



Design Brief

Wateridge Village – Block 5

NCCA22-0243-05

Rohit Communities

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1 DEVELOPMENT OVERVIEW

The development application being submitted for 1255 Hemlock Road is for Site Plan Control Approval. The subject property is located within the Wateridge Village (Phase 4) neighbourhood in Ward 13 Rideau-Rockcliffe and the Ottawa Greenbelt. Located east of Downtown Ottawa, the redevelopment project by Canada Lands Corporation occupies the former Canadian Forces Base (CFB) Rockcliffe lands and is largely characterized by new residential and mixed-use development with a variety of zoning designations.

The proposed development is the construction of one four-storey low-rise residential apartment building, consisting of 89 units, and an accessory utility installation structure to serve the property. The unit mix is a varied collection of 1 bedroom, 2 bedroom, and 3 bedroom units. 89 resident parking stalls are provided below grade, with 8 short-term or visitor parking stalls provided at grade via private approach at the site's northwest edge off Kijigong Street. A combination of 35 secure bicycle parking spaces located in the underground parking area and 14 exterior bicycle parking spaces make up the total provided bicycle spaces. Building A on site has a Gross Floor Area of approximately 6,886 square metres.

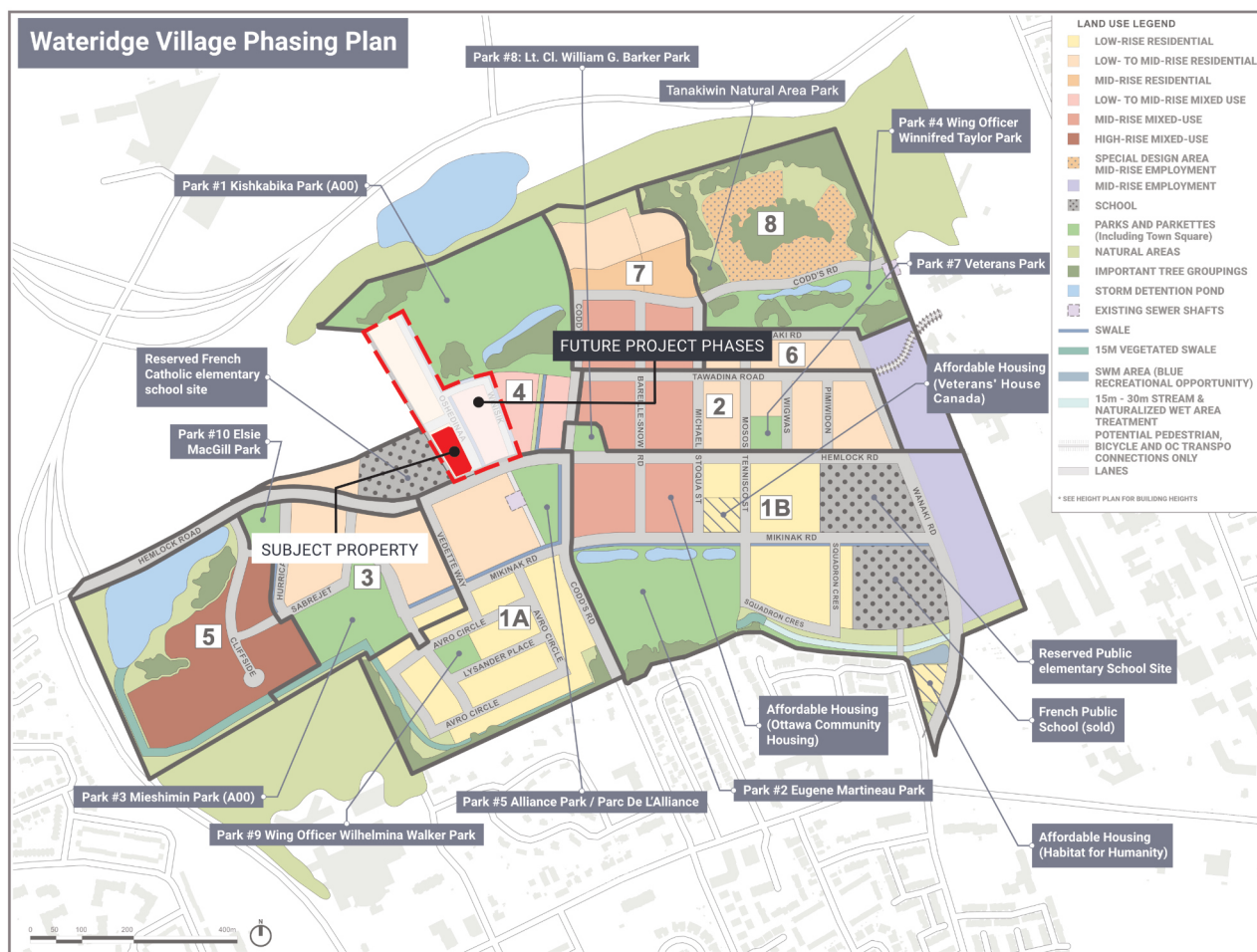


Figure 1.0 – Subject property location on Wateridge Village Phasing Plan (September 2023).

1.1 Site and Policy

The subject property is a vacant corner lot situated on the north side of Hemlock Road at the intersection with Oshedinaa Street, having approximately 34 metres and 87 metres of frontage along Hemlock Road and Oshedinaa Street respectively. The lot has a total area of about 4,174 square metres and is bounded by Kijigong Street to the north and by a Phase 3 site reserved for a French Catholic school (see Figure 1.0). Block 5 is zoned R5Y[2312] – Residential Fifth Density Zone, Subzone Y, Urban Exception 2312 in the City of Ottawa Zoning By-law 2008-250. The overall design of the building meets all applicable setback and yard areas. The site is not yet cleared but has no notable landform features or vegetative cover.



Figure 2.0 – Subject property and surrounding zoning designations (November 2023).

