

Urban Design Brief

4000 Riverside Drive, Ottawa

December 2025



IGP Realty Advisors Inc.

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1.0 Introduction

On behalf of our client, Suncor Energy Inc., we have prepared this Urban Design Brief to examine the Official Plan policies and Urban Design Guidelines for the lands located at 4000 Riverside Drive in the City of Ottawa, herein after referred to as the subject site shown on Figure 1.

Figure 1: Subject site at 4000 Riverside Drive in the City of Ottawa



2.0 Project Description

The subject site, 4000 Riverside Drive, is located on the South-West corner lot at the intersection of Riverside Drive and Hunt Club Road in Ottawa, Ontario. The site is in the vicinity of the Ottawa International Airport to the south-east. The property is currently occupied by a gas bar, a convenience store, and a car wash. The gas bar and convenience store are located near the centre of the property away from the frontage with the gas bar canopy connected to the store. A separate building houses the car wash at the rear of the site.

The Ottawa Official Plan By-law 2021-386 was adopted in November 2021, with subsequent amendments. Under this Official Plan, the subject site is designated as Outer Urban - Ottawa International Airport Economic District.

The Zoning by-law 2008-250 Consolidation was adopted in November 2023. The subject site is zoned IL - Light Industrial.

The City of Ottawa is developing a new comprehensive Zoning By-law for approval by Council in 2025. Under Draft 1 of the new zoning by-law, this site is designated as EDA1 - Special District Zones. The transition from Industrial Zone to Special District Zones would not incur any changes to the policies that apply to the site.

The City of Ottawa has developed Urban Design Guidelines, two of which are for Gas Stations and Drive-Through Facilities. These documents along with the policies of the Official Plan and Zoning by-law guide this design brief.

Figure 2: Subject site at 4000 Riverside Drive and surrounding uses



2.1 Public realm conditions around the site

According to the Official Plan By-law 2021-386, public realm refers to all of those private- and publicly-owned spaces and places which are freely available to the public to see and use.

South:

A restaurant with drive-through facilities, with landscaping buffering the two areas. Further south are undeveloped lands with a forested area.

East:

Riverside Drive, with bicycle lanes for public use. A sidewalk borders the road. On the opposite side of the road, various retail and commercial options are available for public use. Landscaping buffers both areas from Riverside Drive.

North:

Hunt Club Road, with bicycle lanes for public use. A sidewalk borders the road. Landscaping and larger trees buffer the site and the undeveloped lands on the opposite side of the road.

West:

Undeveloped lands with trees and a meadow, next to the Michael JE Sheflin Bridge crossing the Rideau River.

2.2 Surrounding Views

Figure 3: Corresponding Figures Map

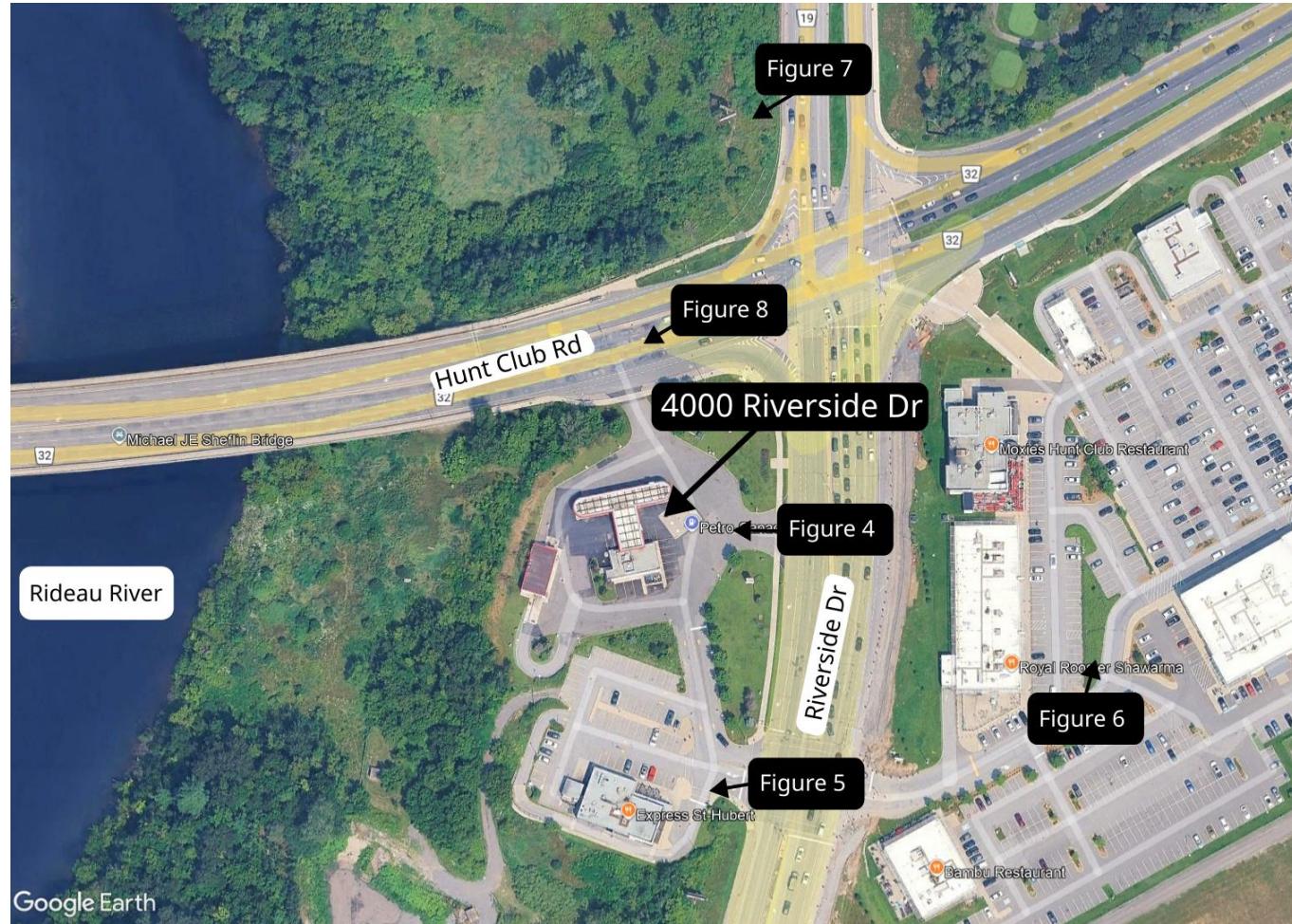


Figure 4: Subject Site at 4000 Riverside Drive



Figure 5: Restaurant, south of the subject site



Figure 6: Commercial retail, east of the subject site



Figure 7: Vacant lands, north of the subject site



Figure 8: Hunt Club Road and Michael JE Shelfin Bridge, west of the subject site



3.0 Proposed Development Site Plan Application

The proposed modifications to the site are to add a quick serve restaurant with drive-through to the existing convenience store building. The existing garbage enclosure will be changed to the Earth Bin garbage and recycling system. An exterior walk-in cooler and external Sanimax Tank will be located next to the proposed extended building.

Modifications to the curbs will be made for the drive-through stacking lanes, with additional parking spaces at the north-east corner of the site. The exterior pylon signs will be updated with the gas and restaurant uses.

A patio facing the frontage along Riverside will be accessed with a new sidewalk connection to the Riverside Drive and Hunt Club Road intersection. Bike parking will accompany the proposed development, located along the Riverside Drive frontage. Additional landscaping has been provided around the site as shown in the submitted landscape plan.

