PROPOSED SWALE ELEVATION	99.00(S)
PROPOSED SLOPE	1.0%
PROPOSED TERRACING (3:1 MAX)	-----
PROPOSED BARRIER CURB	=====
PROPOSED RETAINING WALL	=====
PROPOSED DRAINAGE SWALE	-----
PROPOSED CONCRETE SIDEWALK	-----
ENTRY/EXIT LOCATION, ELEVATION & LEVEL	BF, 1R, OH MAIN L1
1R = ONE RISER, OH = OVERHEAD DOOR	FF=74.30

No.	Revisions	Date
5	REISSUED FOR SITE PLAN APPLICATION	JUN. 05, 2024
4	ISSUED FOR SHORING PERMIT	APR. 26, 2024
3	REISSUED FOR SITE PLAN APPLICATION	MAR. 15, 2024
2	ISSUED FOR SITE PLAN APPLICATION	NOV. 16, 2023
1	ISSUED FOR REVIEW	SEP. 29, 2023



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Stamp:

Client: **SINA**
 3030 BOUL. LE CARREFOUR, SUITE 1200,
 LAVAL, QUÉBEC

Project: **RESIDENTIAL BUILDING**
 788 MARCH ROAD,
 OTTAWA, ON

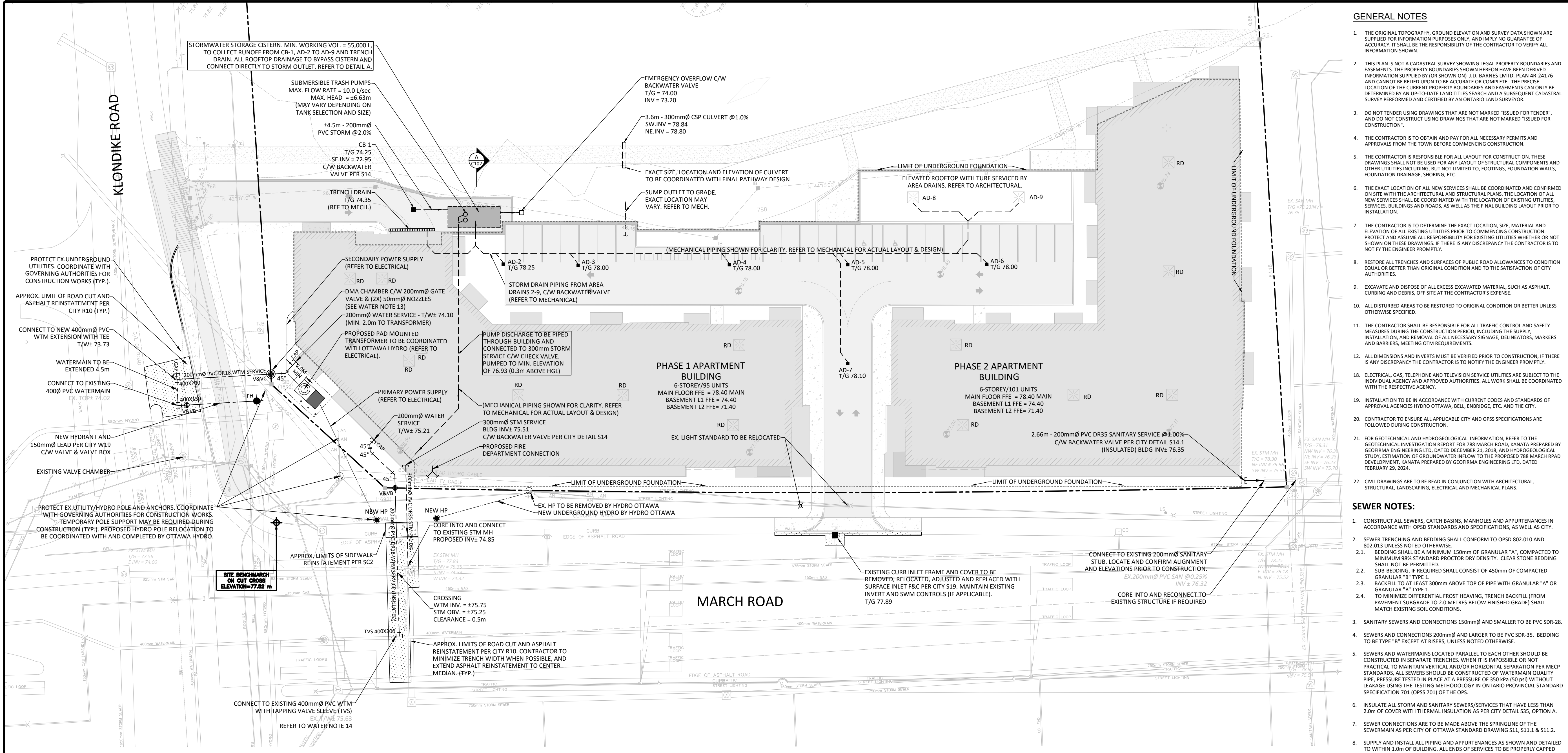
Drawing Title: **SITE GRADING PLAN**

Scale: 1:300	Project Number: CO-24-1519
Drawn By: RP	Checked By: JH
Designed By: RP	Drawing Number: C101

FILENAME: U:\Information\2024\CO-24-1519\1519-01\Site Grading\CO-24-1519-01.dwg
 DATE PLOTTED: Wednesday, June 05, 2024, 10:45:10 AM
 LAST SAVED: Wednesday, June 05, 2024, 10:45:10 AM
 LAST PLOTTED: Wednesday, June 05, 2024, 10:45:10 AM

D07-00-000

Appendix D: Site Servicing Plan



GENERAL NOTES

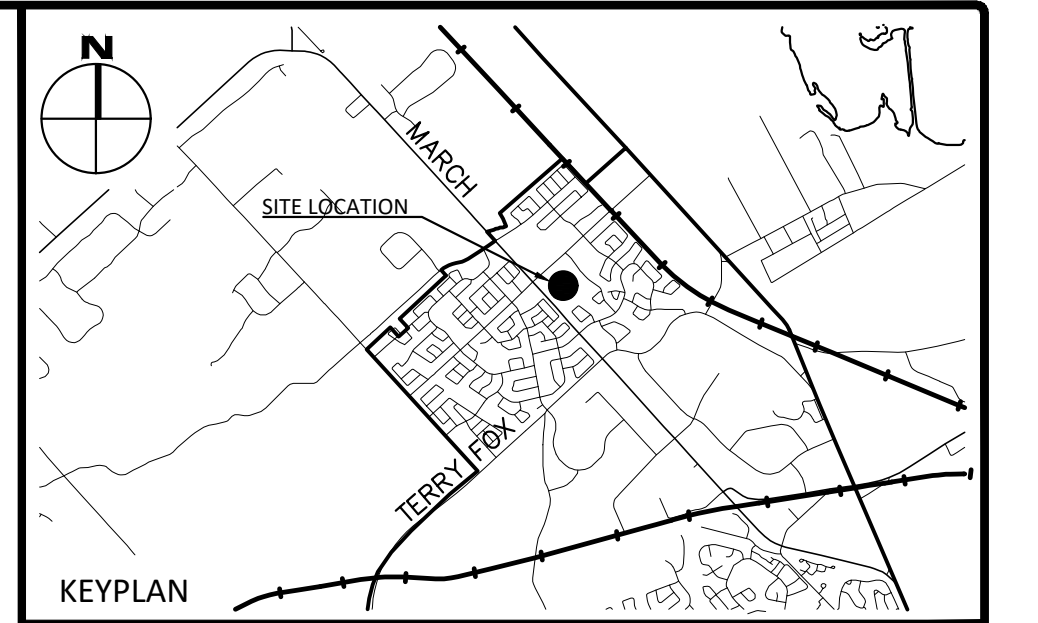
1. THE ORIGINAL TOPOGRAPHY, GROUND ELEVATION AND SURVEY DATA SHOWN ARE SUPPLIED FOR INFORMATION PURPOSES ONLY, AND IMPLY NO GUARANTEE OF ACCURACY. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY ALL INFORMATION SHOWN.
2. THIS PLAN IS NOT A CADASTRAL SURVEY SHOWING LEGAL PROPERTY BOUNDARIES AND EASEMENTS. THE PROPERTY BOUNDARIES SHOWN HEREON HAVE BEEN DERIVED FROM INFORMATION SUPPLIED BY (OR SHOWN ON) I.D. BARNES L.M.P. PLAN 48-24176 AND CANNOT BE RELIED UPON TO BE ACCURATE OR COMPLETE. THE PRECISE LOCATION OF THE CURRENT PROPERTY BOUNDARIES AND EASEMENTS CAN ONLY BE DETERMINED BY AN UP-TO-DATE LAND TITLES SEARCH AND A SUBSEQUENT CADASTRAL SURVEY PERFORMED AND CERTIFIED BY AN ONTARIO LAND SURVEYOR.
3. DO NOT TRENCH USING DRAWINGS THAT ARE NOT MARKED "ISSUED FOR TENDER", AND DO NOT CONSTRUCT USING DRAWINGS THAT ARE NOT MARKED "ISSUED FOR CONSTRUCTION".
4. THE CONTRACTOR IS TO OBTAIN AND PAY FOR ALL NECESSARY PERMITS AND APPROVALS FROM THE TOWN BEFORE COMMENCING CONSTRUCTION.
5. THE CONTRACTOR IS RESPONSIBLE FOR ALL LAYOUT FOR CONSTRUCTION. THESE DRAWINGS SHALL NOT BE USED FOR ANY LAYOUT OF STRUCTURAL COMPONENTS AND OTHER UTILITIES INCLUDING, BUT NOT LIMITED TO, FOOTINGS, FOUNDATION WALLS, FOUNDATION DRAINAGE, SHORING, ETC.
6. THE EXACT LOCATION OF ALL NEW SERVICES SHALL BE COORDINATED AND CONFIRMED ON SITE WITH THE ARCHITECTURAL AND STRUCTURAL PLANS. THE LOCATION OF ALL NEW SERVICES SHALL BE COORDINATED WITH THE LOCATION OF EXISTING UTILITIES, SERVICES, BUILDINGS AND ROADS, AS WELL AS THE FINAL BUILDING LAYOUT PRIOR TO INSTALLATION.
7. THE CONTRACTOR IS TO DETERMINE THE EXACT LOCATION, SIZE, MATERIAL AND ELEVATION OF ALL EXISTING UTILITIES PRIOR TO COMMENCING CONSTRUCTION. PROTECT AND ASSUME ALL RESPONSIBILITY FOR EXISTING UTILITIES WHETHER OR NOT SHOWN ON THESE DRAWINGS. IF THERE IS ANY DISCREPANCY THE CONTRACTOR IS TO NOTIFY THE ENGINEER PROMPTLY.
8. RESTORE ALL TRENCHES AND SURFACES OF PUBLIC ROAD ALLOWANCES TO ORIGINAL CONDITION OR BETTER THAN ORIGINAL CONDITION AND TO THE SATISFACTION OF CITY AUTHORITIES.
9. EXCAVATE AND DISPOSE OF ALL EXCESS EXCAVATED MATERIAL, SUCH AS ASPHALT, CURBING AND DEBRIS, OFF SITE AT THE CONTRACTOR'S EXPENSE.
10. ALL DISTURBED AREAS TO BE RESTORED TO ORIGINAL CONDITION OR BETTER UNLESS OTHERWISE SPECIFIED.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL AND SAFETY MEASURES DURING THE CONSTRUCTION PERIOD, INCLUDING THE SUPPLY, INSTALLATION, AND REMOVAL OF ALL NECESSARY SIGNAGE, DELINEATORS, MARKERS AND BARRIERS, MEETING OTM REQUIREMENTS.
12. ALL DIMENSIONS AND INVERTS MUST BE VERIFIED PRIOR TO CONSTRUCTION, IF THERE IS ANY DISCREPANCY THE CONTRACTOR IS TO NOTIFY THE ENGINEER PROMPTLY.
13. ELECTRICAL, GAS, TELEPHONE AND TELEVISION SERVICE UTILITIES ARE SUBJECT TO THE INDIVIDUAL AGENCY AND APPROVED AUTHORITIES. ALL WORK SHALL BE COORDINATED WITH THE RESPECTIVE AGENCY.
14. INSTALLATION TO BE IN ACCORDANCE WITH CURRENT CODES AND STANDARDS OF APPROVAL AGENCIES HYDRO OTTAWA, BELL, ENBRIDGE, ETC. AND THE CITY.
15. CONTRACTOR TO ENSURE ALL APPLICABLE CITY AND OPCS SPECIFICATIONS ARE FOLLOWED DURING CONSTRUCTION.
16. FOR GEOTECHNICAL AND HYDROLOGICAL INFORMATION, REFER TO THE GEOTECHNICAL INVESTIGATION REPORT FOR 788 MARCH ROAD, KANATA PREPARED BY GEORHMA ENGINEERING LTD., DATED DECEMBER 21, 2018, AND HYDROLOGICAL STUDY, ESTIMATION OF GROUNDWATER INFLOW TO THE PROPOSED 788 MARCH ROAD DEVELOPMENT, KANATA PREPARED BY GEORHMA ENGINEERING LTD., DATED FEBRUARY 29, 2024.
17. CIVIL DRAWINGS ARE TO BE READ IN CONJUNCTION WITH ARCHITECTURAL, STRUCTURAL, LANDSCAPING, ELECTRICAL AND MECHANICAL PLANS.

SEWER NOTES:

1. CONSTRUCT ALL SEWERS, CATCH BASINS, MANHOLES AND APPURTENANCES IN ACCORDANCE WITH OPCS STANDARDS AND SPECIFICATIONS, AS WELL AS CITY.
2. SEWER TRENCHING AND BEDDING SHALL CONFORM TO OPCS 802.010 AND 802.013 UNLESS NOTED OTHERWISE.
 - 2.1. BEDDING SHALL BE A MINIMUM 150mm OF GRANULAR "A", COMPACTED TO MINIMUM 98% STANDARD PROCTOR DRY DENSITY. CLEAR STONE BEDDING SHALL NOT BE PERMITTED.
 - 2.2. SUB-BEDDING, IF REQUIRED SHALL CONSIST OF 450mm OF COMPACTED GRANULAR "B" TYPE 1.
 - 2.3. BACKFILL TO AT LEAST 300mm ABOVE TOP OF PIPE WITH GRANULAR "A" OR GRANULAR "B" TYPE 1.
 - 2.4. TO MINIMIZE DIFFERENTIAL FROST HEAVING, TRENCH BACKFILL (FROM PAVEMENT SURFACE TO 2.0 METRES BELOW FINISHED GRADE) SHALL MATCH EXISTING SOIL CONDITIONS.
3. SANITARY SEWERS AND CONNECTIONS 150mm AND SMALLER TO BE PVC SDR-28.
4. SEWERS AND CONNECTIONS 200mm AND LARGER TO BE PVC SDR-35. BEDDING TO BE TYPE "B" EXCEPT AT RISERS, UNLESS NOTED OTHERWISE.
5. SEWERS AND WATERMANS LOCATED PARALLEL TO EACH OTHER SHOULD BE CONSTRUCTED IN SEPARATE TRENCHES. WHEN IT IS IMPOSSIBLE OR NOT PRACTICAL TO MAINTAIN VERTICAL AND/OR HORIZONTAL SEPARATION PER MSCP STANDARDS, ALL SEWERS SHOULD BE CONSTRUCTED OF WATERMAIN QUALITY PIPE, PRESSURE TESTED IN PLACE AT A PRESSURE OF 350 kPa (50 psi) WITHOUT LEAKAGE USING THE TESTING METHODOLOGY IN ONTARIO PROVINCIAL STANDARD SPECIFICATION 701 (OPSS 701) OF THE OPS.
6. INSULATE ALL STORM AND SANITARY SEWERS/SERVICES THAT HAVE LESS THAN 2.0m OF COVER WITH THERMAL INSULATION AS PER CITY DETAIL S35, OPTION A.
7. SEWER CONNECTIONS ARE TO BE MADE ABOVE THE SPRINGLINE OF THE SEWERMAIN AS PER CITY OF OTTAWA STANDARD DRAWING S11, S11.1 & S11.2.
8. SUPPLY AND INSTALL ALL PIPING AND APPURTENANCES AS SHOWN AND DETAILED TO WITHIN 1.0m OF BUILDING. ALL ENDS OF SERVICES TO BE PROPERLY CAPPED AND LOCATED WITH "2"x4" LONG MARKERS.
9. CONTRACTOR TO TELEVISION (CCTV) ALL PROPOSED SEWERS ON SITE, OUTLET CONNECTION TO THE MAIN AND PIPES 150mm OR GREATER PRIOR TO BASE COURSE ASPHALT. UPON COMPLETION OF CONTRACT, THE CONTRACTOR IS RESPONSIBLE TO FLUSH AND CLEAN ALL SEWERS & APPURTENANCES.
10. DYE TESTING IS TO BE COMPLETED ON SANITARY SERVICE TO CONFIRM PROPER CONNECTION TO SANITARY SEWER MAIN.

