

Project Heritage Statement

Clemow Development

160 Clemow Avenue, Ottawa, Ontario



Prepared By:



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1.0 Introduction

1.1 Overview

The proposed project is located at 160 Clemow Avenue, which is located within the Clemow-Monkland Driveway and Linden Terrace Heritage Conservation District (HCD), as designated under Part V of the Ontario Heritage Act. The policies and guidelines of the associated Heritage Conservation District Plan (HCD Plan) apply to this property.

The Owner is proposing to sever the parcel and develop a new detached dwelling on the severed parcel (eastern part). The heritage building (residence) on the western part of the property is to be retained. The new dwelling on the eastern part of the property will be developed in consideration of the style and massing of the existing heritage building and the “Contributing Properties” within the “Streetscape Character Analysis” and the guidelines of the HCD Plan.

The additions to the eastern side yard of the heritage building (carport and family room), and the shed in the southeast corner of the property are proposed to be removed. Please refer to the attached Streetscape Character Analysis (SCA) and drawings.

The Illing family (Owners) are long-standing members of the GCA and are active members of the Glebe community. They have also been strong supporters of the conservation of the heritage of the neighbourhood and have participated as members of the GCA Heritage Committee in the past.

Anne and Wolf Illing purchased the property in 1974, where their daughter Michelle and her 3 siblings were raised. Michelle and her husband Grant Stewart also chose to raise their children in the Glebe and have also been living in the family home for over 20 years. Next year, the family will be celebrating 50 years in the house!

Over the years, the family has cared for the house and the land and now wishes to develop the property in a manner that respects the heritage of the property and the intent of the conservation district. Grant Stewart is an architect with experience with heritage projects and his firm is the leading the design. Both houses will be retained by the family – Anne and Wolf Illing will retain 160 Clemow Ave. and their daughter Michelle and son-in-law Grant will take ownership of the new house.

The owner/architect had a pre-application meeting with City Heritage Staff (and other city staff), on January 11, 2024, and received follow-up comments on January 15, 2024. The proposed development presented at the meeting was to retain the carport of the heritage house, and the proposed dwelling with a reverse slope driveway to an integrated garage at the basement level. This would allow the entire above grade portion of the house to be living area, provide an accessible entrance (from the basement level), and result in elimination of additional driveway (hardscape) to provide parking at the rear of the property. Staff did not support the proposed “integrated reverse slope garage” as they noted Policy 9.3.1 of the HCD Plan states “are not consistent with the historic character of the district”. Further discussion at the meeting centred around the consideration of removing the carport and providing a shared driveway with rear yard parking. Reduction of paving/hardscape in the front yard was also discussed and strongly supported by staff in keeping with the streetscape. The design has been revised based on comments from that meeting, in particular, the proposed shared driveway and rear yard parking in consideration of the guidelines and policy of the HCD Plan.

The owner/architect also had an initial discussion with two members of the Glebe Community Association Heritage Committee to advise them of the proposed project.

The family has been approached on several occasions by developers as the lot is significant (75'x110') and is adjacent to the largest under-developed lot on Bank St. (specifically 680-690 Bank St). There is a high likelihood that if it were to be acquired by a developer, it could be used to expand the Bank St. lots and justify a significant development like others already built along the Bank St. corridor in the Glebe. Alternatively, a triplex would also be a likely development option, especially since the property is an end condition that abuts the commercial zone.

The proposal will help to protect the conservation district by adding a new single family home on available land and reducing the potential for development that does not respect the heritage nature of Clemow Ave. and the Glebe. The proposal includes a design for the new house whose structure, location and material respect the intent of the Heritage Conservation District with little impact on the density and services of the area.

1.2 Site Location and Development

The residence is located at the east end of the street (block), abutting a commercial property, creating an “end condition”. In consideration of this, the property is in a unique position to support the proposed development, and the following key points should be taken under consideration:

- The property is oversized at 75’ wide allowing for severance without affecting the heritage residence and the proposed addition of a single family would have negligible impact on the currently provided City and utility services.
- The location of the heritage house on the property with the removal of the carport and family room, as discussed with Heritage to restore the residence to more closely resemble the original condition, allows for the feasibility of the severance and proposed new residence.
- The property abuts a commercial property and as such is an “end condition”; the proposed development would provide a buffer to the commercial Bank St. zone, and reduces future risk of other less desirable development of the property that would be incompatible with the HCD.

1.3 Heritage, Streetscape Character & Analysis

Heritage

The residence at 160 Clemow Avenue was designed by the architect John William Hurrell Watts, and construction was completed in 1913. The property is identified as a “Contributing Property” in the HCD Plan. Renovations and additions have occurred over time, but the building retains much of it’s original character and materials. In fact, the current owners have done many renovations over the years to restore the property to it’s original style and condition.

As noted elsewhere, the intent of this project is to retain the heritage home and remove the carport and family room additions to the east side of the structure, returning it closer to it’s original state.

Streetscape Character & Analysis

The analysis of the streetscape character has led to the following design approaches for proposed new dwelling:

- The main door faces the street as this is the dominant form of the properties reviewed, and in general throughout the HCD.
- A walkway to the sidewalk in keeping with the character of the street is proposed.
- A shared driveway with carports or garages at the rear of the property is also proposed, as this is the dominant form of the street. This was noted as the option the heritage staff supported, in keeping with the character of the HCD (and HCD Plan policy).
- The project proposes to significantly reduce the current amount of hardscaping in the front yard of the property by replacing it with soft landscaping, as recommended by the City Heritage Staff and in keeping with the HCD.

2.0 Proposed Development

2.1 Description

The development proposes to sever the parcel and develop a new detached dwelling on the severed parcel (eastern part). The heritage building (residence) on the western part of the property is to be retained. The additions to the eastern side of the heritage building (carport and family room), and the shed in the southeast corner of the property are proposed to be removed. Section 8.0 New Construction in the HCD Plan provides guidance for the proposed dwelling, and the project considers the Policies and Guidelines stated in the section as outlined below.

Features of the proposed development (refer to attached drawings for additional detail):

- The lot will be severed to allow for the construction of a new single family home on the side yard of 160 Clemow Ave. abutting the Bank Street commercial zone.
- The carport and family room will be removed, as discussed with Heritage to restore the residence closer to the original condition.
- In keeping with the policies and guidelines of Section 8.0 of the HCD Plan, the massing and material of the proposed dwelling considers the houses on the street through the use of brick, the fenestration pattern, the two-storey projection for the living room and master bedroom, providing a respectful design without copying other dwellings. This approach both contributes to the heritage character of the HCD

(Subsection 8.2, Policy 1), and balances the new building with the historic character of the HCD without imitating the neighbouring buildings (Subsection 8.2 New Construction, Guideline a), in addition to other policies and guidelines of the section. As noted in the HCD Plan, brick is the primary cladding material in the District.