Figure 9: Proposed Site Plan by K-Paul Architects,

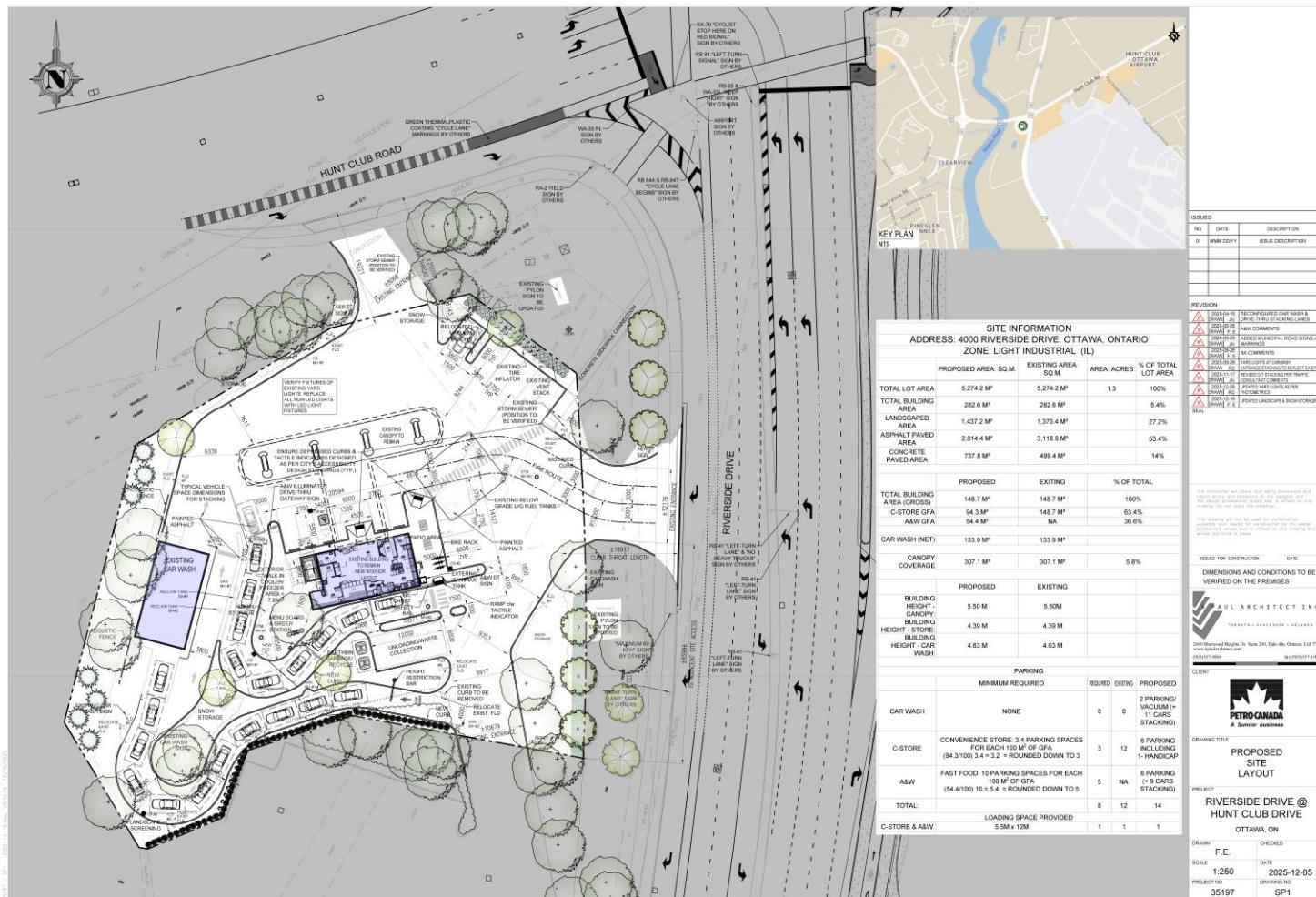
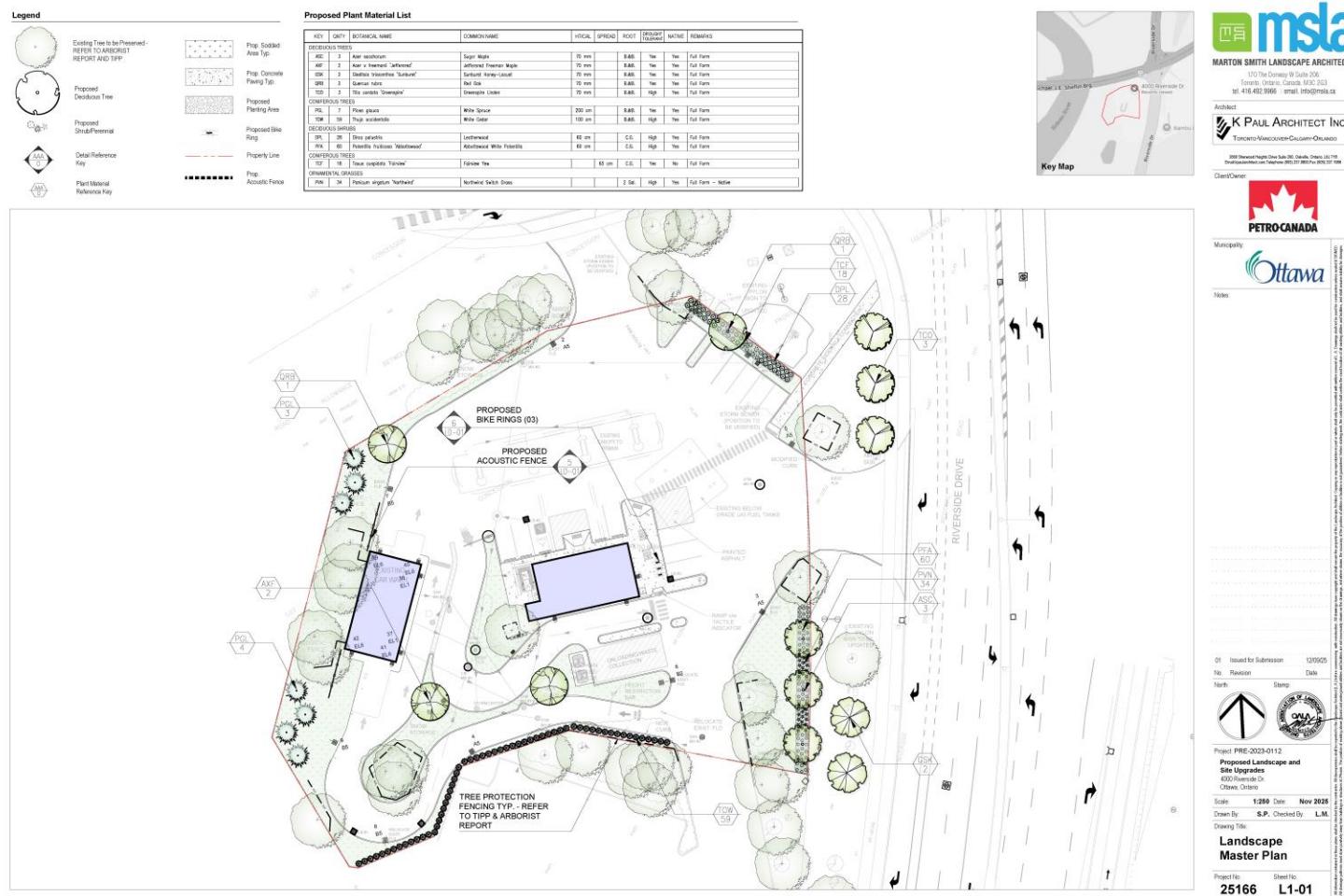