WATERMAIN NOTES

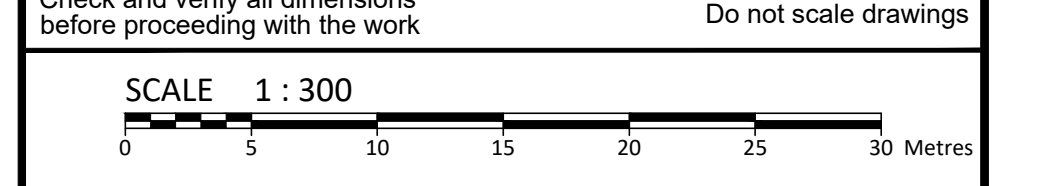
1. CONSTRUCT ALL WATERMANS AND APPURTENANCES IN ACCORDANCE WITH OPCS STANDARDS AND SPECIFICATIONS, AS WELL AS CITY STANDARDS.
2. WATERMANS AND/OR WATER SERVICES ARE TO HAVE A MINIMUM COVER OF 2.0m. INSULATE ALL WATERMANS AND SERVICES THAT HAVE LESS THAN 2.4m COVER WITH THERMAL INSULATION AS PER CITY DETAIL W22.
3. IF THE WATERMAIN MUST BE DEFLECTED TO MEET ALIGNMENT, ENSURE THAT THE AMOUNT OF DEFLECTION USED IS EQUAL TO OR LESS THAN HALF OF THAT WHICH IS RECOMMENDED BY THE MANUFACTURER.
4. THERMAL INSULATION OF WATERMANS AT OPEN STRUCTURES AS PER CITY DETAIL W23.
5. VALVES TO BE OPERATED BY CITY STAFF ONLY.
6. NO CONNECTION TO EXISTING WATER NETWORK SHALL BE COMPLETED UNTIL A WATER PERMIT IS OBTAINED FROM THE CITY. CITY TO BE PRESENT FOR WATERMAIN CONNECTION, CONNECTION, EXCAVATION, BACKFILLING AND REINSTATEMENT TO BE COMPLETED BY CONTRACTOR.
7. IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PERFORM ANY WATERMAIN CONNECTION(S) REQUIRED. THIS SHALL BE COMPLETED IN THE PRESENCE OF A DESIGNATED MUNICIPAL WATER OPERATOR AND THE SELECTED CONTRACTOR SHALL PROVE TO THE SATISFACTION OF THE CITY THAT THEY ARE COMPETENT TO PERFORM THE WORKS PRIOR TO INITIATING CONSTRUCTION.
8. CONCRETE THRUST BLOCKS TO CONFORM TO OPCS 1103.010 AND OPCS 1103.020.
9. ALL WATERMAIN TO BE CLASS 150 DR-18 OR APPROVED EQUIVALENT.
10. ALL WATERMAIN TO BE EQUIPPED WITH TRACER WIRE.
11. AS PER CITY GUIDELINE, THE MINIMUM VERTICAL CLEARANCE BETWEEN WATERMAIN AND SEWER/UTILITY IS 0.25m FOR CROSSING OVER THE SEWER, AS PER CITY DETAIL W25.2 FOR CROSSING UNDER SEWER. THE MINIMUM VERTICAL CLEARANCE IS 0.5m AS PER CITY DETAIL W25. FOR CROSSING UNDER SEWER, ADEQUATE STRUCTURAL SUPPORT FOR THE SEWERS IS REQUIRED TO PREVENT EXCESSIVE DEFLECTION OF JOINTS AND SETTLING. THE LENGTH OF WATER PIPE SHALL BE CENTERED AT THE POINT OF CROSSING SO THAT THE JOINTS WILL BE EQUIDISTANT AND AS FAR AS POSSIBLE FROM THE SEWER.
12. SWABBING, CHLORINATION AND CONTINUITY TESTING FOR PROPOSED WATER SERVICES IS TO FOLLOW CITY OF OTTAWA SPECIAL PROVISIONS #SP-4491 & SP-4494.
13. WATER SERVICES FOR BUILDING TO INCLUDE (1) ONE DISTRICT METERING AREA (DMA) CHAMBER, USING A STANDARD 200mm DIAMETER ISOLATION VALVE AND (2) TWO 50mm DIAMETER STANDARD NOZZLES TAPPED ON EACH SIDE OF THE ISOLATION VALVE. VALVES TO INCLUDE STANDARD CATHODIC PROTECTION IN ACCORDANCE WITH CITY STANDARD SPECIFICATIONS.
14. NO MECHANICAL OR VIBRATION EQUIPMENT IS TO BE USED WITHIN A PROXIMITY OF 3.0m OF THE 400mm BACKSPOUN WATERMAIN ON MARCH ROAD. ALL CONSTRUCTION IS TO BE COORDINATED WITH THE CITY OF OTTAWA DRINKING WATER SERVICES AND AUTHORIZED INSPECTORS PRIOR TO EXCAVATION.



LEGEND

LEGAL BOUNDARY	---
EXISTING FENCE	---
EXISTING SANITARY SEWER	---
EXISTING SANITARY STRUCTURE	○
EXISTING STORM STRUCTURE	○
EXISTING CATCHBASIN	○
EXISTING SANITARY STRUCTURE	●
EXISTING FIRE HYDRANT	+
EXISTING VALVE & VALVE BOX	+
EXISTING HYDRO POLE	○
EXISTING HYDRO	○
EXISTING UTILITIES	○
PROPOSED STORM SEWER	XX.Xxm - XXXmm STM @ X.XXX%
PROPOSED SANITARY SEWER	XX.Xxm - XXXmm SAN @ X.XXX%
PROPOSED WATERMAIN	XX.Xxm - XXXmm WTR @ X.XXX%
PROPOSED CULVERT	---
PROPOSED STORM MAINTENANCE HOLE	○
PROPOSED STORM DITCH INLET MAINTENANCE HOLE	○
PROPOSED SANITARY MAINTENANCE HOLE	○
PROPOSED WATER VALVE/HYDRANT	+

No.	Revisions	Date
5	REISSUED FOR SITE PLAN APPLICATION	JUN. 05, 2024
4	ISSUED FOR SHORING PERMIT	APR. 26, 2024
3	REISSUED FOR SITE PLAN APPLICATION	MAR. 15, 2024
2	ISSUED FOR SITE PLAN APPLICATION	NOV. 16, 2023
1	ISSUED FOR REVIEW	SEP. 29, 2023



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Stamp: J. D. J. HEWSON, PROFESSIONAL ENGINEER, 105056243, 06/05/2024, PROVINCE OF ONTARIO

Client: **SINA**
 3030 BOUL. LE CARREFOUR, SUITE 1200,
 LAVAL, QUÉBEC

Project: **RESIDENTIAL BUILDING**
 788 MARCH ROAD,
 OTTAWA, ON

Drawing Title: **SITE SERVICING PLAN**

Scale: 1:300	Project Number: CO-24-1519
Drawn By: RP	Checked By: JH
Designed By: RP	Drawing Number: C102

STORMWATER STORAGE CISTERN. MIN. WORKING VOL. = 55,000 L. TO COLLECT RUNOFF FROM CB-1, AD-2 TO AD-9 AND TRENCH DRAIN. ALL ROOFTOP DRAINAGE TO BYPASS CISTERN AND CONNECT DIRECTLY TO STORM OUTLET. REFER TO DETAIL A.

SUBMERSIBLE TRASH PUMPS
 MAX. FLOW RATE = 10.0 l/sec
 MAX. HEAD = 46.63m
 (MAY VARY DEPENDING ON TANK SELECTION AND SIZE)

EMERGENCY OVERFLOW C/W BACKWATER VALVE
 T/G = 74.00
 INV = 73.20

3.6m - 300mm CSP CULVERT @ 1.0%
 SW INV = 78.84
 NE INV = 78.80

EXACT SIZE, LOCATION AND ELEVATION OF CULVERT TO BE COORDINATED WITH FINAL PATHWAY DESIGN

SUMP OUTLET TO GRADE. EXACT LOCATION MAY VARY. REFER TO MECH.

LIMIT OF UNDERGROUND FOUNDATION

ELEVATED ROOFTOP WITH TURF SERVICED BY AREA DRAINS. REFER TO ARCHITECTURAL

(MECHANICAL PIPING SHOWN FOR CLARITY. REFER TO MECHANICAL FOR ACTUAL LAYOUT & DESIGN)

STORM DRAIN PIPING FROM AREA DRAINS 2-9, C/W BACKWATER VALVE (REFER TO MECHANICAL)

PUMP DISCHARGE TO BE PIPED THROUGH BUILDING AND CONNECTED TO 300mm STORM SERVICE C/W CHECK VALVE. PUMPED TO MIN. ELEVATION OF 76.93 (0.3m ABOVE HGL)

PHASE 1 APARTMENT BUILDING
 6-STORY/95 UNITS
 MAIN FLOOR FFE = 78.40
 BASEMENT L1 FFE = 74.40
 BASEMENT L2 FFE = 71.40

PHASE 2 APARTMENT BUILDING
 6-STORY/101 UNITS
 MAIN FLOOR FFE = 78.40
 BASEMENT L1 FFE = 74.40
 BASEMENT L2 FFE = 71.40

2.66m - 200mm PVC DR35 SANITARY SERVICE @ 1.00%
 C/W BACKWATER VALVE PER CITY DETAIL S14-1 (INSULATED) BLDG INV = 76.35

SECONDARY POWER SUPPLY (REFER TO ELECTRICAL)

DMA CHAMBER C/W 200mm GATE VALVE & (2) 50mm NOZZLES (SEE WATER NOTE 13)

200mm WATER SERVICE - T/W = 74.10 (MIN. 2.0m TO TRANSFORMER)

PROPOSED PAD MOUNTED TRANSFORMER TO BE COORDINATED WITH OTTAWA HYDRO (REFER TO ELECTRICAL)

PRIMARY POWER SUPPLY (REFER TO ELECTRICAL)

200mm WATER SERVICE T/W = 75.21

300mm STM SERVICE BLDG INV = 75.51

C/W BACKWATER VALVE PER CITY DETAIL S14

PROPOSED FIRE DEPARTMENT CONNECTION

NEW HP

CORE INTO AND CONNECT TO EXISTING STM MH PROPOSED INV = 74.85

EXISTING MH T/G = 77.89

W INV = 74.32

CROSSING WTM INV = +75.75

STM OBV = +75.25

CLEARANCE = 0.5m

APPROX. LIMITS OF ROAD CUT AND ASPHALT REINSTATEMENT PER CITY R10. CONTRACTOR TO MINIMIZE TRENCH WIDTH WHEN POSSIBLE, AND EXTEND ASPHALT REINSTATEMENT TO CENTER MEDIAN. (TYP.)

APPROX. LIMITS OF SIDEWALK REINSTATEMENT PER SC2

CONNECT TO EXISTING 400mm PVC WTM WITH TAPPING VALVE SLEEVE (TVS) T/G = 79.83

REFER TO WATER NOTE 14

WATERTIGHT, INSULATED, LOCKABLE ACCESS HATCH FOR SEDIMENT CLEANOUT / MAINTENANCE

WATERTIGHT, INSULATED, LOCKABLE ACCESS HATCH FOR PUMP ACCESS / MAINTENANCE, C/W LADDER ACCESS

150mm HIGH LOAD RIGID INSULATION - EXTEND 1.5m BELOW TOP OF TANK.

100mm ALLOY AIR VENT C/W INSECT SCREEN, TO BE MINIMUM 900mm ABOVE FINISHED GRADE

EMERGENCY OVERFLOW STRUCTURE PER OPSD-705.030 (OR APPROVED EQUIV.) C/W GRATE & OPEN BOTTOM. LOCATION HAS BEEN REVOLVED FOR CLARITY. ACTUAL LOCATION SHOWN ON PLAN. SPILL ELEVATION = 74.00

STONEDUST PEDESTRIAN PATHWAY PER CITY DETAIL SC25

2024 GROUNDWATER LEVEL VARIES - 72.66 TO 73.57

REFER TO GEORHMA HYDROLOGICAL REPORT DATED FEB. 29, 2024

2024 SHIRLEY'S BROOK WATER (ICE) ELEVATION - 71.85

300mm EMERGENCY OUTLET PIPE C/W BACKWATER VALVE CONCRETE RETAINING WALL PER OPSD-3120.100 (ACTUAL SIZE MAY VARY)

(X2) SUBMERSIBLE TRASH PUMPS
 RELEASE RATE = 10.0 l/sec
 MAX HEAD = 46.63m
 (HEAD MAY VARY DEPENDING ON TANK SELECTION AND EXACT SIZE)
 REFER TO MECHANICAL FOR PUMP, FLOAT AND ELECTRICAL DETAILS

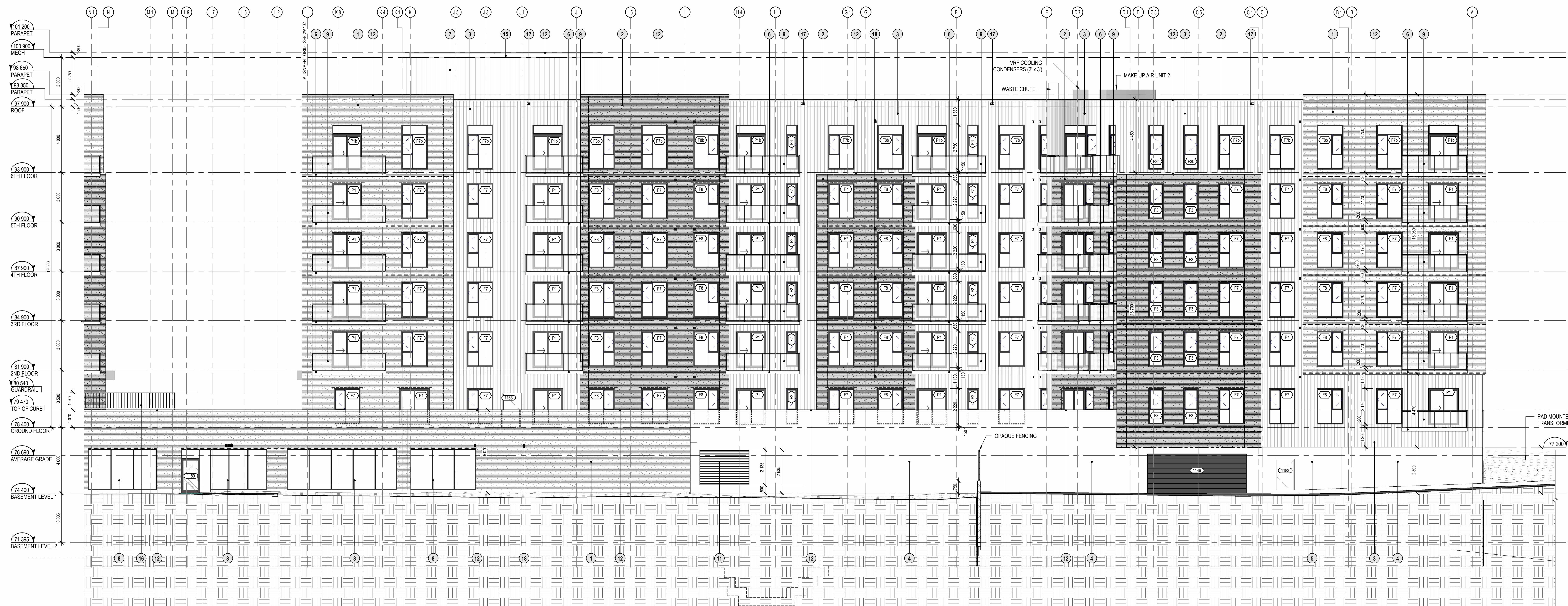
CONCRETE BALLAST WEIGHTS (DESIGNED BY OTHERS)

DETAIL - STORMWATER STORAGE CISTERN
 1:75

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 DATE PLOTTED: Wednesday, June 05, 2024 10:45:15 AM
 LAST PLOTTED: Wednesday, June 05, 2024 10:45:15 AM

D07-00-000

Appendix E: Building Elevations



NORTH ELEVATION - BUILDING "A"
1:100



SOUTH ELEVATION - BUILDING "A"
1:100

ELEVATION KEYNOTE LEGEND	
1	ARCHITECTURAL MASONRY B/C BRICK/CRAFT - "WALNUT OR MENDOTA GREY"
2	ARCHITECTURAL MASONRY B/C BRICK/CRAFT - "SPIN"
3	STEEL BRUSH METAL NORWOOD MIN - "BLACK WALNUT"
4	ACRYLIC - LIGHT GREY COLOR
5	ACRYLIC - CHARCOAL COLOR
6	EXPOSED CONCRETE
7	METAL CLADDING PANELS - LIGHT GREY COLOR
8	WINDOW WALL DOUBLE GLASS WITH ALUMINUM PROFILE - BLACK COLOR
9	GLAZING WITH TINTED GLASS WITH ALUMINUM PROFILE - BLACK COLOR
10	WINDOW SCREEN WITH FRIGTED GLASS WITH ALUMINUM PROFILE - BLACK COLOR - 800MM OF HEIGHT
11	GLASSING - COLOR MATCHED TO ADJACENT MATERIAL
12	METAL FLASHING - COLOR MATCHED TO ADJACENT MATERIAL
13	ADJACENT MATERIAL
14	METAL PANEL FOR ENTRANCE CANOPY - BLACK COLOR
15	GALVANIZED STEEL OUTLET
16	GALVANIZED STEEL CHANNEL WITH FINGER
17	PRE-FINISHED METAL SCOPER
18	VENT COVER - SEE MECHANICAL DRAWINGS
19	WINDOW SCREEN WITH PERFORATED ALUMINUM - BLACK COLOR

ELEVATION LEGEND	
	EXTERIOR FINISH REFER TO ELEVATION KEYNOTE LEGEND
	WINDOW TYPE REFER TO WINDOW SCHEDULE
	ANODIZED ALUMINUM WALL BOX VENTILATION COVER COORDINATE WITH MECHANICAL ENGINEER DRAWINGS FOR LOCATIONS.
	GALVANIZED STEEL STRUCTURAL STEEL ANGLE LEVELS AND LOCATIONS TO BE COORDINATED WITH THE ARCHITECTURAL DETAILS / SECTIONS, MASONRY COURSING, STEEL SHOP DRAWINGS AND STRUCTURAL ENGINEER DRAWINGS.
	DROPPED GALVANIZED STEEL STRUCTURAL STEEL ANGLE LEVELS AND LOCATIONS TO BE COORDINATED WITH THE ARCHITECTURAL DETAILS / SECTIONS, FLOOR SLAB PLANS, MASONRY COURSING, STEEL SHOP DRAWINGS AND STRUCTURAL ENGINEER DRAWINGS.
	MASONRY EXPANSION JOINT REFER TO PLAN DETAILS

NOTES:
1. EXHAUST VENT LOCATION TO BE VERIFIED AND COORDINATED WITH MECHANICAL ENGINEER DRAWINGS.