- The front landscaping, including the mature blue spruce in the front eastern yard, will be retained and the front yard will be reconfigured to remove a significant amount of the hardscape/driveway and will be replaced by landscaping. This is in keeping with the policies and guidelines of Section 9.0 of the HCD Plan, in particular, the conservation of the soft landscaped character of the front yards and mature trees (Private Landscape, Policy 1 and Guidelines a and e).
- A walkway from the main (front) entrance to the sidewalk in keeping with the character of the street. This is in keeping with the policies and guidelines of Section 9.0 of the HCD Plan, in particular, linear walkways perpendicular to the sidewalks leading to the front door (Subsection 9.2 Private Landscape, Policy 2 and Guideline b).
- A single shared driveway to the rear of the property will be added with carports or garages to provide parking at the back of the properties, following the dominant pattern of the street. There will be no impact on the boulevard as no new curb cut is required. Maintaining the existing pattern of shared driveway and rear yard parking is noted in the policies and guidelines in Subsection 9.3 Parking and Driveways of the HCD Plan.
- The property abuts a commercial property (“end condition”), with the proposed development providing a buffer to the commercial, and reduces future risk of other less desirable development of the property, that would be incompatible with the HCD.

3.0 Appendices

3.1 Appendix A – Streetscape Character Analysis Drawings

3.2 Appendix B – Drawings

Appendix A – Streetscape Character Analysis Drawings

Clemow Development

160 Clemow Avenue, Ottawa, Ontario

Streetscape Character Analysis (Character Groups)

- 1 Attached Garages and Carports
 - A. No front-facing attached garage or carport
 - B. Front-facing attached garage or carport
- 2 Access and Parking Character
 - A. No driveways along lot lines abutting a street
 - B. Individual single-wide driveways and Shared driveway
 - C. Individual double-wide driveways
 - D. Legally-established front yard parking
- 3 Main Door Character
 - A. Main door faces the front lot line and the street, or is accessed by a structure located along the front wall of the dwelling but does not face the front lot line and street
 - B. Main door does not face the front lot line and doesn't face the street



182 Clemow (1B, 2B, 3A)



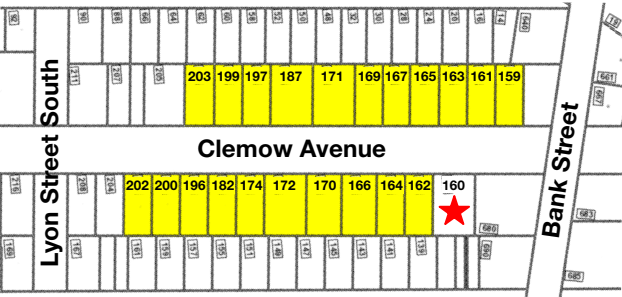
196 Clemow (1A, 2B, 3A)



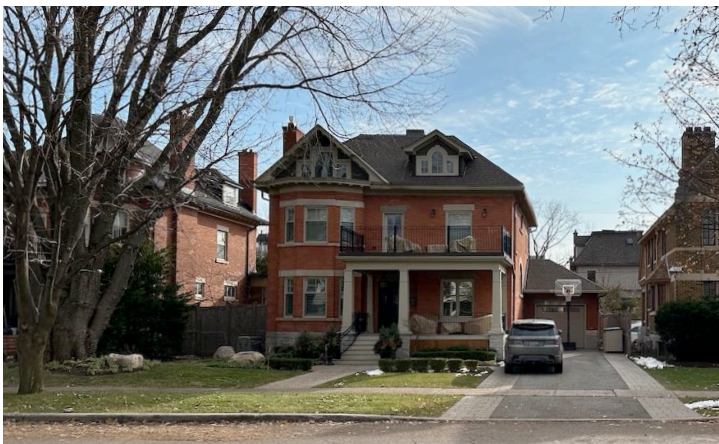
200 Clemow (1A, 2B, 3A)



202 Clemow (1A, 2B, 3A)



Streetscape Key Plan



166 Clemow (1B, 2B, 3A)



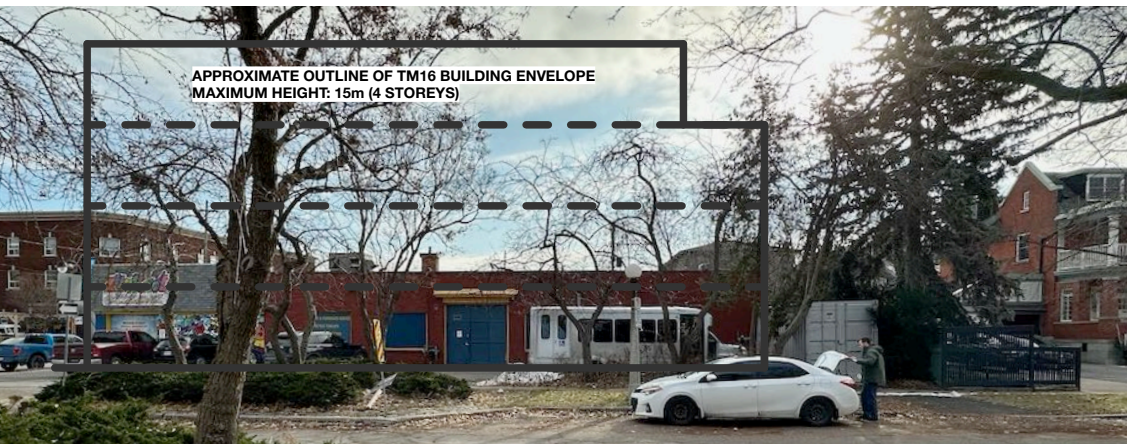
170 Clemow (1A, 2B, 3A)



172 Clemow (1A, 2B, 3A)



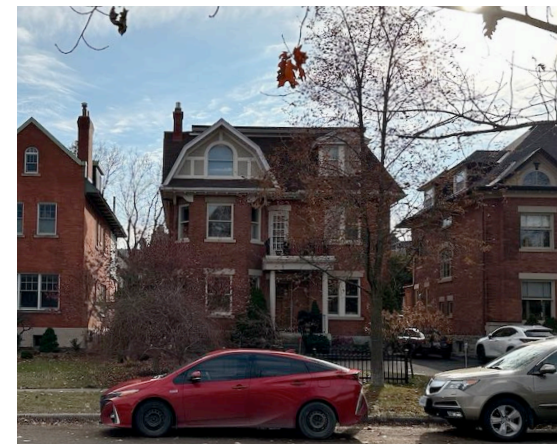
174 Clemow (1A, 2B, 3A)



680 Bank Street (included as adjacent lot / end condition)



160 Clemow (1B, 2B, 3A)



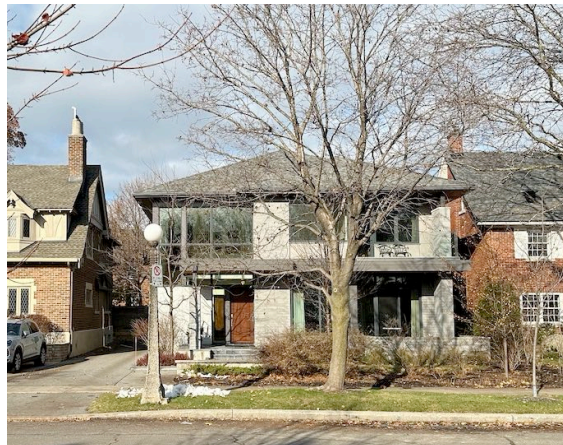
162 Clemow (1A, 2B, 3A)