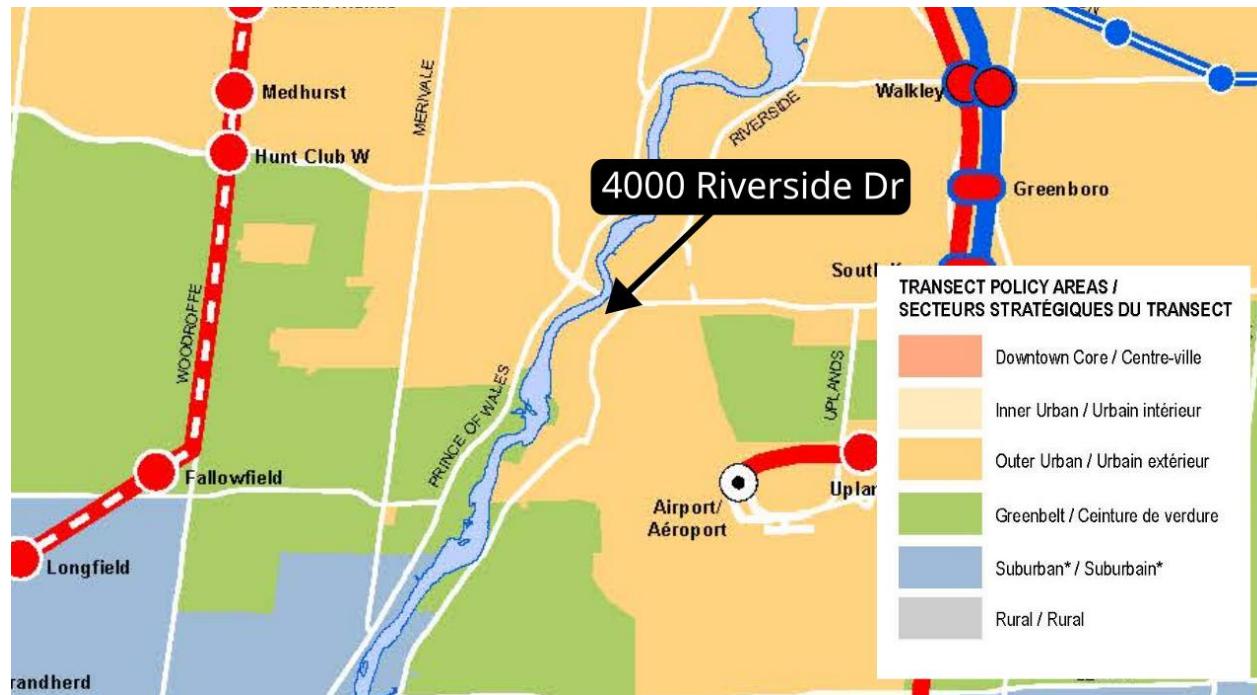
Figure 10: Proposed Landscape Plan by Marton Smith Landscape Architects



4.0 Legislation and Policy Overview

4.1 Official Plan Policies

Figure 11: Subject site's Official Plan designation



Official Plan Designation: Outer Urban – Ottawa International Airport Economic District

4.6 Urban Design

Policy 4.6.2 Protect views and enhance Scenic Routes including those associated with national symbols

4) *Development abutting Scenic Routes, as identified on Schedule C13, shall contribute to conserving or creating a desirable context by such means as:*

- a) Protecting the opportunity to view natural and cultural heritage features;*
- b) Preserving and restoring landscaping, including but not limited to distinctive trees and vegetation along the right of way;*
- c) Orienting buildings towards the Scenic Route and providing direct pedestrian access, where appropriate; and*
- d) Providing screening by way of opaque fencing or landscape buffers to hide surface parking lots or outside storage; and*
- e) Managing the intensity and spill-over of lighting on adjacent parcels.*

The proposed development's landscaping along Riverside Drive and Hunt Club Road provides screening from the development.

5) *Where Scenic Routes are also identified as Scenic Capital Entry Routes on Schedule C13, development and capital projects should also:*

- a) Enhance the opportunity for views and vistas towards national symbols, cultural landscapes and other features of the Capital; and*
- b) Contribute to the image of Ottawa as the Capital city by providing landscape and aesthetic improvements, including buildings that enhance the urban character, where possible.*

The proposed development's landscaping along Riverside Drive and Hunt Club Road provides screening from the development.

5.3 Outer Urban Transect

Policy 5.3.1 Recognize a suburban pattern of built form and site design

1) The Outer Urban Transects established pattern of built form and site design is suburban as described in Table 8, above and is predominantly reflective of the classic suburban model, and in some areas the conventional suburban model. Over the

medium- to long-term, this area will evolve toward an urban (15- minute) model as outlined in Table 8. This Plan allows for this evolution to happen gradually.

The subject site conforms to current suburban built form and design.

2) The Outer Urban Transect is generally characterized by low- to mid-density development. Development shall be:

- a) Low-rise within Neighbourhoods and along Minor Corridors;*
- b) Generally Mid- or High-rise along Mainstreets, except where the lot is too small to provide a suitable transition to abutting low-rise areas, in which case only low-rise development shall be permitted; and*
- c) Mid- or High-rise in Hubs.*

The proposed development's low-rise building conforms to the surrounding low-rise commercial retail uses.

3) In the Outer Urban Transect, the City shall support the rapid transit system and begin to introduce urban environments through the designation and overlay policies of this Plan, by:

- a) Supporting the introduction of mixed-use urban developments at strategic locations close to rapid transit stations; and*
- b) Targeting Hubs and selected segments of Mainstreets for mid-density and mixed-use development to reinforce or establish an urban pattern as described in Table 6.*

Hunt Club Road is designated as a Transit Priority Corridor. The city revised and improved bus service along this road in April 2025. A new sidewalk connection towards the intersection of Riverside Drive and Hunt Club Road will provide additional access to the restaurant. No housing is proposed for the site.

4) In the Outer Urban Transect, the Zoning By-law shall provide for a range of dwelling unit sizes in:

- a) Multi-unit dwellings in Hubs and on Corridors;*
- b) Predominantly ground-oriented forms in Neighbourhoods located away from frequent street transit and Corridors, with Low-rise multi-unit dwellings permitted near rapid transit and frequent street transit routes; and*

c) *In Hubs, a range of housing types to accommodate individuals not forming part of a household*

No housing is proposed for the site.

Policy 5.3.2 Enhance mobility options and street connectivity in the Outer Urban Transect

1) *The transportation network for the Outer Urban Transect shall:*

a) Acknowledge the existing reality of automobile-dependent built form that characterizes the Outer Urban Transect while taking opportunities as they arise to improve the convenience and level of service for walking, cycling and public transit modes;

b) Further to a), introducing mid-block connections to, from and within residential areas, particularly where doing so would materially reduce walking and cycling distances imposed by discontinuous street networks; and

c) Reducing automobile trips into the Inner Urban and Downtown Core Transects while improving first- and last-kilometre transportation options at the Outer Urban trip ends by:

i) Establishing park-and-ride facilities at strategic locations near rapid transit stations; and

ii) Maximizing direct pedestrian access from residential areas to street transit stops.