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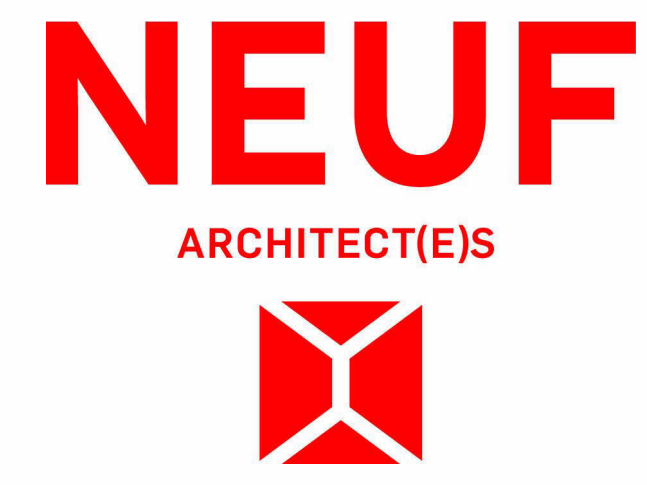
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13109

NO.	REVISION	DATE (aa-mm-jj)
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VÉRIFIÉ PAR - Checked by
B.R.

DATE (aa-mm-jj)
2024-06-04

ÉCHELLE - Scale
1 : 100

TITRE DU DESSIN - Drawing Title
ELEVATIONS - BUILDING "A"

NO. DESSIN - Drawing Number	NO. DESSIN - Day Number
B	A400

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ELEVATION KEYNOTE LEGEND	
NOTE	DESCRIPTION
1	ARCHITECTURAL MASONRY B/C: ARBESCRETT, "WALNUT OR MENDOTA GREY"
2	ARCHITECTURAL MASONRY B/C: ARBESCRETT, "SPAL"
3	STEEL BRUSH IAC METAL, NORWOOD MIN. "BLACK WALNUT"
4	ACRYLIC, LIGHT GREY COLOR
5	ACRYLIC, CHARCOAL COLOR
6	EXPOSED CONCRETE
7	METAL CLADDING PANEL, LIGHT GREY COLOR
8	WINDOW WALL DOUBLE GLASS WITH ALUMINUM PROFILE, BLACK COLOR
9	QUARTZ, WITH TINTED GLASS WITH ALUMINUM PROFILE, BLACK COLOR
10	PRIVATE SCREEN WITH FRIGTES GLASS WITH ALUMINUM PROFILE, BLACK COLOR, 1800MM OF HEIGHT
11	CLADDING, COLOR MATCHED TO ADJACENT MATERIAL
12	METAL FLASHING, COLOR MATCHED TO ADJACENT MATERIAL
13	INTERIOR SPOT
14	METAL PANEL FOR ENTRANCE CANOPY, BLACK COLOR
15	GALVANIZED STEEL OUTLET
16	GALVANIZED STEEL, CHARCOAL WITH POCKETS
17	PRE-FINISHED METAL SCOPPER
18	VENT COVER, SEE MECHANICAL DRAWINGS
19	PRIVATE SCREEN WITH PERFORATED ALUMINUM, BLACK COLOR

ELEVATION LEGEND	
	EXTERIOR FINISH REFER TO ELEVATION KEYNOTE LEGEND
	WINDOW TYPE REFER TO WINDOW SCHEDULE
	ANODIZED ALUMINUM WALL BOX VENTILATION COORDINATE WITH MECHANICAL ENGINEER DRAWINGS FOR LOCATIONS.
	GALVANIZED STEEL STRUCTURAL STEEL ANGLE, LEVEL AND LOCATIONS TO BE COORDINATED WITH THE ARCHITECTURAL DETAILS / SECTIONS, MASONRY COURSING, STEEL SHOP DRAWINGS AND STRUCTURAL ENGINEER DRAWINGS.
	DROPPED GALVANIZED STEEL STRUCTURAL STEEL ANGLE, LEVEL AND LOCATIONS TO BE COORDINATED WITH THE ARCHITECTURAL DETAILS / SECTIONS, FLOOR FINISH PLANS, MASONRY COURSING, STEEL SHOP DRAWINGS AND STRUCTURAL ENGINEER DRAWINGS.
	MASONRY EXPANSION JOINT REFER TO VAN DETAILS
NOTES: 1 EXHAUST VENT LOCATION TO BE VERIFIED AND COORDINATED WITH MECHANICAL ENGINEER DRAWINGS.	



NORTH-WEST CORNER ELEVATION - BUILDING "A"
1:100



SOUTH-EAST CORNER ELEVATION - BUILDING "A"
1:100



WEST ELEVATION - BUILDING "A"
1:100



EAST ELEVATION - BUILDING "A"
1:100

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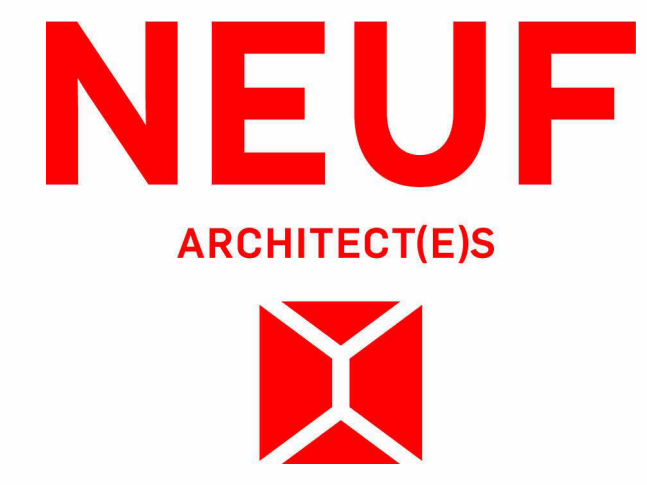
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2024-06-04

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TITRE DU DESSIN / Drawing Title
ELEVATIONS - BUILDING "A"

REVISION / Revision
B

NO. DESSIN / Drawing Number
A401



NORTH ELEVATION - BUILDING "B"
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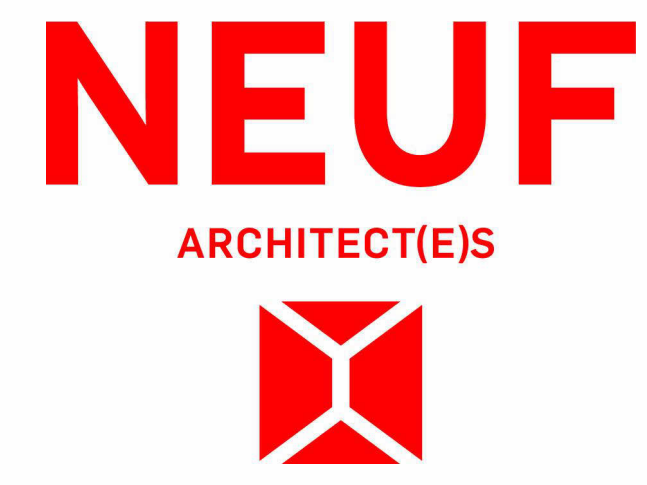
A402 SOUTH ELEVATION - BUILDING "B"
1:100

ELEVATION KEYNOTE LEGEND	
NOTE	DESCRIPTION
1	ARCHITECTURAL MASONRY B.C. ARBISCREAF - "VANUIT OR MONTREIL GREY"
2	ARCHITECTURAL MASONRY B.C. ARBISCREAF - "SPAL"
3	STEEL BRING MAC METAL NORWOOD MIN - "BLACK WILNUT"
4	ACRYLIC LIGHT GREY COLOR
5	ACRYLIC CHARCOAL COLOR
6	EXPOSED CONCRETE
7	METAL CLADDING PANEL LIGHT GREY COLOR
8	WINDOW WALL DOUBLE GLASS WITH ALUMINUM PROFILE - BLACK COLOR
9	GUARDRAIL WITH TAMPED GLASS WITH ALUMINUM PROFILE - BLACK COLOR
10	FRONT SCREEN WITH FRIGTED GLASS WITH ALUMINUM PROFILE - BLACK COLOR - 800MM OF HEIGHT
11	CLADDING - COLOR MATCHED TO ADJACENT MATERIAL
12	METAL FLASHING - COLOR MATCHED TO ADJACENT MATERIAL
13	ADJACENT MATERIAL
14	METAL PANEL FOR ENTRANCE CANOPY - BLACK COLOR
15	ALUMINUM PANEL FOR ENTRANCE CANOPY - BLACK COLOR
16	PRE-FINISHED METAL SCOFFER
17	VENT CLADDING SEE MECHANICAL DRAWINGS
18	FRONT SCREEN WITH FRIGTED GLASS WITH ALUMINUM PROFILE - BLACK COLOR

ELEVATION LEGEND	
○	EXTERIOR FINISH REFER TO ELEVATION KEYNOTE LEGEND
□	INDOOR TYPE REFER TO WINDOW SCHEDULE
▨	ANODIZED ALUMINUM WALL BOX VENTILATION LOUVER. COORDINATE WITH MECHANICAL ENGINEER DRAWINGS FOR LOCATION.
104	GALVANIZED STEEL STRUCTURAL SELF ANGLE. LEVELS AND LOCATIONS TO BE COORDINATED WITH THE ARCHITECTURAL DETAILS/ SECTION, MASONRY COURSE, STEEL SHOP DRAWINGS AND STRUCTURAL ENGINEER DRAWINGS.
105	PROFILING GALVANIZED STEEL STRUCTURAL SELF ANGLE. LEVELS AND LOCATIONS TO BE COORDINATED WITH THE ARCHITECTURAL DETAILS/ SECTION, FLOOR PLATE PLANS, MASONRY COURSE, STEEL SHOP DRAWINGS AND STRUCTURAL ENGINEER DRAWINGS.
106	MASONRY EXPANSION JOINT REFER TO PLAN DETAILS

NOTES:
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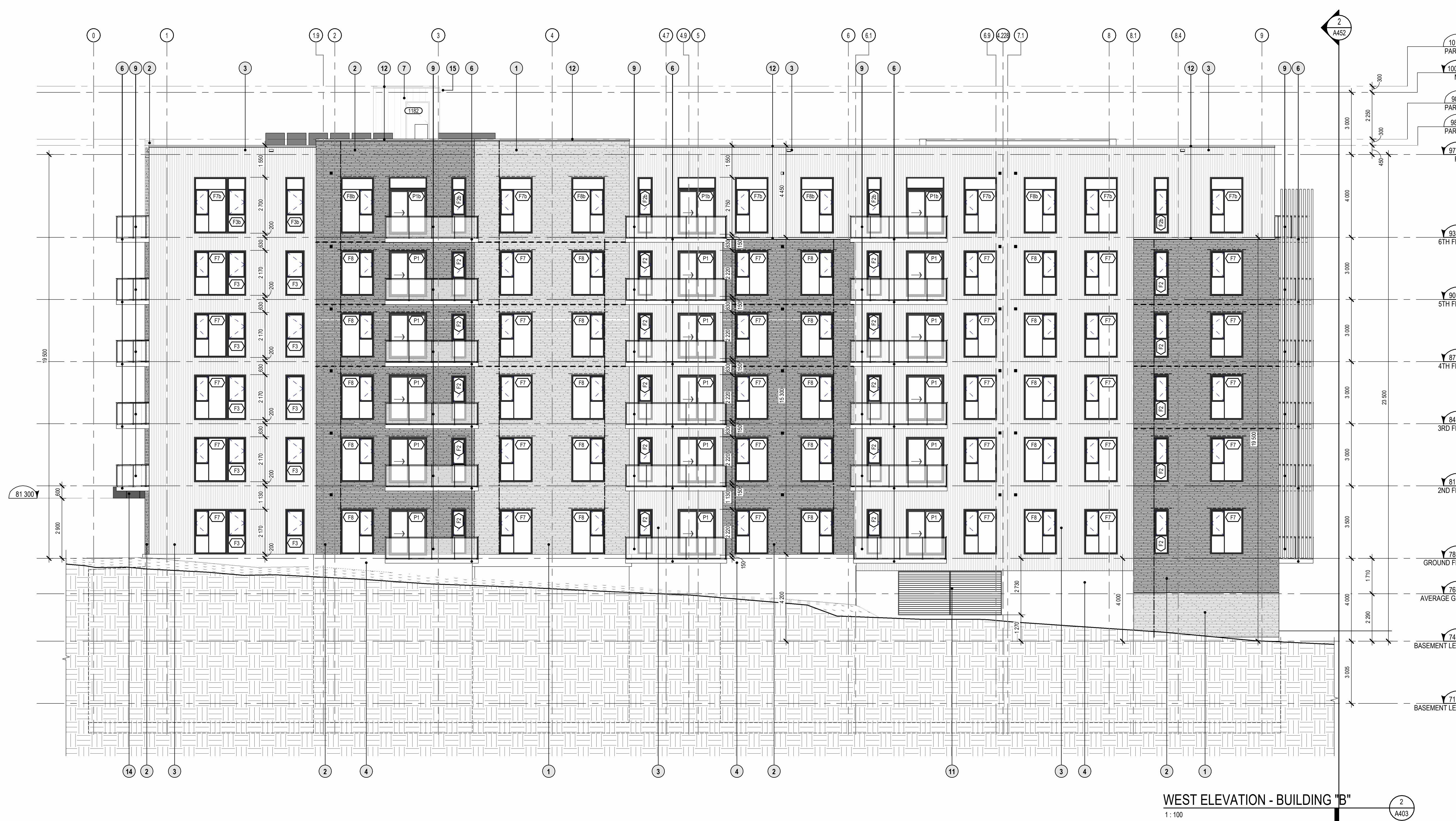
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TITRE DU DESSIN Drawing Title
ELEVATIONS - BUILDING "B"

REVISION	Revision	NO. DESSIN	Dwg Number
B		A402	

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WEST ELEVATION - BUILDING "B"
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A403 EAST ELEVATION - BUILDING "B"
1:100

ELEVATION KEYNOTE LEGEND	
NOTE	DESCRIPTION
1	ARCHITECTURAL MASONRY B.C. ARBESKRAFT "WALNUT OR MENDOTA GRAY"
2	ARCHITECTURAL MASONRY B.C. ARBESKRAFT "SPAL"
3	STEEL BRUSH METAL NORWOOD MIN. "BLACK WALNUT"
4	ACRYLIC LIGHT GREY COLOR
5	ACRYLIC CHARCOAL COLOR
6	EXPOSED CONCRETE
7	METAL CLADDING PANEL LIGHT GREY COLOR
8	WINDOW WALL DOUBLE GLASS WITH ALUMINUM PROFILE - BLACK COLOR
9	GLAZING WITH TINTED GLASS WITH ALUMINUM PROFILE - BLACK COLOR
10	FRONT SCREEN WITH FRIGTED GLASS WITH ALUMINUM PROFILE - BLACK COLOR - 800MM OF HEIGHT
11	CLADDING - COLOR MATCHED TO ADJACENT MATERIAL
12	METAL FLASHING - COLOR MATCHED TO ADJACENT MATERIAL
13	ADJACENT MATERIAL
14	METAL PANEL FOR ENTRANCE CANOPY - BLACK COLOR
15	GLAZING WITH TINTED GLASS WITH ALUMINUM PROFILE - BLACK COLOR
16	GLAZING WITH TINTED GLASS WITH ALUMINUM PROFILE - BLACK COLOR
17	PRE-FINISHED METAL SCOPPER
18	VENT CLADDING SEE MECHANICAL DRAWINGS
19	FRONT SCREEN WITH FRIGTED GLASS WITH ALUMINUM PROFILE - BLACK COLOR

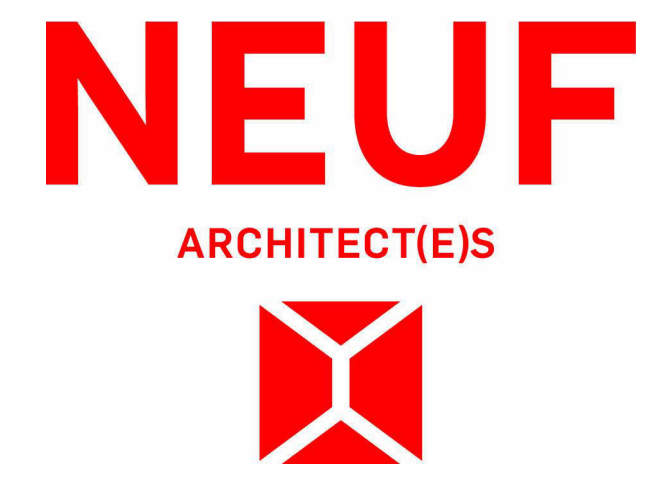
ELEVATION LEGEND	
○	EXTERIOR FINISH REFER TO ELEVATION KEYNOTE LEGEND
□	WINDOW TYPE REFER TO WINDOW SCHEDULE
▨	ANODIZED ALUMINUM WALL BOX VENTILATION LOUVER. COORDINATE WITH MECHANICAL ENGINEER DRAWINGS FOR LOCATION.
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106	MASONRY EXPANSION JOINT