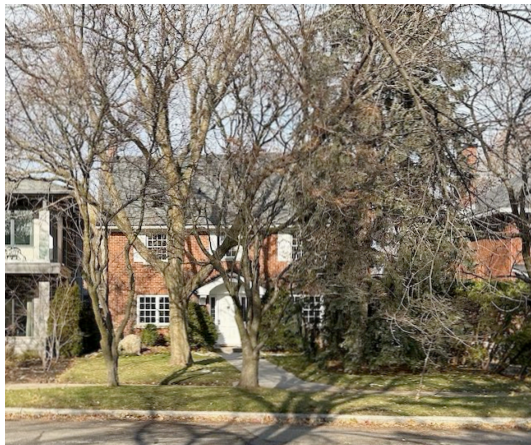
164 Clemow (1A, 2B, 3A)

Streetscape Character Analysis (Character Groups)

- 1 Attached Garages and Carports
 - A. No front-facing attached garage or carport
 - B. Front-facing attached garage or carport
- 2 Access and Parking Character
 - A. No driveways along lot lines abutting a street
 - B. Individual single-wide driveways and Shared driveway
 - C. Individual double-wide driveways
 - D. Legally-established front yard parking
- 3 Main Door Character
 - A. Main door faces the front lot line and the street, or is accessed by a structure located along the front wall of the dwelling but does not face the front lot line and street
 - B. Main door does not face the front lot line and doesn't face the street



163 Clemow (1A, 2B, 3A)

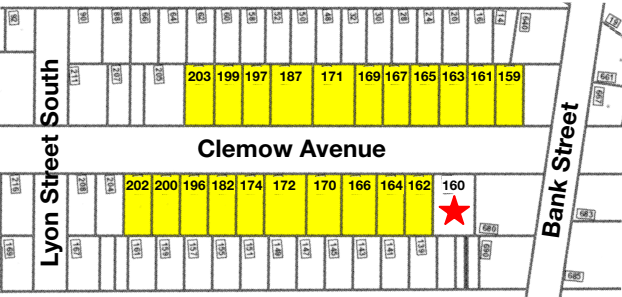


161 Clemow (1A, 2B, 3A)



159 Clemow (1A, 2B, 3A)

Bank Street



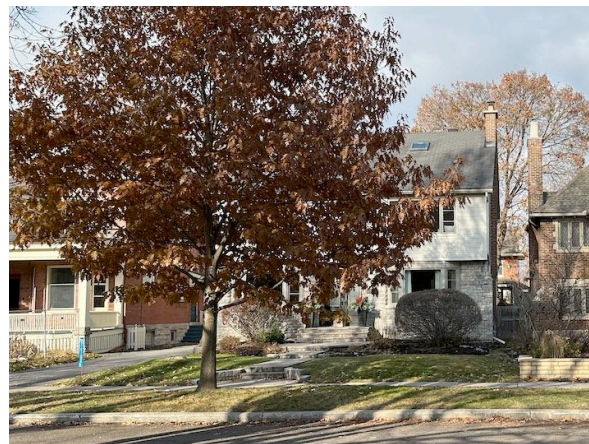
Streetscape Key Plan



171 Clemow (1B, 2B, 3A)



169 Clemow (1A, 2B, 3A)



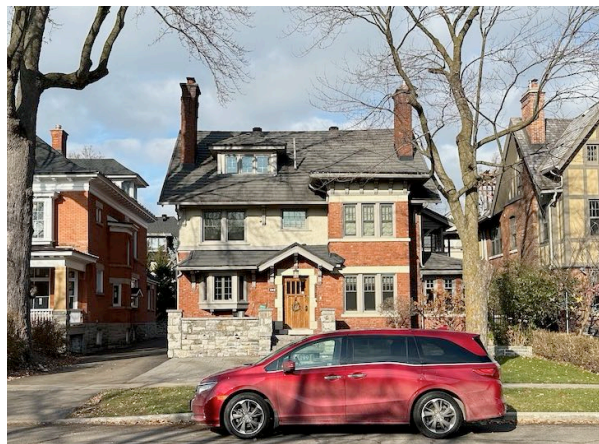
167 Clemow (1A, 2B, 3A)



165 Clemow (1A, 2B, 3A)



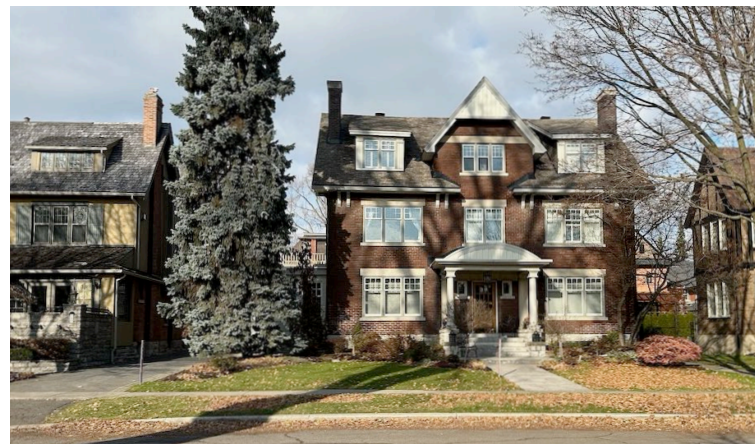
203 Clemow (1A, 2B, 3A)