The proposed development supports the overall goals of the Former CFB Rockcliffe Secondary Plan, a document that guides the future development on these lands. The guiding vision of the Former CFB Rockcliffe Community Design Plan (2015) the document which forms the basis of the policies implemented by the Secondary Plan, is that the community will be walkable, cycling-supportive, transit-oriented and built at a human scale. The proposed development incorporates an increased supply of bicycle parking spaces, street-oriented units that allow for increased mobility and interaction with the public realm, and the property is located along Hemlock Road which acts as a collector road for the community, leading to a current bus route to the east. Human scale design is met through the low-rise block character, which transitions between the adjacent low-density neighbourhoods surrounding Wateridge Village and the future higher density context of the community, while also allowing for greater access to light and views within the Wateridge Village neighbourhood and towards the escarpment leading down to the Ottawa River. Based on these criteria, the design for Block 5 embraces the implementing goals of the Former CFB Rockcliffe Secondary Plan.

Additionally, the Wateridge Village Urban Design Guidelines and Architectural Controls provide specificity surrounding the means by which to achieve the policies contained in the Community Design Plan and the Secondary Plan. For low- to mid-rise residential, the following general design guidelines were given particular attention:

- Long frontages should be varied through the use of windows, different materials, variable façade and setbacks, colours, or architectural features such as bays or porches.

Though the frontage along Oshedinaa Street is significant, we have taken care to provide considerable attention to the volumetric play and vertical emphasis to reduce the impact of a long frontage. Multiple types of balconies, large quantities of windows, and the undulating façade along the length of Oshedinaa Street help to break up the length and allow for variety in the 4-storey building mass.

- Large horizontal expanses of windows and upper story balconies are encouraged in units facing parks and streets to promote casual surveillance.

The design for the subject property incorporates plenty of windows and large balconies on all four building levels along both Hemlock Road and Oshedinaa Street to provide for a sense of casual surveillance and security along the public realm.