NOTES:
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 TITRE DU DESSIN Drawing Title

ELEVATIONS - BUILDING "B"

REVISION Revision NO. DESSIN Drawing Number
B A403

Appendix F: Floor Plans

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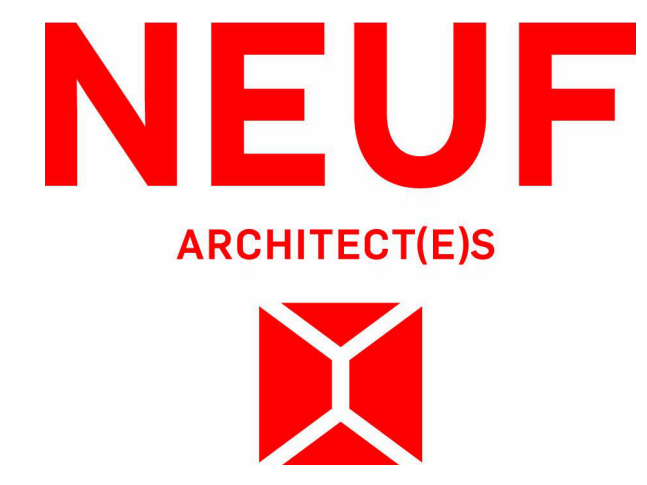
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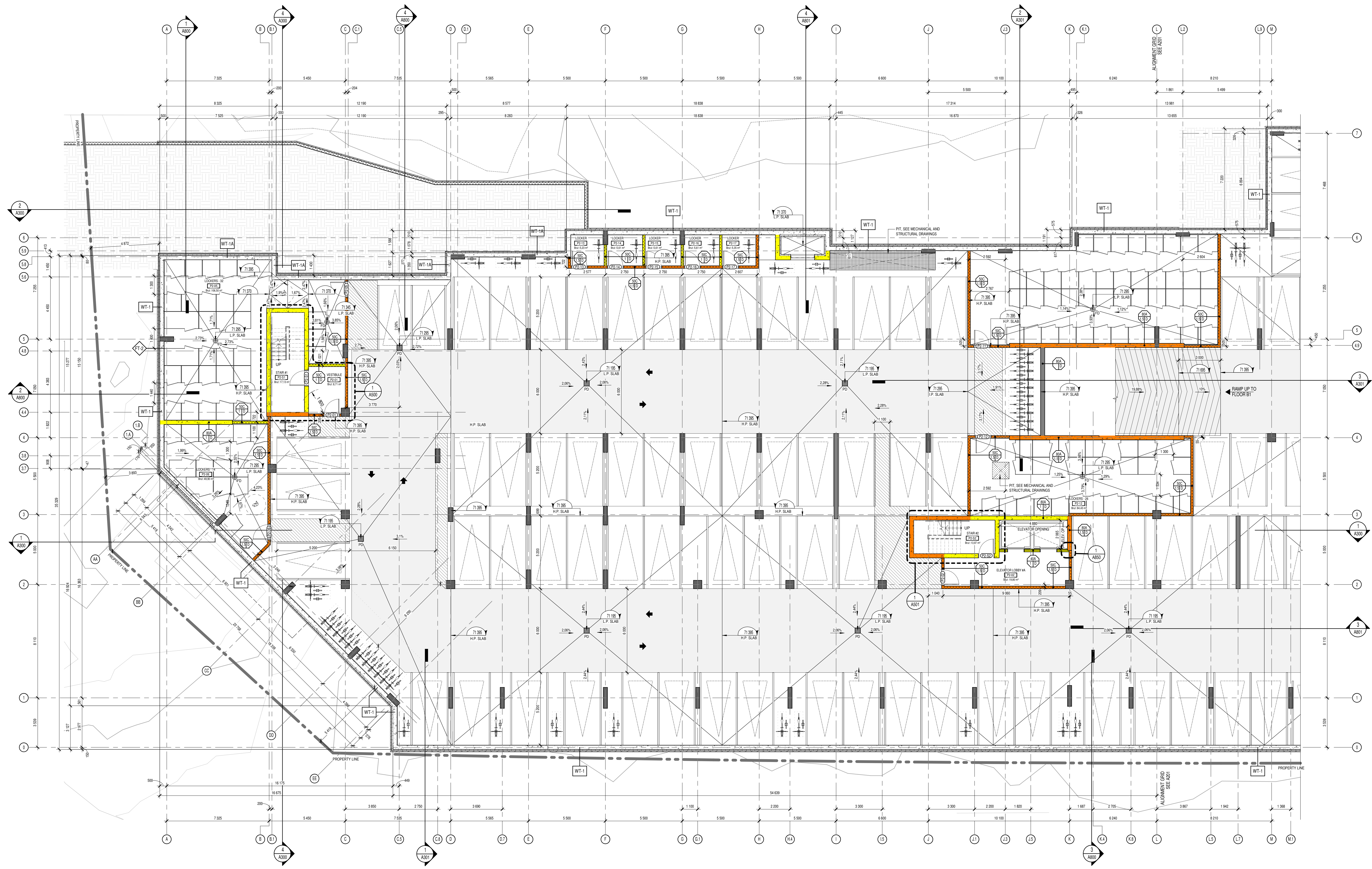
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TITRE DU DESSIN / Drawing Title
BUILDING "A" BASEMENT LEVEL 2

NO. DESSIN / Drawing Number
B A200

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A200 - BUILDING "A" BASEMENT L2
 1:100

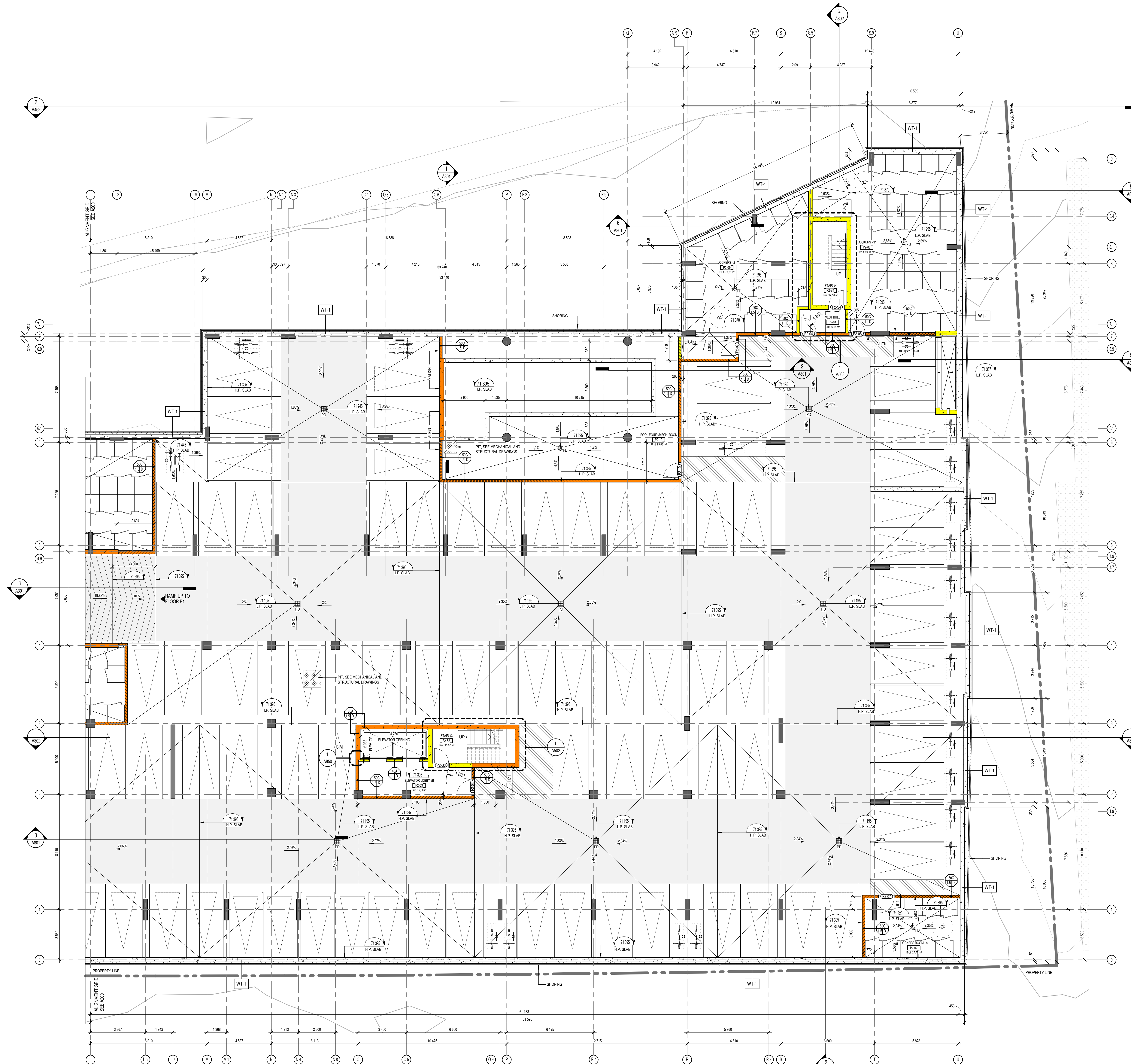


TABLEAU - P2 CAR PARKINGS		TABLEAU - P2 LOCKERS		TABLEAU - P2 BICYCLE PARKINGS	
TYPE DE STATIONNEMENT	QTE	TYPE DE STATIONNEMENT	QTE	TYPE DE STATIONNEMENT	QTE
SMALL 2400mm X 4600mm	8	CASIER	174	VELO AISLE 1.5	30
STANDARD 2600mm X 5200mm	127				
TOTAL:	135	TOTAL:	174	TOTAL:	40

A201 - BUILDING "B" BASEMENT L2
1:100

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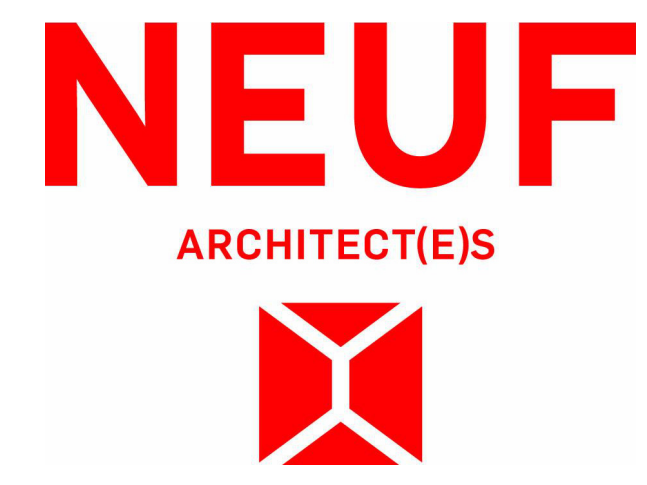
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 TITRE DU DESSIN / Drawing Title: BUILDING "B" BASEMENT LEVEL 2

REVISION / Revision: B
 NO. DESSIN / Drawing Number: A201

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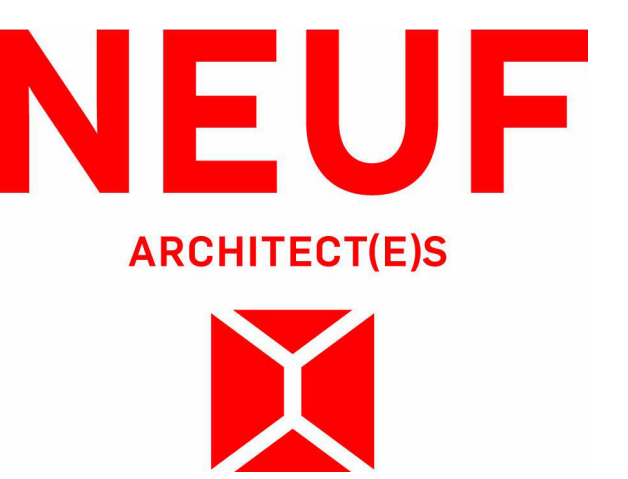
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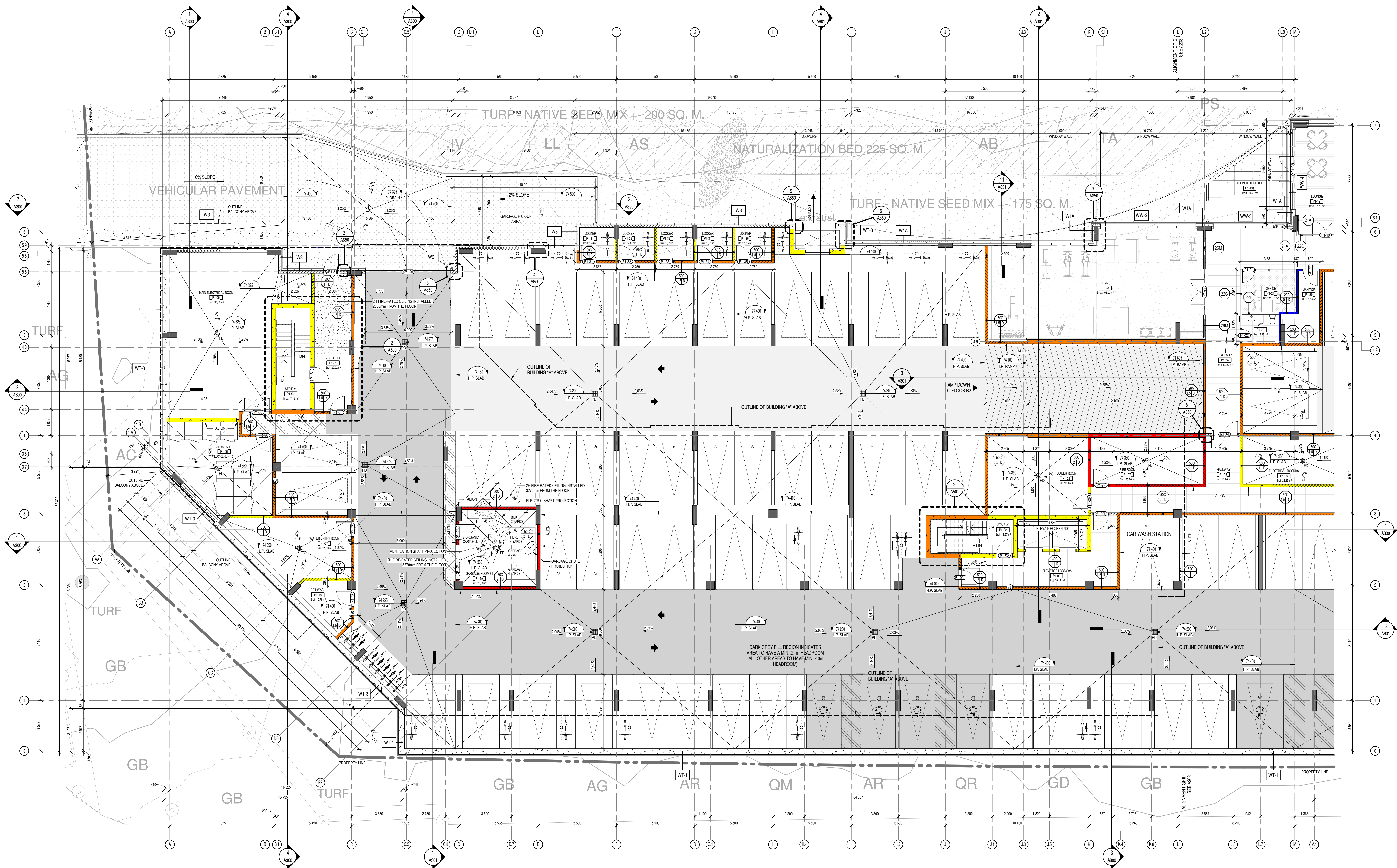
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TITRE DU DESSIN / Drawing Title
BUILDING "A" BASEMENT LEVEL 1

REVISION / Revision NO. DESSIN / Day Number
B A202



A202 - BUILDING "A" BASEMENT L1
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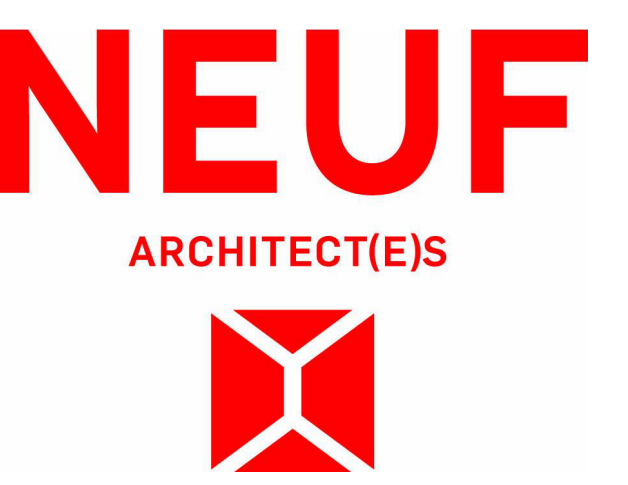
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TITRE DU DESSIN / Drawing Title
BUILDING "B" BASEMENT LEVEL 1