The city has installed bicycle lanes along Riverside Drive and Hunt Club Road and has improved the intersection with bicycle infrastructure, as shown in the proposed Site Plan. The addition of the sidewalk connection and bicycle parking will improve access to the site.

2) *When reconstructing arterials, the City shall set the stage for their future evolution to include, immediately upon reconstruction, a recognition of these streets' broader function as multimodal corridors and as public space that unites and connects neighbourhoods instead of dividing them, and shall implement designs that maintain the arterial function but also provide, within the right of way, for an edge that is calmer, designed for slower vehicular traffic, better integrated into the residential fabric of the adjacent neighbourhoods and fully supportive of the development of street-fronting buildings with active frontages.*

The city has installed bicycle lanes along Riverside Drive and Hunt Club Road and has improved the intersection with bicycle infrastructure, as shown in the proposed Site Plan. The addition of the sidewalk connection and bicycle parking will improve access to the site.

3)In the Outer Urban Transect areas, all streets within Hubs and within an Evolving Neighborhood Overlay shall be identified as access streets.

Not applicable to the subject site.

Policy 5.3.3 Provide direction to the Hubs and Corridors located within the Outer Urban Transect

1)Within Hubs, except where a secondary plan or area-specific policy specifies different heights, permitted building heights are as follows:

- a) Up to 300 metre radius or 400 metres walking distance of an existing or planned rapid transit station, whichever is greatest, at least 3 storeys and up to High-rise; and*
- b) Outside the area described by Policy*

a), up to High-rise where the parcel is of sufficient size to allow for a transition in built form massing.

Not applicable to the subject site.

2)Parking in Outer Urban Hubs shall be managed as follows:

- a) Minimum parking requirements may be reduced or eliminated; and*
- b) Surface parking within 300 metre radius or 400 metres walking distance, whichever is greatest, of an existing or planned rapid transit station, shall be located in the interior of the block, behind or beside the building and if located beside, shall not introduce a built-edge gap along the street that is wider than the widest building along the same frontage on the same site.*

Not applicable to the subject site.

3) *Along Mainstreets, permitted building heights are as follows, subject to appropriate height transitions, stepbacks and angular planes:*

- a) On sites that front on segments of streets whose right-of-way (after widening requirements have been exercised) is 30 metres or greater as identified in Schedule C16 for the planned street context, and where the parcel is of sufficient size to allow for a transition in built form massing, not less than 2 storeys and up to High-rise; and*
- b) On sites that front on segments of streets whose right-of-way is narrower than 30 metres as identified in Schedule C16 for the planned street context, generally, up to 9 storeys except where a secondary plan or area-specific policy specifies different heights.*

The subject site is adjacent to Hunt Club Road and Riverside Drive, both classified as minor corridors east and north of their intersection.

4) *Along Minor Corridors, permitted building heights, subject to appropriate height transitions and stepbacks shall not be less than 2 storeys and up to 6 storeys except where a secondary plan or area- specific policy specifies different heights.*

The subject site is adjacent to Hunt Club Road and Riverside Drive, both classified as minor corridors east and north of their intersection.

Policy 5.3.4 Provide direction to Neighbourhoods located within the Outer Urban Transect

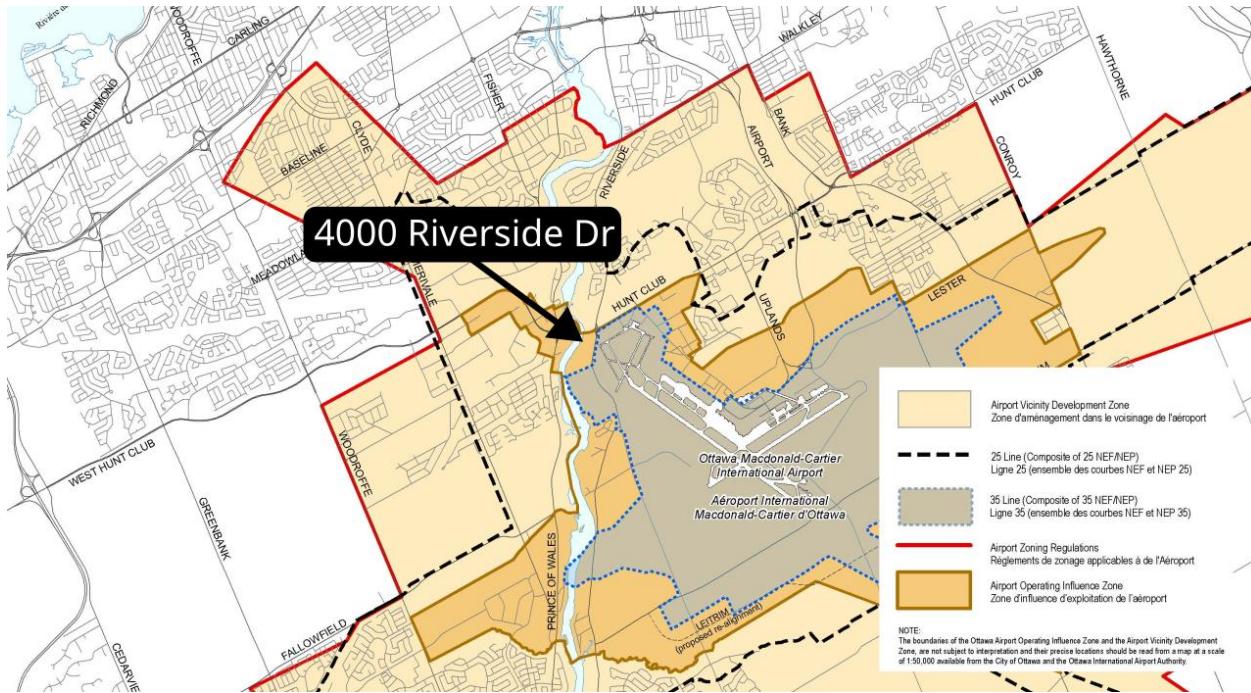
1) *Neighbourhoods located in the Outer Urban area shall accommodate residential growth to meet the Growth Management Strategy as outlined in Section 3. The Zoning By-law shall implement development standards that transition away from a suburban model and move towards urban built forms as described in Table 6 as applicable and that:*

- a) Allows and supports a wide variety of housing types with a focus on lower density missing-middle housing which generally reflects the existing built form context of the neighbourhood, which may include new housing types that are currently not contemplated in this Plan;*

- b) The application, as appropriate, of Zoning By-law development standards to be applied as one lot for zoning purposes to support missing middle housing;*
- c) Generally provides for up to 3 storeys height permission, and where appropriate 4 storeys height permission to allow for ground oriented higher-density Low-rise residential development;*
- d) Provides an emphasis on regulating the maximum built form envelope that frames the public right of way; and*
- e) In appropriate locations, to support the production of missing middle housing, prohibit lower-density typologies.*

Not applicable to the subject site.

Figure 12: Subject site's location in the Ottawa International Airport Economic District



6.6 Special Districts

Policy 6.6.3.1 Ottawa International Airport Economic District

1) To encourage the integration of airport planning with land use planning and transit, the City will undertake a secondary plan in partnership with the airport authority, Government of Canada and the National Capital Commission. This will set the stage for future transit-supportive development by creating opportunities for additional service-oriented and commercial land uses.

According to the latest 2018 Airport Master Plan (Land Use Plan amended 2022), the site is designated aviation/non-aviation commercial area.

