

SINGLE CROSSWALK INTERSECTION  
Mineral Street at LANE 1  
Les Emmerson Drive (N) at LANE 1  
SCALE 1:400

*G. Wildman*

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DEPARTMENT, CITY OF OTTAWA

APPROVED  
By Geraldine Wildman at 2:43 pm, Jan 09, 2026

## NOT FOR CONSTRUCTION

### TOPOGRAPHIC INFORMATION

TOPOGRAPHIC INFORMATION PROVIDED BY J.D. BARNES LIMITED,  
PROJECT No. 16-10-127-00, SURVEY DATED APRIL 10, 2018.

### LEGAL INFORMATION

SITE CONCEPT PLAN PROVIDED BY Q4A ARCHITECTS, PROJECT No. 24006-SPI,  
RECEIVED ON NOVEMBER 28, 2024.

### ELEVATION NOTE

ELEVATIONS SHOWN ON THIS PLAN ARE RELATED TO GEODETIC DATUM  
AND ARE REFERRED TO THE PUBLISHED BENCH MARK No. 0011964U3710. ELEVATION=71.724m

9	W.L.	25-10-10	REVISED BLOCK 8 HOUSE CONNECTIONS
8	W.L.	25-09-02	REVISED PER ADS CHAMBER DESIGN
7	W.L.	25-08-15	4th SUBMISSION
6	W.L.	25-08-06	REVISED BLOCK 10 GRADING
5	W.L.	25-07-23	3rd SUBMISSION
4	W.L.	25-06-25	REVISED PER SWM DESIGN
3	W.L.	25-05-16	REVISED PER SWM DESIGN
2	W.L.	25-01-17	2nd SUBMISSION
1	X.W.	24-08-30	1st SUBMISSION
No.	BY	DATE	DESCRIPTION