RÉVISION / Revision
B

NO. DESSIN / Drawing Number
A203

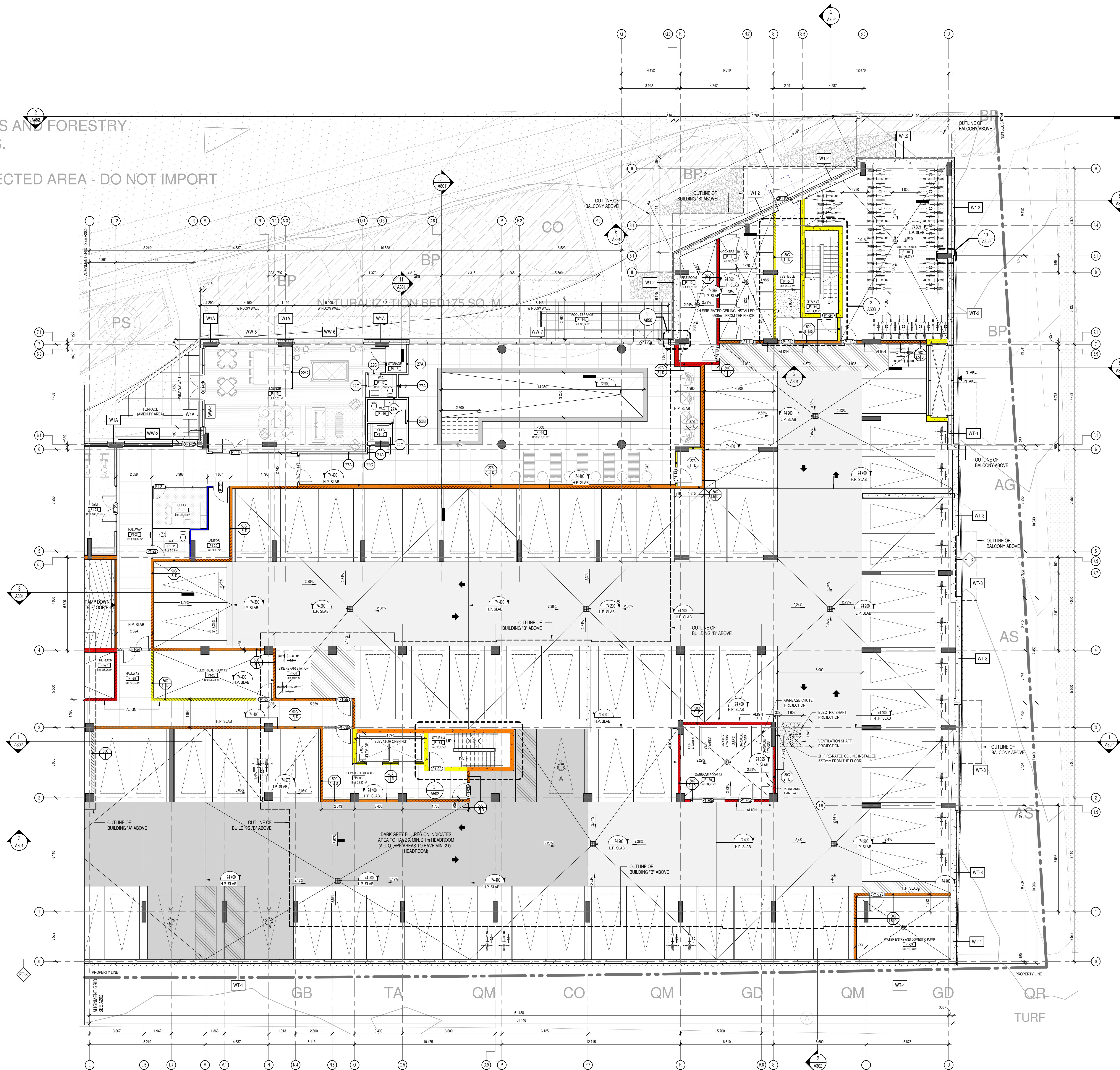


TABLEAU - P1 CAR PARKINGS	
TYPE DE STATIONNEMENT	QTE.
SMALL 2400mm X 4600mm	5
STANDARD 2600mm X 5200mm	105
HANDICAPÉ 2600mm X 5200mm Type B	4
HANDICAPÉ 3400mm X 5200mm Type A	3
TOTAL:	117

TABLEAU - P1 LOCKERS	
TYPE DE STATIONNEMENT	QTE.
CASIER	20
TOTAL:	20

TABLEAU - P1 BICYCLE PARKINGS	
TYPE DE STATIONNEMENT	QTE.
VELO AISLÉ 1.5	40
VELO	14
TOTAL:	54

A203 - BUILDING "B" BASEMENT L1
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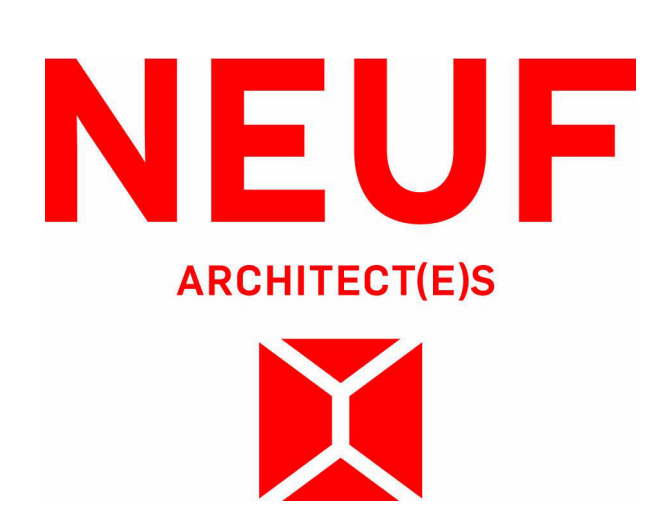
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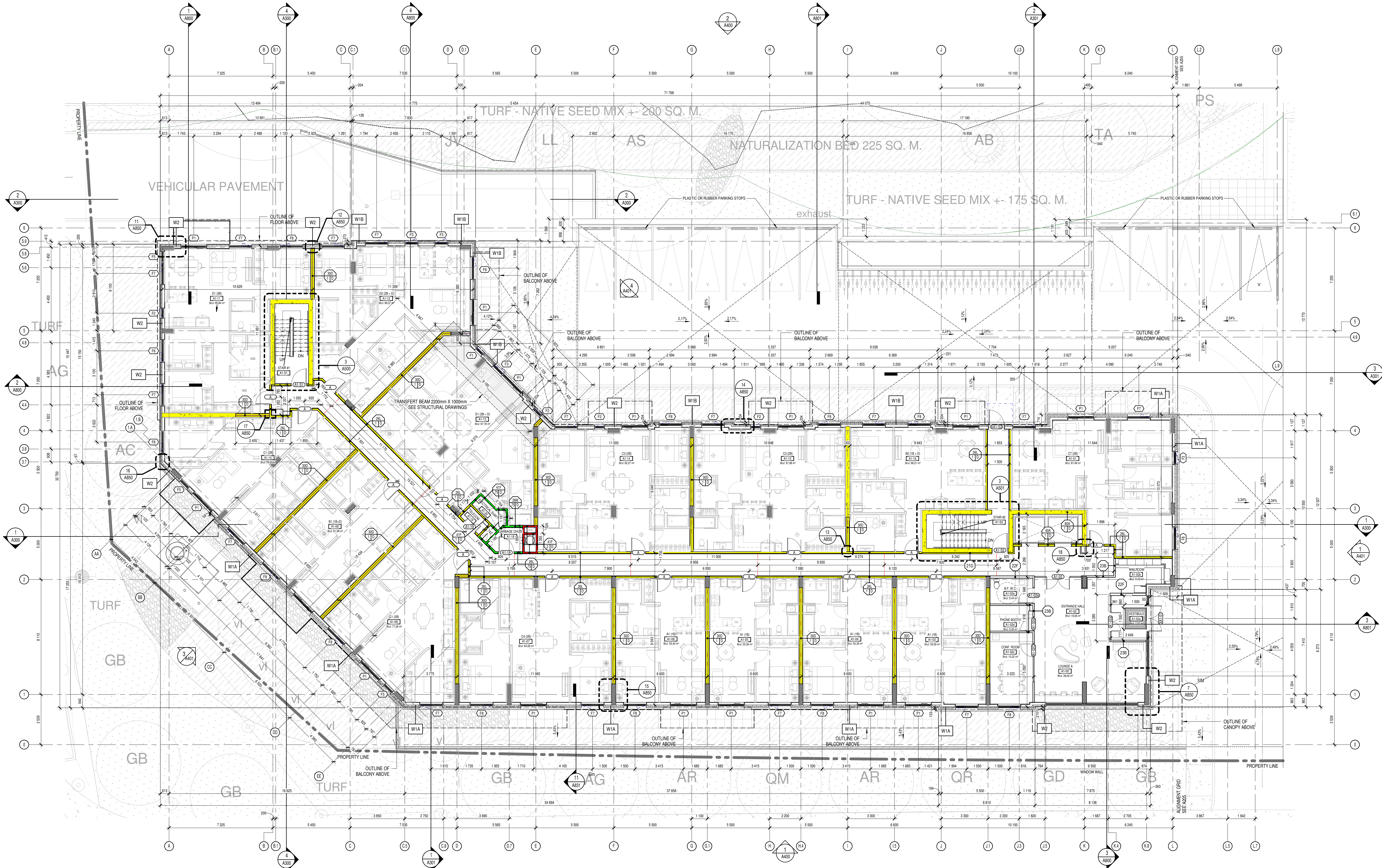


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TITRE DU DESSIN / Drawing Title BUILDING "A" - GROUND FLOOR

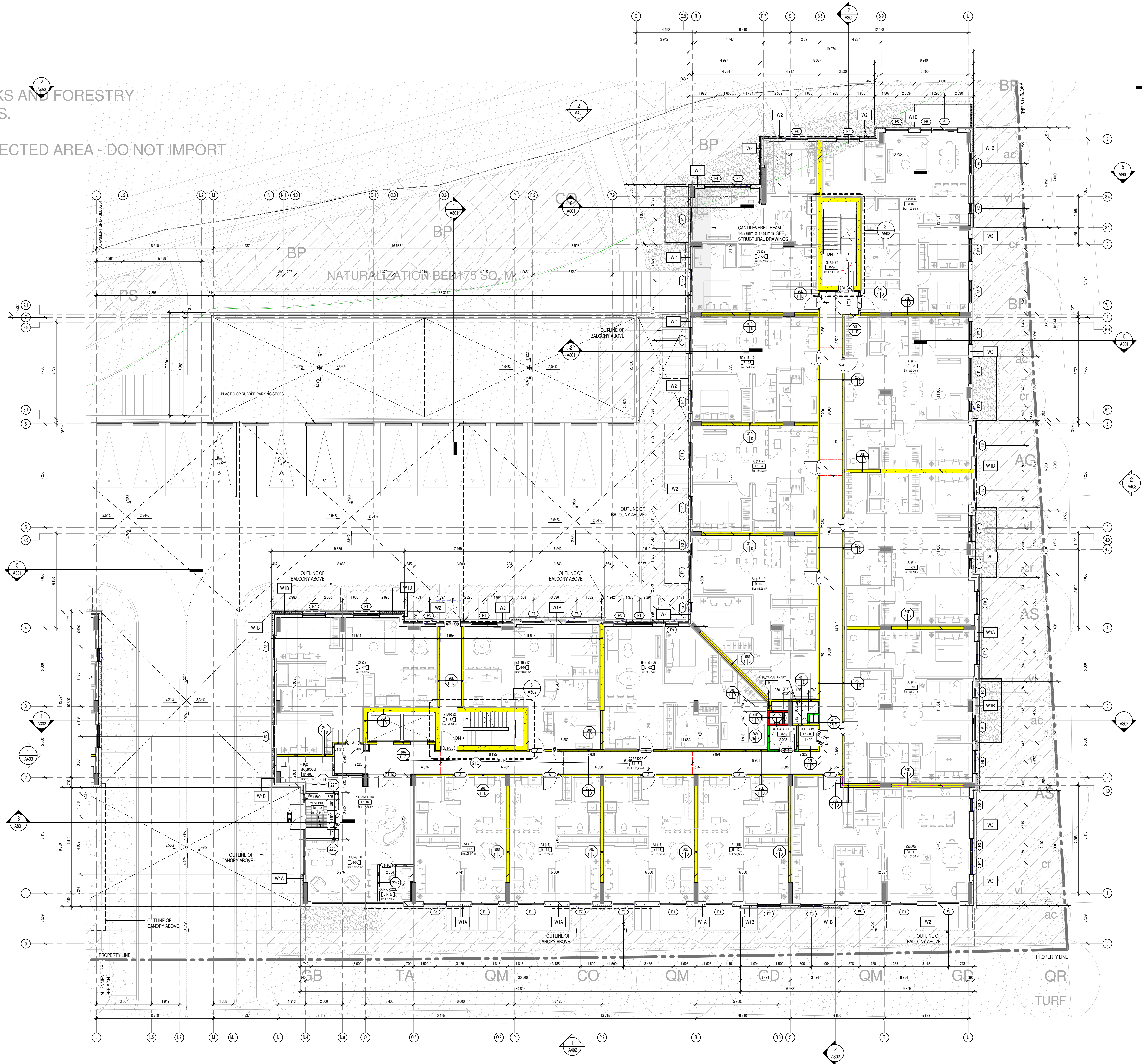
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A204 - BUILDING "A" - GROUND FLOOR
 1:100

ST PATH ON SITE WITH CITY PARKS AND FORESTRY VEGETATION, NATURAL FEATURES.

WOOD, EARTH ETC) FROM PROTECTED AREA - DO NOT IMPORT



A205 - BUILDING "B" - GROUND FLOOR
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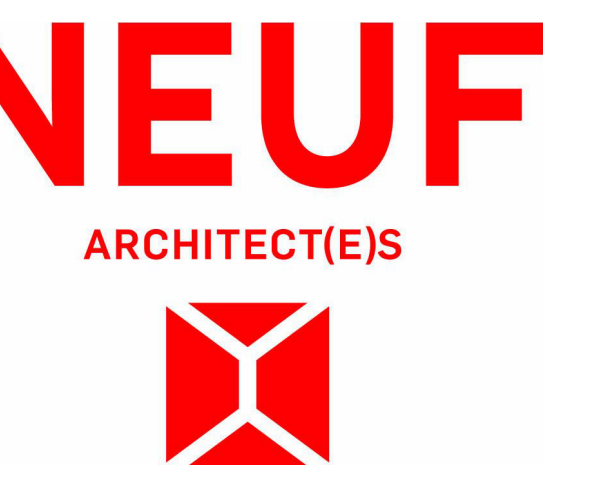
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TITRE DU DESSIN / Drawing Title
BUILDING "B" - GROUND FLOOR