2)Recognize the airport's function as a gateway to the City will encourage and where applicable, develop partnerships with the Airport Authority and other levels of government to provide high-quality urban places, landscaping, buildings, transit stations, area parks, pedestrian and cycling improvements and increased passenger convenience through wayfinding, connections to convenient transit and pathways for active transportation.

The city has installed bicycle lanes along Riverside Drive and Hunt Club Road and has improved the intersection with bicycle infrastructure, as shown in the site layout plan. The addition of the sidewalk connection and bicycle parking will improve access to the site.

3)Until a secondary plan as per Policy 1) has been approved, on lands shown as “Ottawa International Airport Special Economic District” on Schedule B6 and notwithstanding land use permissions within Subsection 6.5 Mixed Industrial, allow a mix of land uses such as industrial and commercial to provide an economic base around the airport and accessible by O-Train and street transit service. Development should enhance the airport’s role as a key economic generator. Ideal businesses have a relationship with the airport, such as mixed research and development, storage and distribution, machinery repair and maintenance and accommodations for travelers. These uses may locate within multi-storey buildings where there are no conflicts with height restrictions.

The proposed development will feature a convenience store and quick serve restaurant with drive-through, offering commercial uses to the airport area.

4)Development will be subject to the restrictions imposed by the Airport Vicinity Development Zone as shown on Schedule C13 and as described in Subsection 10.2.2.

The subject site is within the Airport Vicinity Development Zone’s 25 Line and Airport Operating Influence Zone. No noise study is required as per the City’s Pre-consultation’s Study and Plan Identification List.

5)Development within runway approach surfaces will be subject to the overall building height provisions of the Federal Airport Zoning Regulations. Elsewhere in the Ottawa International Airport Economic District, until height provisions are determined in a secondary plan, Subsection 6.6.1 Policy 1 d) will apply.

The proposed development is maintaining the existing height of the buildings.

6)In the wider airport area beyond the area restricted by noise controls, residential and business clusters will be developed along Corridors and around future O-Train stations leading to the airport. These areas include the former Canadian Forces Uplands Air Force Base, lands west of the airport, along the Rideau River in the vicinity of Limebank Road. Employment uses will include private employers and public services such as paramedic and police installations as well as public transit stations, government agencies laboratories and offices.

The subject site is located west of the airport, along the Rideau River.

7)Encourage an employment area that achieves a modal share with a high percentage of employees that rely on street transit, O-Train and active transportation connections. To reduce the amount of vehicle traffic utilizing the existing access road infrastructure, new development will integrate into, and extend into its site, the existing surrounding pattern of public streets and sidewalks and reinforce or introduce a grid pattern that is transit oriented. The Airport Parkway will continue to be the primary means of road access to the passenger terminal building and core development area.

Not applicable to the subject site.

8)There will be no development of land identified as being environmentally significant. The Greenbelt linkage is part of land reserved for environmental purposes which will form part of a potential future link between the Greenbelt lands to the west of the airport and the Leitrim Wetland.

Not applicable to the subject site.

Policy 10.2.2 Protection of airport and aircraft operations

1)Development inside the Airport Vicinity Development Zone, as shown on Schedule C14, shall be consistent with applicable City, provincial and federal guidelines and regulations. In the event of a variation between City, provincial or federal guidelines, the most restrictive provisions shall be applied.

2)A noise control study consistent with the Council-approved Environmental Noise Control Guidelines is required as part of a complete application for any development proposal within the 25 Line (Composite of 25 NEF/NEP), as shown on Schedule C14.

No noise study is required as per the City's Pre-consultation's Study and Plan Identification List.

6)Development shall comply with the Ottawa Macdonald-Cartier International Airport Zoning Regulations as enacted under the federal Aeronautics Act.

The proposed development complies with the Ottawa Macdonald-Cartier International Airport Zoning Regulations.

4.2 Urban Design Guidelines

4.2.1 Urban Design Guidelines for Drive-Through Facilities

1) Streetscape and Built Form

Guideline 1: Respond to the positive elements of the context through such means as building height, setbacks, building orientation and architectural styles.

The building and canopy heights of 4.39 m and 5.50 m respectively are set back away from the street.

Guideline 2: Locate buildings close to the street to help define the street edge.

The building and canopy are set back from the street to provide access to the refueling truck, while providing pedestrian access to the site with a sidewalk connection to the intersection. The TIA provided with this application illustrates the truck maneuvering plan around the existing tanks, which are being kept at the northeast corner of the site.

Guideline 3: Provide ample landscaping, in combination with building orientation, to enhance the streetscape and define the street edge when setting buildings back from the street is unavoidable.

The subject site's landscaping is provided along both Riverside Drive and Hunt Club Road.

Guideline 4: Provide weather protection at the main building entrance, for areas close to public transit stops, bicycle parking, walkways, and in places with pedestrian amenities.

The canopy provides weather protection at the main building entrance.

Guideline 5: Locate public amenities close to the building entrances.

The bike rack and patio area are located to the east of the building next to the walkway connection.

Guideline 6: Locate interior uses such as seating areas, employee rooms, offices, waiting areas and lobbies, which have the potential for clear windows, along street-facing walls.

The interior layout of the building provides natural lighting facing the gas bar.

Guideline 7: Make the majority of the pedestrian level façade facing the street highly transparent with clear glass windows and doors that animate public streets and maximize views in and out of the building.

The standard building façade provides windows at the entrance of the building facing the gas bar.

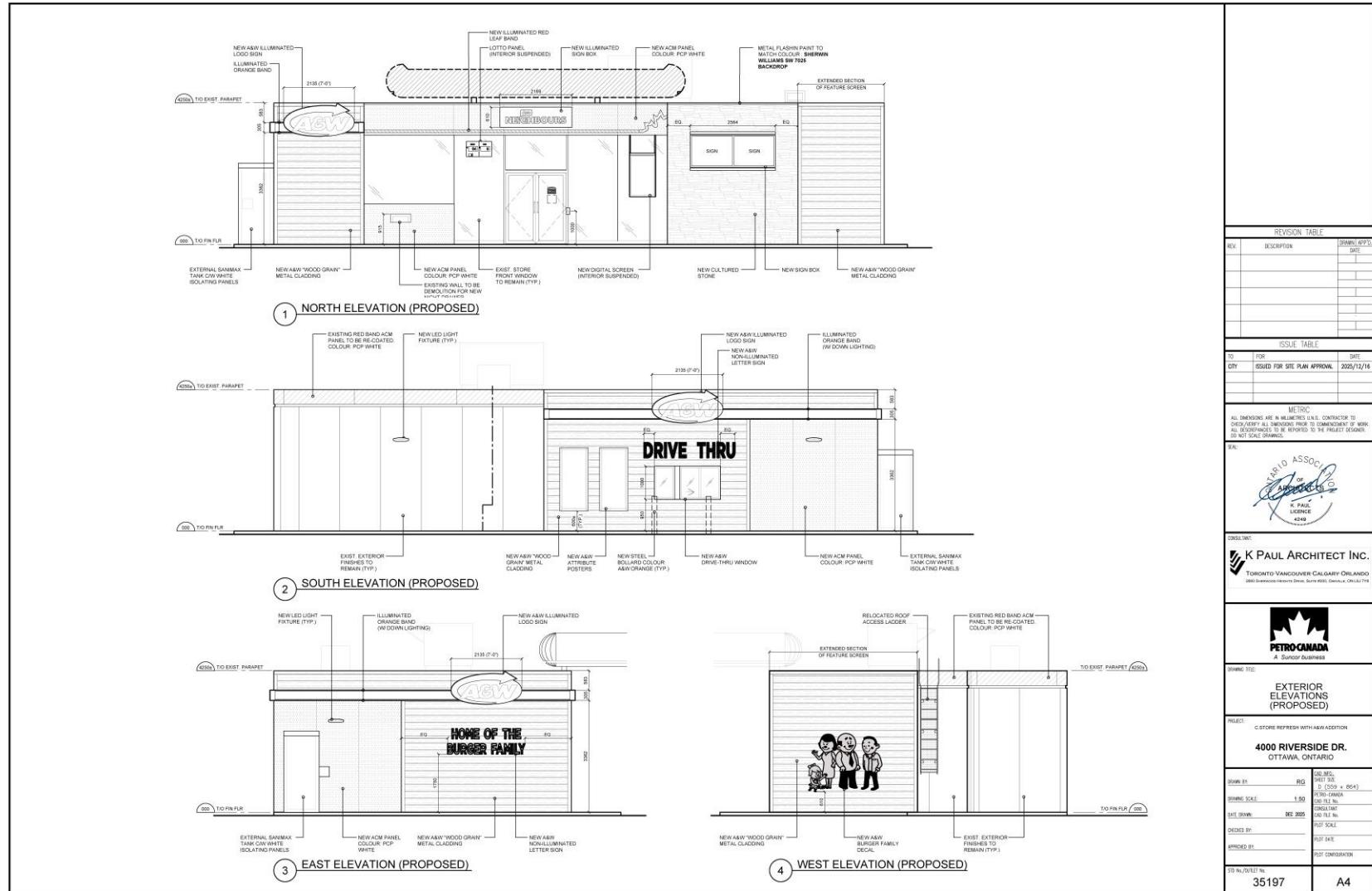
Guideline 8: Landscape the area in front of blank walls that face public streets and use projections, recesses, arcades, awnings, colour and texture to reduce the visual size of any unglazed walls.