Architectural controls specific to low-rise apartment buildings addressed through this design include:

- The design of the building should consider the overall form and rhythm of building elements to create a consistent and attractive building street façade that reinforces a human scale environment.

Harmony has been created in the building design through the use of varying element styles (i.e. more than one window treatment, more than one balcony design, more than one cladding material, etc.) that are implemented using a system of rules, as well as purposeful exceptions to the rules to add visual interest and create multiple distinct facades across the building.

- Building facades should provide visual interest through use of materials, colours, sophisticated wall articulation, windows and doors and style-appropriate architectural detailing.

A sophisticated palette of classic materials compliments the more traditional nods of the proposed architectural style, while streamlined primary building entrances and unexpected details bring in a contemporary nod, firmly placing the project both within its traditional inspirations and its modern architectural context.

- Main entrances should be designed as a focal point of the building and should face the street. They should be recessed or covered and provide visibility to interior lobbies to allow for safe and convenient arrival and departure from the buildings.

The front entrance is located along the active Hemlock Road corridor in a gesture that extends the full height of the building to signal its presence along the active street. The entrance is covered and presents itself prominently, providing visibility to the front lobby and an obvious place of arrival and departure for the site.

- Residential apartments are encouraged to include covered private open space (i.e. balconies/terraces) where feasible to enhance the private living environment of residents.

The vast majority of units include private amenity space in the form of patios and balconies, of which all are covered with the exception of those located on the fourth level.

- Parking should be provided in a nonobtrusive manner. Surface parking areas should be screened from street view through the use of landscaping or building location.

Onsite surface parking is provided off the rear yard, sheltered from view by landscaping elements and the building when traveling along Oshedinaa Street.

1.2 Context

Block 5 maintains a lower density residential character at the edge of the Wateridge Village development. Significant parks have been planned northeast of the site (Kishkabika Park) and already constructed to the southeast (Alliance Park). The lot owner has also secured Block 4 to the east (General Mixed Use zoning) and Block 6 to the north, allowing for a largely unified design between the three contiguous blocks. As a collector road within the core of the community, Hemlock Road increases in activity level east of Block 5, with an increased proportion of commercial and retail services within this zone. Much development east of Codd's Road has already begun, with several multi-family residential buildings under construction, while many lower-density dwellings have reached occupancy. The development of Block 5 will add further definition to the activity along Hemlock Road and will fill out the western edge of the Wateridge community.



Figure 3.0 – Existing conditions key plan for views shown in Figures 4.0 and 5.0 (July 2023).



Figure 4.0 – View from intersection of Hemlock Road and Oshedinaa Street, looking northwest into the site (May 2023).



Figure 5.0 – View along Hemlock Road, looking northwest along Block 5 south property line (May 2023).



Figure 6.0 – Subject property and surrounding key destinations.

1.3 Site Connectivity

The Wateridge Village community is characterized by new residential construction in a variety of types and densities. Key destinations within a close range include lands owned by the National Capital Commission slated for a future cultural institution and the Canada Aviation & Space Museum to the northwest, the National Research Council Campus to the east, Montreal Road as a major arterial and commercial network to the south, and Montfort Hospital to the southwest. The site is close in proximity to the Ottawa River and future park space northeast of the site.

Given the site is part of a greenfield development following the closure of the CFB Rockcliffe base, roadways, sidewalks and other urban features are still under development. With that in mind, Hemlock Road has seen a great deal of construction activity, and the development of Block 5 will build off this activity to pull development north towards the future Kishkabila Park. New sidewalks and building accesses will be constructed along Hemlock Road, Oshedinaa and Kijigong Streets, as well as connecting the proposed internal roadway and sidewalks to Kijigong Street at the north site boundary. Additionally, pedestrian connections to Blocks 4 and 6 have been planned in conjunction with their ongoing site design to provide easy flow between the three blocks.