CITY OF  
OTTAWA



PROJECT No. 24-1398

BARRHAVEN  
CONSERVANCY  
DEVELOPMENT  
CORPORATION

BCDC EAST STACKED  
CONDO SITE PLAN

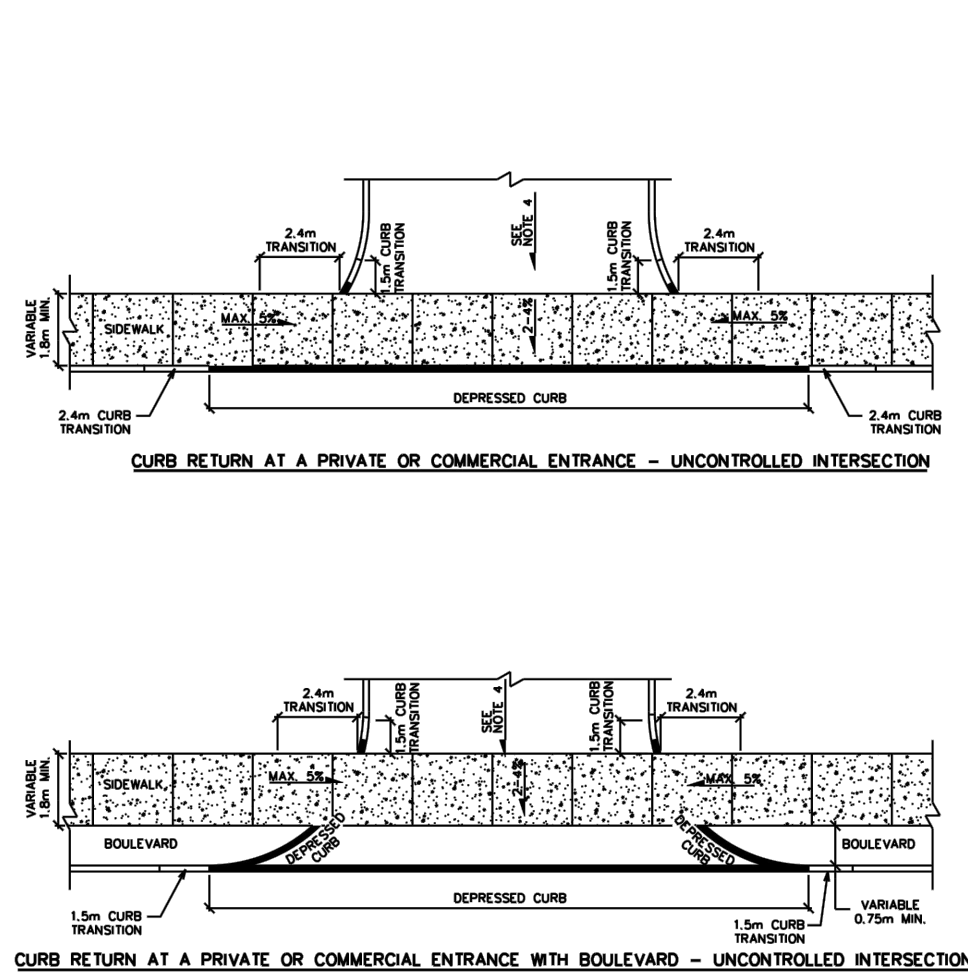


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TWSI DETAILS

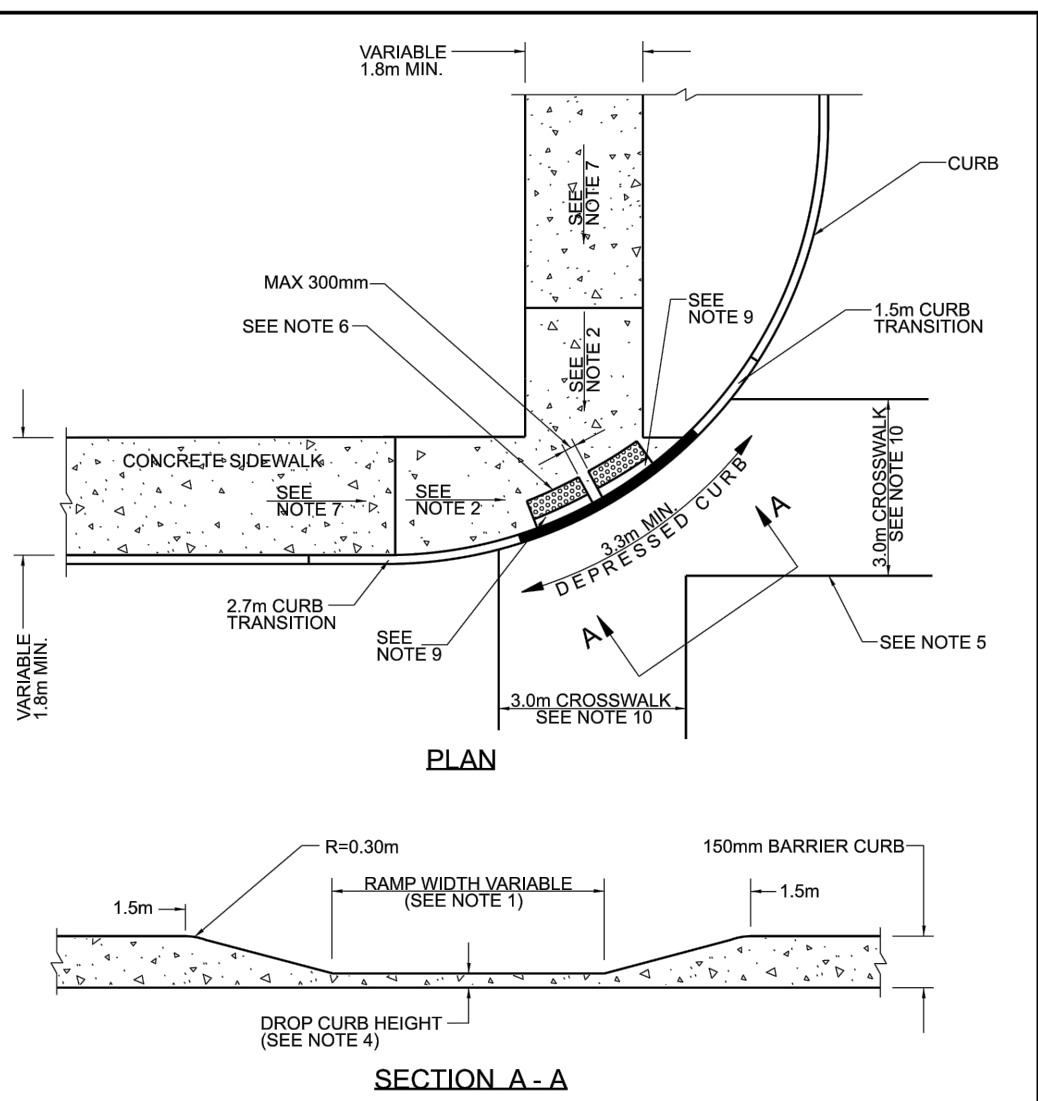
DRAWN BY: M.S.	CHECKED BY: W.L.	SHEET No.
DESIGNED BY: W.L.	CHECKED BY: C.M.	4 OF 15
SCALE: 1:500	DATE: AUGUST 2024	