Landscaping is provided along both Riverside Drive and Hunt Club Road around the subject site.

Guideline 9: Coordinate architectural detail and character within an overall design concept for all building sides and components.

The proposed development's exteriors provide a stylistic design with the addition of cultured stone finish and wood grain metal cladding.

Figure 13: Proposed Building Elevation Design by K Paul Architects



2) Pedestrians and Cyclists

Guideline 10: Provide an unobstructed 2.0 metre wide sidewalk in the public right-of-way, across private access driveways. Ensure little or no change in elevation.

The existing sidewalk in the public right-of-way is 2.0 metres wide.

Guideline 11: Distinguish walkways from driving surfaces by using varied paving treatments and by raising walkways to curb level.

The walkway around the building is elevated with two ramps for accessibility at the front of the building. Painted asphalt distinguishes the sidewalk connection from the building to Riverside Drive and the access to the Earth Bin.

Guideline 12: Provide customer entrance doors that are close to parking areas.

The building's entrance faces the parking area adjacent to the gas bar.

Guideline 13: Provide customer entrance doors clearly visible from public streets and directly accessible from the public sidewalk.

The building's entrance faces Hunt Club Road and a sidewalk connection is provided to Riverside Drive.

Guideline 14: Provide an unobstructed 2.0 metre wide pedestrian walkway between the public sidewalk (and/or parking areas) and building entrances.

A new 2.0 metres wide walkway and crosswalk connect the sidewalk along Riverside Drive to the building, with tactile walking surface indicators.

Guideline 15: Use landscaping or similar means to delineate pedestrian walkways and pedestrian access to the buildings.

Landscaping has been provided at the walkway connection.

Guideline 16: Locate required bicycle parking close to the building entrance in a manner that does not impede pedestrian movement.

The bike rack is located to the east of the building next to the walkway connection and does not impede pedestrian movement.

3) Vehicles and Parking

Guideline 17: Locate surface parking areas and stacking lanes at the side or rear of buildings.

Surface parking is located in front of the main entrance and at the corner of the lot, screened with landscaping. Stacking lanes are located away from the street, behind the main building. Existing stacking lanes for car wash are at the back of the lot.

Guideline 18: Minimize the number and width of driveways from the public street while ensuring they meet the requirements of the Private Approach By-law.

By-law 2003-447 Private Approach By-law:

Section 11: a) private approach shall have a minimum width of 2.4 metres and a maximum width of 9.0 metres, and in no case shall the width exceed 50% of the frontage on which the approach or approaches are located.

The existing entrances on Hunt Club Road and Riverside Drive are 8.0 m and 12.1 m respectively. Their width provides access for the refueling truck.

Guideline 19: Locate vehicular access points to the sites as far away as possible from street intersections. Locate vehicle access points to corner sites on the secondary street.

The two vehicle access points on Riverside Drive and Hunt Club Road are located to provide access to the refueling truck.

Guideline 20: Locate stacking lanes away from adjacent sensitive uses, such as residential and outdoor amenity areas, to reduce the impacts of noise and pollution that could be caused by stacking cars on such uses. Use landscaping and fencing to help buffer potential impacts.

The stacking lanes are located away from sensitive uses, buffered by landscaping.

Guideline 21: Locate the start point to the stacking lane at the rear of the site so that queued vehicles do not block traffic along the public streets or the movement of other vehicles on site.

The stacking lane starting points are located away from the street.

Guideline 22: Avoid locating the stacking lane between the building and the public street.

The stacking lanes are located to the rear of the lot, away from the streets.

Guideline 23: Provide escape lanes and the appropriate number of queuing spaces as required by the Zoning By-law to create efficient stacking lanes and to minimize on-site conflicts.

The number of queuing spaces conforms to the Zoning By-law for the car wash use. The restaurant queuing spaces are supported by the Traffic Impact Assessment, to reduce the total number from 11 spaces to 9 spaces. A Minor Variance application to the Committee of Adjustment is required for this application.

Guideline 24: Separate stacking lanes from parking areas and driveways using landscaped islands, decorative pavement, pervious islands and painted lines.

Painted lines separate the stacking lanes from the parking area.

Guideline 25: Design the on-site circulation to minimize the conflicts between pedestrians and vehicles.

Stacking lanes and drive-through exits are located at the side and rear of the building, away from the main entrance.

Guideline 26: Provide separate stacking lanes when two drive-through uses exist on the same site.

Each of the drive-through uses (car wash and restaurant) have their own separate stacking lanes.

Guideline 27: Provide only the minimum number of parking spaces required by the Zoning By-law.

A total of 14 parking spaces are provided for the site, for a better customer experience.

4) Landscape and Environment

Guideline 28: Plant street trees between 7.0 to 10.0 metres apart along public streets. Plant trees in permeable surface areas, with approximately 10.0 square metres of soil area per tree.

The Landscape Plan prepared for this application adheres to the requirements.

Guideline 29: Select trees, shrubs and other vegetation considering their tolerance to urban conditions, such as road salt and heat. Give preference to native species of the region that are of equal suitability.

The Landscape Plan prepared for this application adheres to the requirements.

Guideline 30: Provide a minimum 3.0 metre wide landscape area along the edge of a site where parking areas, drive lanes or stacking lanes are adjacent to a public street. Use trees, shrubs and low walls to screen cars from view while allowing eye level visibility into the site.

A 3.0 metre buffer is provided next to the parking area at the north-east of the site.

Guideline 31: Provide a minimum 2.5 metre wide landscape area along the site's side and rear yards in order to provide screening and enhance site environmental benefits.