The site is well-located along vehicular networks, with Hemlock serving as a primary artery in and out of the Wateridge community. With access to Montreal Road via Codd's Road and direct future access to Aviation Parkway planned from Hemlock Road, vehicular traffic to and from the subject property will be easy and efficient. The proposed development is also well-located within the City public transportation network, with higher frequency routes along Montreal Road to the south and near-immediate access to OC Transpo Route 27 to the east at Wanaki Road. As the Wateridge Village community continues to develop, it is further anticipated that public transportation services will increase in the area.

2 URBAN DESIGN OBJECTIVES

2.1 Pre-Application Consultation Urban Design Issues

The following are responses to urban design issues identified for Block 5 during the Pre-Application Consultation meeting held on March 20, 2023:

- Below grade parking ramp should not conflict with the public realm. Consider shifting it so that it is more internal to the block.

The revised site design takes this concern into consideration – we have altered the location of the underground parking ramp access so as to limit its impact on the public realm. We have also simplified and straightened out the internal access road to reduce the amount of asphalt on site.

- Consider nighttime light spill with current parking configuration.

In order to reduce nighttime light spill into rear-facing units, we have relocated the surface parking stalls to the outermost edge of the site, facing in the opposite direction. We have also increased the buffer zone between the internal road and the building to allow for greater distance from the parking area, and to provide more landscaping to soften the impact of both light and noise due to vehicular traffic.

- Please provide grade-related units along Oshedinaa Street.

We have further refined our site design to include at-grade entrances to units along Oshedinaa Street, providing for increased porch culture, a sense of security, and greater interaction between these residential units and the public realm along this important community roadway.

- Ensure that the landscape plan provides sufficient planting along the public realm and within the courtyard.

As demonstrated in the landscape plan, our adjusted design provides increased and diverse planting in both type and scale, both along Oshedinaa Street to soften the space between the building and the public realm, and within the site interior to create a sense of privacy from the rear-facing units and the internal road.

2.2 Pre-Application Consultation Urban Design Objectives

The following are site-specific Urban Design Objectives derived from the Pre-Application Consultation with City staff, addressing the comprehensive plan for Blocks 5, 6 and 4:

- Fostering a sense of neighbourhood character through the use of grade-oriented suites with usable porches and balconies for increased interaction with the public realm, creating a lively residential quality.
- Providing greater articulation of the massing along main roadways to impart pleasant variation and depth to building façades.

- Coordinating and consolidating internal roadways across the overall development to simplify intersections; aligning features for visual clarity within the urban environment; and reducing the amount of asphalt to allow for as much soft landscaping and open outdoor space as is feasible.
- Harmonious landscaping strategies across the entire development to contribute to the Oshedinaa corridor leading to the future Kishkabika Park.
- Locating surface parking area and internal roadways away from main roadways to reduce the impact of such features on the public realm.

3 DESIGN PROPOSAL

3.1 Massing and Scale

The basic massing of the proposed development is that of an L-shaped building positioned prominently at the southeast corner of the lot. Though the building is lengthy, the massing is broken up by the following moves:

- The 3:1 ratio of materiality that differentiates the bottom three levels from the top level, with key locations breaking through this division to provide further articulation and visual relief,
- The variety of treatments for differing balcony conditions at corners, at cantilevered balconies, and at grouped balconies,
- The undulating articulation of walls that balances horizontality and verticality, and
- Depth created by both the recessed balconies and the dark material language provided at select locations across the entire building.



Figures 7.0 and 7.1 – Looking northeast and northwest towards the site along Hemlock Road (August 2024).



Figure 7.2 – Looking northwest along Oshedinaa Street (August 2024).



Figures 7.3 and 7.4 – Looking southwest along Oshedinaa Street and south from Kijigong Street (August 2024).



Figure 7.4 – Looking southeast along west property line (August 2024).

The building is also inherently human scale as it is only four storeys and less than 16m in height. The proposed development acts as a transition between the lower scale reserved French Catholic school site to the west (still zoned as low-density R5Y residential at the time of submission) and the rising density along Hemlock to the east of the site. Sites to the north and south of the proposed development are designated as low-density residential, with the addition of a park to the southeast. In general, the proposed development meets the requirements of its place within the overall Wateridge Village community, providing gentle density and appropriate scale.