- NOTES:
- ALL DIMENSIONS ARE IN MILLIMETRES UNLESS SHOWN OTHERWISE.
  - CURB DETAILS SEE SC1.1, SC1.2 AND SC1.3.
  - SIDEWALK DETAILS SEE SC2 AND SC3.
  - MAXIMUM SLOPE VARIES, SEE PRIVATE APPROACH BYLAW.
  - UNCONTROLLED INTERSECTION MEANS AN ENTRANCE NOT LOCATED AT A TRAFFIC SIGNAL OR ALL-WAY STOP CONTROL.

 CURB RETURN ENTRANCES -  
UNCONTROLLED INTERSECTIONS

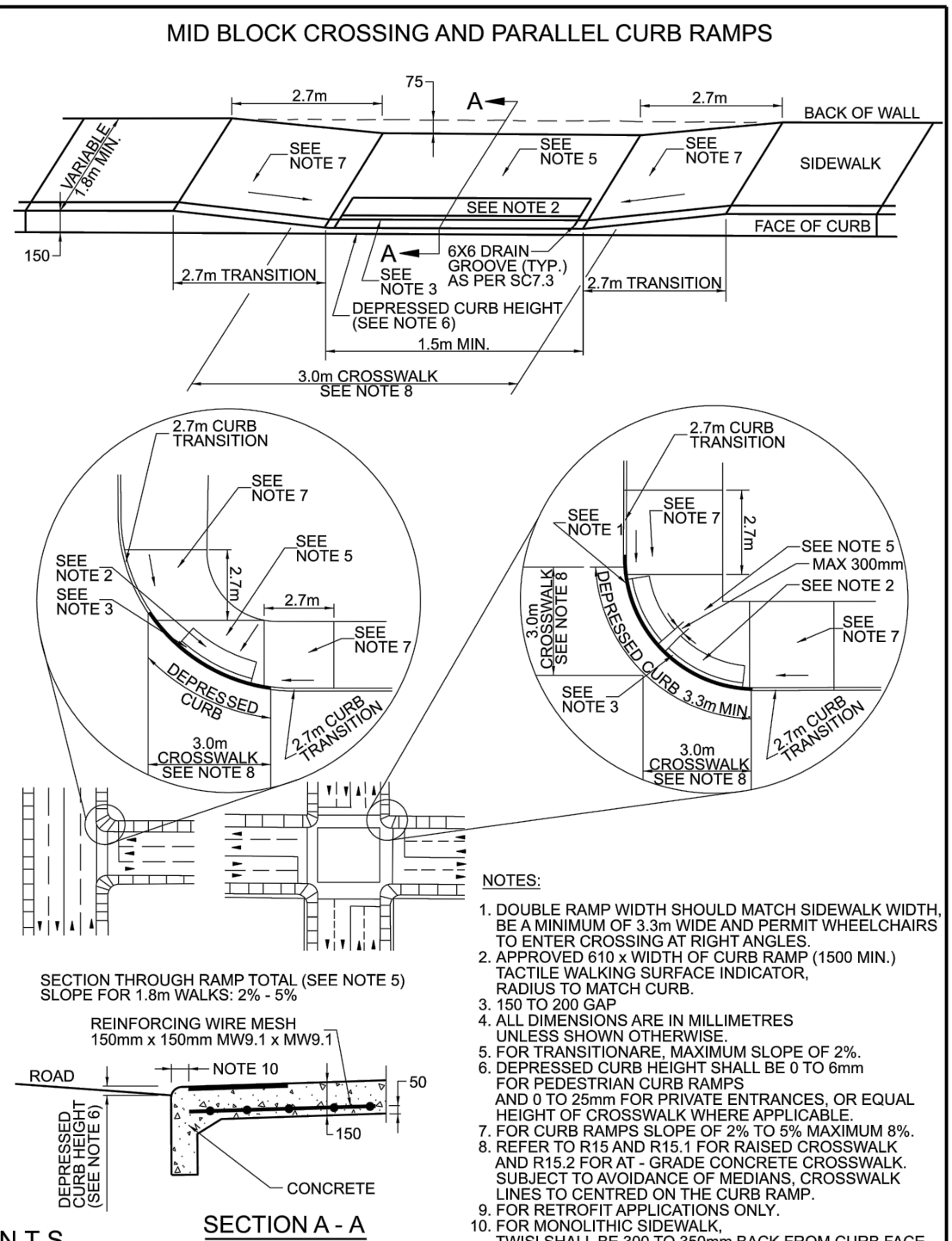
DATE: MARCH 2007  
REV: MARCH 2021  
DWG. No.: SC7.1