199 Clemow (1A, 2B, 3A)



197 Clemow (1A, 2B, 3A)



187 Clemow (1A, 2B, 3A)

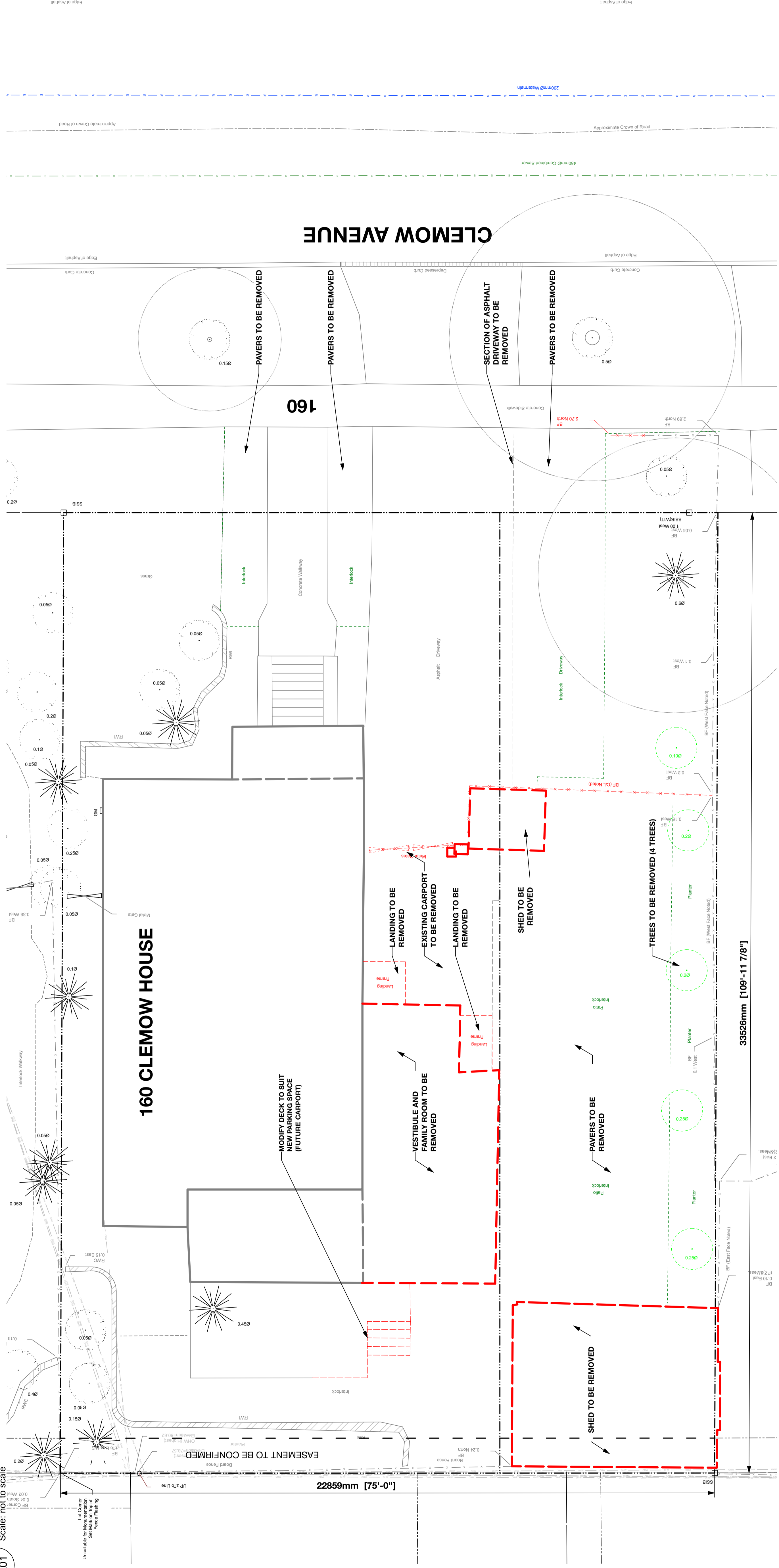
Appendix B – Drawings

Clemow Development

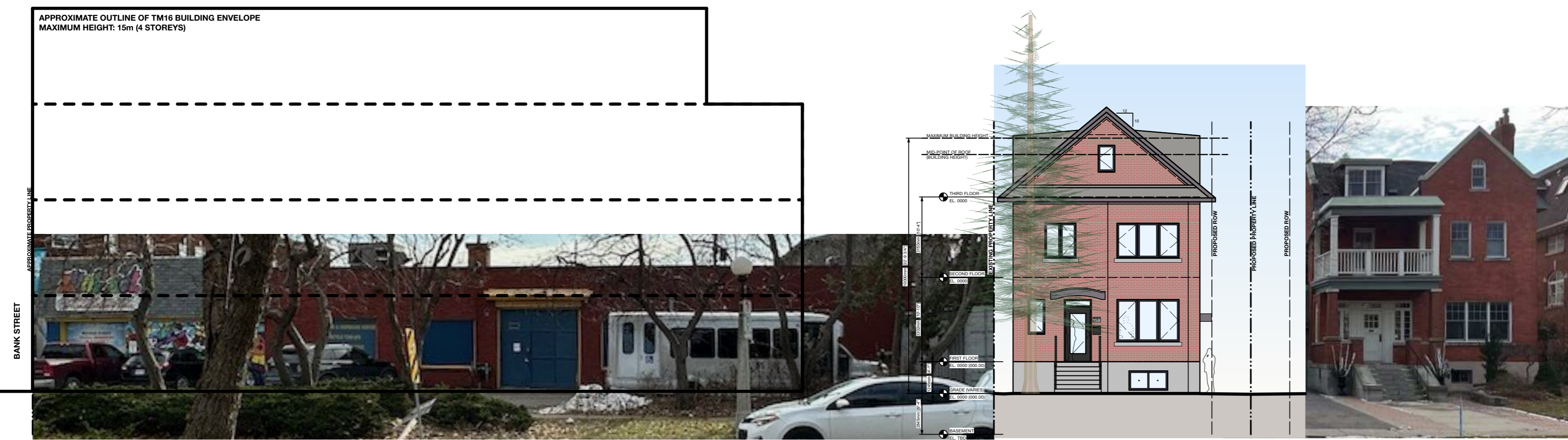
160 Clemow Avenue, Ottawa, Ontario



2 STREETSCAPE
A01 Scale: not to scale

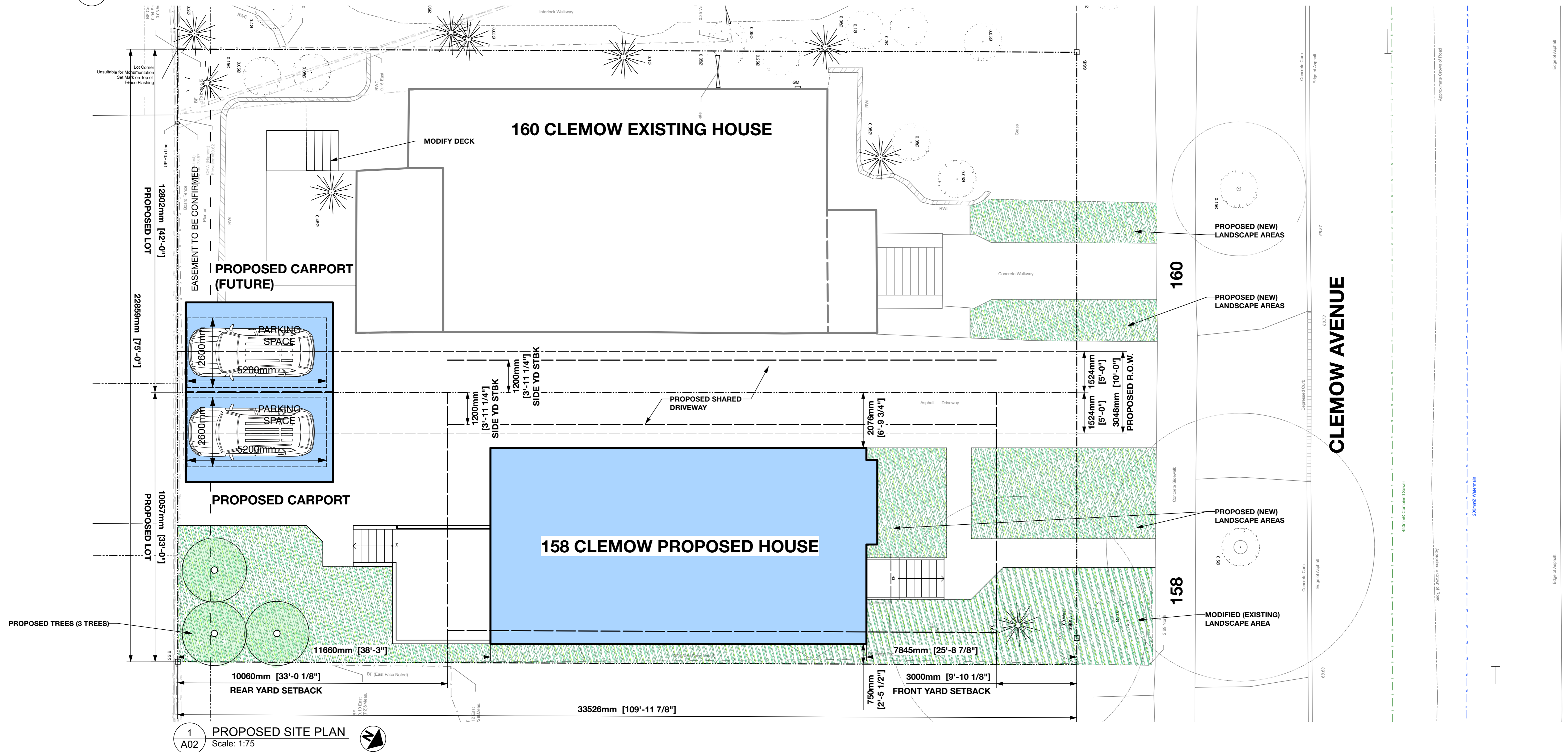


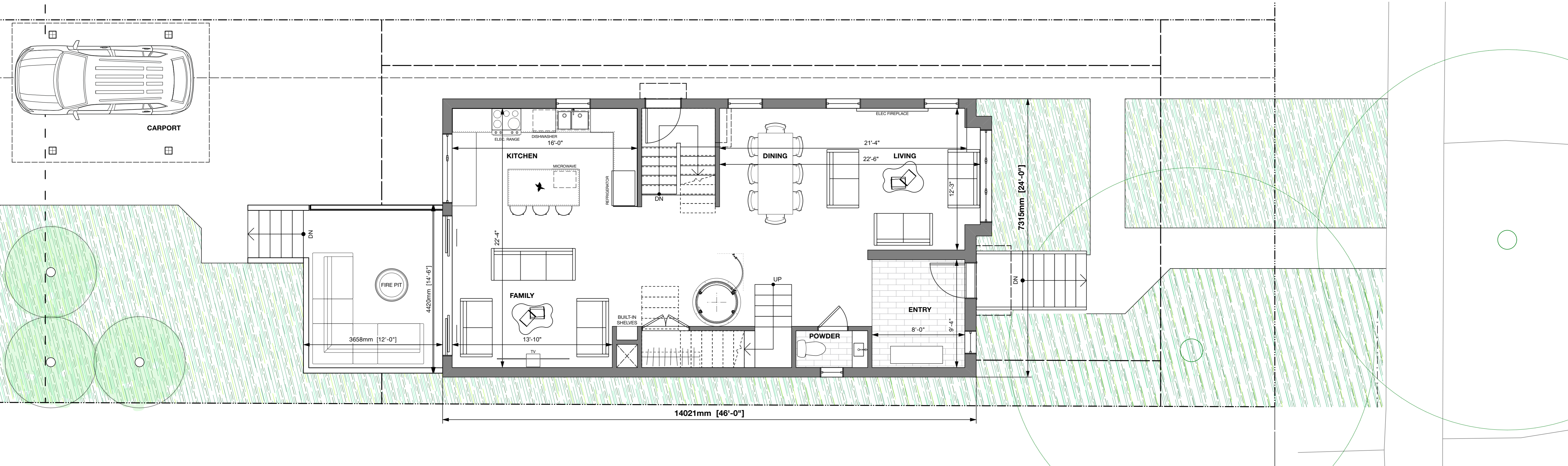
1 EXISTING SITE PLAN
A01 Scale: 1:75



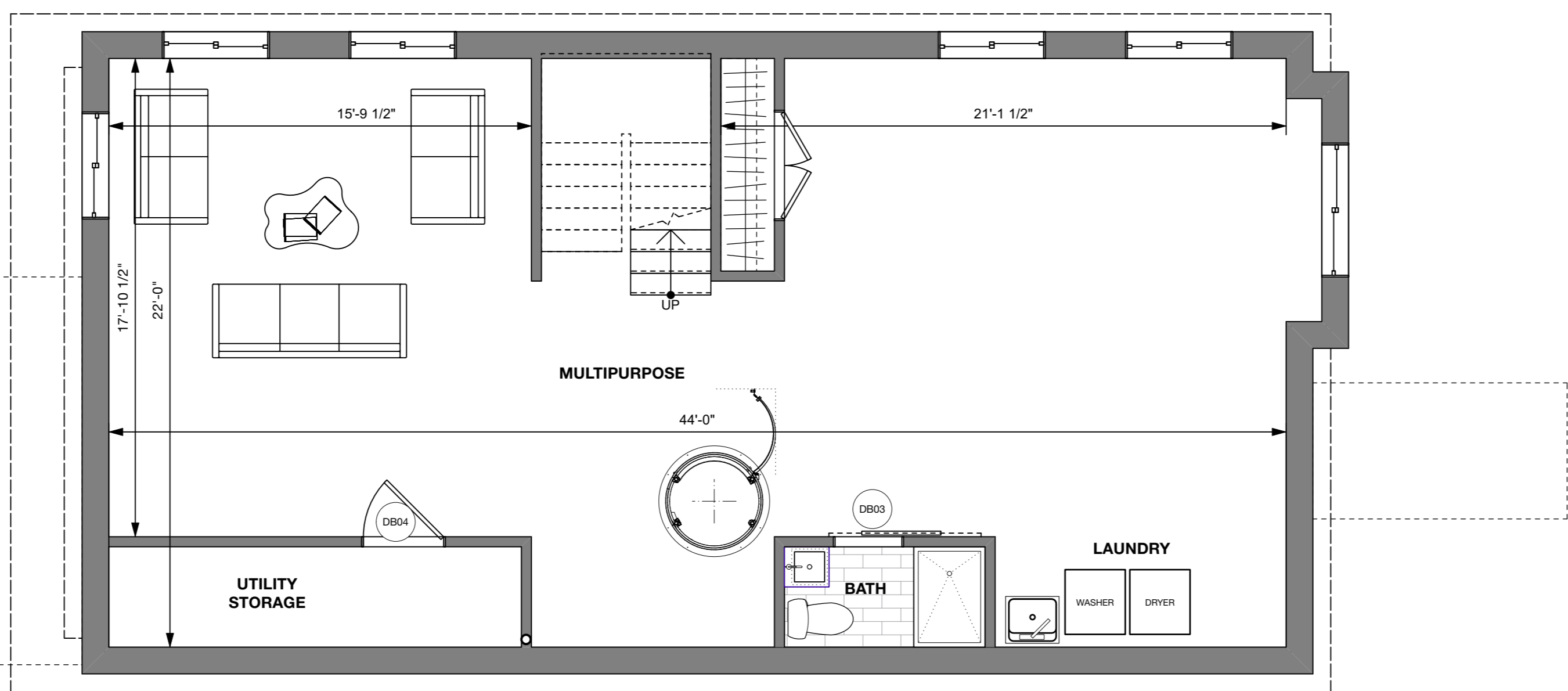
160 Clemow Zoning:	R1MM H(10)*	*Maximum Height Limit = 10m							
I Sub-Zone	II Minimum Lot Width (m)	III Minimum Lot Area (m ²)	IV Maximum Building Height (m)	V Minimum Front Yard Setback (m)	VI Minimum Corner Side Yard Setback (m)	VII Minimum Rear Yard Setback (m)	VIII Minimum Interior Side Yard Setback (m)	IX Maximum Lot Coverage (And Other Provisions)	X End Notes (see Table 156B)
MM *5	15	450	8.5	3	3	10.06m *7 (30% lot depth)	1.2. *4	na	4,5,7
4 Despite the minimum interior side yard setback provision in column VIII of Table 156A, the combined minimum required interior side yard setback									
(i) 36 metres or greater; must increase by 1 m for each additional 1 m of lot width, to a maximum of 40% of the lot width, and									
(ii) with one yard no less than the minimum interior side yard setback of the applicable zone or subzone. (By-law 2020-288)									
5 Where development is proposed on a corner lot, See Part V, Section 144:									
(i) Despite the minimum front yard setback provision in column V of Table 156A, on an interior lot with a lot width									
(ii) no part of an attached or detached garage or carport may be located closer to the front lot line than the front wall of the principal building.									
(iii) See Part V – Section 144 for yard setbacks and corner lot regulations, and Section 139 and 140 for garage regulations. (By-law 2020-288)									
7 For lots located within S. 342 and whose rear lot line abuts an R1, R2, R3 or R4 zone, see Part V, Section 144 – Alternative Yard Setbacks for Low-rise Dwellings in the Greenbelt. In all other cases the minimum rear yard setback is 25% of the lot depth which must comprise at least 25% of the area of the lot, however it may not be less than 6.0 m and need not exceed 7.5 m, except on lots with depths of 15 metres or less, in which case the minimum rear yard setback is 4 m. (By-law 2022-103)									