The proposed development is sensitive to the existing grades proposed along the edge of the site, with the exception of the west side that shares a lot line with the reserved school site. Along this edge, the design proposes to gently raise the grades to achieve better landscaping and interaction between the two lots. Otherwise, the existing grades on site have been captured to provide ideal design conditions such as a slightly raised ground floor level to increase privacy of at-grade units and favourable buffer conditions between building and public realm.

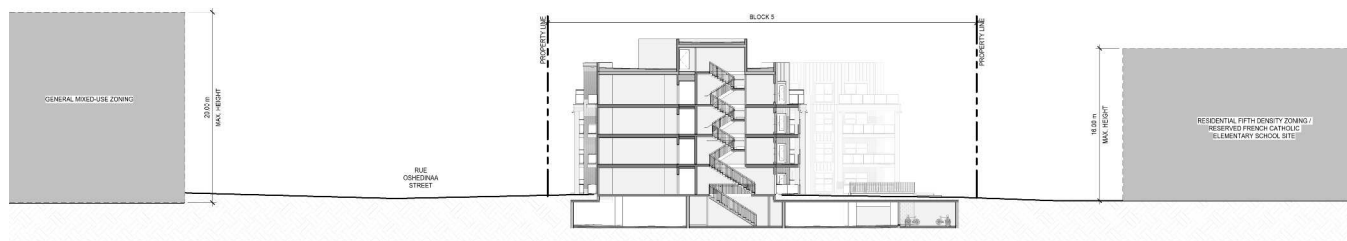


Figure 8.0 – Cross section showing transitions between subject property and adjacent uses (August 2024).