REVISION / Revision
B

NO. DESSIN / Drawing Number
A205

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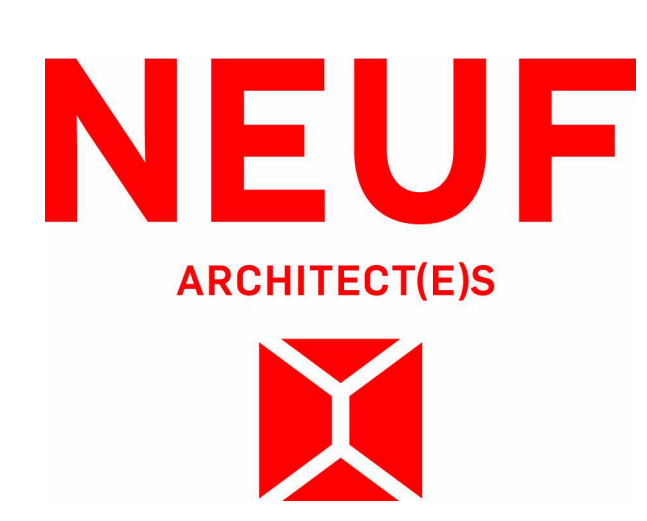
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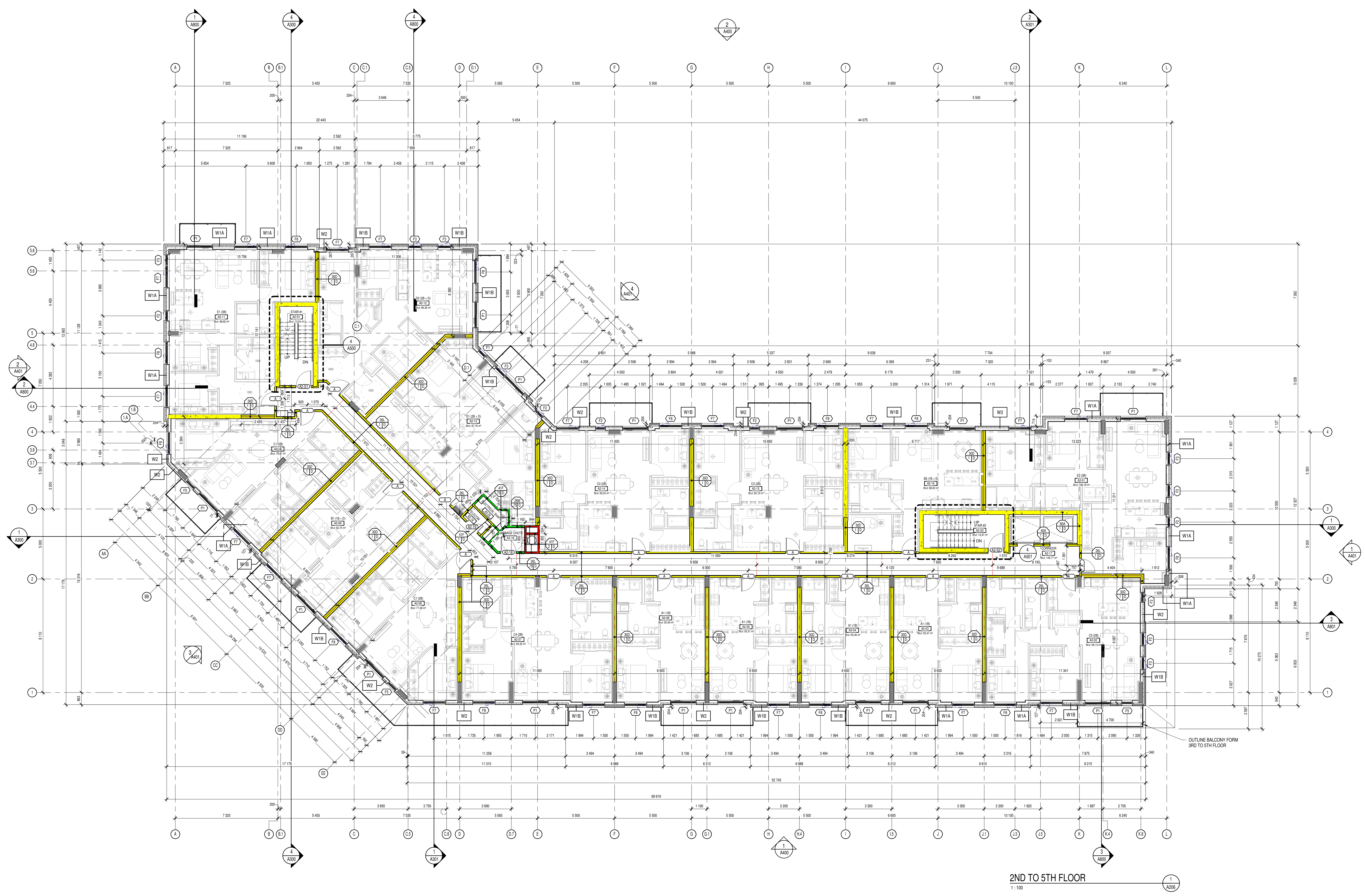
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ÉCHELLE / Scale
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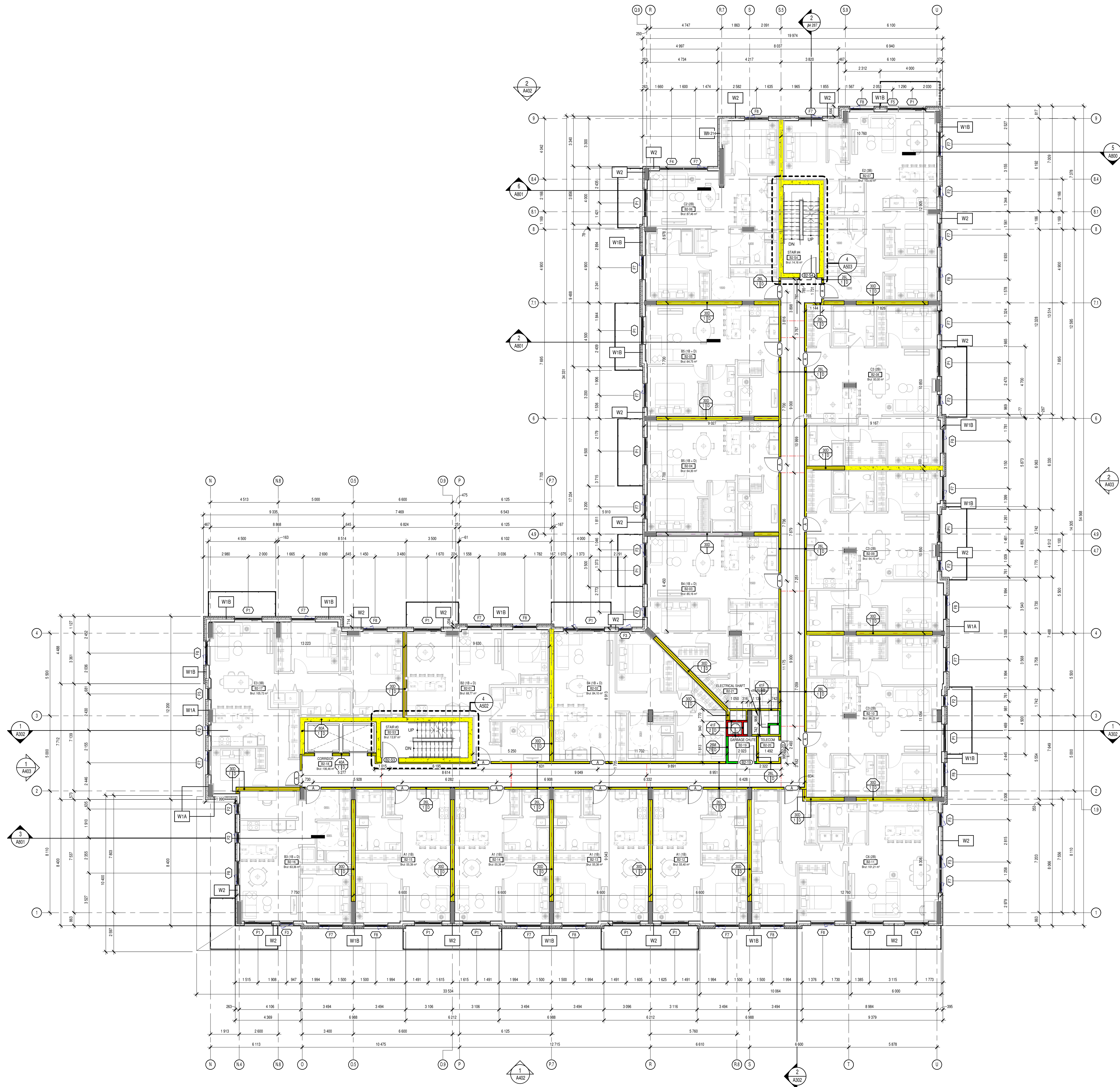
TITRE DU DESSIN / Drawing Title
BUILDING "A" - 2ND TO 5TH FLOOR

REVISION / Revision
B

NO. DESSIN / Day Number
A206



2ND TO 5TH FLOOR
 1:100



2ND TO 5TH FLOOR
1:100

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 T 613 733 5709 foteni.com

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GJA Inc. Gino, J. Aiello
 110 Dabury Road, Unit 9, Ottawa, ON K2T 0C2
 T 613 288 5100 gja.com

INGÉNIEUR TRANSPORT: Transportation Engineering
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 1223 Somerset Street, Suite 100, Ottawa, ON K1J 7T2
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CIVIL: Civil
McINTOSH PERRY
 115 Wellington Road R/R 3 Cars, Ottawa, ON K0A 1L0
 T 613 836 2184 mcintoshperry.com

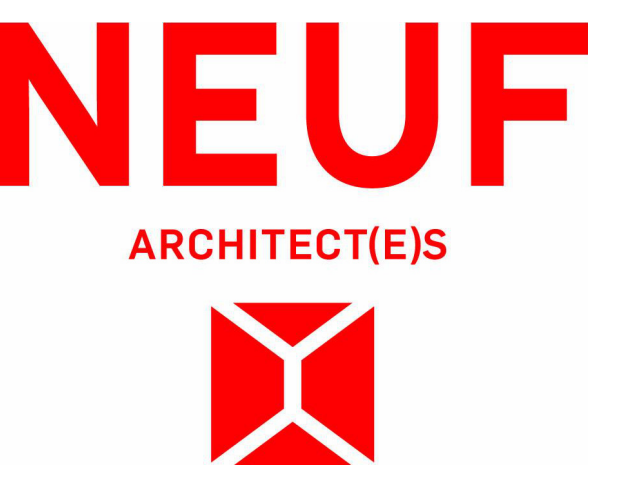
STRUCTURE: Structure
RJC Engineers
 1545 Carling Avenue, Suite 304, Ottawa, ON K1Z 6P9
 T 613 714 7000 rjc.ca

ARCHITECTES: Architect
NEUF architect(e)s
 630 boul. René Lévesque O. 32e étage, Montréal, QC H3B 1S6
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INGÉNIEUR MÉCANIQUE: Mechanical Engineer
BUILDing Consultants
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INGÉNIEUR ÉLECTRIQUE: Electrical Engineer
BUILDing Consultants
 6455 rue Doris-Lussier, Suite 110, Boisbriand, QC J7H 0E9
 T 514 718 6841 building-consultants.ca

SCEAU / Seal



OUVRAGE: Project
788 MARCH ROAD RESIDENTIAL

EMPLACEMENT: Location
OTTAWA, ON

NO PROJET: No.
13109

NO	REVISION	DATE (aa-mm-jj)
B	FOR FOUNDATION PERMIT	2024-04-26

Preliminary
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CONSTRUCTION

DESSINÉ PAR: Drawn by
N.S.H/B.R.

DATE (aa-mm-jj)
2024-06-04

TITRE DU DESSIN: Drawing Title
BUILDING "B" - 2ND TO 5TH FLOOR

VÉRIFIÉ PAR: Checked by
B.R.

ÉCHELLE: Scale
1 : 100

REVISION: Revision
B

NO. DESSIN: Drawing Number
A207

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 T 613 733 5709 fotenn.com

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GJA Inc. Gino, J. Aiello
 110 Dabury Road, Unit 9, Ottawa, ON K2T 0C2
 T 613 288 5100 gja.com

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 1223 Somerset Street, Suite 100, Ottawa, ON K1J 7T2
 T 613 738 4160 parsons.com

CIVIL: Civil
McINTOSH PERRY
 115 Wellington Road R#3 Cals, Ottawa, ON K0A 1L0
 T 613 836 2184 mcintoshperry.com

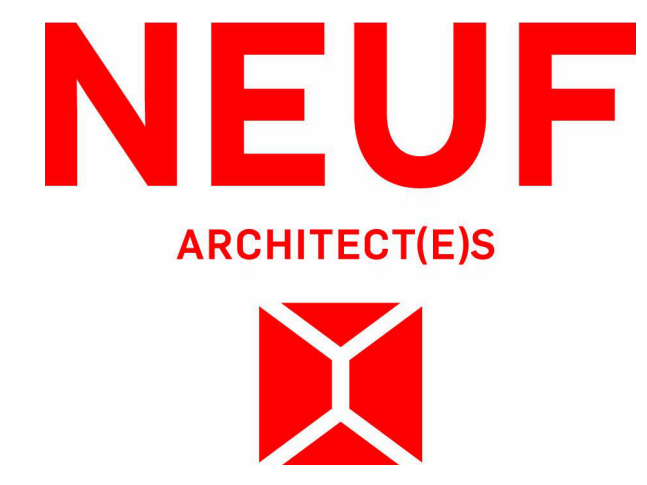
STRUCTURE: Structure
RJC Engineers
 1545 Carling Avenue, Suite 304, Ottawa, ON K1Z 6P9
 T 613 714 7000 rjc.ca

ARCHITECTES: Architect
NEUF architect(e)s
 690 boul. René Lévesque O. 30e étage, Montréal, QC H3B 1B6
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BUILDING CONSULTANTS
 6455 rue Dorval-Leslie, Suite 110, Boisbriand, QC J7H 0E9
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SCEAU / Seal



OUVRAGE: Project
788 MARCH ROAD RESIDENTIAL

EMPLACEMENT: Location
OTTAWA, ON

NO PROJET No.
13109

NO REVISION DATE (aa.mm.jj)
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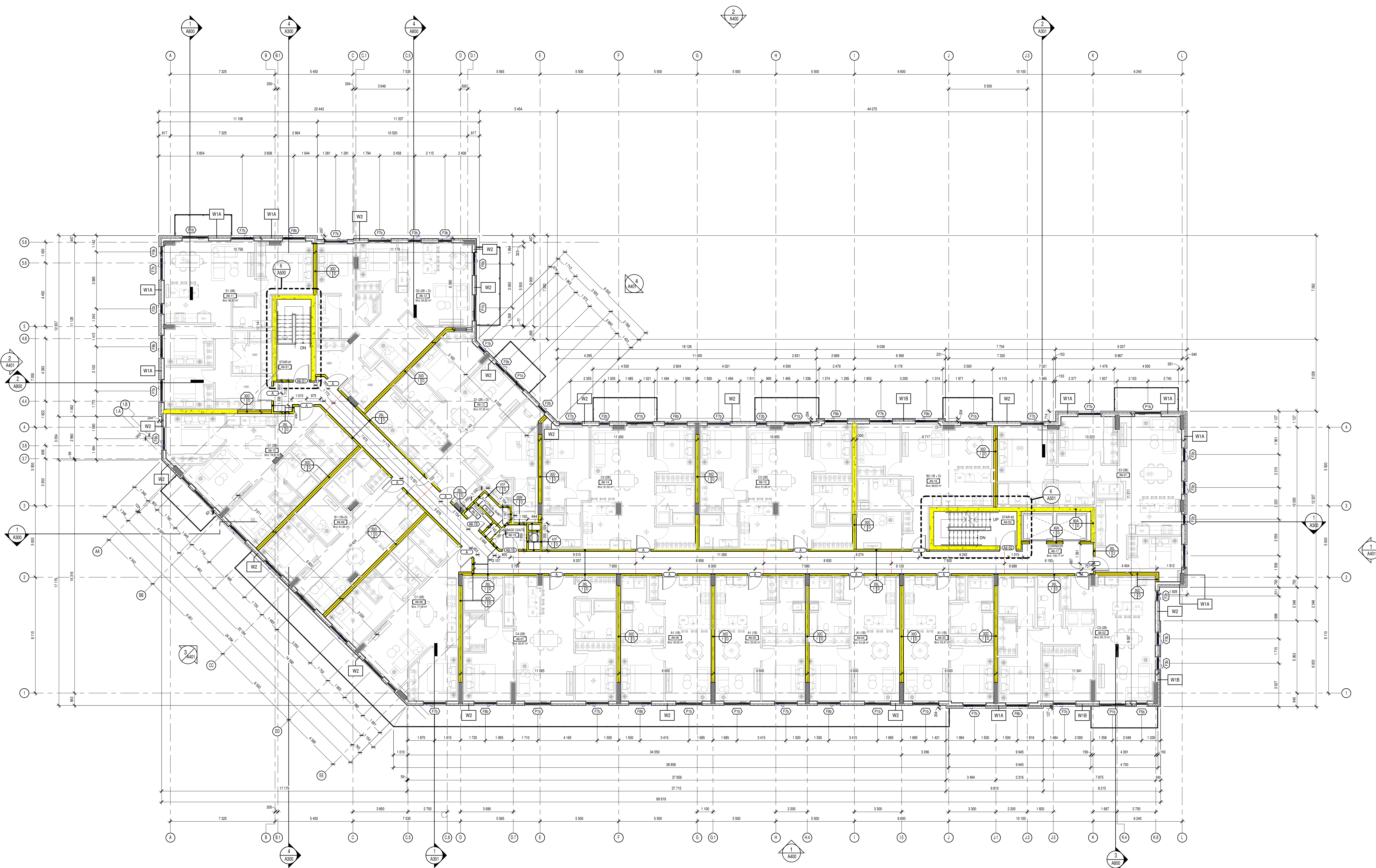
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DATE (aa.mm.jj)
2024-06-04