A 1.5 metre landscape buffer is provided around the rear and side yard conforming to the Zoning By-law.

Guideline 32: Provide a minimum 3.0 metre wide landscape area, which may include a solid wall or fence in addition to planting, at the edges of sites that are adjacent to residential or institutional properties.

Not applicable to the subject site.

Guideline 33: Protect and feature heritage, specimen and mature trees on the site by minimizing grade changes and preserving permeable surfaces.

The Tree Inventory and Preservation Plan Report prepared for this application shows the removal of 2 trees, preserving 43 trees and tree polygon.

Guideline 34: Use green building technologies such as green roofs, drip irrigation, and other Leadership in Energy and Environmental Design (LEED) approaches.

Not applicable to the subject site.

Guideline 35: Use sodded areas and shrub beds to collect, store and filter stormwater in order to improve groundwater recharge.

The existing Stormwater Management plan effectively stores and filters groundwater.

Guideline 36: Divide large parking areas into smaller and well-defined sections using soft and hard landscaping in order minimize the amount of paved area.

The subject site's total paved area is separated through different uses (parking, drive isles, canopy, stacking lanes).

5) Signs

Guideline 37: Locate and design ground-mounted and wall-mounted signs to complement the character and scale of the area. Integrate landscape features with ground-mounted signs.

The subject site's existing pylon signage will be updated to reflect the uses.

Guideline 38: Use pavement markings and directional signs to enhance clarity of movement patterns on site.

Pavement markings denote the drive-through stacking lanes and parking spaces. Existing signage for the car wash provides direction around the site.

Guideline 39: Design buildings to include defined spaces to accommodate signs that respect building scale, architectural features, signage uniformity and established streetscape design objectives.

The proposed development's exteriors provide a stylistic design with the addition of cultured stone finish and wood grain metal cladding.

Guideline 40: Restrict temporary and portable signs. Prohibit billboards, revolving signs and roof signs on private property.

Not applicable to the subject site.

Guideline 41: Design sign illumination to be task-oriented and avoid glare/light spillover toward adjacent land uses.

The building is located away from sensitive uses, reducing spillover into adjacent uses. The existing pylon signage will be updated. The landscape plan shows additional landscaping to the south and west of the property.

6) Servicing and Utilities

Guideline 42: Design garbage enclosures that are external to the building with the same materials as the building and ensure that the wall height is sufficient to completely conceal garbage dumpsters.

The proposed Earth Bins are not attached to the main building, located in between the two drive-throughs.

Guideline 43: Enclose all utility equipment within buildings or screen them from both public streets and private properties to the rear. These include utility boxes, garbage and recycling container storage, loading docks and ramps, and air conditioner compressors.

The existing transformers are screened with landscaping.

Guideline 44: Locate noise-generating areas, including ordering board speakers, outdoor loading areas and garbage storage, away from sensitive uses such as residential areas and schools.

The ordering board and Earth Bins are located away from the roads.

Guideline 45: Buffer potential noise impacts with building structures, landscaped berms or attenuation fencing (minimum 1.8 metre in height) in front with landscaping.

The ordering boards are buffered by landscaping and the carwash by an acoustic fence.

Guideline 46: Design lighting so that there is no light spillage, glare or light cast over adjacent uses. Direct and/or shield lighting sources away from adjacent residential properties and provide screening as necessary.

The lighting plan shows minimal spillage onto adjacent uses.

Guideline 47: Use efficient white light sources to reduce energy costs and to create a natural colour balance for safety and security.

The existing and proposed lighting will be replaced with LED lighting fixtures.

Guideline 48: Plan the site to include areas for temporary snow storage without conflicting with site circulation, landscaping and utility boxes.

The subject site snow storage area is located away from potential conflicts.

Guideline 49: Provide views and clear sightlines between the site and surrounding uses to ensure sufficient safety and comfort levels.

The site is visible from the roads, while being screened with existing and proposed landscaping.

4.2.2 Urban Design Guidelines for Gas Stations

1) Streetscape and Built Form

Guideline 1: Respond to the positive elements of the surrounding context through such means as building height, setbacks, building orientation and architectural styles.

The building and canopy heights of 4.39 m and 5.50 m respectively are set back away from the street.

Guideline 2: Locate building structures (such as car washes, convenience stores, and canopies) close to the street to help define the street edge.

The building and canopy are set back from the street to provide access to the refueling truck, while providing pedestrian access to the site with a sidewalk connection to the intersection. The TIA provided with this application illustrates the truck maneuvering plan around the existing tanks, which are being kept at the northeast corner of the site.

Guideline 3: Design all sides of buildings and pump islands with a consistent architectural style to enhance the streetscape.

The proposed development's exteriors provide a stylistic design with the addition of cultured stone finish and wood grain metal cladding.

Guideline 4: Use clear windows for the car wash façades facing public streets. They animate the street by providing views into the car wash and act as poster cases for advertising.

The car wash is located at the back of the site, with windows facing towards the street.

Guideline 5: Provide ample landscaping, in combination with building orientation, to enhance the streetscape and define the street edge when setting building structures back from the street is unavoidable.

The subject site's landscaping is provided along both Riverside Drive and Hunt Club Road.

Guideline 6: Provide transparent windows and doors for retail buildings to ensure visibility between the store, the pump islands and surrounding streets.

The proposed development's main building windows face the gas pumps and Hunt Club Road.

2) Pedestrians and Cyclists

Guideline 7: Provide an unobstructed 2.0 metre wide pedestrian walkway between the public sidewalk (and/or parking areas) and building entrances.

A new 2.0 metres wide walkway and crosswalk connect the sidewalk along Riverside Drive to the building, with tactile walking surface indicators.

Guideline 8: Distinguish walkways from driving surfaces by using varied paving treatments and by raising walkways to curb level.

The walkway around the building is elevated with two ramps for accessibility at the front of the building. Painted asphalt distinguishes the sidewalk connection from the building to Riverside Drive and the access to the Earth Bin.

Guideline 9: Provide an unobstructed 2.0 metre wide sidewalk in the public right-of-way across private access driveways. Ensure little or no change in elevation.

The existing sidewalk in the public right-of-way is 2.0 metres wide.

Guideline 10: Locate required bicycle parking close to the building entrance in a manner that does not impede pedestrian movement.

The bike rack is located to the east of the building next to the walkway connection and does not impede pedestrian movement.

3) Vehicles and Parking

Guideline 11: Locate vehicular access points to the site as far away as possible from street intersections.

The two vehicle access points on Riverside Drive and Hunt Club Road are located to provide access to the refueling truck.

Guideline 12: Minimize the number and width of driveways from the public street while ensuring that they meet the requirements of the Private Approach By-law. Avoid conflicts with pedestrians along the street by defining a narrower car entrance while allowing for tanker truck turning.

By-law 2003-447 Private Approach By-law:

Section 11: a) private approach shall have a minimum width of 2.4 metres and a maximum width of 9.0 metres, and in no case shall the width exceed 50% of the frontage on which the approach or approaches are located.

The existing entrances on Hunt Club Road and Riverside Drive are 8.0 m and 12.1 m respectively. Their width provides access for the refueling truck.

Guideline 13: Allow a sufficient driving distance from the car wash exit to the public street to minimize tracking water onto the street during winter conditions.

A 50 m distance exists between the car wash exit and the exit onto Hunt Club Road.

Guideline 14: Locate stacking lanes away from adjacent sensitive uses, such as residential and outdoor amenity areas, to reduce the impacts of noise and pollution that could be caused by stacking cars on such uses. Use landscaping and fencing to help buffer potential impacts.