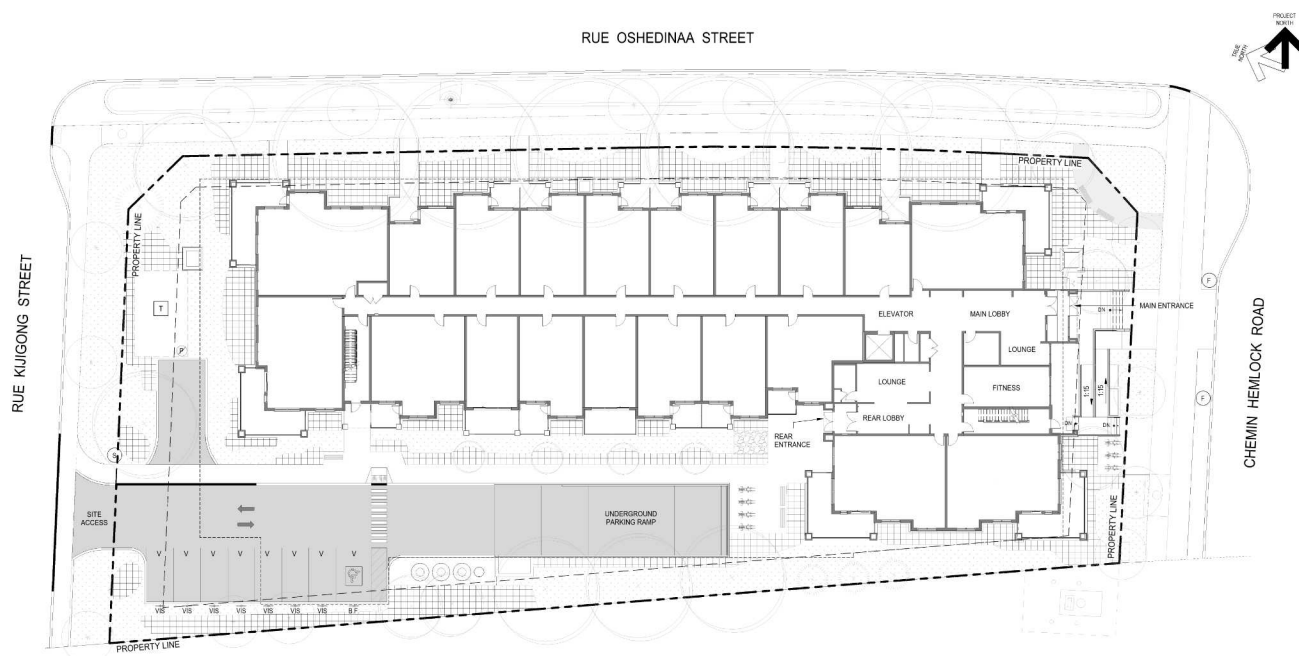


Figure 9.0 – Ground floor plan demonstrating relationship of units and common areas to public realm (August 2024).

3.2 Public Realm

The proposed development has been carefully placed at the intersection of the two most prominent edges of the block to provide maximum interaction between the building and the public realm. Animation at the street level is achieved by the placement and frequency of private patios and balconies along Oshedinaa Street and by providing large amounts of glazing along Oshedinaa Street and especially at Hemlock Road to increase interaction between the building activity and the public realm while adding to a sense of casual surveillance. Though this will likely be the first block under construction in the immediate vicinity, the proposed design anticipates activity focused on the intersection of Hemlock Road and Oshedinaa Street. Architectural responses to this reality include the building's placement weighted to this corner, a landscaped public plaza at this intersection, street-oriented units facing Oshedinaa Street, and the placement of service site features away from public view to promote more attractive spaces.

The landscaping strategy marries the language of plantings and features proposed for the overall development with those that complement the design of the building. Several types of deciduous trees and shrubs have been carefully selected to provide year-round interest and variation in terms of species, size, colour, flowers and fruit. The design emphasizes the residential nature of the project by acting as a transition between public and private space, while balancing scale so as to not impede views to and from the site. Landscaping features provide moments of pause and relaxation (as in the landscaped plaza at the corner of Hemlock and Oshedinaa). Landscaping is also working to screen unsightly but necessary site elements from view as much as possible while still allowing easy access to these elements (such as utility equipment, surface parking areas and waste pickup areas).