- NOTES:
- CURB RAMP WIDTH MATCH SIDEWALK WIDTH, BE A MINIMUM OF 1.5m WIDE.
  - FOR TRANSITION AREA, MAXIMUM SLOPE OF 2%.
  - ALL DIMENSIONS ARE IN MILLIMETRES UNLESS SHOWN OTHERWISE.
  - DEPRESSED CURB HEIGHT SHALL BE 0 TO 6mm FOR PEDESTRIAN CURB RAMP.
  - SUBJECT TO AVOIDANCE OF MEDIAN, CROSSWALK LINES TO CENTRED ON THE CURB RAMP.
  - APPROVED 610 x WIDTH OF CURB RAMP (1500 MIN.) TACTILE WALKING SURFACE INDICATOR (TWSI).
  - RADIUS TO MATCH CURB, DRAIN GROOVES AS PER SC7.
  - FOR CURB RAMP SLOPE OF 2% TO 5% MAXIMUM 8%.
  - FOR RETROFIT APPLICATIONS ONLY.
  - 150 TO 200mm GAP FOR MONOLITHIC SIDEWALK, TWSI SHALL BE 300 TO 350mm BACK FROM CURB FACE.
  - REFER TO R15 AND R15.1 FOR RAISED CROSSWALK AND R15.2 AT - GRADE CONCRETE CROSSWALK.