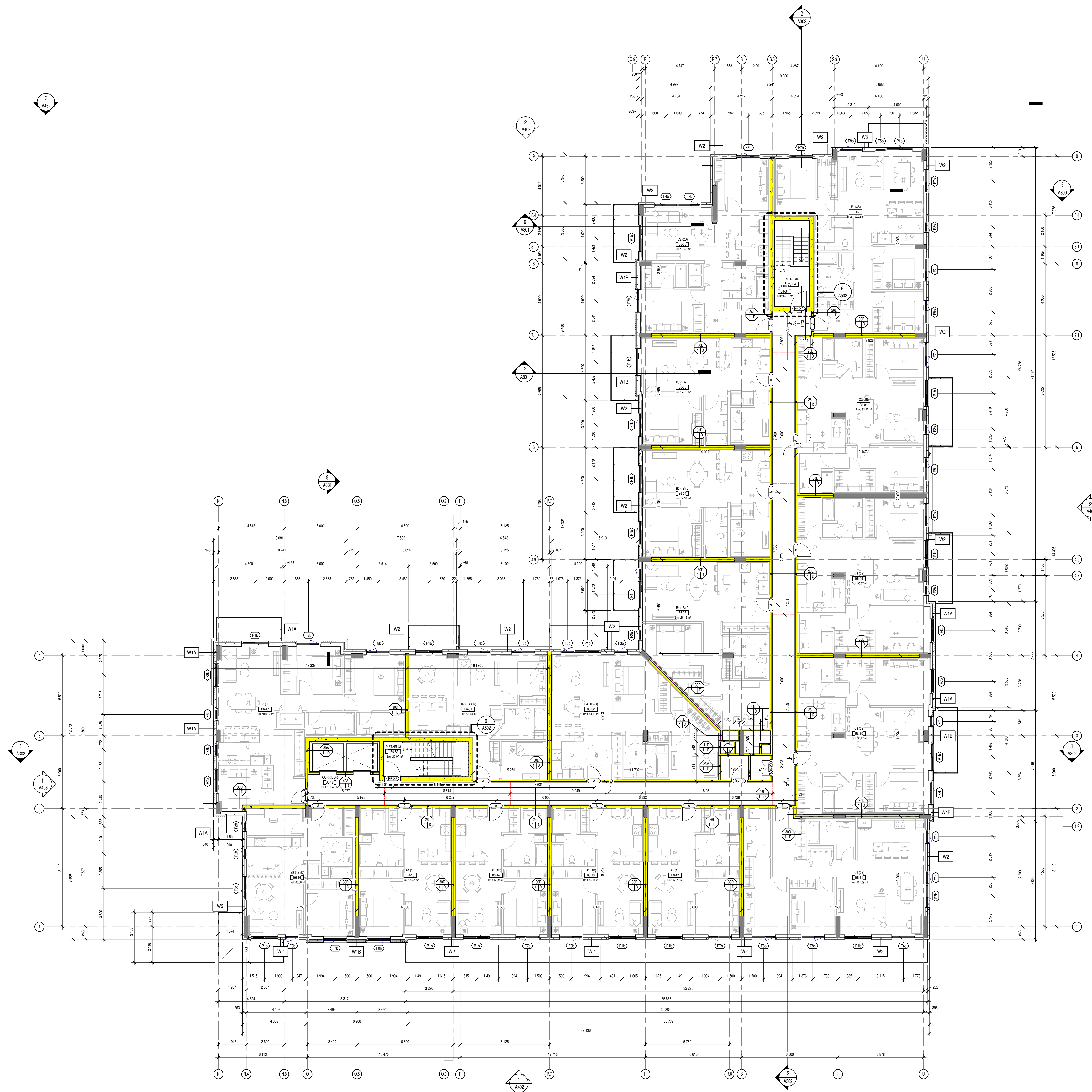
ÉCHELLE: Scale
1 : 100

TITRE DU DESSIN: Drawing Title
BUILDING "A" - 6TH FLOOR

REVISION: Revision NO. DESSIN: Draw Number
B A208



A208 - BUILDING "A" - 6TH FLOOR
 1:100



A209 - BUILDING "B" - 6TH FLOOR
1:100

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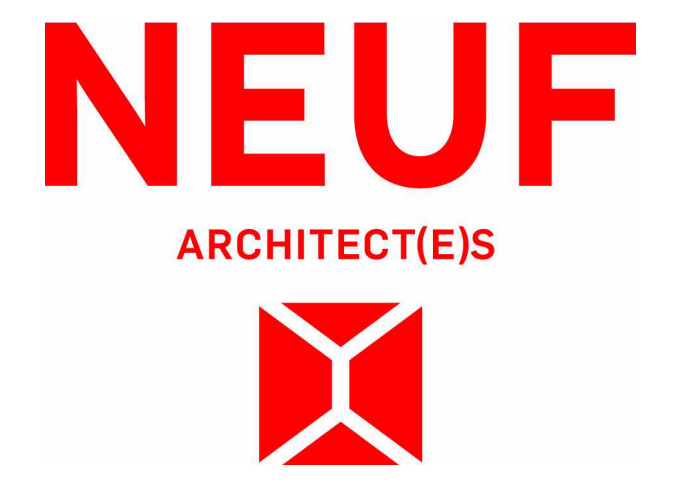
STRUCTURE, Structure
RJC Engineers
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EMPLACEMENT Location
OTTAWA, ON

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TITRE DU DESSIN Drawing Title
BUILDING "B" - 6TH FLOOR

REVISION Revision **NO. DESSIN Drawing Number**
B A209



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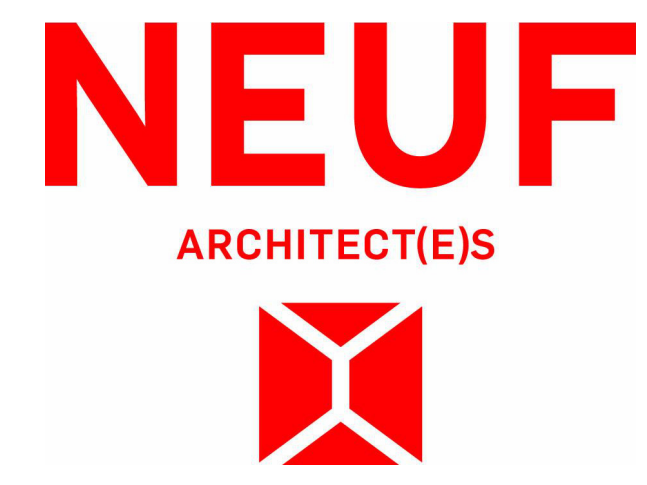
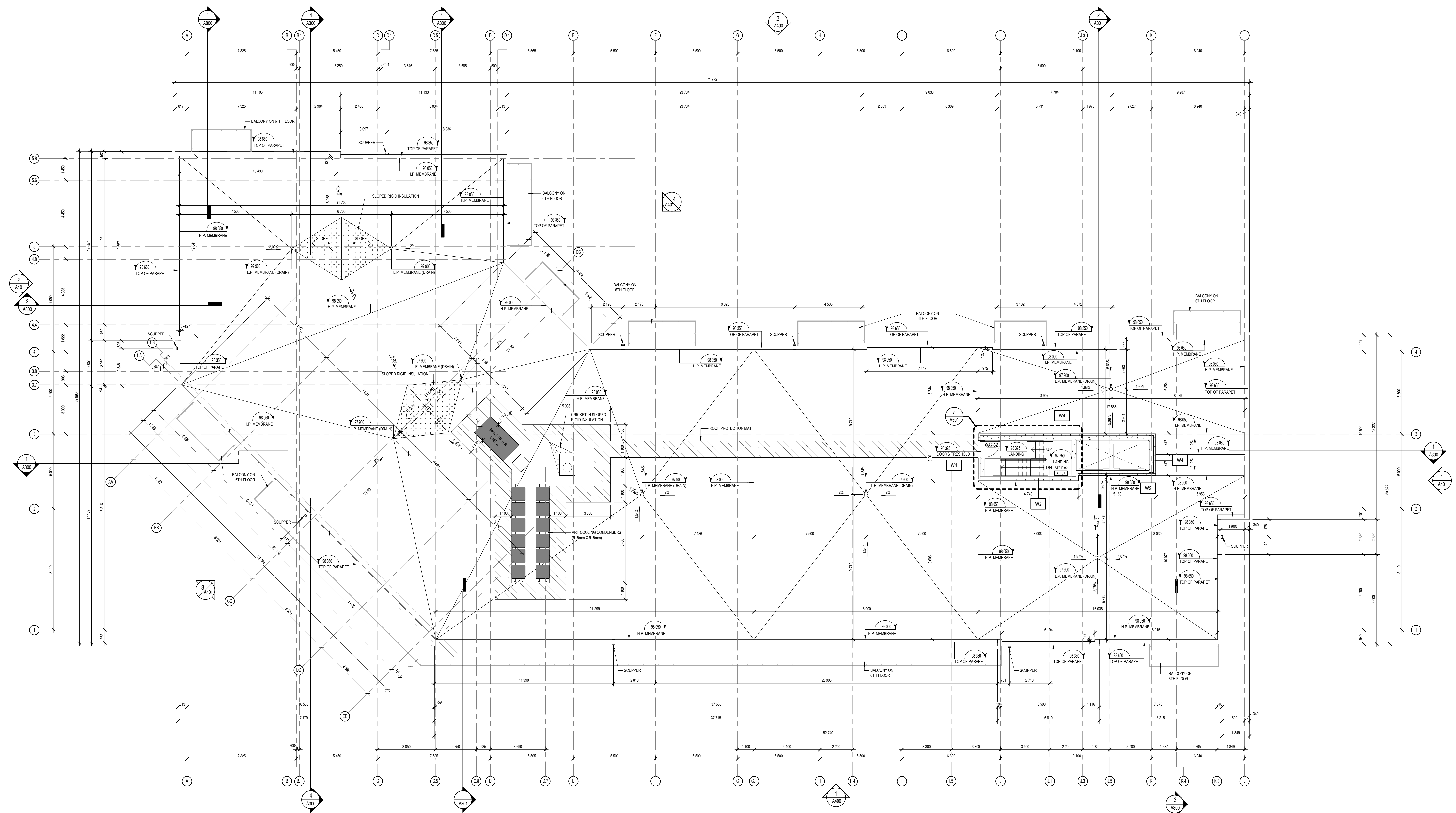
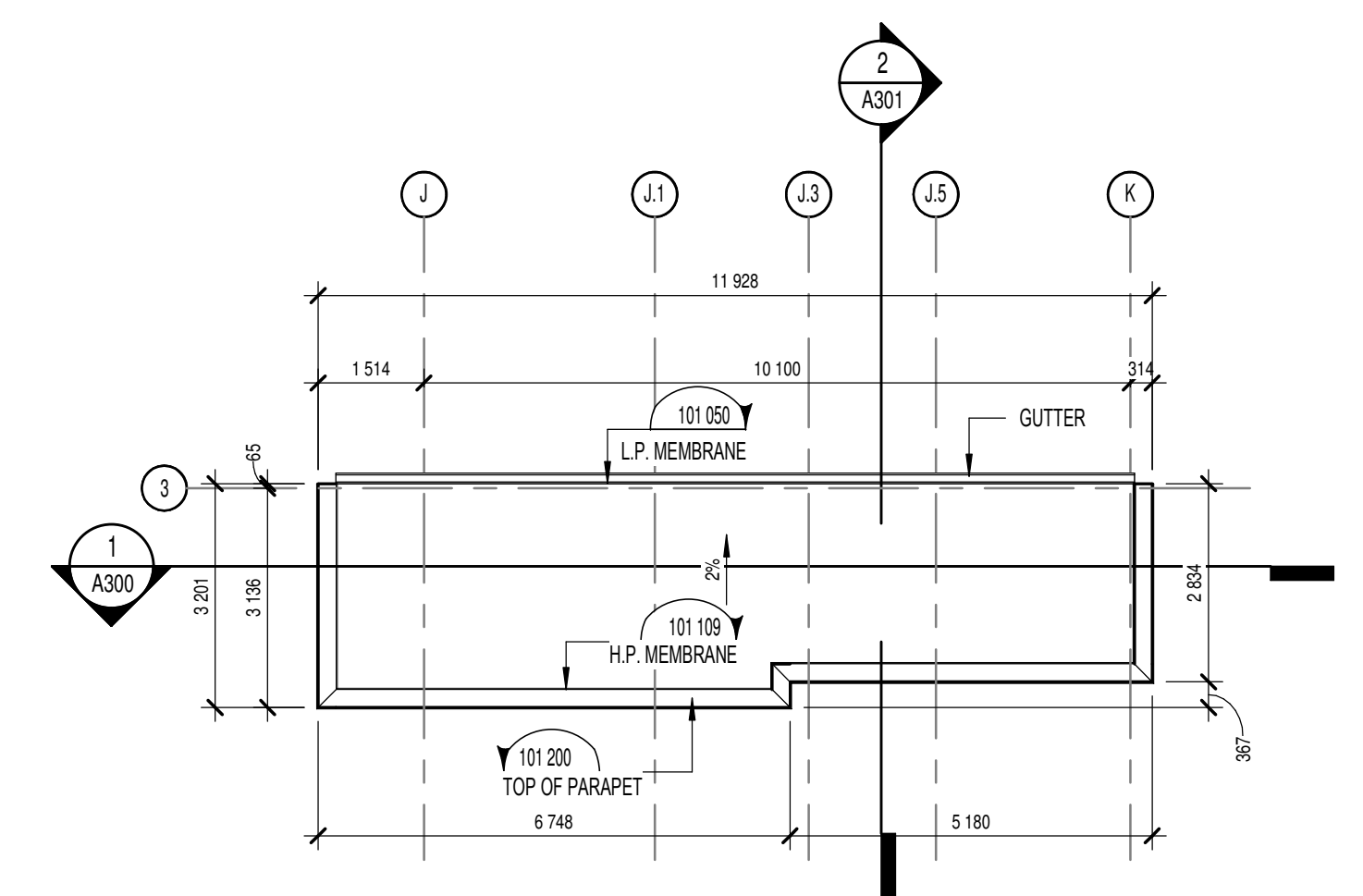
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RJC Engineers
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788 MARCH ROAD RESIDENTIAL

EMPLACEMENT/ Location
OTTAWA, ON

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TITRE DU DESSIN/ Drawing Title
BUILDING "A" - ROOF PLAN

REVISION/ Revision
B

NO. DESSIN/ Draw Number
A210

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 T 613 836 2184 mcintoshperry.com

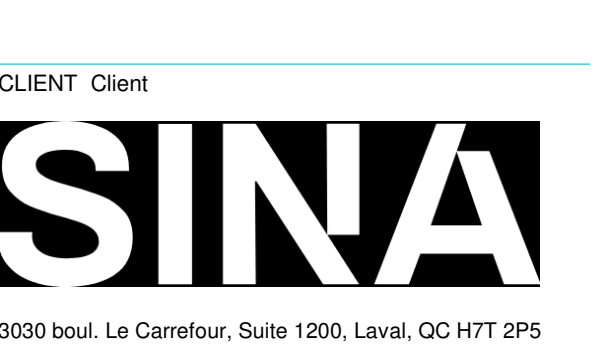
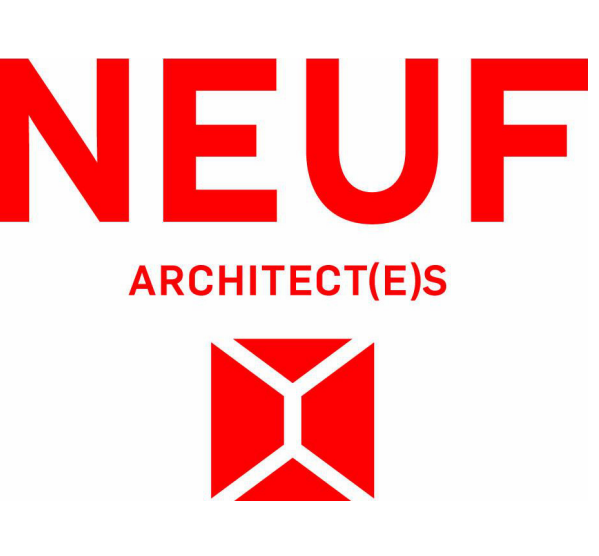
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RJC Engineers
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NEUF architect(e)s
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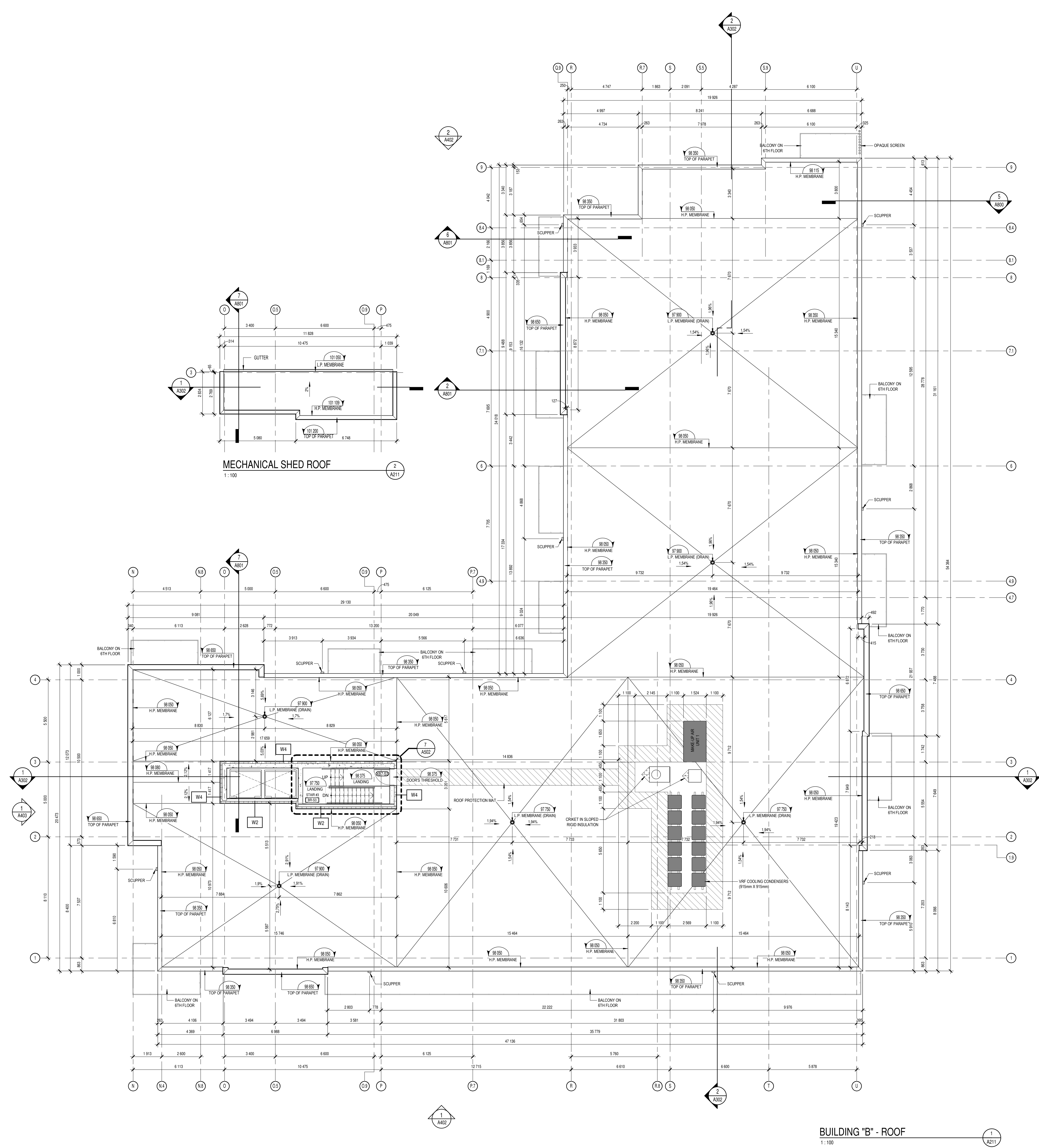
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 2024-06-04 1 : 100