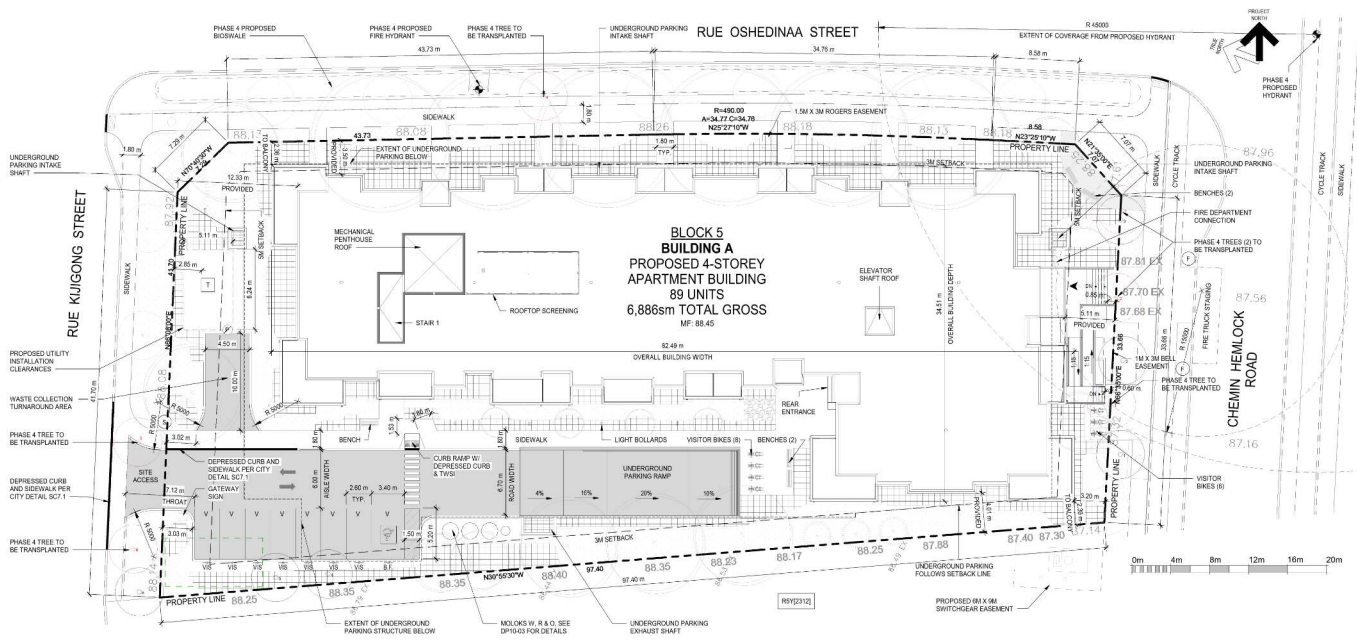


Figure 10.0 – Site plan from roof level (August 2024).