2 STREETScape
A02 Scale: not to scale



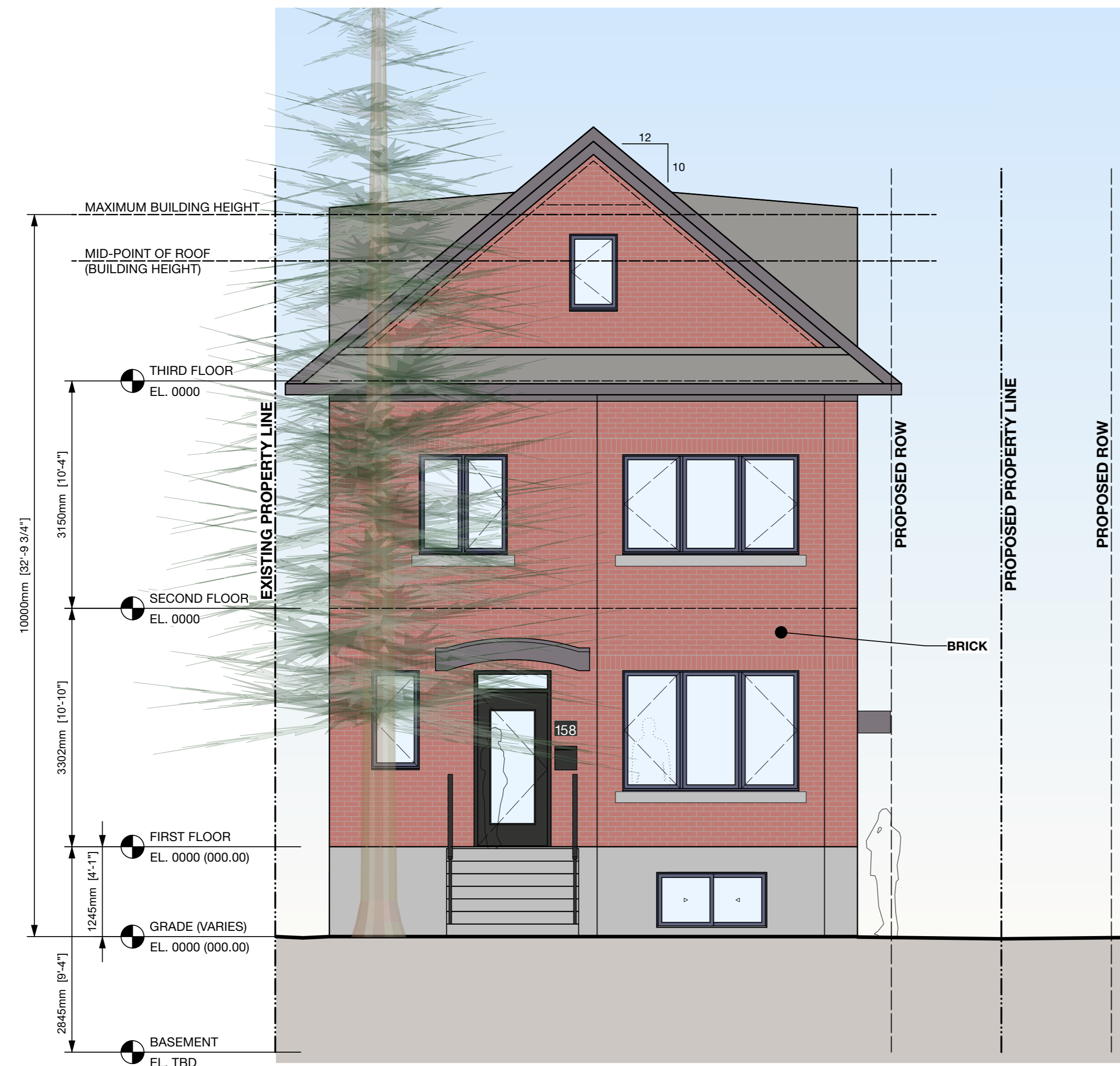


2 GROUND FLOOR PLAN
A03 Scale: 1:50

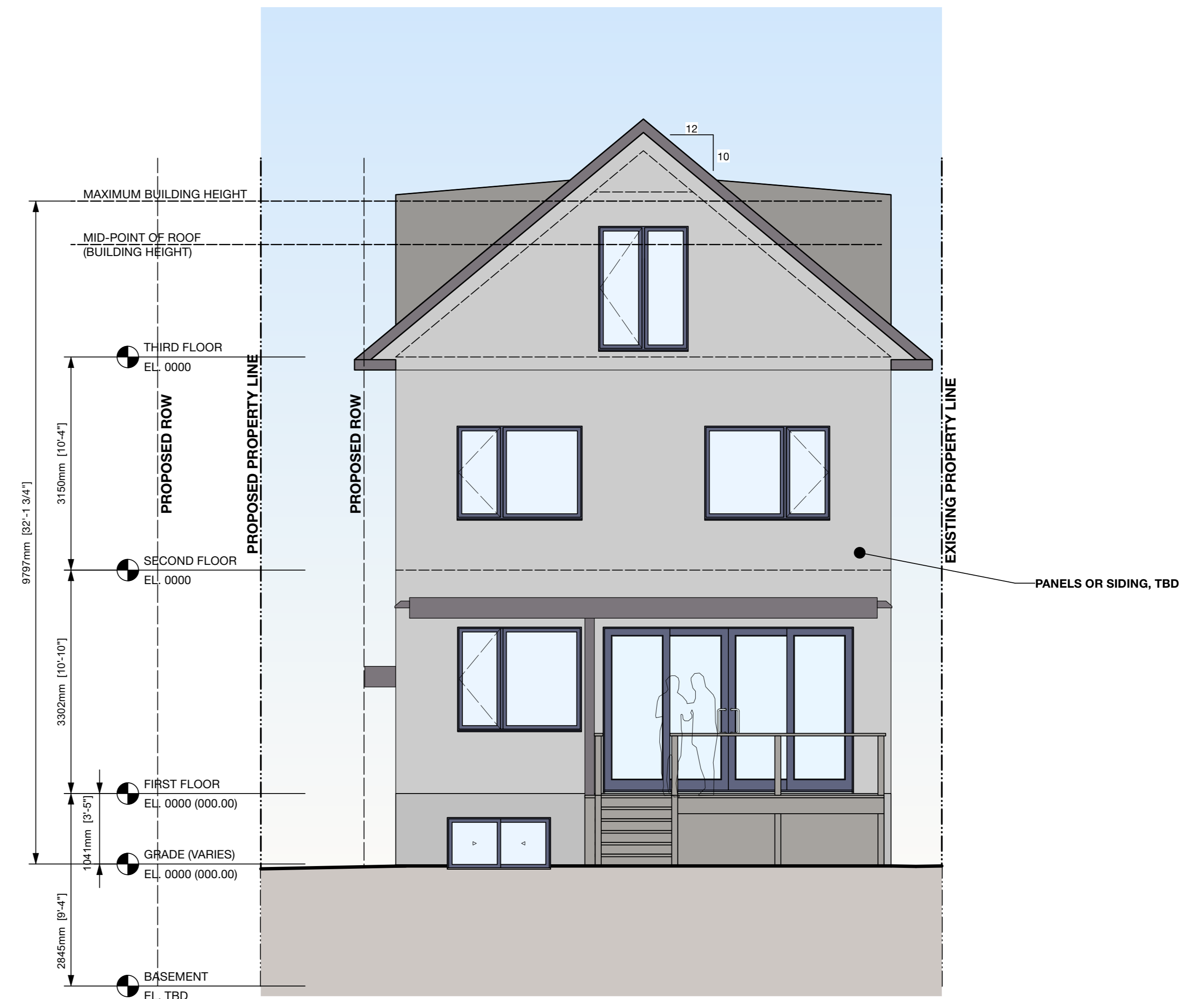


1 BASEMENT FLOOR PLAN