The stacking lanes are located away from sensitive uses, buffered by landscaping.

Guideline 15: Locate access points for stacking lanes away from public streets and driveways so that queued vehicles do not block the traffic along public streets or the movement of other vehicles on site.

The stacking lane access points are located away from the street.

Guideline 16: Provide separate stacking lanes when two drive-through uses (such as a car wash and a drive-through convenience store) exist on the same site.

Each of the drive-through uses (car wash and restaurant) have their own separate stacking lanes.

Guideline 17: Provide escape lanes and the appropriate number of queuing spaces as required by the Zoning By-law to create efficient stacking lanes and to minimize on-site conflicts.

The number of queuing spaces conforms to the Zoning By-law for the car wash use. The restaurant queuing spaces are supported by the Traffic Impact Assessment, to reduce the total number from 11 spaces to 9 spaces. A Minor Variance application to the Committee of Adjustment is required for this application.

Guideline 18: Design the on-site circulation to minimize the conflicts between pedestrians and vehicles.

Stacking lanes and drive-through exits are located at the side and rear of the building, away from the main entrance.

Guideline 19: Design the on-site circulation to facilitate unobstructed forward movement by tanker trucks and the safe unloading of fuels. Provide adequate clearance for fuel delivery trucks under canopies.

The on-site configuration allows unobstructed fueling truck entry and exit.

Guideline 20: Separate stacking lanes from parking areas and driveways by using landscaped islands, decorative pavement, pervious islands and painted lines.

Painted lines separate the stacking lanes from the parking area.

Guideline 21: Provide only the minimum number of required parking spaces required by the Zoning By-law.

A total of 14 parking spaces are provided for the site, for a better customer experience.

4) Landscape and Environment

Guideline 22: Provide a minimum 3.0 metre wide landscape area along the edge of a site where parking areas, driveways or stacking lanes are adjacent to a public street. Use trees, shrubs and low walls to screen cars from view while allowing eye level visibility into the site.

A 3.0 metre buffer is provided next to the parking area at the north-east of the site.

Guideline 23: Plant street trees between 7.0 and 10.0 metres apart along public streets. Plant trees in permeable surface areas, with approximately 10.0 square metres of soil area per tree.

The Landscape Plan prepared for this application adheres to the requirements.

Guideline 24: Select trees, shrubs and other vegetation considering their tolerance to urban conditions, such as road salt or heat. Give preference to native species of the region that are of equal suitability.

The Landscape Plan prepared for this application adheres to the requirements.

Guideline 25: Provide a minimum 2.5 metre wide landscape area along the site's side and rear yards in order to provide screening and enhance site environmental benefits.

A 1.5 metre landscape buffer is provided around the rear and side yard conforming to the Zoning By-law.

Guideline 26: Provide a minimum 3.0 metre wide landscape area, which may include a solid wall or fence in addition to planting, at the edges of sites that are adjacent to residential or institutional properties.

Not applicable to the subject site.

Guideline 27: Protect and feature heritage, specimen and mature trees on site by minimizing grade changes and preserving permeable surfaces.

The Tree Inventory and Preservation Plan Report prepared for this application shows the removal of 2 trees, preserving 43 trees and tree polygon.

Guideline 28: Provide significant architectural or landscape features at the corner on corner sites in order to emphasize the public streets and enhance the streetscape.

The Landscape Plan prepared for this application shows additional landscaping that buffers the parking area from the street intersection.

Guideline 29: Use green building technologies such as green roofs, drip irrigation, and other Leadership in Energy and Environmental Design (LEED) approaches.

Not applicable to the subject site.

Guideline 30: Use sodded areas and shrub beds to collect, store and filter stormwater in order to improve groundwater recharge.

The existing Stormwater Management plan effectively stores and filters groundwater.

5) Signs

Guideline 31: Use pavement markings and directional signage to enhance clarity and ease of movement patterns on site.

Pavement markings denote the drive-through stacking lanes and parking spaces. Existing signage for the car wash provides direction around the site.

Guideline 32: Design buildings to include defined spaces to accommodate signs that respect building scale, architectural features, signage uniformity and established streetscape design objectives.

The proposed development's exteriors provide a stylistic design with the addition of cultured stone finish and wood grain metal cladding.

Guideline 33: Restrict temporary and portable signs. Prohibit billboards, revolving signs and roof signs on private property.

Not applicable to the subject site.

Guideline 34: Design sign illumination to be task-oriented and avoid glare and light spillover toward adjacent land uses.

The building is located away from sensitive uses, reducing spillover into adjacent uses. The existing pylon signage will be updated. The landscape plan shows additional landscaping to the south and west of the property.

Guideline 35: Locate and design ground-mounted and wall-mounted signs to complement the character and scale of the area and promote an active, pedestrian friendly environment. Integrate landscape features with ground-mounted signs.

The subject site's existing pylon signage will be updated to reflect the uses.

6) Servicing and Utilities

Guideline 36: Locate noise-generating areas, including auto service bays, car wash openings, vacuum stations, outdoor loading areas, garbage storage and stacking lanes, away from sensitive uses such as residential areas and schools.

The ordering board and Earth Bins are located away from the roads.

Guideline 37: Buffer potential noise impacts with building structures, landscaped berms or attenuation fencing (minimum 1.8 metre in height) with landscaping in front.

The ordering boards are buffered by landscaping and the carwash by an acoustic fence.

Guideline 38: Enclose all utility equipment within buildings or screen them from both public streets and private properties to the rear. These include utility boxes, garbage and recycling container storage, loading docks and ramps, and air conditioner compressors.

The existing transformers are screened with landscaping.

Guideline 39: Design external garbage enclosures with the same materials as the building and ensure that the wall height is sufficient to completely conceal garbage dumpsters.

The proposed Earth Bins are not attached to the main building, located in between the two drive-throughs.

Guideline 40: Provide views and clear sightlines between the site, surrounding uses, and public streets to ensure sufficient safety and comfort levels.

The site is visible from the roads, while being screened with existing and proposed landscaping.

Guideline 41: Plan the site to include areas for temporary snow storage without conflicting with site circulation, landscaping and utility boxes.

The subject site snow storage area is located away from potential conflicts.

Guideline 42: Design lighting so that there is no light spillage, glare or light cast over adjacent uses. Direct and/or shield lighting sources away from adjacent residential properties and provide screening as necessary.

The lighting plan shows minimal spillage onto adjacent uses.

Guideline 43: Use efficient white light sources to reduce energy costs and to create a natural colour balance for safety and security.

The existing and proposed lighting will be replaced with LED lighting fixtures.

Guideline 44: Set rooftop mechanical equipment back from the edge of the building and screen it to minimize the visual impact.

The rooftop equipment is back from the edge of the building and screened from view.

5.0 Conclusion

This Urban Design Brief reviewed the land use planning merits of a proposed Site Plan Application for the lands municipally known as 4000 Riverside Drive in the City of Ottawa to permit the lands to be redeveloped. The proposed application conforms to the policies of the City of Ottawa Official Plan, City of Ottawa Urban Design Guidelines for Drive-Through Facilities and Gas Stations.

The proposed application will permit the subject lands to be redeveloped containing a gas station, car wash and commercial building which includes a convenience store and restaurant with a drive-through. The application efficiently uses the land for commercial redevelopment within an industrial area, which will continue to utilize municipal infrastructure.

In our professional opinion, the application represents good planning.

Sincerely,

IGP Realty Advisors Inc.