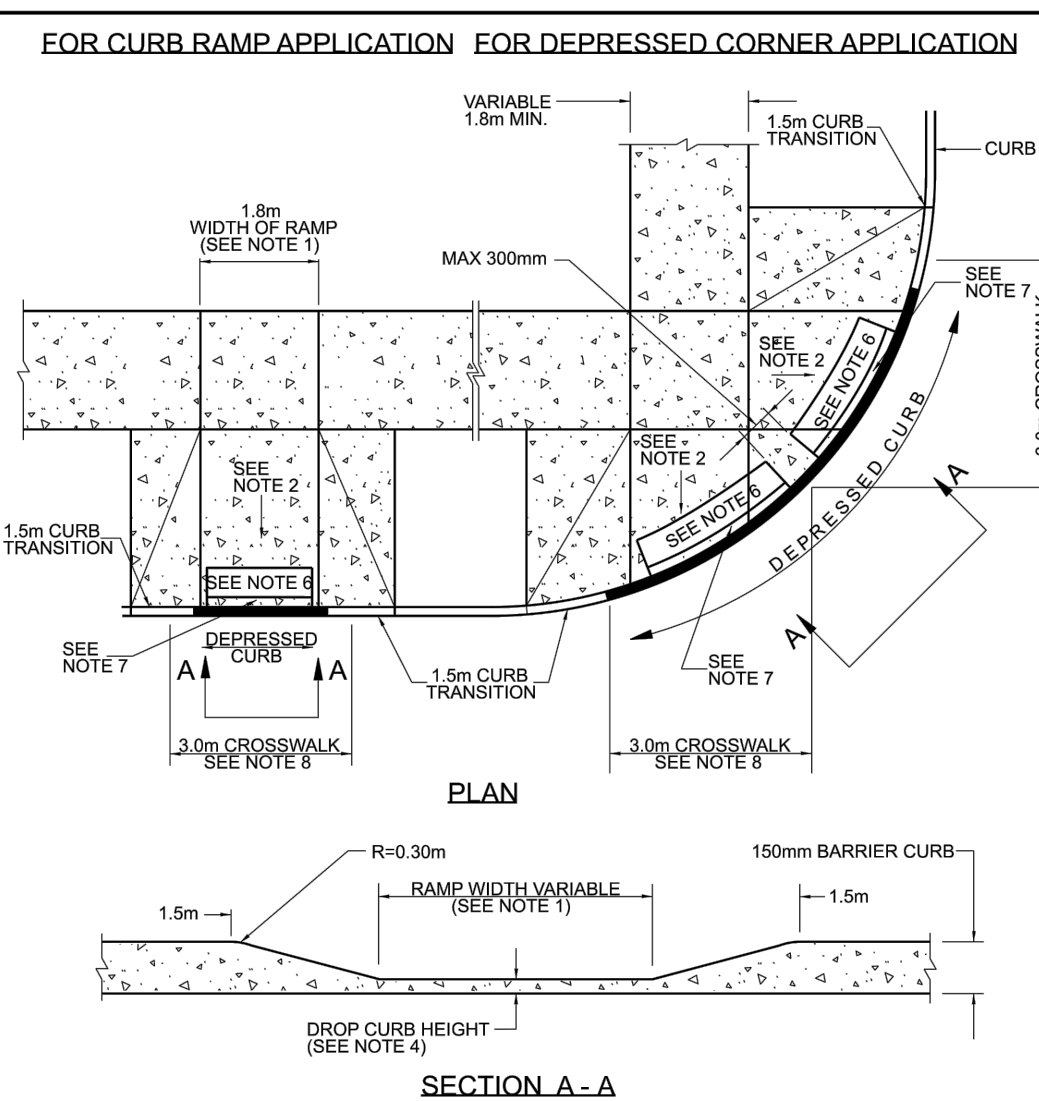
 PEDESTRIAN CURB RAMP AT  
INTERSECTION WITH BOULEVARD  
AND ADJACENT SIDEWALK

DATE: MARCH 2007  
REV: MARCH 2022  
DWG. No.: SC7.2



 PEDESTRIAN CURB RAMP  
WITHOUT BOULEVARD

DATE: MAY 2001  
REV: MARCH 2022  
DWG. No.: SC6



- NOTES:
- CURB RAMP WIDTH MATCH SIDEWALK WIDTH, BE A MINIMUM OF 1.5m WIDE.
  - FOR CURB RAMP SLOPE OF 2% TO 5% MAXIMUM 8%.
  - ALL DIMENSIONS ARE IN MILLIMETRES UNLESS SHOWN OTHERWISE.
  - DEPRESSED CURB HEIGHT SHALL BE 0 TO 6mm FOR PEDESTRIAN CURB RAMP.
  - SUBJECT TO AVOIDANCE OF MEDIAN, CROSSWALK LINES TO CENTRED ON THE CURB RAMP.
  - APPROVED 610 x WIDTH OF CURB RAMP (1500 MIN.) TACTILE WALKING SURFACE INDICATOR (TWSI).
  - RADIUS TO MATCH CURB, ALL TWSI SHALL HAVE 6mm WIDE AND 6mm DEEP DRAIN GROOVES.
  - AT CORNERS BETWEEN TWSI AND CURB, PANEL JOINTS MAY BE ADAPTED FOR USE IF TOUCHING.
  - TOP OF TWSI SHALL BE ALIGNED AND LEVEL WITH ADJACENT SURFACE.
  - 150 TO 200 GAP FOR MONOLITHIC SIDEWALK, TWSI SHALL BE 300 TO 350mm BACK FROM CURB SURFACE.
  - REFER TO R15 AND R15.1 FOR RAISED CROSSWALK AND R15.2 AT - GRADE CONCRETE CROSSWALK.

 PEDESTRIAN CURB RAMP  
WITH BOULEVARD

DATE: MAY 2001  
REV: MARCH 2022  
DWG. No.: SC7