A03 Scale: 1:50

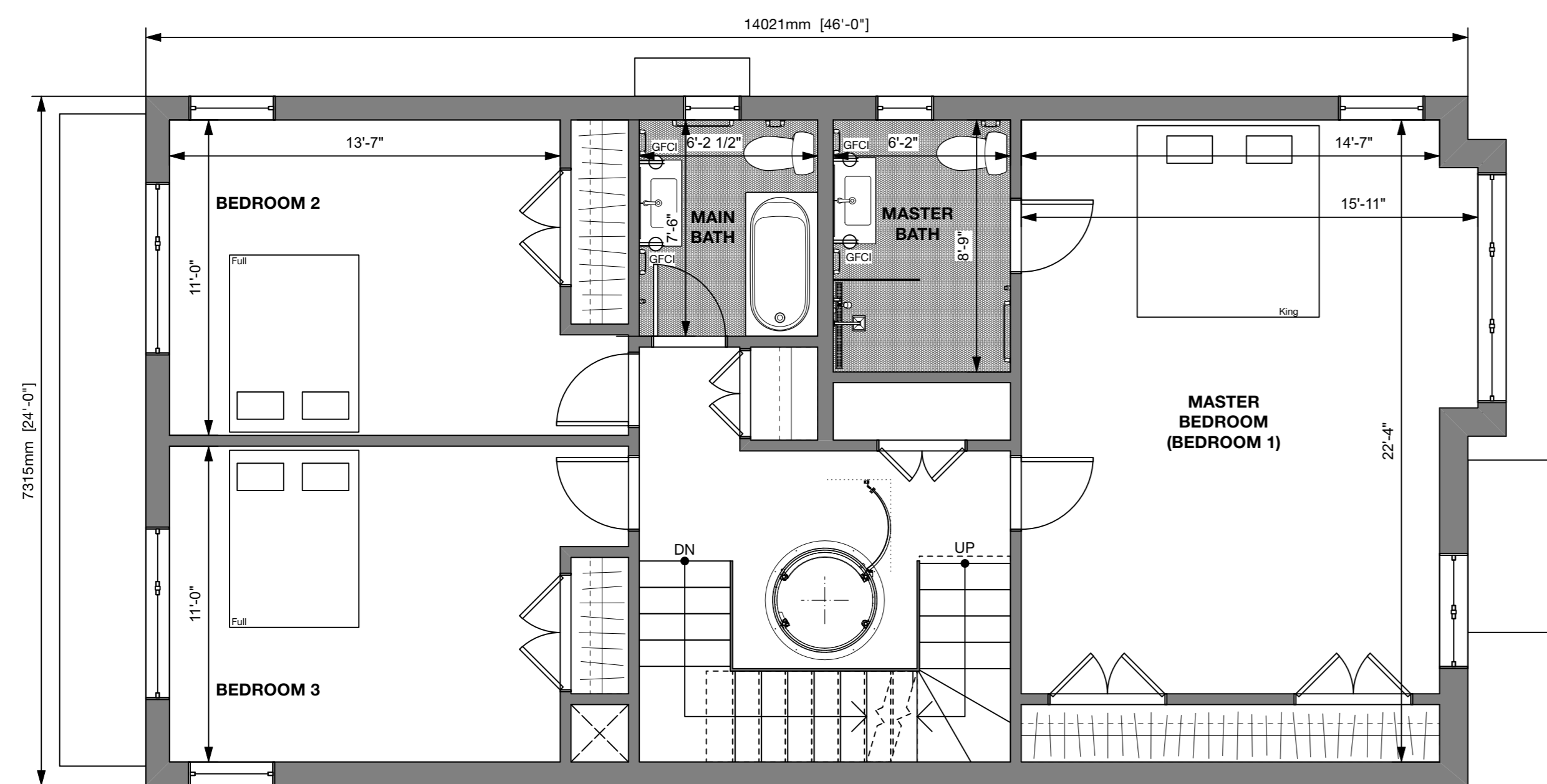




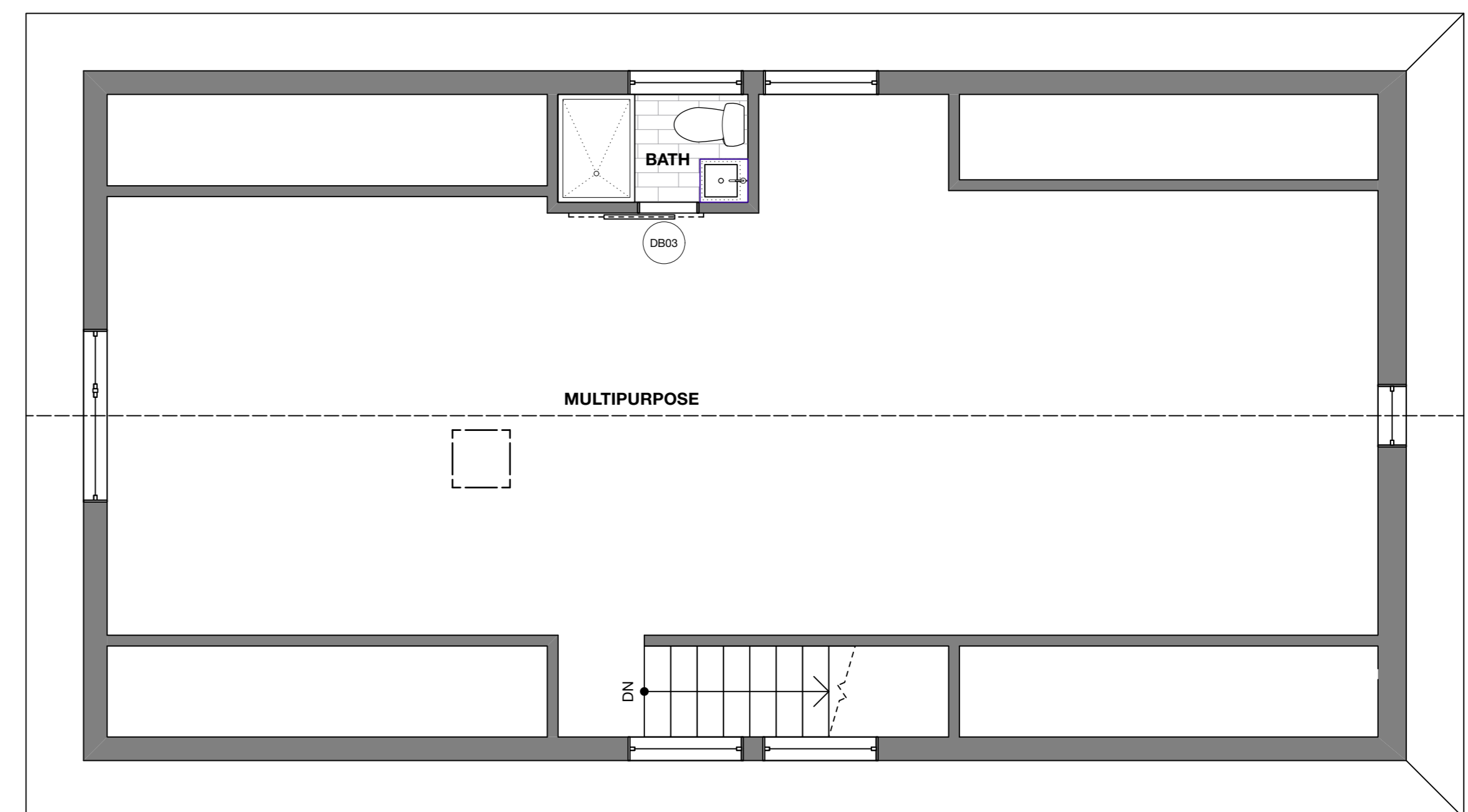
3 NORTH ELEVATION
A04 Scale: 1:50



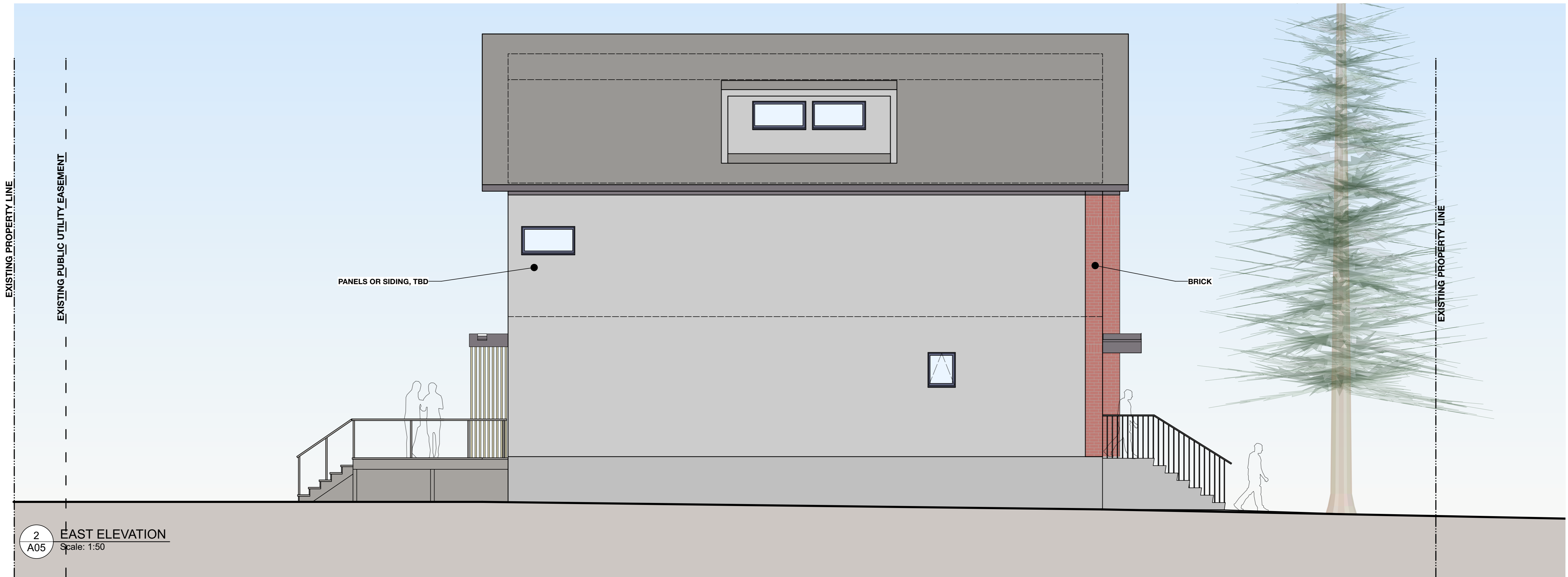
4 SOUTH ELEVATION
A04 Scale: 1:50



1 SECOND FLOOR PLAN
A04 Scale: 1:50



2 THIRD FLOOR PLAN
A04 Scale: 1:50



2 EAST ELEVATION
A05 Scale: 1:50



1 WEST ELEVATION
A05 Scale: 1:50