TITRE DU DESSIN Drawing Title
BUILDING "B" - ROOF PLAN

REVISION Revision NO. DESSIN Draw Number
B A211



BUILDING "B" - ROOF
 1 : 100

Appendix G: Shadow Analysis

SUN SHADOW STUDY

788 MARCH ROAD, KANATA, ONTARIO



SI
NA

NEUF
ARCHITECT(E)S

February 19th 2024

SUMMER SOLSTICE
- JUNE -

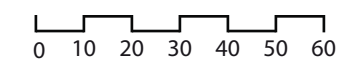
2024-02-19



LEGEND

- PROPERTY LINE
- PROPOSED SHADOW OUTLINE (21.2m)
- AS-OF-RIGHT SHADOW OUTLINE (18m)
- NEW NET SHADOW
- PROPERTY LOT
- PROPOSED BUILDING
- AS-OF-RIGHT BUILDING
7.5m Rear Yard Setback
18m Height
1.8m Private Way Setback

SCALE



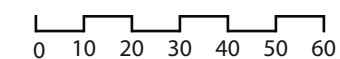
2024-02-19



LEGEND

- - - - - PROPERTY LINE
- PROPOSED SHADOW OUTLINE (21.2m)
- AS-OF-RIGHT SHADOW OUTLINE (18m)
- █ NEW NET SHADOW
- █ PROPERTY LOT
- █ PROPOSED BUILDING
- ▨ AS-OF-RIGHT BUILDING
7.5m Rear Yard Setback
18m Height
1.8m Private Way Setback

SCALE



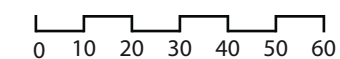
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LEGEND

- PROPERTY LINE
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- AS-OF-RIGHT SHADOW OUTLINE (18m)
- NEW NET SHADOW
- PROPERTY LOT
- PROPOSED BUILDING
- AS-OF-RIGHT BUILDING
7.5m Rear Yard Setback
18m Height
1.8m Private Way Setback

SCALE



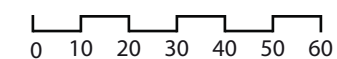
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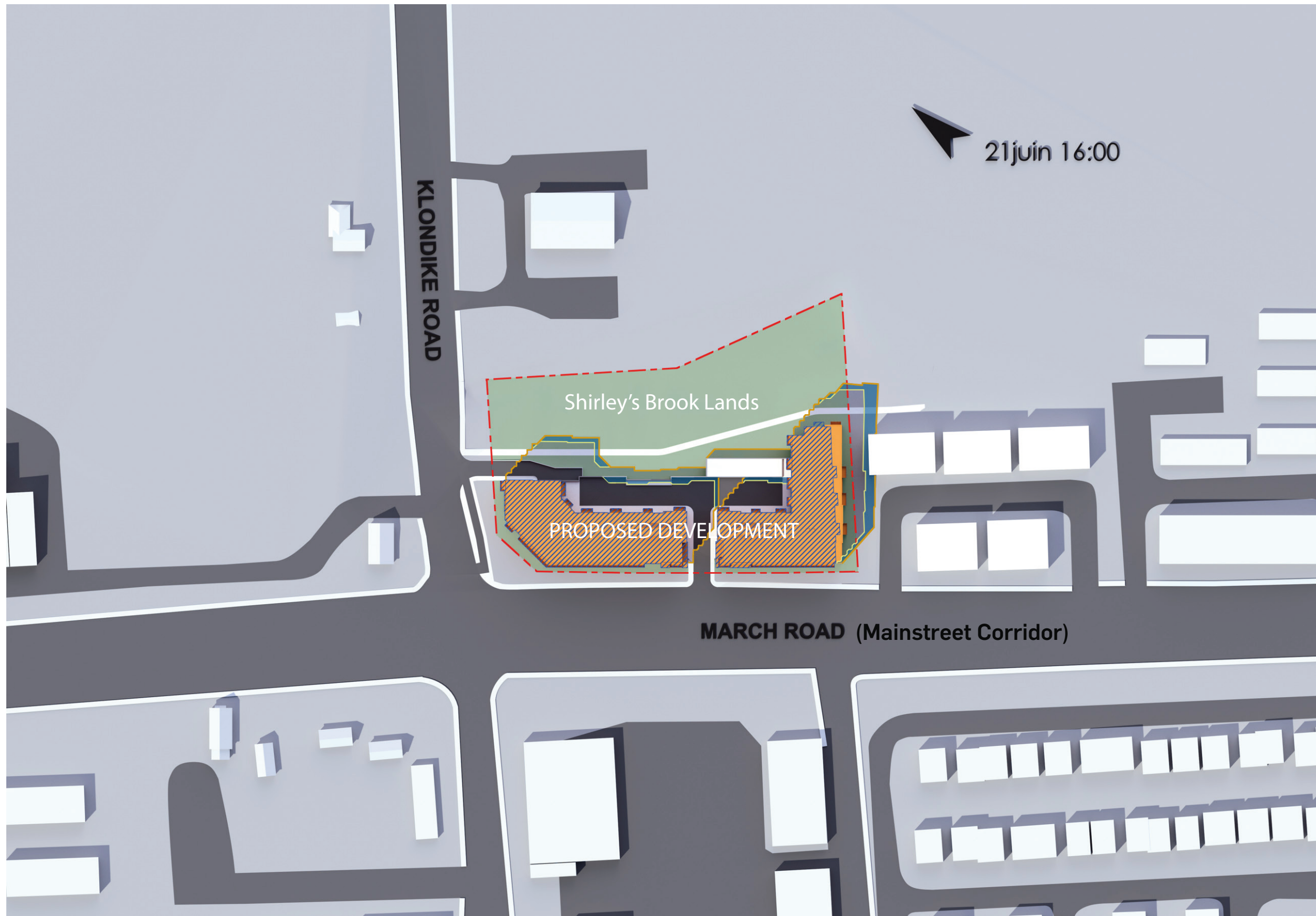


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- PROPERTY LINE
- PROPOSED SHADOW OUTLINE (21.2m)
- AS-OF-RIGHT SHADOW OUTLINE (18m)
- NEW NET SHADOW
- PROPERTY LOT
- PROPOSED BUILDING
- AS-OF-RIGHT BUILDING
*7.5m Rear Yard Setback
18m Height
1.8m Private Way Setback*

SCALE

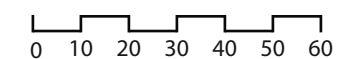




LEGEND

- PROPERTY LINE
- PROPOSED SHADOW OUTLINE (21.2m)
- AS-OF-RIGHT SHADOW OUTLINE (18m)
- NEW NET SHADOW
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18m Height
1.8m Private Way Setback

SCALE

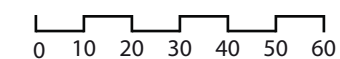




LEGEND

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- PROPOSED SHADOW OUTLINE (21.2m)
- AS-OF-RIGHT SHADOW OUTLINE (18m)
- NEW NET SHADOW
- PROPERTY LOT
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- AS-OF-RIGHT BUILDING
7.5m Rear Yard Setback
18m Height
1.8m Private Way Setback

SCALE

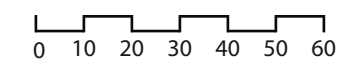




LEGEND

- PROPERTY LINE
- PROPOSED SHADOW OUTLINE (21.2m)
- AS-OF-RIGHT SHADOW OUTLINE (18m)
- NEW NET SHADOW
- PROPERTY LOT
- PROPOSED BUILDING
- AS-OF-RIGHT BUILDING
7.5m Rear Yard Setback
18m Height
1.8m Private Way Setback

SCALE



AUTUMN EQUINOX
- **SEPTEMBER** -

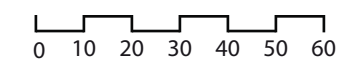
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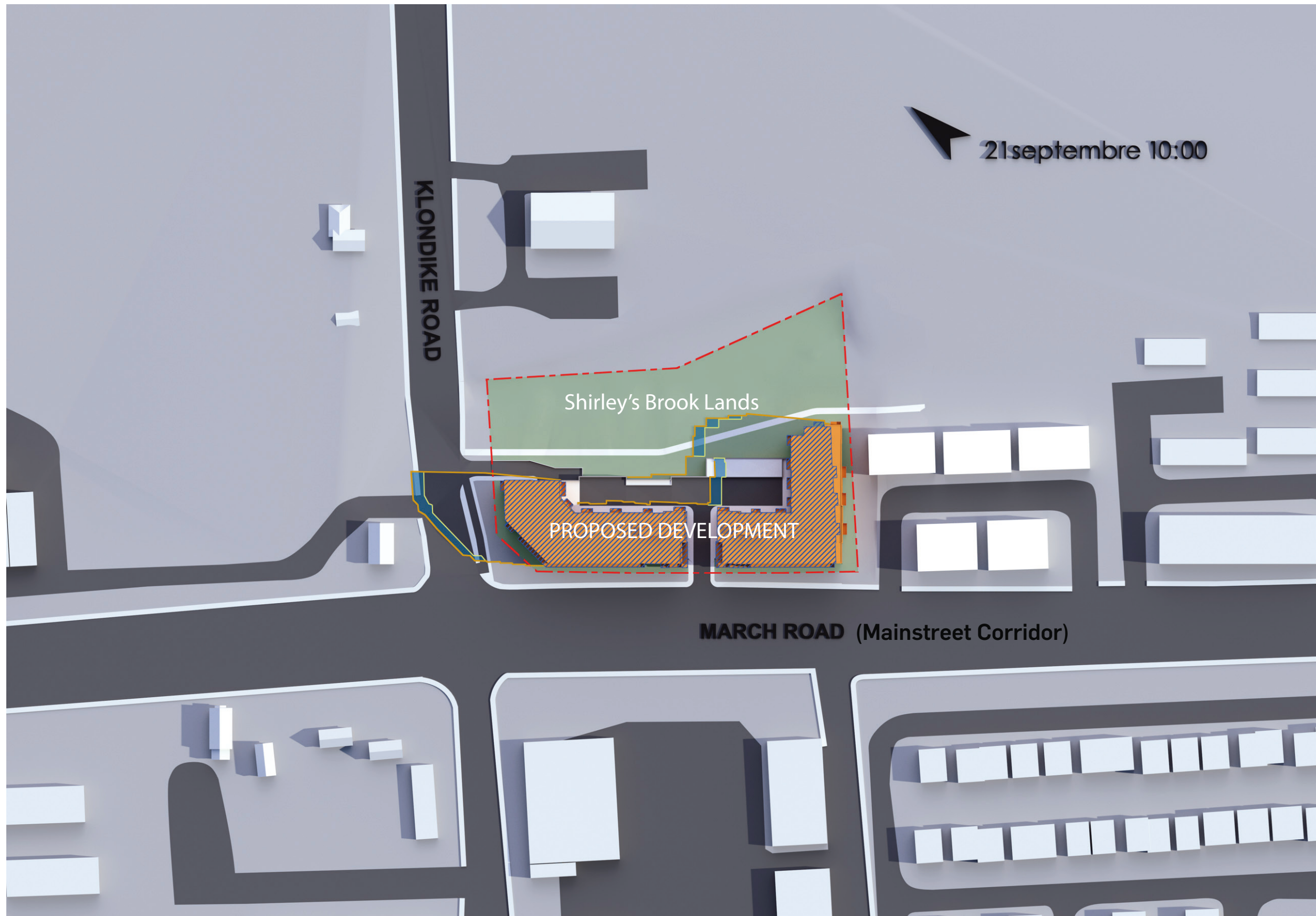
LEGEND

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- AS-OF-RIGHT SHADOW OUTLINE (18m)
- NEW NET SHADOW
- PROPERTY LOT
- PROPOSED BUILDING
- AS-OF-RIGHT BUILDING
7.5m Rear Yard Setback
18m Height
1.8m Private Way Setback

SCALE



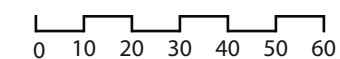
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- AS-OF-RIGHT SHADOW OUTLINE (18m)
- NEW NET SHADOW
- PROPERTY LOT
- PROPOSED BUILDING
- AS-OF-RIGHT BUILDING
7.5m Rear Yard Setback
18m Height
1.8m Private Way Setback

SCALE



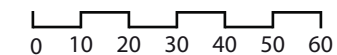
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LEGEND

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- AS-OF-RIGHT SHADOW OUTLINE (18m)
- NEW NET SHADOW
- PROPERTY LOT
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- AS-OF-RIGHT BUILDING
7.5m Rear Yard Setback
18m Height
1.8m Private Way Setback

SCALE



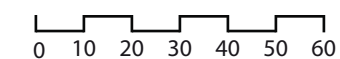
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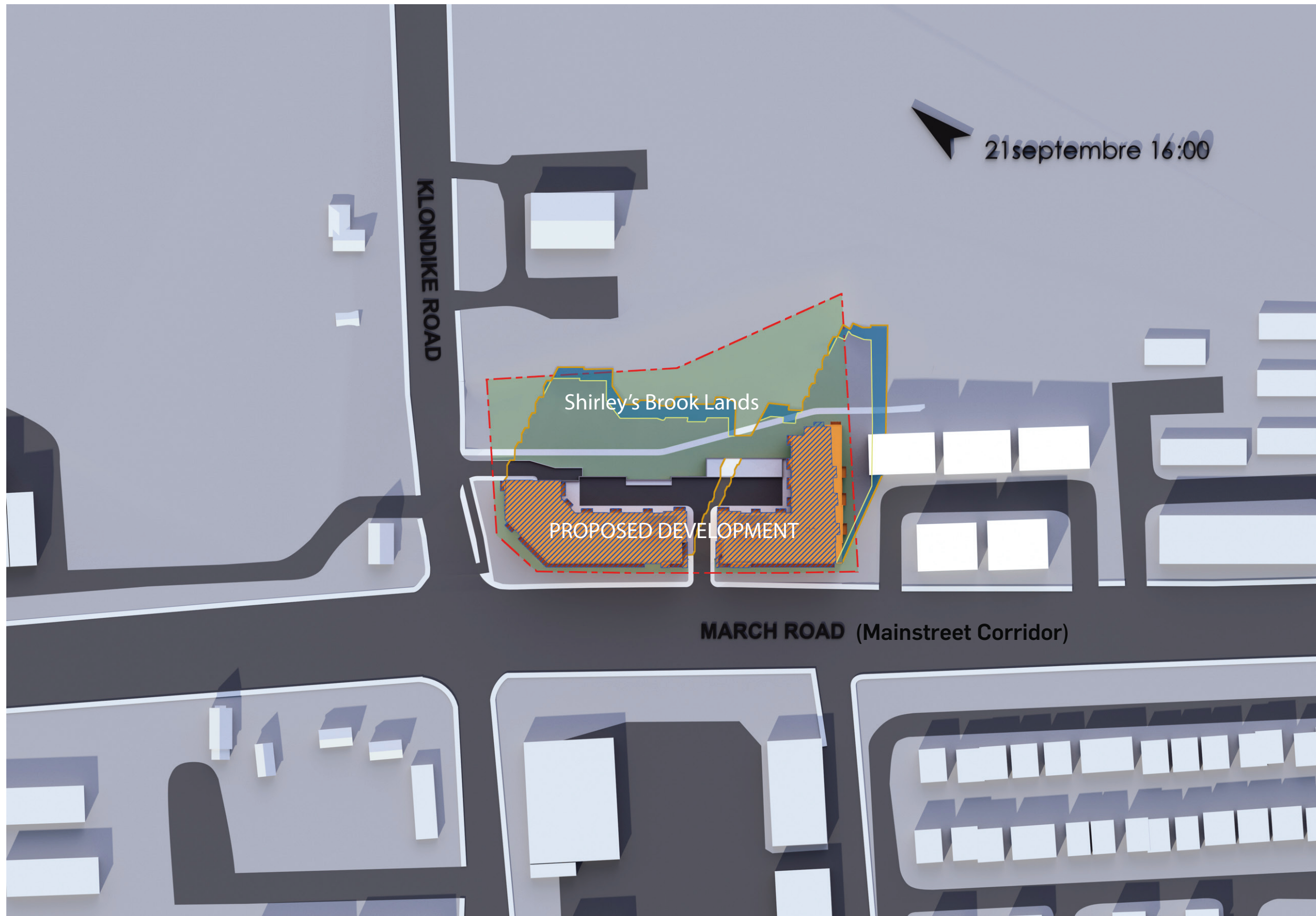
LEGEND

- PROPERTY LINE
- PROPOSED SHADOW OUTLINE (21.2m)
- AS-OF-RIGHT SHADOW OUTLINE (18m)
- NEW NET SHADOW
- PROPERTY LOT
- PROPOSED BUILDING
- AS-OF-RIGHT BUILDING
7.5m Rear Yard Setback
18m Height
1.8m Private Way Setback

SCALE



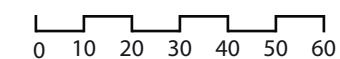
2024-02-19



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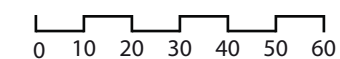
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2024-02-19

WINTER SOLSTICE
- **DECEMBER** -

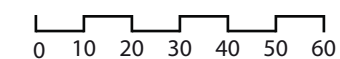
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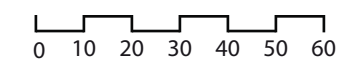




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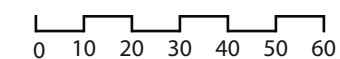
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