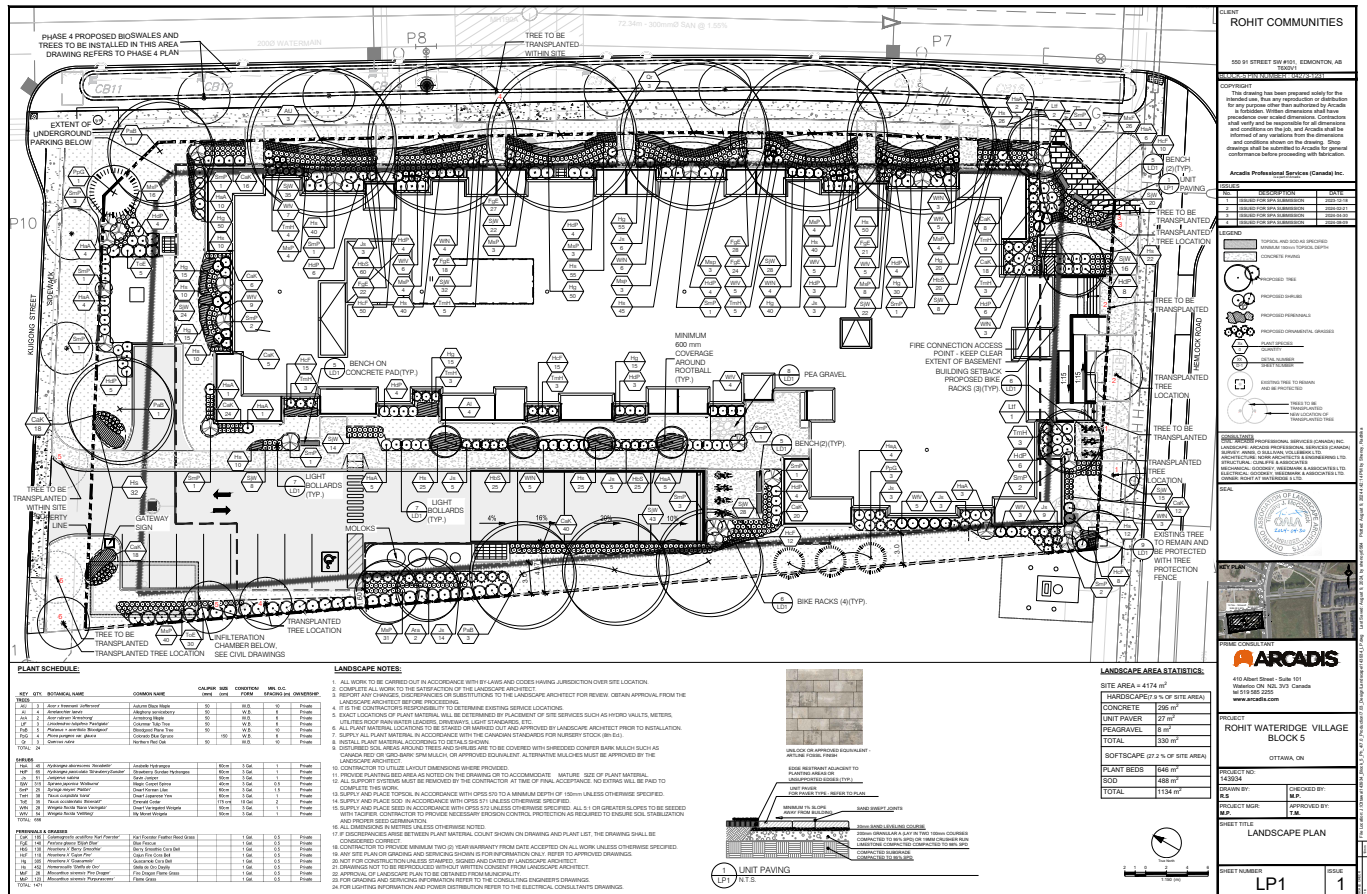


Figure 11.0 – Landscape plan, courtesy of Arcadis (August 2024).

3.3 Building Design

The building design is grounded in the balance between traditional and contemporary features. High quality, resilient materials such as brick masonry and standing seam metal form the basis of the materiality and goes hand-in-hand with the familiar language of traditional architectural styles. The Ottawa context is welcoming to more traditional styles, which led to the selection of timeless characteristics including the faux-Mansard roof and architectural details like cornices and brick column bases at corner balconies.

However, it is also important for architecture to reflect the time in which it was built, and so contemporary play was incorporated to distinguish the building from historical styles. Sleek and simplified trim, railings and profiles provide nods to contemporary design, while the inclusion of bronze-tinted glass at balcony railings provides further departure from historical references. The classic colour scheme and materials reflect a refined palette that is applied in modern ways (refer to Figures 12.0 through 12.4 for graphic representation of these strategies). The overall design is a study of contrast and the dichotomy of the familiar traditional and the current contemporary direction.



Figure 12.0 – South elevation (August 2024).



Figure 12.1 – East elevation (August 2024).



Figure 12v.2 – North elevation (August 2024).



Figure 12.3 – North elevation/section showing rear entry condition (August 2024).



Figure 12.4 – West elevation (August 2024).

3.4 Sustainability

A comprehensive approach regarding sustainability has been adopted for this project. Firstly, the integration of high-efficiency mechanical equipment ensures optimal energy utilization, promoting an eco-friendlier operation of the building. Complementing this, the use of LED light fixtures not only enhances energy efficiency but also aligns with the client’s commitment to sustainable lighting practices. To address water conservation, low flow plumbing fixtures will be selected for use throughout the building. Additionally, the design embraces Low Impact Development (LID) requirements, implementing strategies to manage stormwater runoff and bolster the site’s environmental resilience.

Approximately 10% of the parking spaces will feature electric vehicle (EV) chargers, supporting green transportation initiatives and encouraging the use of electric vehicles. Further, the project will also provide more than the minimum required number of bicycle parking stalls to promote bike usage. Considering the environmentally friendly nature of wood construction, the project harnesses the benefits of sustainable building materials. Wood, as a renewable resource, contributes to lower carbon footprints compared to traditional construction materials, thereby enhancing the overall sustainability profile of the project.

4 RENDERED VIEWS



Figure 13.0 – Looking northwest towards the site at the corner of Hemlock Road and Oshedinaa Street (March 2024).



Figure 13.1 – Looking west towards the site along Oshedinaa Street (March 2024).



Figure 13.2 – Looking southwest towards the site along Oshedinaa Street (March 2024).



Figure 13.3 – Looking southeast towards Block 5 from the northwest corner of the site (March 2024).



Figure 13.4 – Looking at the south elevation and main entry from across Hemlock Road (March 2024).